



ADDENDUM #1
BRIGHAM CITY CONNECTION PROJECT

The following changes are hereby made to the Contract Documents for the above referenced Project:

DRAWINGS: *The project drawings have been updated as follows and shall be replaced with the attached.*

- Civil & Structural Sheets – Removed “100% REVIEW SUBMITTAL - NOT FOR CONSTRUCTION” stamp off of sheets throughout the set.
- Structures Sheets - Concrete Volumes have been updated. Some discrepancies had arisen regarding which type of concrete (AE vs LSF) different bridge elements would be. We have updated the material type for some of the individual elements throughout the plan set and have updated the final total quantities to match on the quantities sheet.

DOCUMENT 00 41 23, BID FORM: *The Bid Schedule has been updated and shall be replaced with the attached.*

- The estimated quantities for the following items have been updated; however, **THE BID UNIT OF LUMP SUM FOR THE ITEM SHALL REMAIN THE SAME**
 - MP 73 – Structural Concrete (Est Qty 1,575 CY)
 - MP 74 – Structural Concrete – Low Shrinkage Fiber (Est Qty 1,373 CY)

ENGINEER’S ESTIMATE: The Engineer’s Estimate has been updated to reflect both for the quantity changes as well as removing the bid contingency amount for better clarity for the contractor. The updated **Engineer’s Estimate is \$22,800,000.**

This Addendum, dated March 25, 2024, is hereby attached to and made part of the Bidding Documents and each Bidder shall acknowledge receipt of this Addendum on the Bid Form.

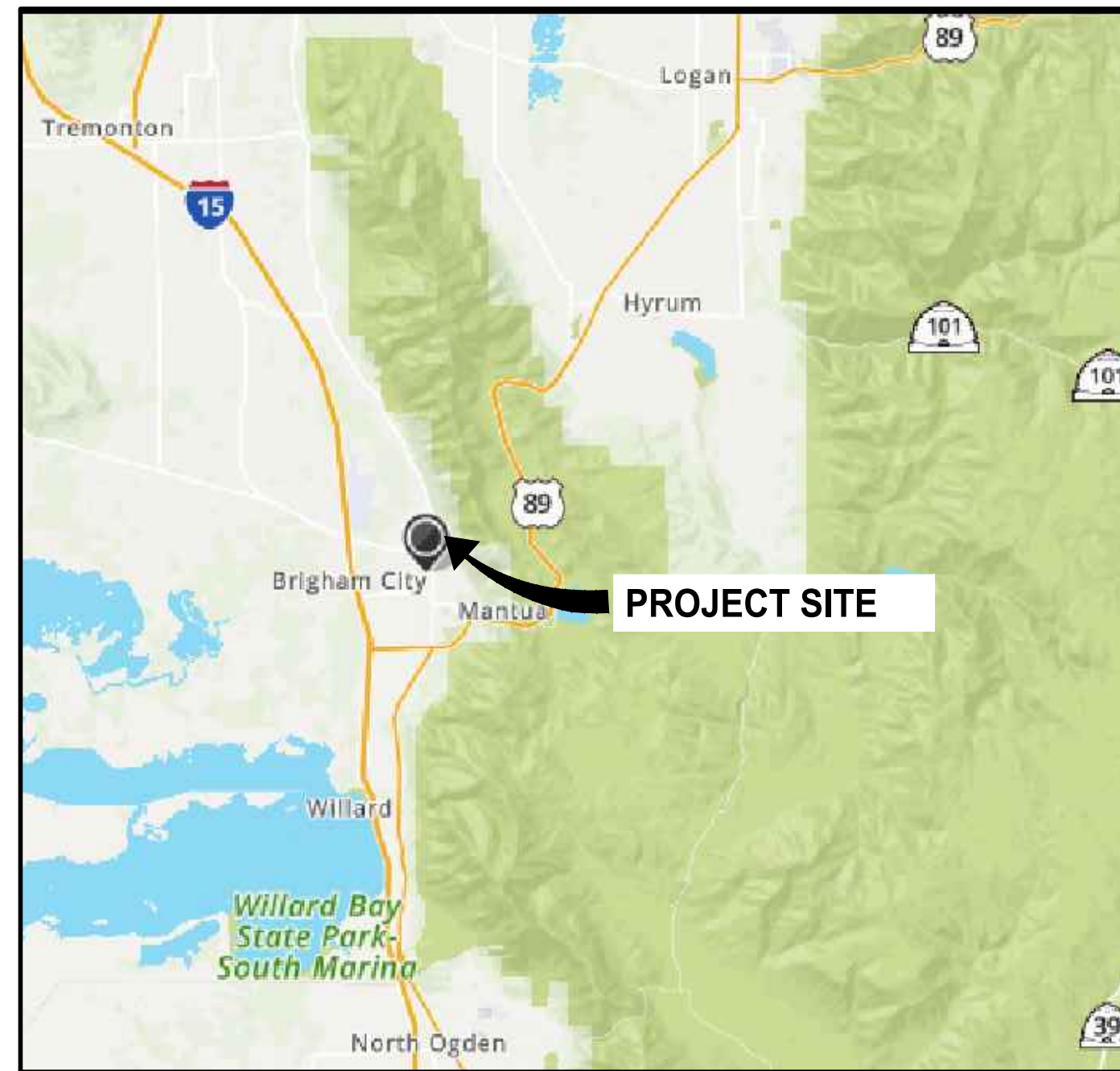
Attachments:

- Drawings
- Bid Form

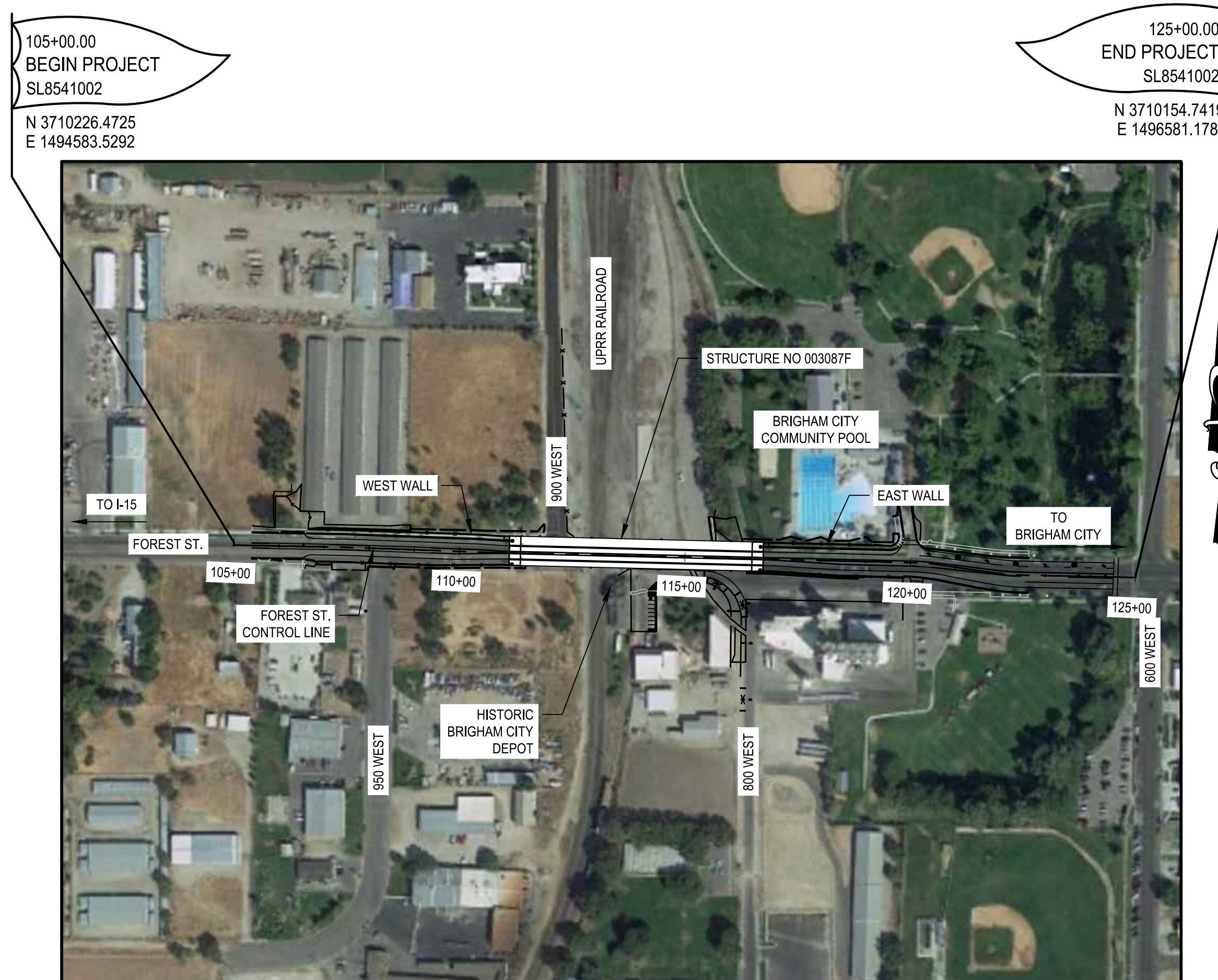
Project Engineer: Adam Birdsall, P.E.
Parametrix
385-341-2834

BRIGHAM CITY CONNECTION PROJECT

Brigham City, Utah



NORTHERN UTAH
NO SCALE



BRIGHAM CITY
NO SCALE

GENERAL PURPOSE OF THIS PROJECT:

CONSTRUCT BRIDGE TO CARRY FOREST STREET OVER EXISTING RAILROAD TRACKS.

CONTACT INFORMATION:

APPLICANT:
BRIGHAM CITY
20 NORTH MAIN STREET
BRIGHAM CITY, UT 84302
(435) 226-1437
CONTACT: TYLER PUGSLEY

OWNER:
BRIGHAM CITY

ENGINEER:
PARAMETRIX CONSULTING
9815 S MONROE ST, SUITE 410
SANDY, UT 84070
(801) 307-3400
CONTACT: ADAM BIRDSALL, P.E.

UTILITY:
ONE-CALL 811

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END PROJECT
SL8541002
N 3710154.7419
E 1496581.1781

105+00.00
BEGIN PROJECT
SL8541002
N 3710226.4725
E 1494583.5292

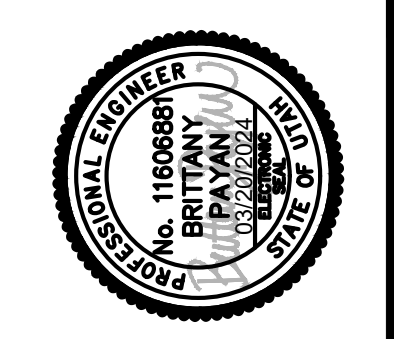
| INDEX TO DRAWINGS | | |
|-------------------|------------------|---|
| SHT NO. | DWG NO. | SHEET TITLE |
| GENERAL | | |
| 1 | 1 | TITLE SHEET, LOCATION AND MAPS, AND INDEX TO DRAWINGS |
| 2 | 1-A | ABBREVIATIONS AND LEGEND |
| 3 | 1-B | GENERAL NOTES |
| 4 | 1-C | STAGING AREA PLAN |
| CIVIL | | |
| 5 | HC-01 | HORIZONTAL CONTROL |
| 6 | SC-01 | SURVEY CONTROL |
| 7 TO 10 | TS-01 TO TS-04 | TYPICAL SECTION |
| 11 TO 31 | DT-01 TO DT-21 | DETAIL |
| 32 TO 44 | SM-01 TO SM-13 | SUMMARY |
| 45 TO 46 | MOT-01 TO MOT-2 | MAINTENANCE OF TRAFFIC |
| 47 TO 48 | RD-01 TO RD-02 | ROADWAY PLAN |
| 49 | RP-01 | ROADWAY PROFILE |
| 50 TO 51 | RMV-01 TO RMV-02 | REMOVAL |
| 52 TO 54 | DR-01 TO DR-03 | DRAINAGE PLAN AND PROFILE |
| 55 TO 56 | EC-01 TO EC-02 | EROSION CONTROL |
| 57 TO 59 | SS-01 TO SS-03 | SIGNING AND STRIPING |
| 60 TO 63 | LT-01 TO LT-04 | LIGHTING |
| STRUCTURE | | |
| 1 TO 59 | S01 TO S59 | STRUCTURE 003087F DRAWINGS |
| WALLS | | |
| 1 TO 11 | W01 TO W11 | WEST AND EAST MSE WALLS DRAWINGS |

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Parametrix

DATE: 03/20/2024
JOB No.: 344-8541-002
DESIGNED: BKP
DRAWN: SLO
CHECKED: AUB
APPROVED: AUB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

TITLE SHEET, LOCATION MAPS, AND INDEX TO DRAWINGS

LAYOUT: G1 PATH: U:\Sofa\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\985ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:19:50 PM



ABBREVIATIONS

| | | | |
|--------|-----------------------------------|------|-----------------------------------|
| APPROX | APPROXIMATE | LF | LINEAR FEET, LINEAR FOOT |
| APWA | AMERICAN PUBLIC WORKS ASSOCIATION | LT | LEFT |
| ASPH | ASPHALT | MH | MANHOLE |
| AVE | AVENUE | MIN | MINIMUM, MINUTE |
| AVG | AVERAGE | MISC | MISCELLANEOUS |
| BF | BLIND FLANGE | MON | MONUMENT |
| BLDG | BUILDING | N | NORTH, NORTHING |
| BLVD | BOULEVARD | NIC | NOT IN CONTRACT |
| BOT | BOTTOM | NO. | NUMBER |
| CB | CATCH BASIN | NTS | NOT TO SCALE |
| CIP | CAST IN PLACE, CAST IRON PIPE | PCC | PORTLAND CEMENT CONCRETE |
| CLR | CLEAR, CLEARANCE | PERF | PERFORATE, PERFORATED |
| CO | COUNTY | PH | PHASE |
| CONC | CONCRETE | PP | POWER POLE |
| CONN | CONNECT, CONNECTION | PVMT | PAVEMENT |
| CONST | CONSTRUCT, CONSTRUCTION | PWR | POWER |
| CONT | CONTINUE, CONTINUOUS | QTY | QUANTITY |
| CONTR | CONTRACTOR | RCP | REINFORCED CONCRETE PIPE |
| COORD | COORDINATE | RD | ROAD |
| CSBC | CRUSHED SURFACING BASE COURSE | RED | REDUCER |
| CSTC | CRUSHED SURFACING TOP COURSE | REF | REFERENCE |
| CTR | CENTER | REQD | REQUIRED |
| CUFT | CUBIC FOOT, CUBIC FEET | ROT | ROTATE |
| CULV | CULVERT | ROW | RIGHT OF WAY |
| CY | CUBIC YARD | RT | RIGHT |
| D | DEPTH, DENSITY, DRAIN, DRAINAGE | S | SOUTH |
| DEMO | DEMOLITION | SCH | SCHEDULE |
| DET | DETAIL | SD | STORM DRAIN |
| DI | DUCTILE IRON | SDMH | STORM DRAIN MANHOLE |
| DIA | DIAMETER | SECT | SECTION |
| DIM | DIMENSION | SEG | SEGMENT |
| DIP | DUCTILE IRON PIPE | SERV | SERVICE |
| DIST | DISTANCE, DISTRICT | SIG | SIGNAL |
| DWG | DRAWING | SL | SLOPE |
| E | EAST, EASTING | SPEC | SPECIFICATION |
| EA | EACH | SQ | SQUARE |
| EL | ELEVATION | SQFT | SQUARE FOOT, SQUARE FEET |
| EOP | EDGE OF PAVEMENT | SQYD | SQUARE YARD, SQUARE YARDS |
| EQUIP | EQUIPMENT | SS | SANITARY SEWER |
| EXIST | EXISTING | SSMH | SANITARY SEWER MANHOLE |
| EXC | EXCAVATE | ST | STREET |
| FCR | FINE CRUSHED ROCK | STA | STATION |
| FG | FINISH GRADE | STD | STANDARD |
| FH | FIRE HYDRANT | SURV | SURVEY |
| FIN | FINISH, FINISHED | SYS | SYSTEM |
| FL | FLOW LINE | TEL | TELEPHONE |
| FM | FORCE MAIN | TEMP | TEMPORARY |
| G | GAS | THK | THICK, THICKNESS |
| GND | GROUND | THRU | THROUGH |
| GR | GRADE | TOB | TOP OF BANK |
| HORIZ | HORIZONTAL | TOC | TOP OF CONCRETE, TOP OF CURB |
| HT | HEIGHT | TOT | TOTAL |
| ID | INSIDE DIAMETER | TOW | TOP OF WALL |
| IE | INVERT ELEVATION | TSD | TEMPORARY STORM DRAIN |
| IN | INCH | TYP | TYPICAL |
| INCL | INCLUDE, INCLUDING | UDOT | UTAH DEPARTMENT OF TRANSPORTATION |
| INSL | INSTALL, INSTALLATION | UG | UNDERGROUND |
| JB | JUNCTION BOX | UP | UTILITY POLE |
| LCPE | LINED CORRUGATED POLYETHYLENE | VAR | VARIES, VARIABLE |
| | | VERT | VERTICAL |
| | | VOL | VOLUME |
| | | W | WATER, WATT, WEST, WIDTH |
| | | WM | WATER METER |
| | | YD | YARD |

LEGEND

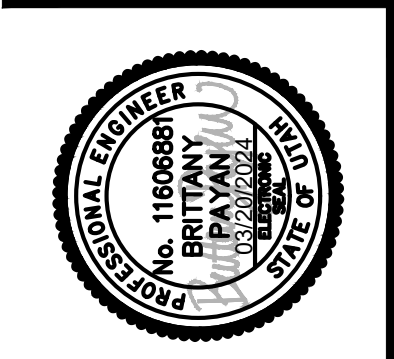
| | |
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| | EXISTING GAS VALVE |
| | EXISTING CATCH BASIN |
| | EXISTING STORM MANHOLE |
| | EXISTING SEWER CLEAN OUT |
| | EXISTING SEWER MANHOLE |
| | EXISTING POWER POLE |
| | EXISTING LIGHT STANDARD |
| | EXISTING TELEPHONE POLE |
| | EXISTING TELEPHONE MANHOLE |
| | EXISTING FIRE HYDRANT |
| | EXISTING WATER METER |
| | EXISTING WATER VALVE |
| | EXISTING WATER MANHOLE |
| | EXISTING IRRIGATION BOX |
| | EXISTING FIBER OPTIC BOX |
| | DROP INLET FIBER ROLL |
| | CHECK DAM FIBER ROLL |
| | EXISTING SIGN |
| | NEW SIGN |
| | EXISTING MAIL BOX |
| | NEW MAIL BOX |
| | EXISTING TREE |
| | NEW TREE |
| | PARKING STOP BLOCK |
| | ABANDONED FIBER OPTIC |
| | ABANDONED SANITARY SEWER |
| | ABANDONED STORM DRAIN |
| | ABANDONED WATER |
| | EXISTING STORM DRAIN |
| | EXISTING SANITARY SEWER |
| | EXISTING POWER |
| | LIGHTING |
| | EXISTING WATER |
| | EXISTING IRRIGATION |
| | EXISTING FIBER OPTIC |
| | RIGHT OF WAY |
| | RAILROAD |
| | EXISTING EDGE OF GRAVEL |
| | EXISTING EDGE OF ASPHALT |
| | NEW EDGE OF ASPHALT |
| | EXISTING EDGE OF CONCRETE |
| | FILL SLOPE |
| | CUT SLOPE |
| | EXISTING BUILDING LINE |
| | EXISTING FENCE |
| | NEW FENCE |
| | SILT FENCE |
| | RETAINING WALL |
| | RETAINING WALL BLOCK |

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| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | AP |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

ABBREVIATIONS AND LEGEND

PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:20:10 PM LAYOUT: 1-B

GENERAL NOTES

1. THE CONTRACTOR SHALL EXAMINE THE DRAWINGS AND SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES OR CONFLICTS BEFORE PROCEEDING WITH THE WORK.
2. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND CONDITIONS AT THE SITE AND SHALL NOTIFY THE ENGINEER OF DISCREPANCIES BETWEEN THE ACTUAL CONDITIONS AND INFORMATION SHOWN ON THE DRAWINGS BEFORE PROCEEDING WITH THE WORK.
3. ALL WORK SHALL CONFORM TO AT LEAST THE MINIMUM STANDARDS OF THE INTERNATIONAL BUILDING CODE (LATEST EDITION), BRIGHAM CITY STANDARDS, AND OTHER REGULATORY AGENCIES EXERCISING AUTHORITY OVER ANY PORTION OF THE WORK WHERE APPLICABLE.
4. SPECIFIC NOTES AND DETAILS SHALL TAKE PRECEDENCE OVER GENERAL NOTES, TYPICAL DETAILS AND SPECIFICATIONS.
5. THE CONTRACTOR SHALL REFER TO THE TECHNICAL PROVISIONS FOR INFORMATION NOT COVERED BY THESE GENERAL NOTES OR THE DRAWINGS.
6. ALL DIMENSIONS SHOWN ON DRAWINGS ARE ASSUMED TO BE IN FEET, UNLESS OTHERWISE NOTED.
7. THE CONTRACTOR SHALL PROVIDE AND BE RESPONSIBLE FOR THE TEMPORARY ERECTION OF BRACING AND SHORING AS REQUIRED FOR STABILITY OF STRUCTURES AND EXCAVATIONS DURING ALL PHASES OF CONSTRUCTION.
8. THE OWNER HAS OBTAINED THE NECESSARY PERMITS REQUIRED TO COMPLETE CONSTRUCTION OF THE PROJECT AS DETAILED. THE CONTRACTOR SHALL OBTAIN UDOT, COUNTY, AND CITY PERMITS WHERE APPLICABLE FOR WORK IN THE PUBLIC RIGHT-OF-WAY.
9. THE CONTRACTOR SHALL BE REQUIRED TO FURNISH ALL TEMPORARY WATER, POWER, OR OTHER UTILITIES AS REQUIRED TO COMPLETE CONSTRUCTION OF THE PROJECT AS DETAILED. WATER FOR FLUSHING AND HYDROSTATIC TESTING CAN BE OBTAINED FROM THE EXISTING PIPELINE AT NO CHARGE TO THE CONTRACTOR.
10. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ALL FLAGGING, BARRICADES, AND TRAFFIC CONTROL AS MAY BE NECESSARY TO ENSURE SAFETY TO THE GENERAL PUBLIC DURING CONSTRUCTION. A TRAFFIC CONTROL PLAN SHALL BE DEVELOPED BY THE CONTRACTOR AND SUBMITTED TO UDOT, BRIGHAM CITY, AND BOX ELDER COUNTRY WHERE APPLICABLE.
11. THE CONTRACTOR SHALL HOLD A VALID UTAH CONTRACTOR'S LICENSE PRIOR TO BEGINNING CONSTRUCTION.
12. THE CONTRACTOR SHALL MAINTAIN CLEAN CONSTRUCTION AREAS. ALL DEBRIS, RUBBISH AND TRASH MUST BE REMOVED FROM THE SITE AND DISPOSED OF IN A LAWFUL MANNER.
13. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN A SET OF DRAWINGS AT THE JOB SITE FOR THE PURPOSE OF RECORDING ALL ACTUAL MEASUREMENTS AND DETAILS TO BE USED IN THE PREPARATION OF "AS BUILTS" OR "RECORD" DRAWINGS UNTIL DRAWINGS HAVE BEEN SUBMITTED TO AND ACCEPTED BY THE ENGINEER.
14. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT BLUE STAKES AT 1-800-662-4111 TO HAVE UNDERGROUND UTILITIES MARKED IN THE FIELD PRIOR TO ALL EARTHWORK OPERATIONS.
15. SEE THE PROJECT SPECIFICATIONS FOR GRADATION AND COMPACTION REQUIREMENTS FOR BEDDING, BACKFILL, BASE AND CRUSHED ROCK SURFACE COURSE.
16. ALL TRENCHES AND EXCAVATIONS SHALL BE CUT, PROTECTED AND SUPPORTED AS PRESCRIBED BY OSHA.
17. THE CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AND MAINTAIN ANY EQUIPMENT NECESSARY TO DE-WATER EXCAVATIONS.
18. IMPORT GRANULAR BACKFILL MATERIAL SHALL BE REQUIRED IN ROADWAY AREAS AS DIRECTED BY CITY ENGINEER TO ACHIEVE 95% DRY DENSITY COMPACTION.
19. THE OPEN ENDS OF ALL PIPELINES UNDER CONSTRUCTION SHALL BE COVERED AND EFFECTIVELY SEALED AT THE END OF THE DAYS WORK.
20. FILL ALL ABANDONED MANHOLES WITH FLOWABLE FILL.
21. FILL ALL ABANDONED VALVES WITH ROAD BASE.

UPRR GENERAL NOTES

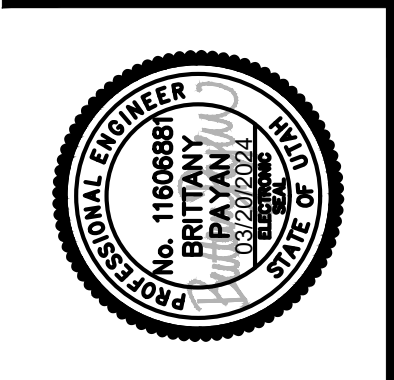
1. **COORDINATION:** USE THE UNION PACIFIC RAILROAD PUBLIC PROJECTS MANUAL AS A GUIDE FOR ASSISTING IN ADMINISTERING, COORDINATING, PLANNING, AND IMPLEMENTING YOUR PROJECT.
2. **DESIGN REVIEW:** PRIOR TO CONSTRUCTION, OBTAIN RAILROAD REVIEW AND APPROVAL OF ALL RELEVANT CONSTRUCTION ITEMS, INCLUDING BUT NOT LIMITED TO, SHORING, TRACK & GROUND MONITORING, ERECTION, DEMOLITION, AND FALSEWORK. ALL DESIGNS MUST ADHERE TO THE MOST RESTRICTIVE PROVISIONS OF THE CURRENT UPRR AND AREMA STANDARDS AND GUIDELINES IN EFFECT AT THE TIME THE WORK IS EXECUTED. ALLOW A MINIMUM OF 4 WEEKS FOR REVIEW AND APPROVAL OF EACH SUBMITTAL. FOR SUBMITTALS NOT IN ACCORDANCE WITH THESE NOTES, LONGER REVIEW TIMES SHALL BE EXPECTED.
3. **OPERATIONS:** PROJECTS SHALL BE DESIGNED SUCH THAT ALL CONSTRUCTION ACTIVITIES AND PHASING WILL NOT COMPROMISE SAFETY NOR IMPACT RAILROAD OPERATIONS.
4. **PASSING TRAINS:** RAILROAD REQUIREMENTS DO NOT ALLOW WORK WITHIN 50 FEET OF TRACK CENTERLINE WHEN A TRAIN PASSES THE WORK SITE AND ALL PERSONNEL MUST CLEAR THE AREA WITHIN 25 FEET OF THE TRACK CENTERLINE AND SECURE ALL EQUIPMENT.
5. **WORK WINDOWS:** CONSTRUCTION ACTIVITIES MUST BE PERFORMED WITHIN NATURALLY OCCURRING TRACK WINDOWS. COORDINATE ALL REQUESTS FOR CONSTRUCTION WORK WINDOWS WITH THE RAILROAD'S DESIGNATED REPRESENTATIVE TO ENSURE THAT THE WORK IS SCHEDULED TO ELIMINATE ANY POTENTIAL DISRUPTION TO THE RAILROAD'S OPERATIONS.
6. **TOP OF RAIL SURVEY:** VERIFY THE ELEVATION OF THE EXISTING TOP-OF-RAIL PROFILE BEFORE STARTING CONSTRUCTION. TOP-OF-RAIL SURVEY SHALL BE PERFORMED FOR 1000 FEET ON EITHER SIDE OF PROPOSED OVERHEAD STRUCTURE. ADDITIONAL VERTICAL CLEARANCE MAY BE REQUIRED FOR ADJUSTMENT OF SAG IN VERTICAL CURVE, FUTURE TRACK RAISE, FLOOD CONSIDERATIONS, CONSTRUCTION AND MAINTENANCE PURPOSES. BRING ALL DISCREPANCIES TO THE ATTENTION OF THE RAILROAD PRIOR TO CONSTRUCTION.
7. **TEMPORARY CONSTRUCTION CLEARANCES:** CONSTRUCTION ACTIVITIES ARE NOT ALLOWED WITHIN THE TEMPORARY CONSTRUCTION CLEARANCE ENVELOPE PER THE CURRENT UPRR GUIDELINES FOR RAILROAD GRADE SEPARATION PROJECTS AND GUIDELINES FOR TEMPORARY SHORING.
8. **PERMANENT CLEARANCE ENVELOPE:** THE MINIMUM PERMANENT VERTICAL CLEARANCE SHALL BE 23'-4" MEASURED FROM TOP OF HIGHEST RAIL TO THE LOWEST OBSTRUCTION UNDER THE STRUCTURE. THE EXTENT OF THIS VERTICAL CLEARANCE SHALL BE A MINIMUM OF 9 FEET TO THE FIELD SIDE OF THE OUTERMOST EXISTING OR FUTURE TRACKS, MEASURED PERPENDICULAR FROM THE CENTERLINE OF SAID TRACKS. IN CURVED TRACK, 9 FEET SHALL BE INCREASED EITHER 6 INCHES TOTAL OR 1.5 INCHES FOR EVERY DEGREE OF CURVE, WHICHEVER IS GREATER. THE PERMANENT VERTICAL CLEARANCE SHALL EXTEND TO COVER ALL EXISTING AND FUTURE TRACKS, INCLUDING THE SPACE IN BETWEEN.
9. **VERIFY PERMANENT CLEARANCES:** ALL PERMANENT CLEARANCES SHALL BE VERIFIED THROUGHOUT CONSTRUCTION. A COMPLIANCE REPORT SHALL BE SUBMITTED TO THE RAILROAD BEFORE PROJECT CLOSING. ALL DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RAILROAD.
10. **PERMANENT CLEARANCES FOR BID:** THE PERMANENT VERTICAL AND HORIZONTAL DESIGN CLEARANCES, WHICH ARE SPECIFIED IN THE BID DOCUMENTS APPROVED BY THE RAILROAD, MUST BE MAINTAINED IN RELATION TO THE TOP-OF-RAIL AND CENTERLINE OF EXISTING AND FUTURE TRACKS, RESPECTIVELY. ANY REDUCTION OF THESE CLEARANCES IS NOT PERMITTED.
11. **DRAINAGE:** THE PROPOSED PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S DITCHES AND/OR DRAINAGE STRUCTURES.
12. **BEFORE YOU DIG:** APPROPRIATE MEASURES FOR THE LOCATION AND PROTECTION OF UPRR FACILITIES SHALL BE ADDRESSED IN THE PLANS AND CONTRACT DOCUMENTS. FOR SPECIFIC RAILROAD REQUIREMENTS AND ADDITIONAL INFORMATION REFER TO WWW.UP.COM/CBUD. ABANDONMENT OF UTILITIES MUST FOLLOW THE UPRR GUIDELINES FOR ABANDONMENT OF SUBSURFACE UTILITY STRUCTURES.

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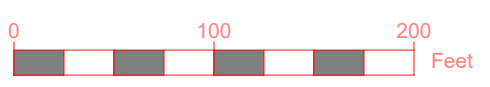
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| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

GENERAL NOTES

LAYOUT: 1-C PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:20:20 PM



DRAWING NO.
4 OF 63
1-C

STAGING AREA PLAN

PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**



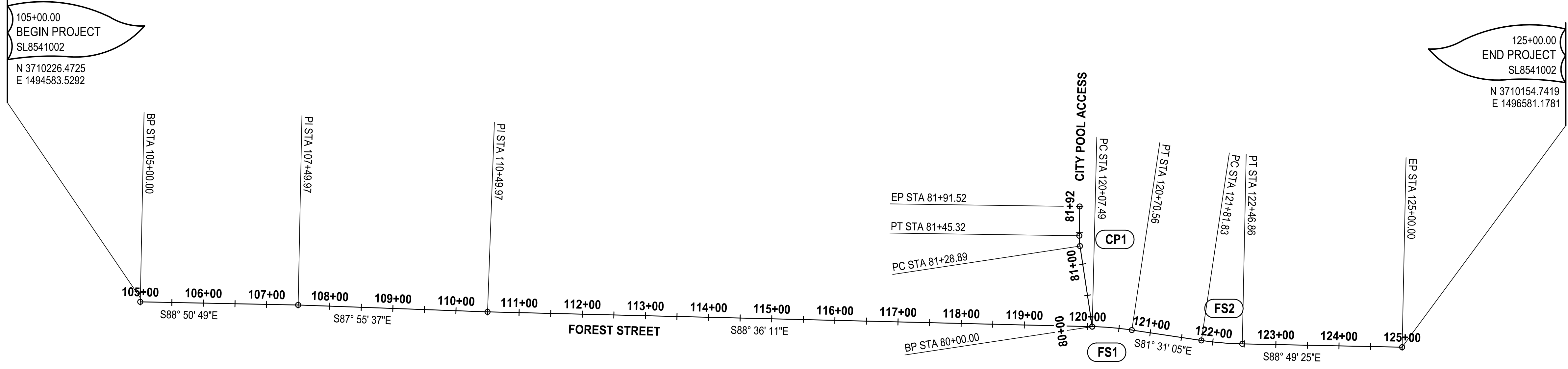
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| 03/20/2024 | BKP | AP |
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| 344-8541-002 | BKP | AP |

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LAYOUT: HC 1 PATH: U:\SAR\Projects\Clients\8541-Brigham_City\344-8541-002_Forest_St_Final_Design\985vcs\CADD\DWG\Civil PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 12:20:30 PM



| CURVE DATA | | | | | | | | |
|------------|-----------------------|-------|--------|--------|---------|----------------|-------------|-------------|
| CURVE ID | HORIZONTAL CURVE DATA | | | | | PI COORDINATES | | |
| | Δ | | RADIUS | LENGTH | TANGENT | PI STATION | NORTHING | EASTING |
| FS1 | 07° 05' 06.65" | RIGHT | 510.00 | 63.07 | 31.57 | 120+39.07 | 3710186.479 | 1496122.054 |
| FS2 | 07° 18' 20.82" | LEFT | 510.00 | 65.03 | 32.56 | 122+14.39 | 3710160.607 | 1496295.541 |
| CP1 | 09° 24' 47.14" | RIGHT | 100.00 | 16.43 | 8.23 | 81+37.12 | 3710322.825 | 1496069.965 |



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Parametrix

DATE: 03/20/2024
 DESIGNED BY: SLO
 DRAWN BY: SLO
 CHECKED BY: AP
 APPROVED BY: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

HORIZONTAL CONTROL



PROJECT DATUM:

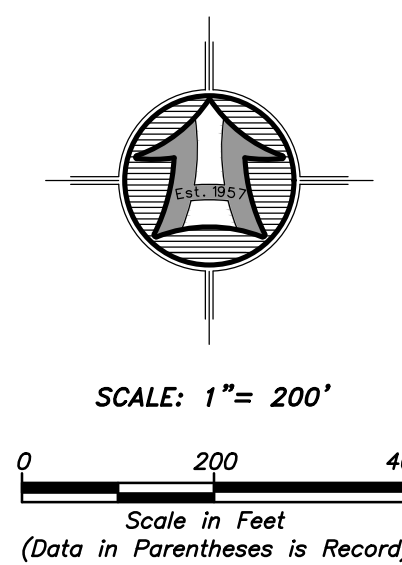
PROJECT COORDINATES ARE MODIFIED STATE PLANE - UTAH NORTH ZONE (GROUND)
ELEVATIONS ARE NAVD 88

PROJECT COMBINED SCALE FACTOR = 0.999768823
SCALED FROM 0,0

PROJECT COORDINATES WERE TRUNCATED BY SUBTRACTING THE FOLLOWING:
-899.138' FROM THE MODIFIED STATE PLANE NORTHING VALUES
-347.887' FROM THE MODIFIED STATE PLANE EASTING VALUES

CONTROL POINTS

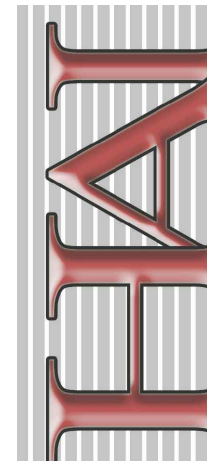
| POINT # | NORTHING | EASTING | ELEVATION | DESCRIPTION |
|---------|--------------|--------------|-----------|----------------------|
| 100 | 3712643.8160 | 1496953.6080 | 4338.062 | CL MON |
| 101 | 3711916.7160 | 1496938.2440 | 4338.016 | CL MON |
| 102 | 3711012.8360 | 1496920.4320 | 4346.886 | CL MON |
| 103 | 3710822.9190 | 1496878.3730 | 4347.221 | SEC COR * PROJECT BM |
| 104 | 3710188.6410 | 1496902.5480 | 4352.285 | CL MON |
| 105 | 3709476.7050 | 1496044.4060 | 4342.095 | CL MON |
| 106 | 3713245.4170 | 1495636.6190 | 4297.119 | CL MON |
| 107 | 3713618.8800 | 1495639.2920 | 4288.931 | CL MON |
| 108 | 3711137.8940 | 1494753.0600 | 4288.212 | RWC HAI |



LEGEND

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| | STREET MONUMENT |
| | FOUND REBAR SET BY HAI |
| | SECTION CORNER |

HANSEN & ASSOCIATES, INC.
Consulting Engineers and Land Surveyors
538 North Main Street, Brigham, Utah 84302
Visit us at www.hai.net
Brigham City, Utah
(435) 723-4491 (801) 399-4905 (435) 732-8272
Celebrating over 65 Years of Business



Drawn By: RS Date: 09/08/2023
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Checked By: _____
Approved By: _____
Scale: 1" = 200'
Drawing File: 21-3-65v19_control.dwg
JOB NUMBER: 21-3-65

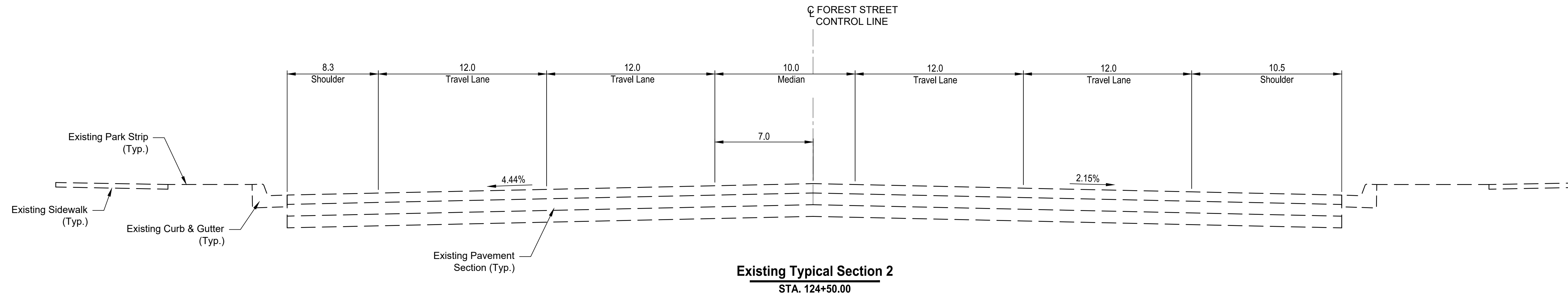
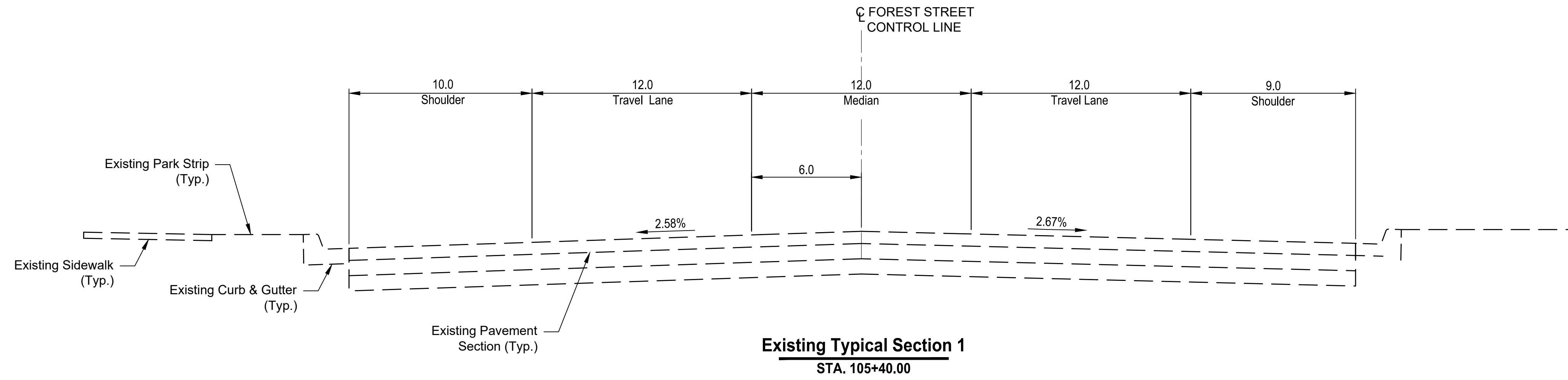
CONTROL SURVEY FOR
FOREST STREET BRIDGE
FOREST STREET & 800 WEST
BRIGHAM CITY, BOX ELDER COUNTY, UTAH
A PART OF THE SOUTHEAST QUARTER OF SECTION 14,
TOWNSHIP 9 NORTH, RANGE 2 WEST, S.L.B.&M.

DRAWING NO.
6 OF 63

SC-01

| No. | Date | By | Revision |
|-----|------|----|----------|
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LAYOUT: TS-01 PATH: u:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995secs\CADD\DWG\Civil PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 12:20:45 PM

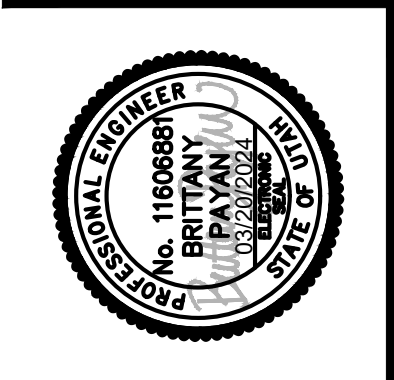


| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH
AT FULL
SCALE IF
NOT
OTHERWISE
SPECIFIED
ACCORDINGLY

Parametrix

| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | AP |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

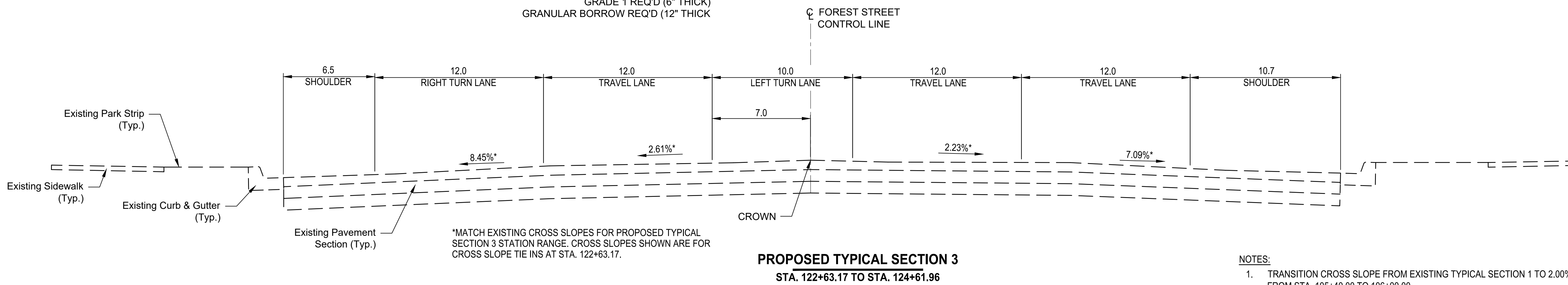
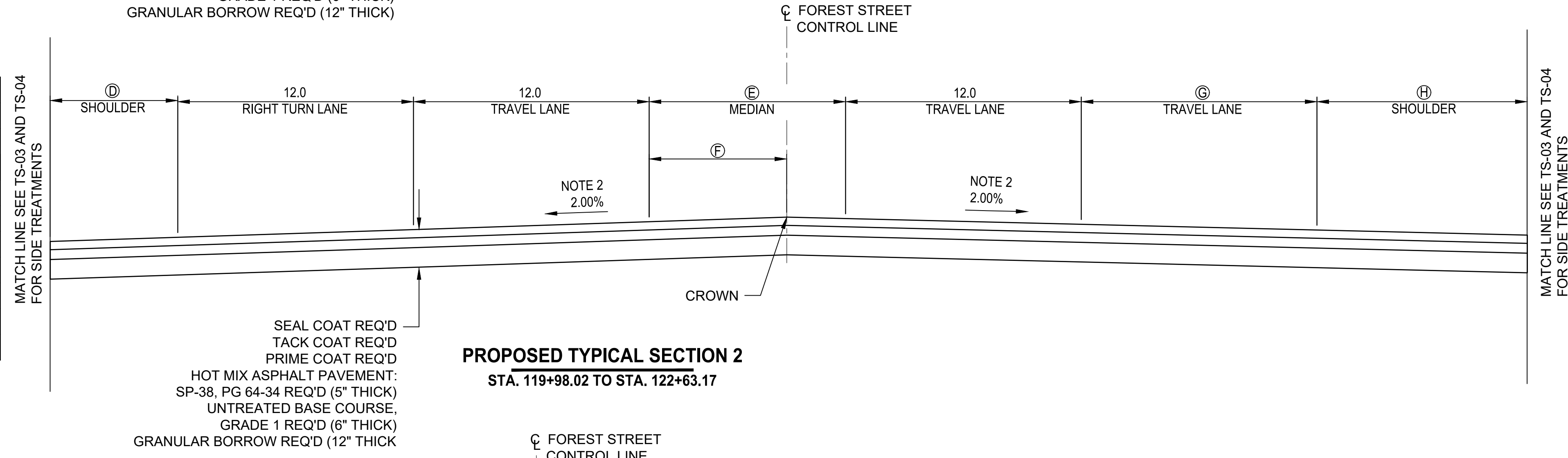
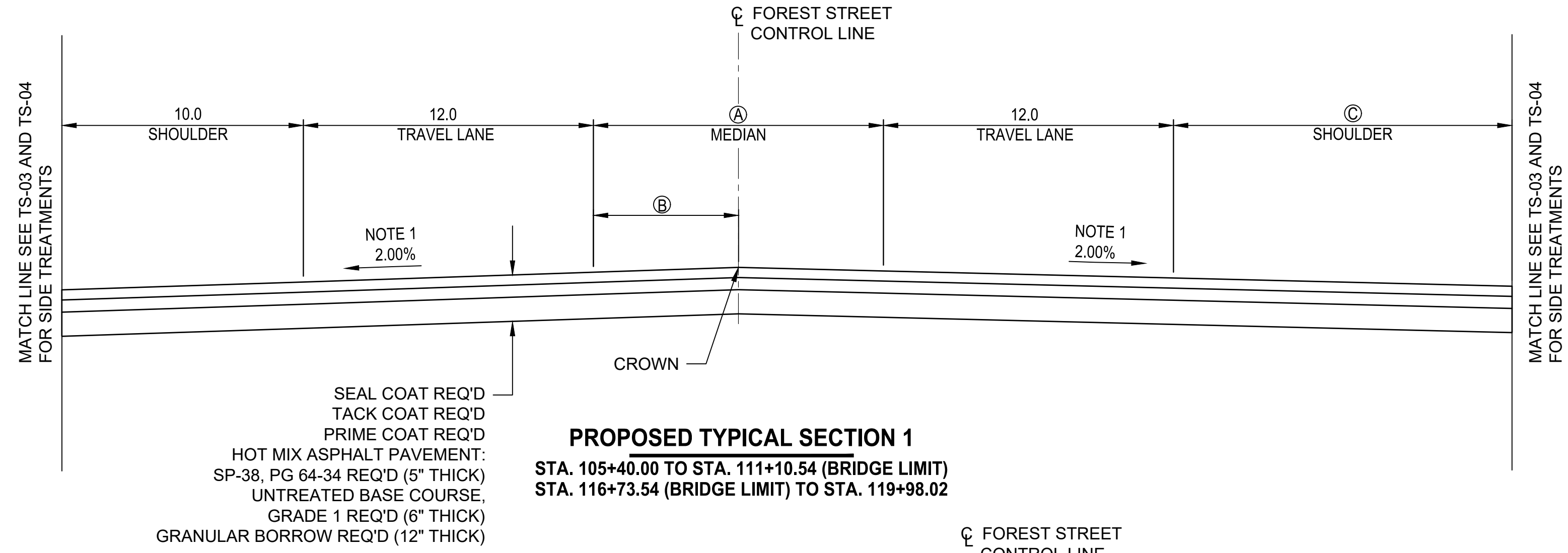
TYPICAL SECTION

DRAWING NO.
7 OF 63
TS-01

LAYOUT: TS-02 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:20:46 PM

| | A | B | C |
|-----------|----------|-----|-------------|
| 105+40.00 | 12.0 | 6.0 | 9.0 |
| 106+52.89 | 12.0 | 6.0 | 9.0 |
| 106+78.23 | 12.0 | 6.0 | 9.0 TO 14.0 |
| 109+46.64 | 12.0 | 6.0 | 14.0 |
| 109+91.03 | 12.0 | 6.0 | IN TAPER |
| 110+41.03 | IN TAPER | 6.0 | IN TAPER |
| 110+91.23 | 6.0 | 4.0 | 12.0 |
| 111+10.54 | 6.0 | 4.0 | 12.0 |
| 116+73.54 | 6.0 | 4.0 | 12.0 |
| 116+93.04 | 6.0 | 4.0 | 12.0 |
| 118+43.04 | 12.0 | 4.0 | 12.0 |
| 119+98.02 | 12.0 | 4.0 | 12.0 |

| | D | E | F | G | H |
|-----------|----------|----------|----------|----------|----------|
| 119+98.02 | 4.0 | 12.0 | 4.0 | 0.0 | 12.0 |
| 120+51.06 | 4.0 | 12.0 | 4.0 | IN TAPER | 12.0 |
| 120+58.18 | 4.0 | IN TAPER | 4.0 | IN TAPER | 12.0 |
| 121+61.31 | 4.0 | IN TAPER | IN TAPER | IN TAPER | 12.0 |
| 121+69.21 | 4.0 | IN TAPER | IN TAPER | IN TAPER | IN TAPER |
| 121+94.21 | IN TAPER | IN TAPER | 7.0 | IN TAPER | IN TAPER |
| 122+01.33 | IN TAPER | 10.0 | 7.0 | IN TAPER | IN TAPER |
| 122+46.86 | 6.5 | 10.0 | 7.0 | IN TAPER | IN TAPER |
| 122+57.00 | 6.5 | 10.0 | 7.0 | 12.0 | 10.7 |
| 122+63.17 | 6.5 | 10.0 | 7.0 | 12.0 | 10.7 |



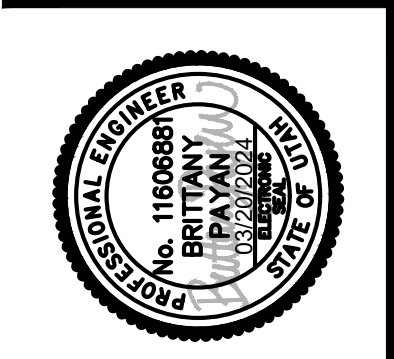
- NOTES:
1. TRANSITION CROSS SLOPE FROM EXISTING TYPICAL SECTION 1 TO 2.00% FROM STA. 105+40.00 TO 106+00.00.
 2. TRANSITION CROSS SLOPE FROM 2.00% TO PROPOSED TYPICAL SECTION 3 FROM STA. 121+50.00 TO 122+63.27.

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE, IF NOT OTHERWISE INDICATED

Parametrix

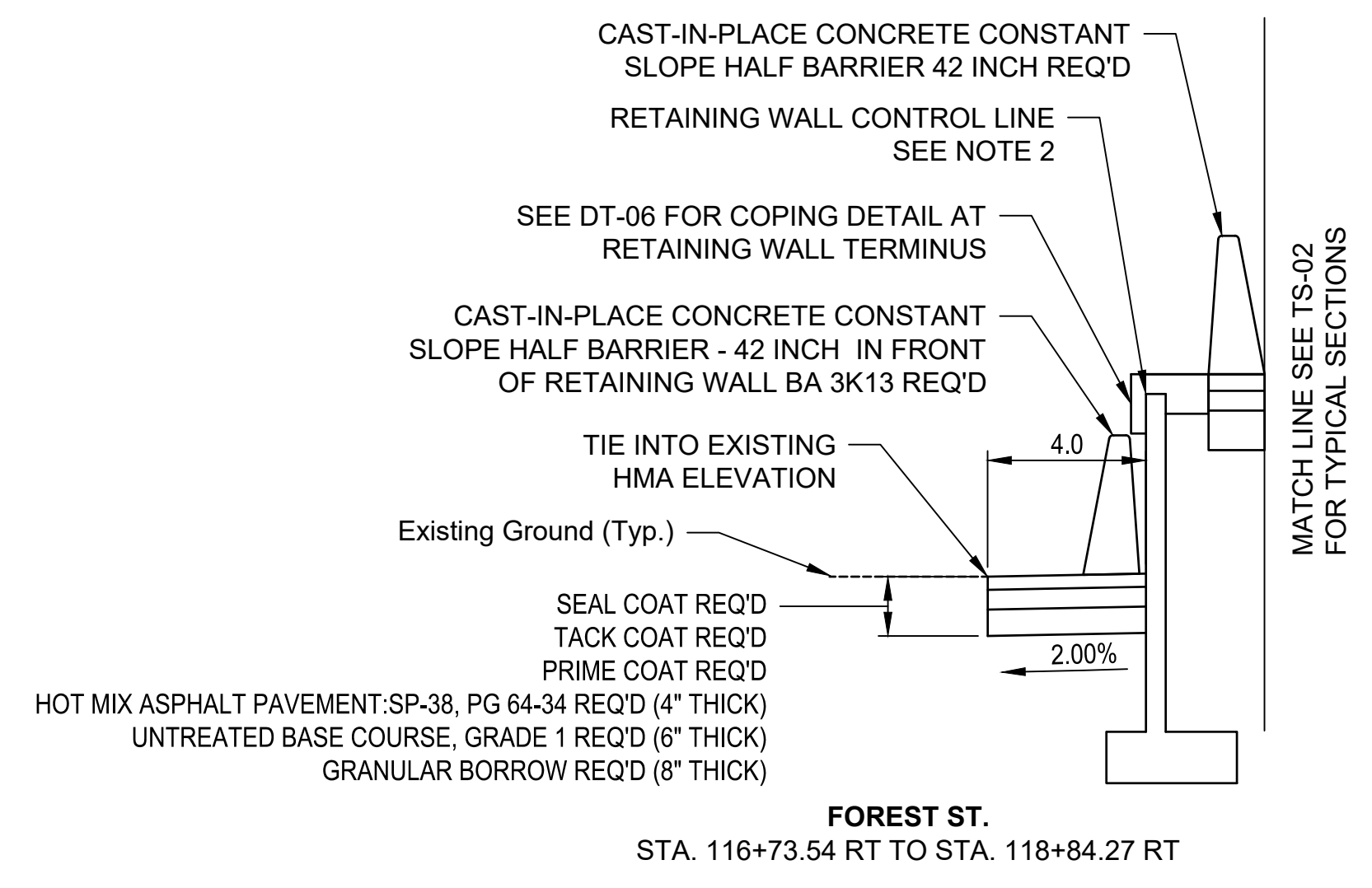
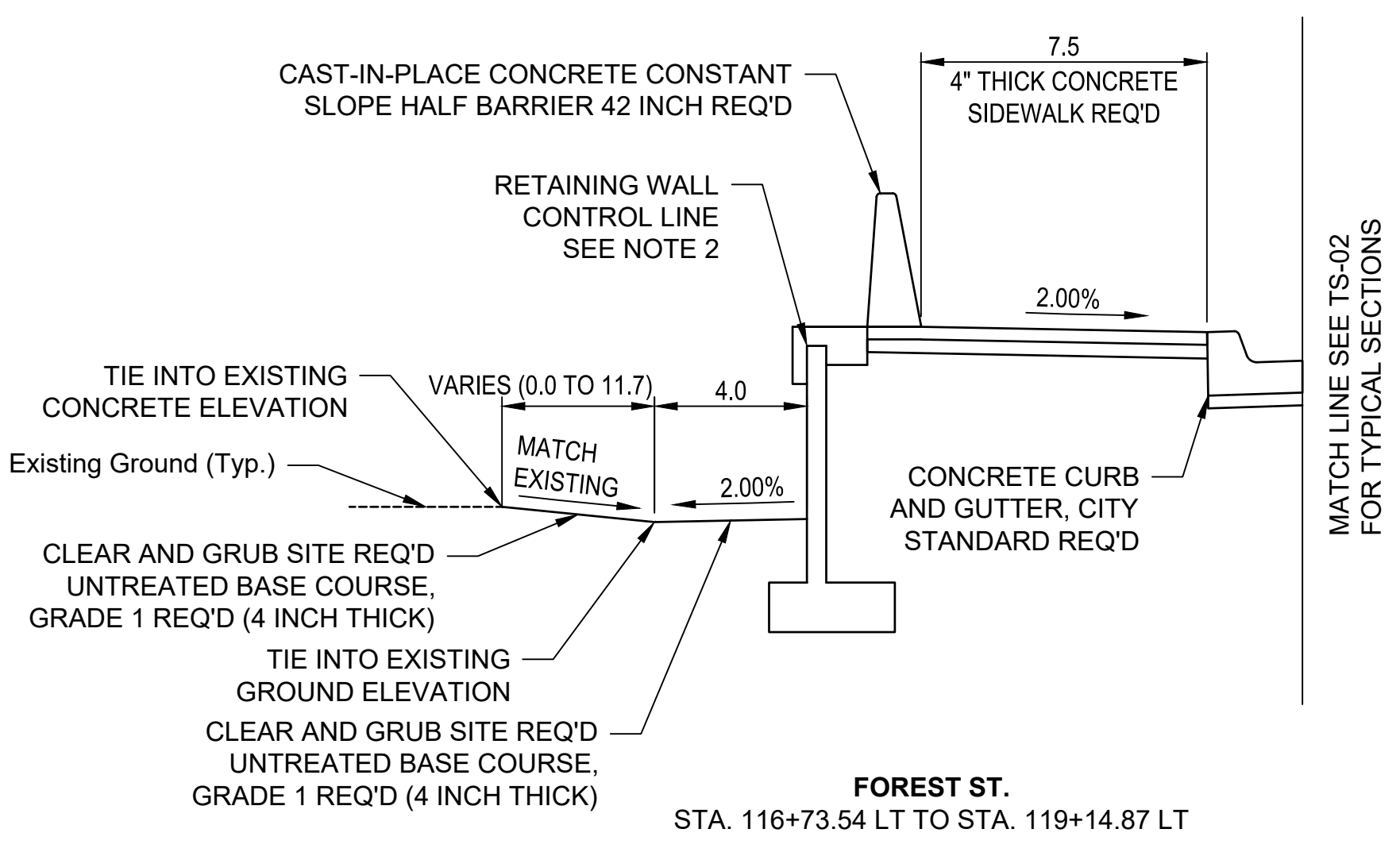
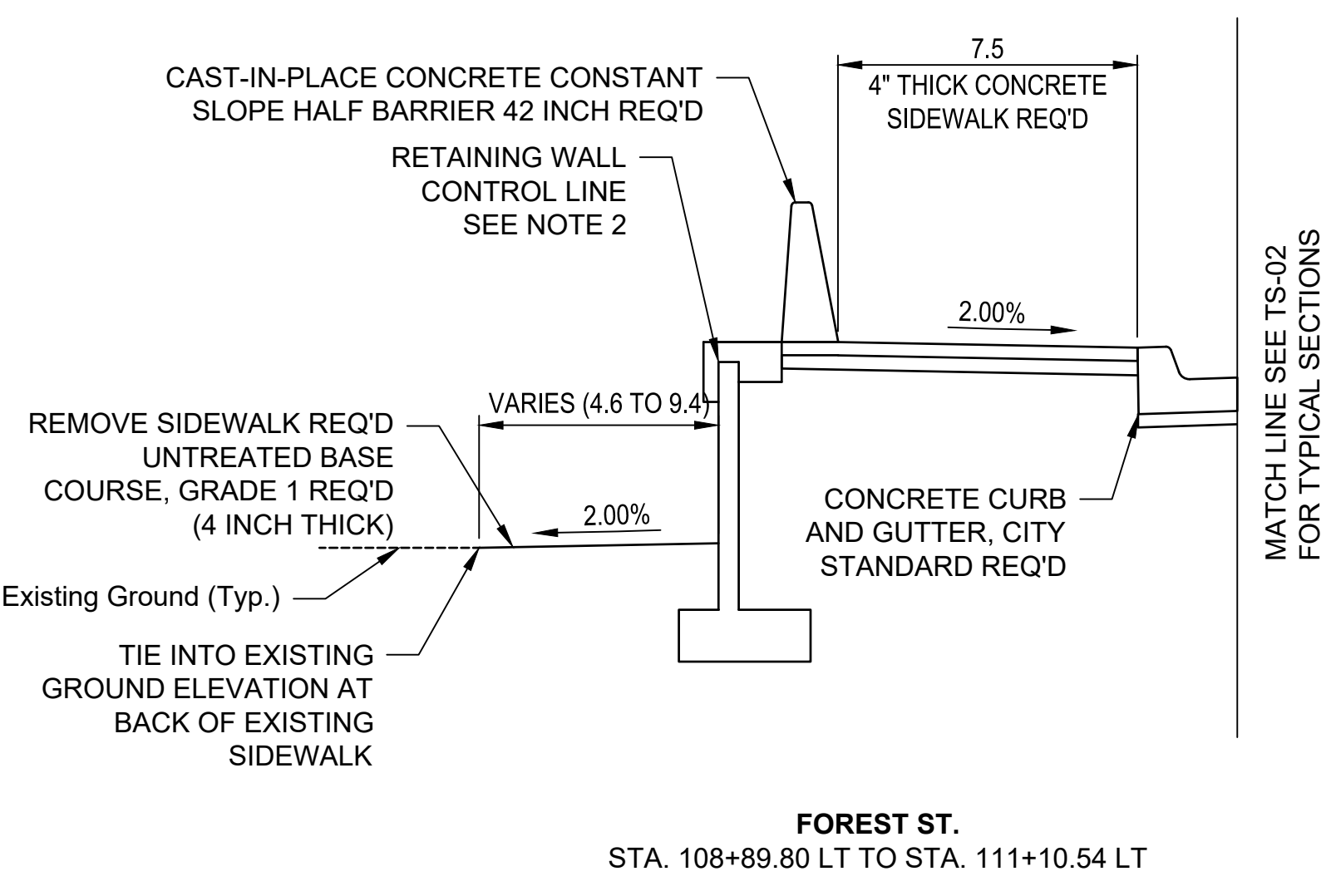
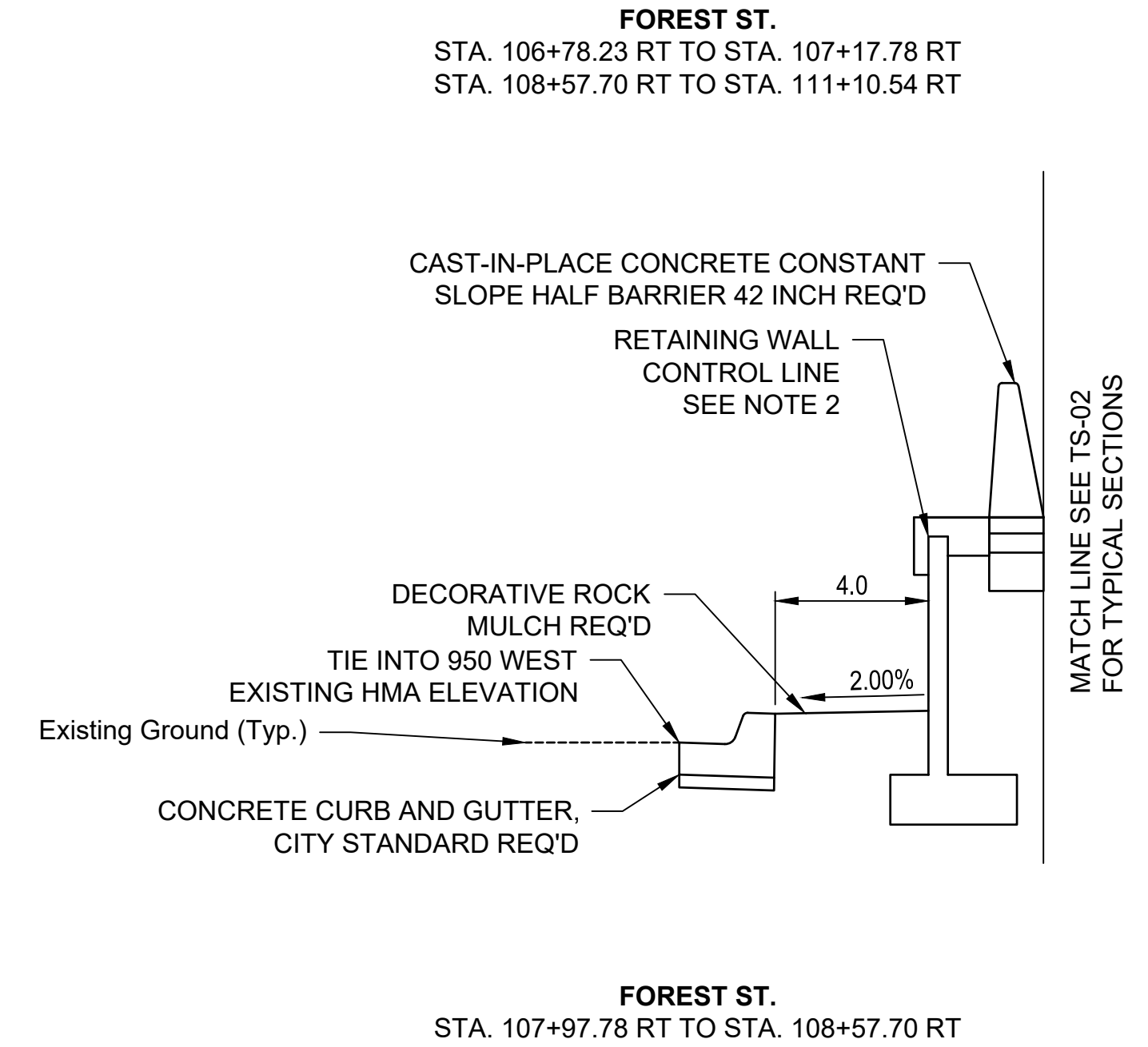
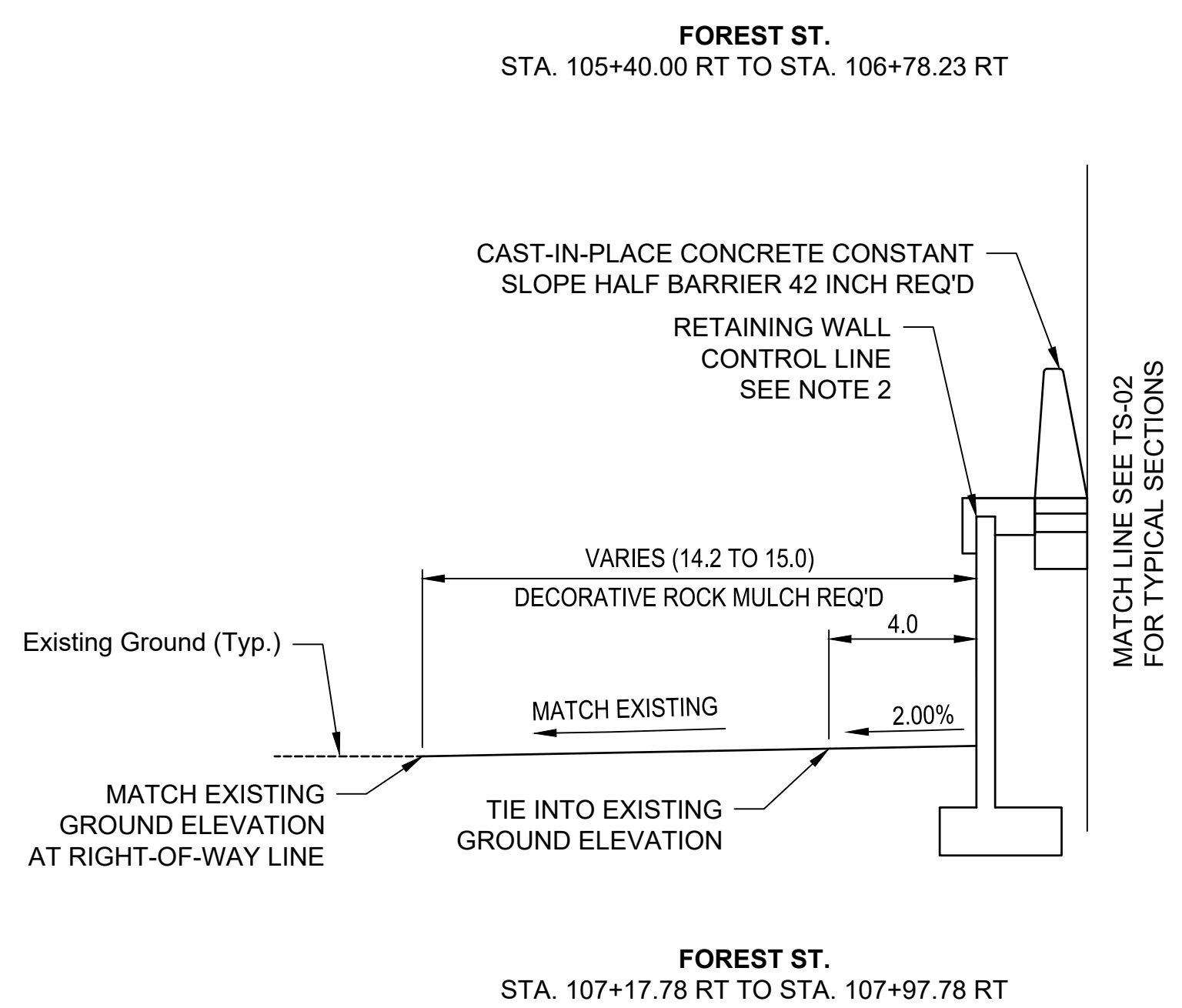
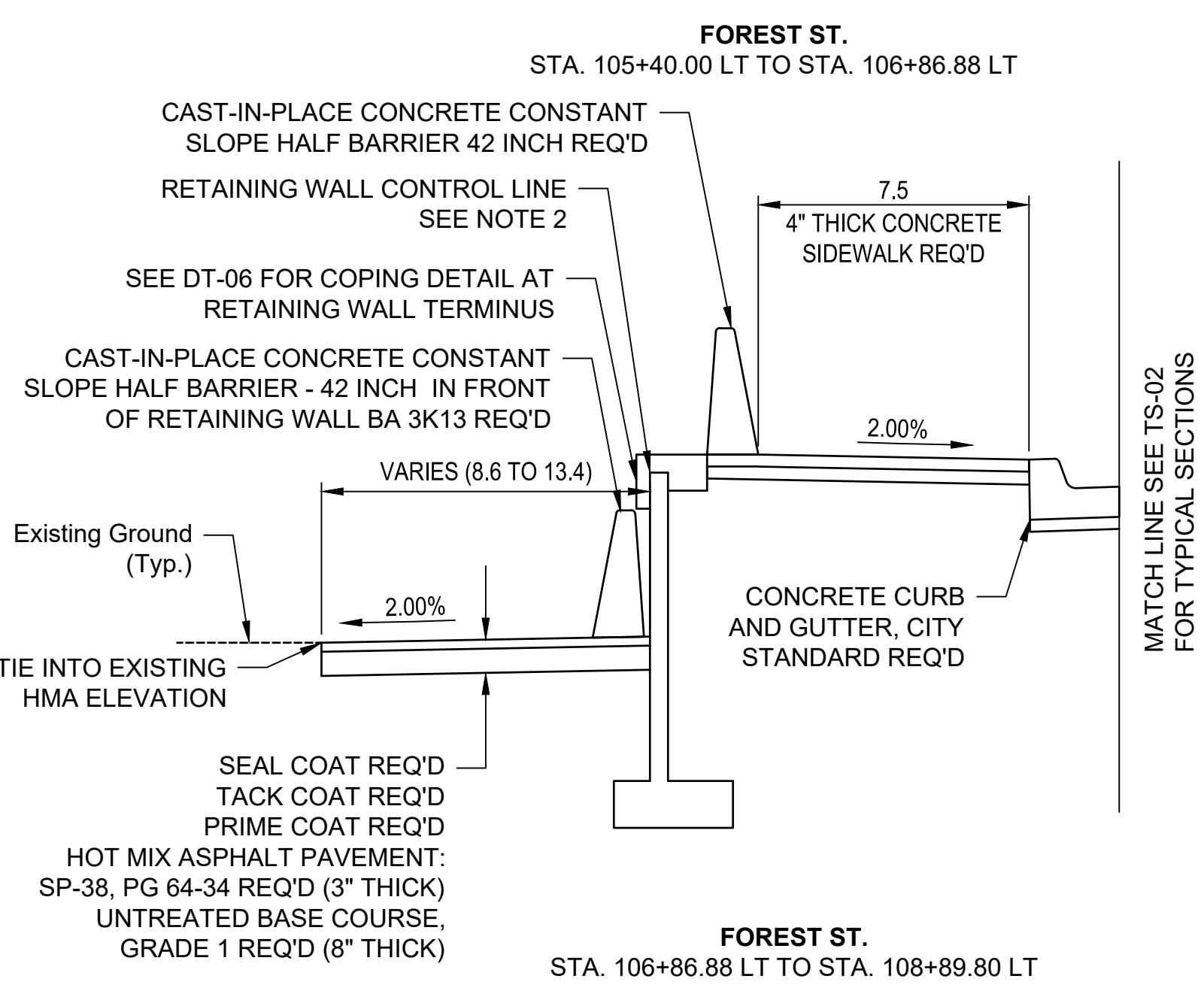
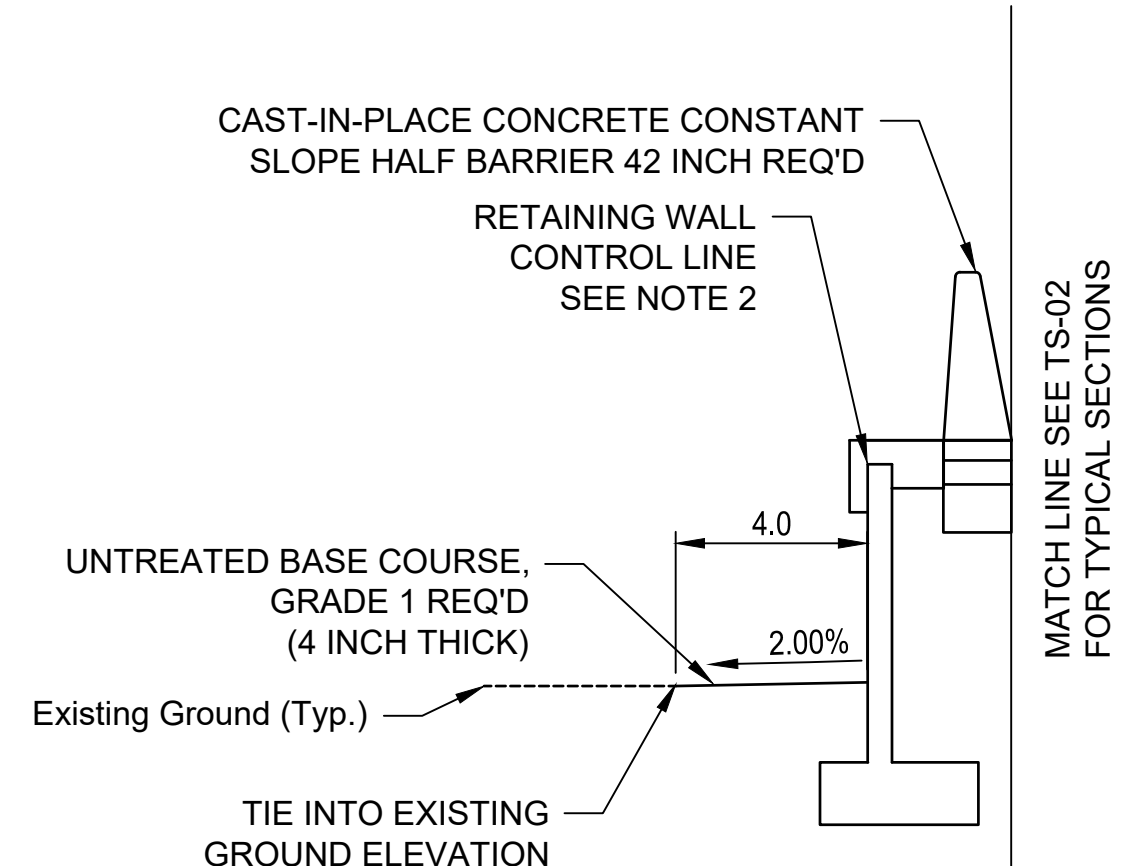
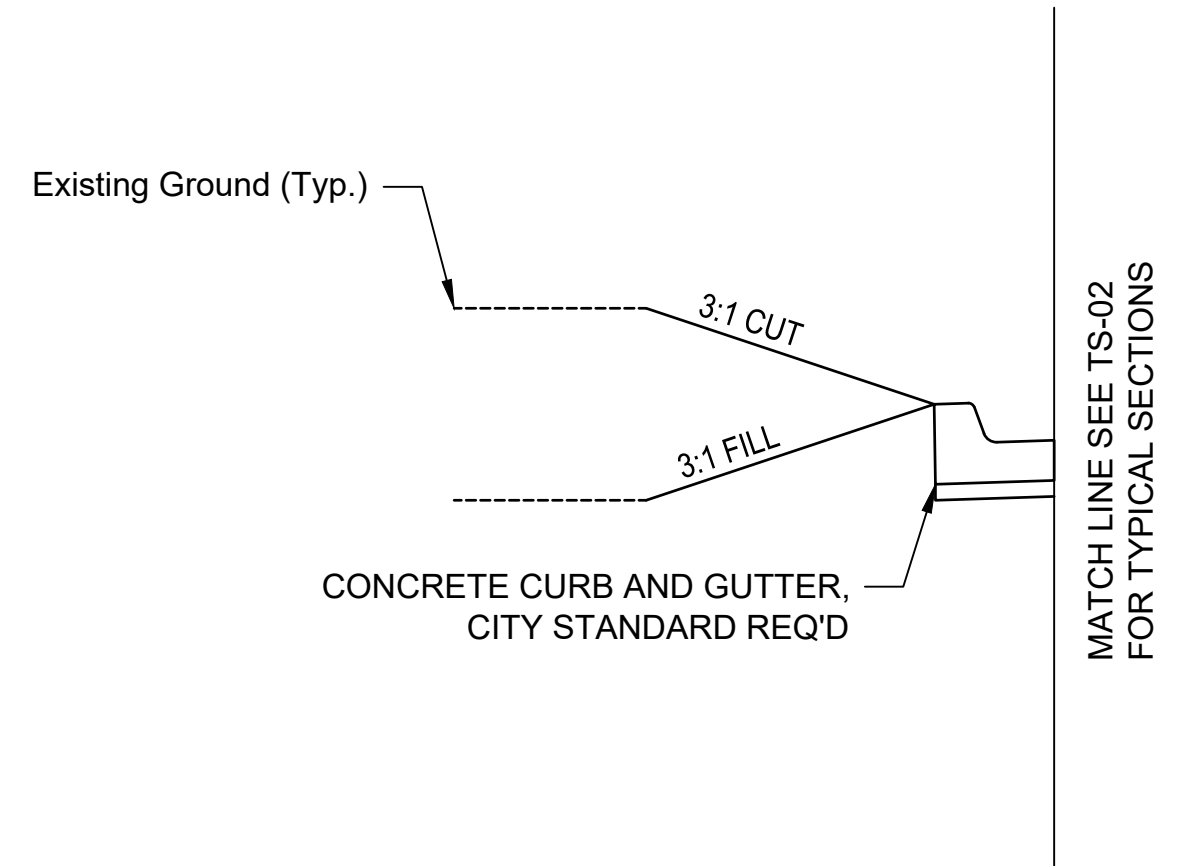
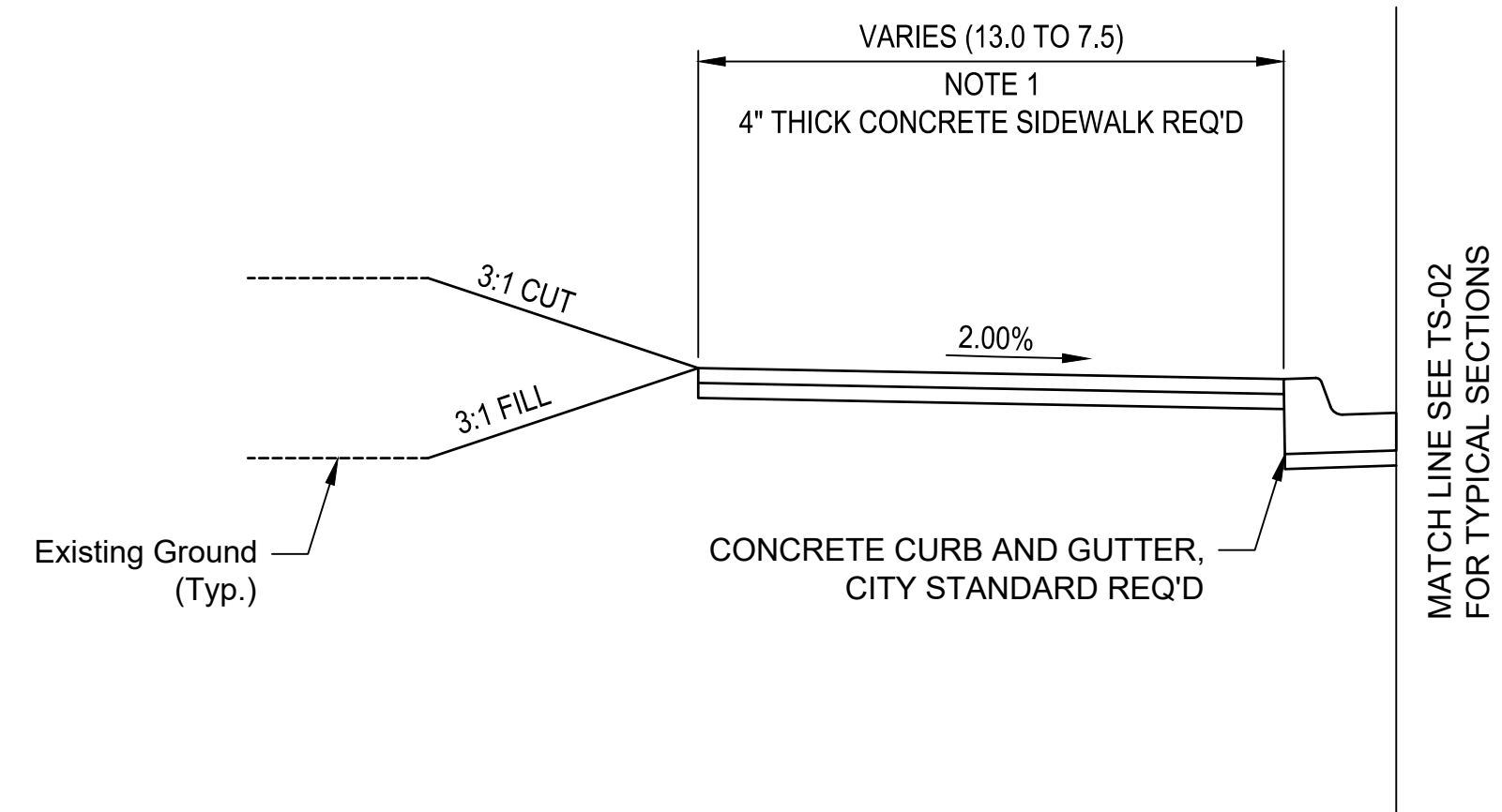
DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: AP
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

TYPICAL SECTION

LAYOUT: TS-03
 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995Secs\CADD\DWG\Civil
 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:20:51 PM



SIDE TREATMENTS

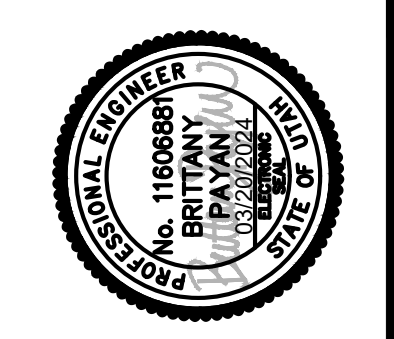
- NOTES:
- SEE ROADWAY SHEETS FOR OFFSET WIDTHS AND LOCATIONS.
 - SEE STRUCTURE SHEETS FOR RETAINING WALL DETAILS.
 - SEE BRIGHAM CITY STANDARD DRAWINGS FOR CURB AND GUTTER AND SIDEWALK DETAILS.
 - SEE UDOT STD DWG BA 3K13 FOR 42 INCH HALF BARRIER PLACEMENT IN FRONT OF RETAINING WALLS.

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

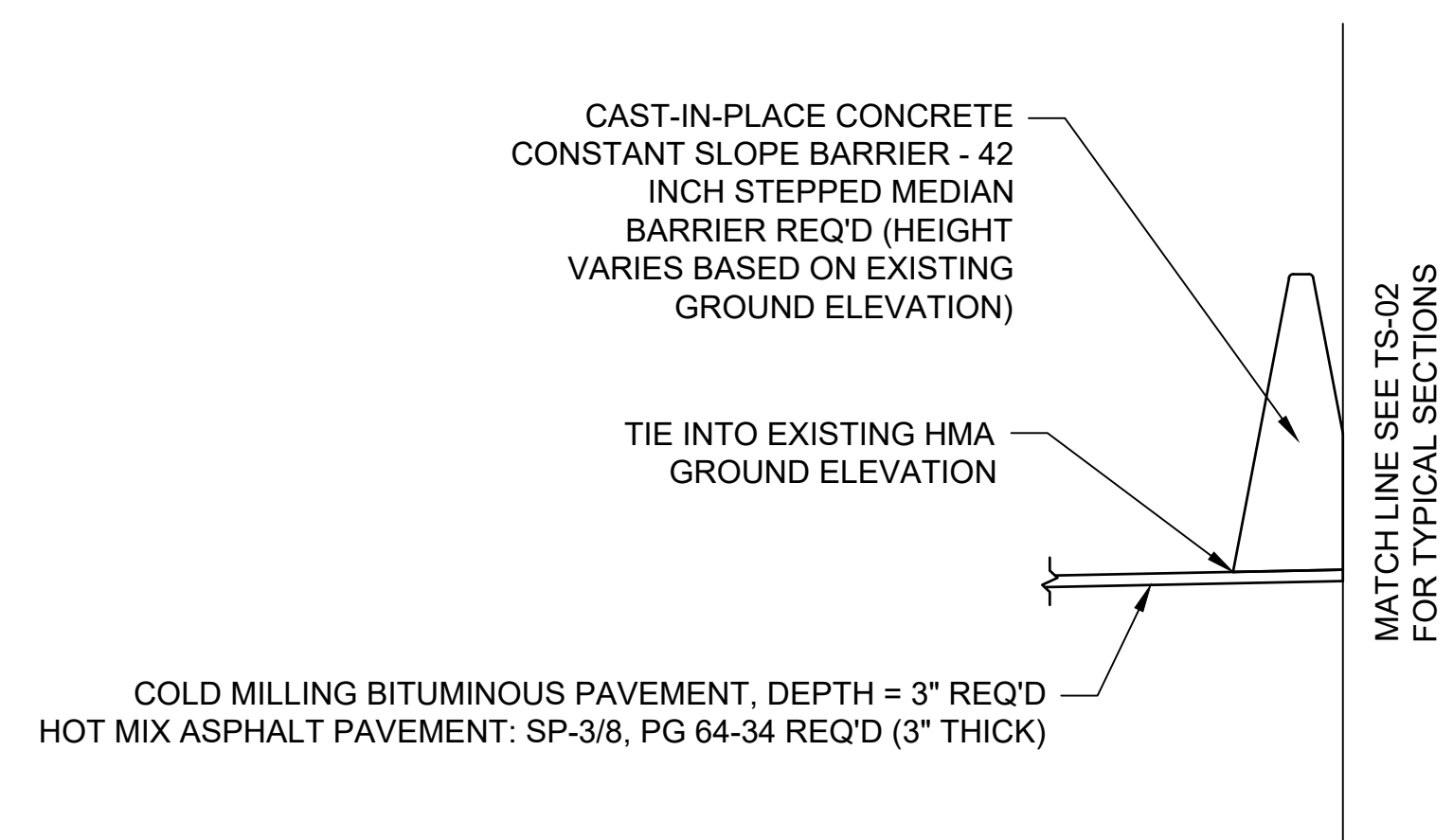
DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: AP
 APPROVED: AP



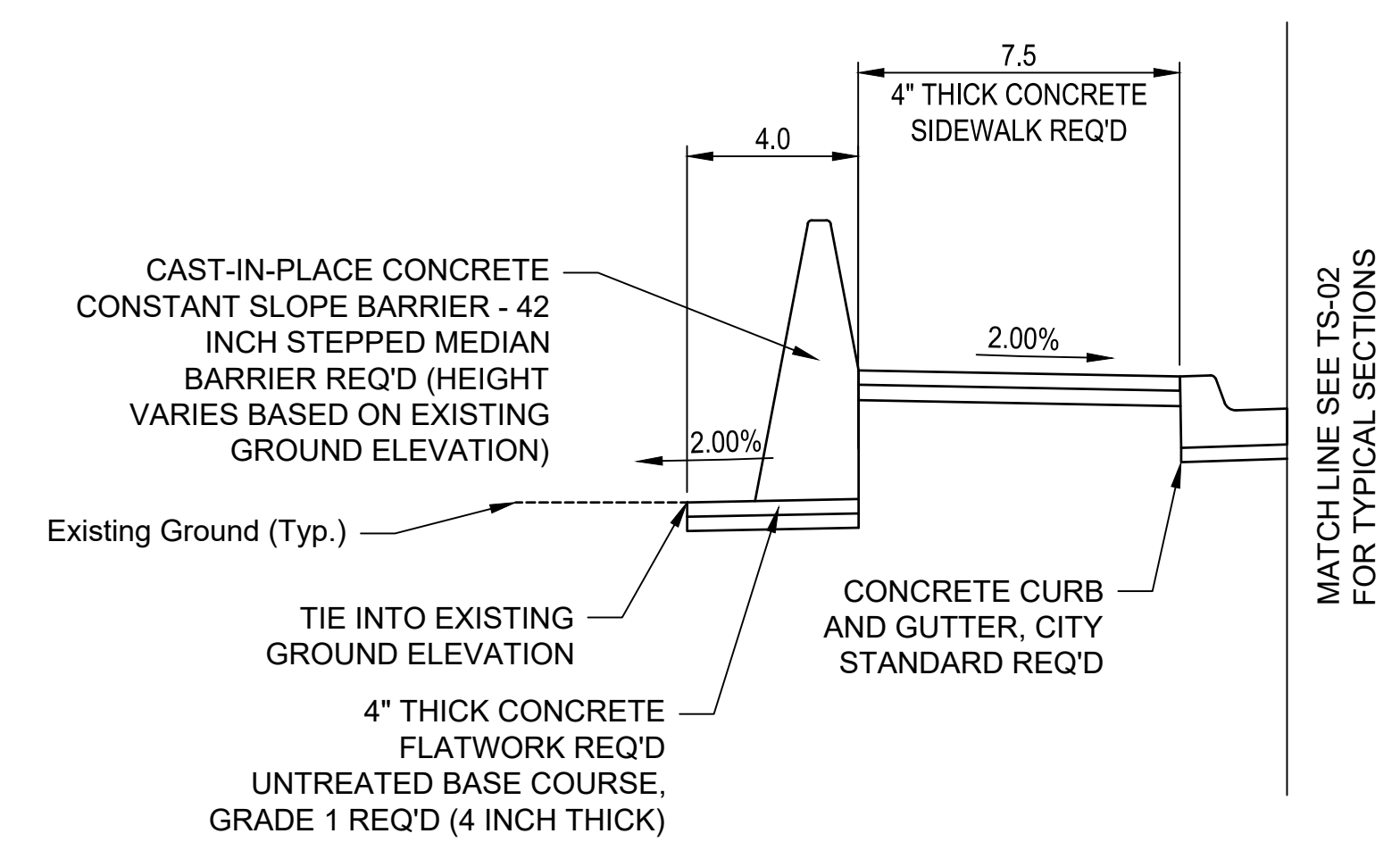
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

TYPICAL SECTION

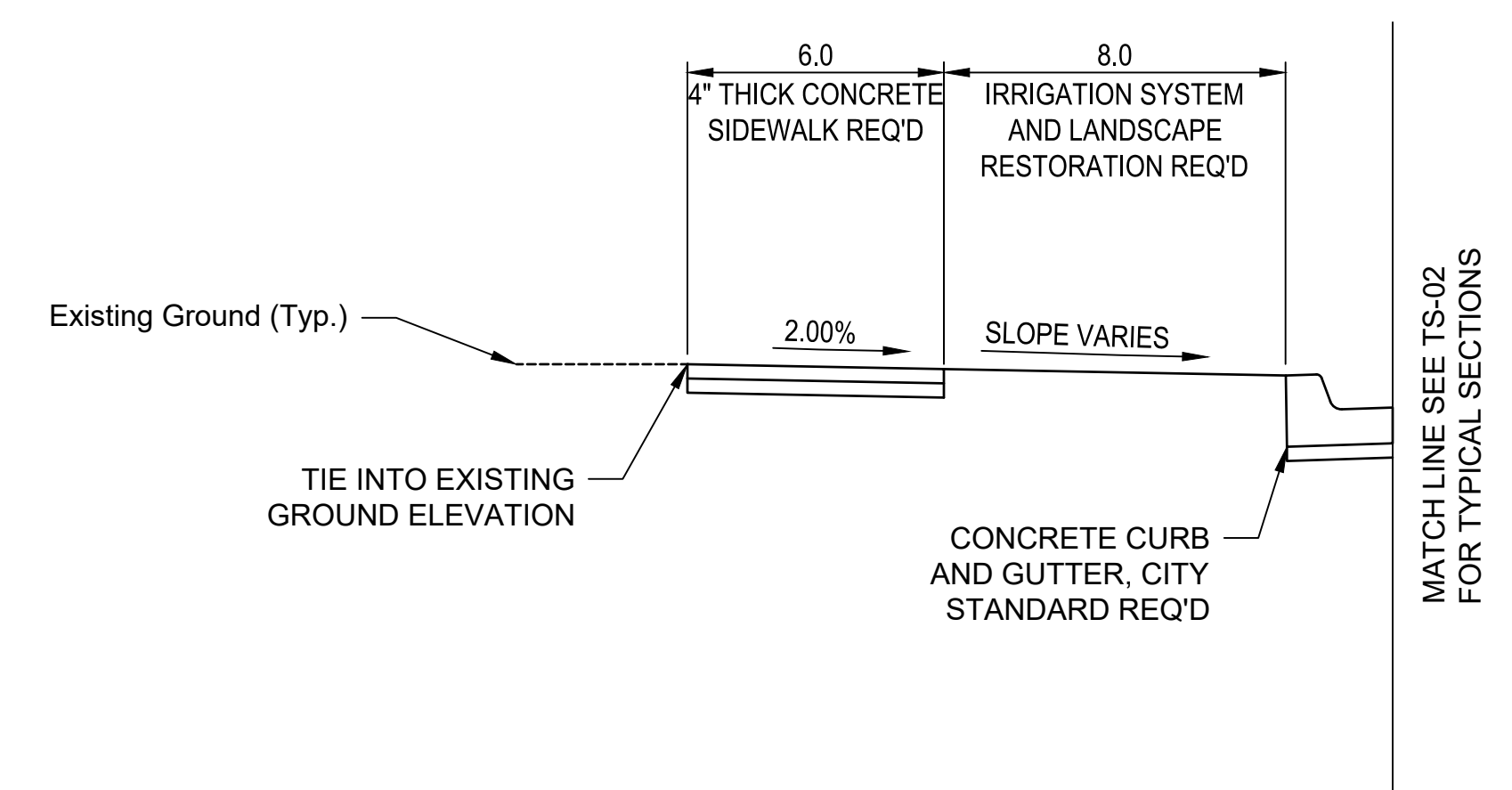
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 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:20:56 PM
 LAYOUT: TS-04



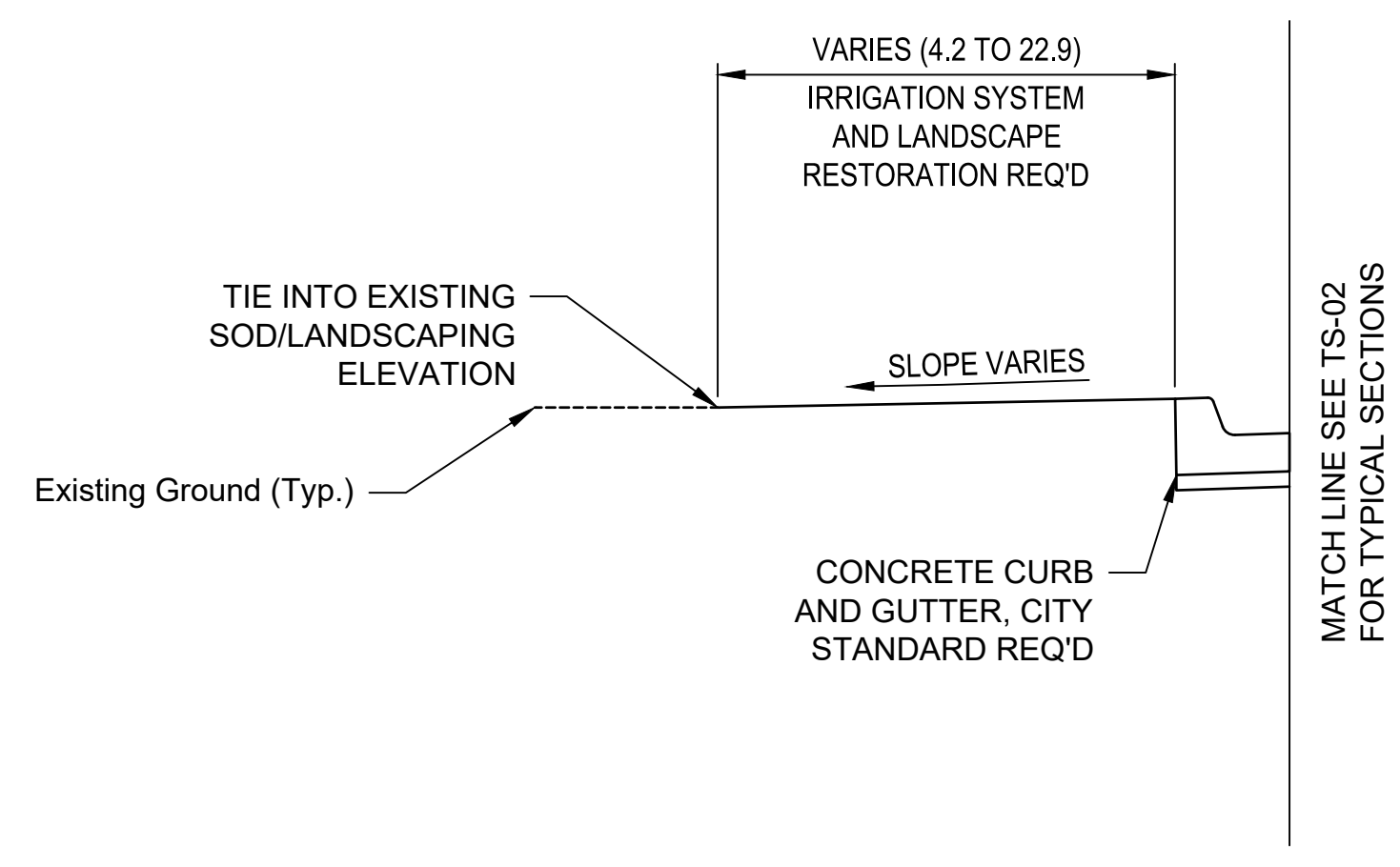
FOREST ST.
 STA. 118+84.27 RT TO STA. 119+86.02 RT



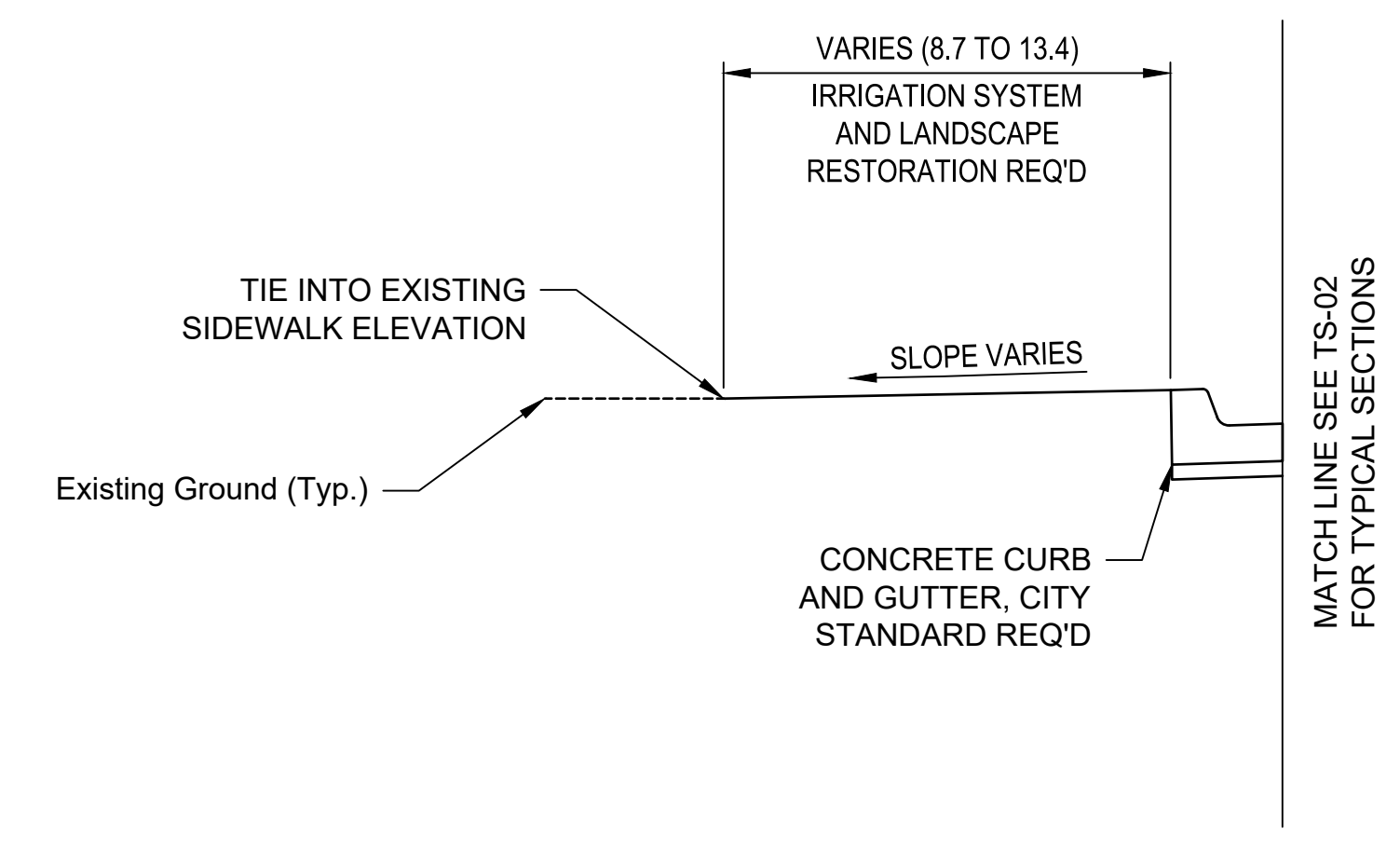
FOREST ST.
 STA. 119+14.87 LT TO STA. 119+54.46 LT



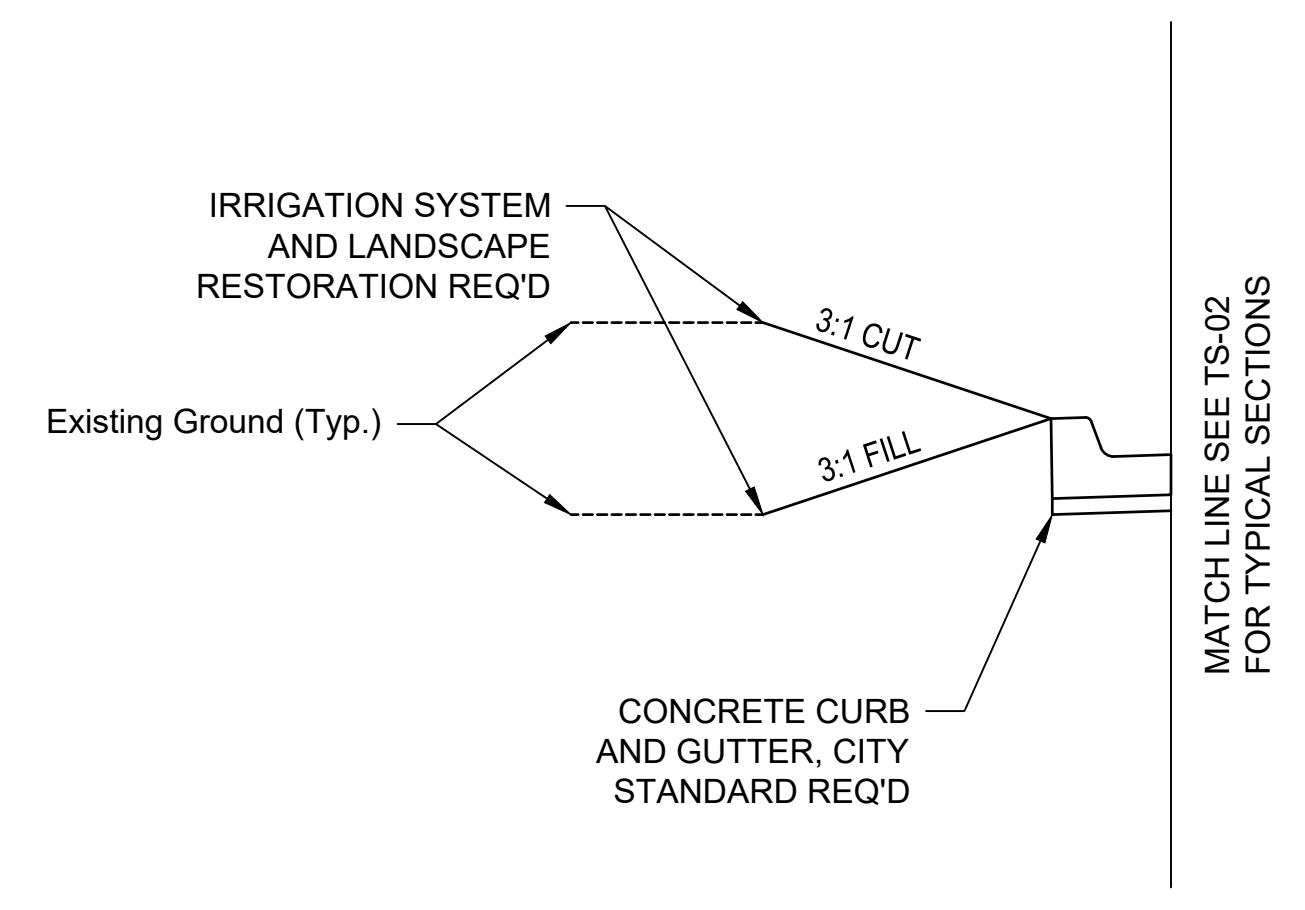
FOREST ST.
 STA. 120+18.58 LT TO STA. 122+63.17 LT



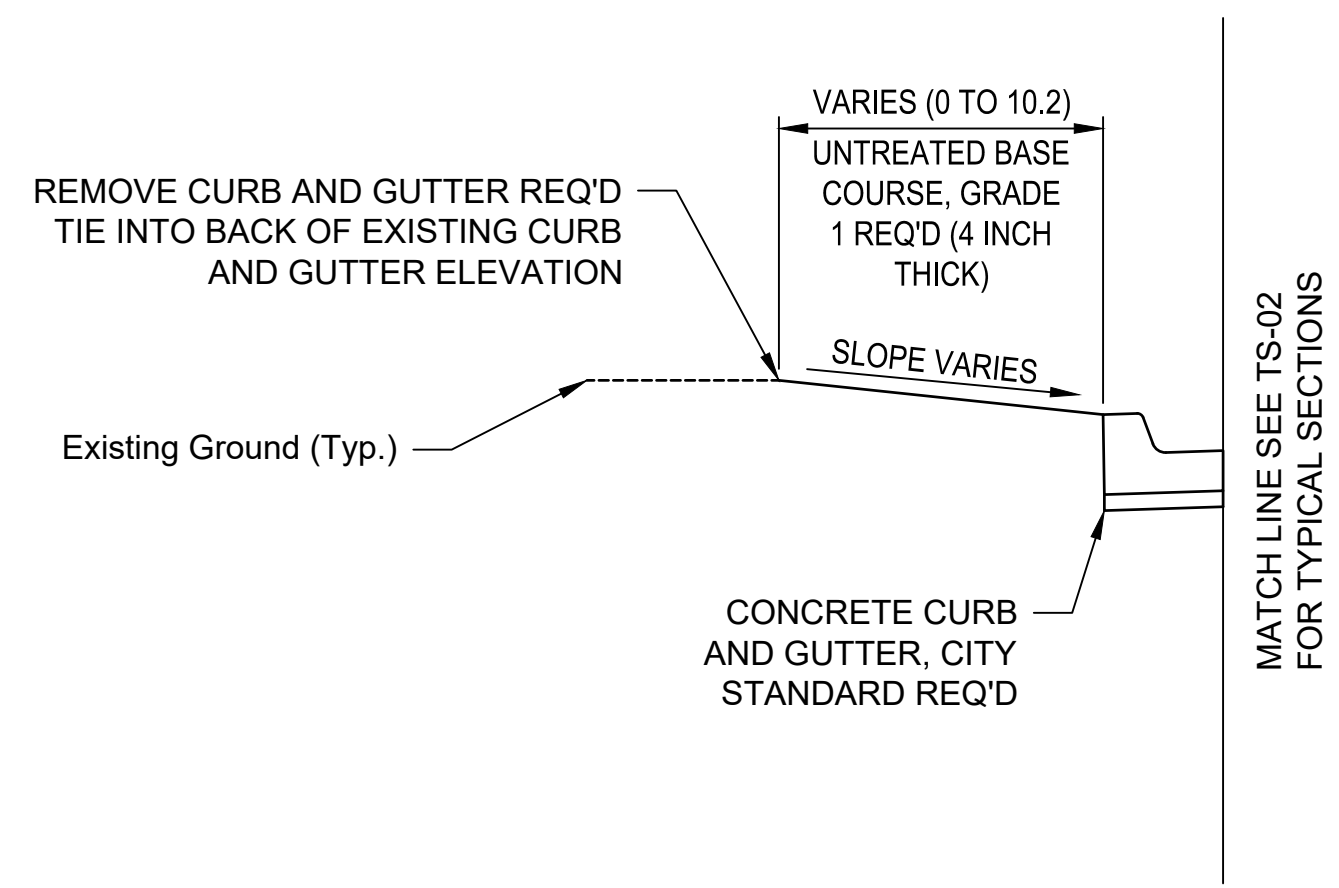
FOREST ST.
 STA. 120+36.00 RT TO STA. 121+84.05 RT



FOREST ST.
 STA. 121+84.05 RT TO STA. 122+63.17 RT



CITY POOL ACCESS
 STA. 80+47.68 RT TO STA. 81+50.00 RT



CITY POOL ACCESS
 STA. 81+08.07 LT TO STA. 81+50.00 LT

SIDE TREATMENTS

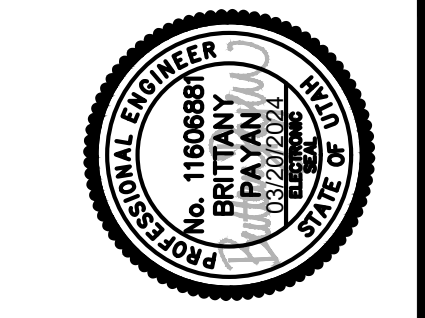
- NOTES:**
1. SEE ROADWAY SHEETS FOR OFFSET WIDTHS AND LOCATIONS.
 2. SEE STRUCTURE SHEETS FOR RETAINING WALL DETAILS.
 3. SEE BRIGHAM CITY STANDARD DRAWINGS FOR CURB AND GUTTER AND SIDEWALK DETAILS.

| REVISIONS | DATE | BY |
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ONE INCH
 AT FULL
 SCALE. IF
 NOT
 ACCORDINGLY

Parametrix

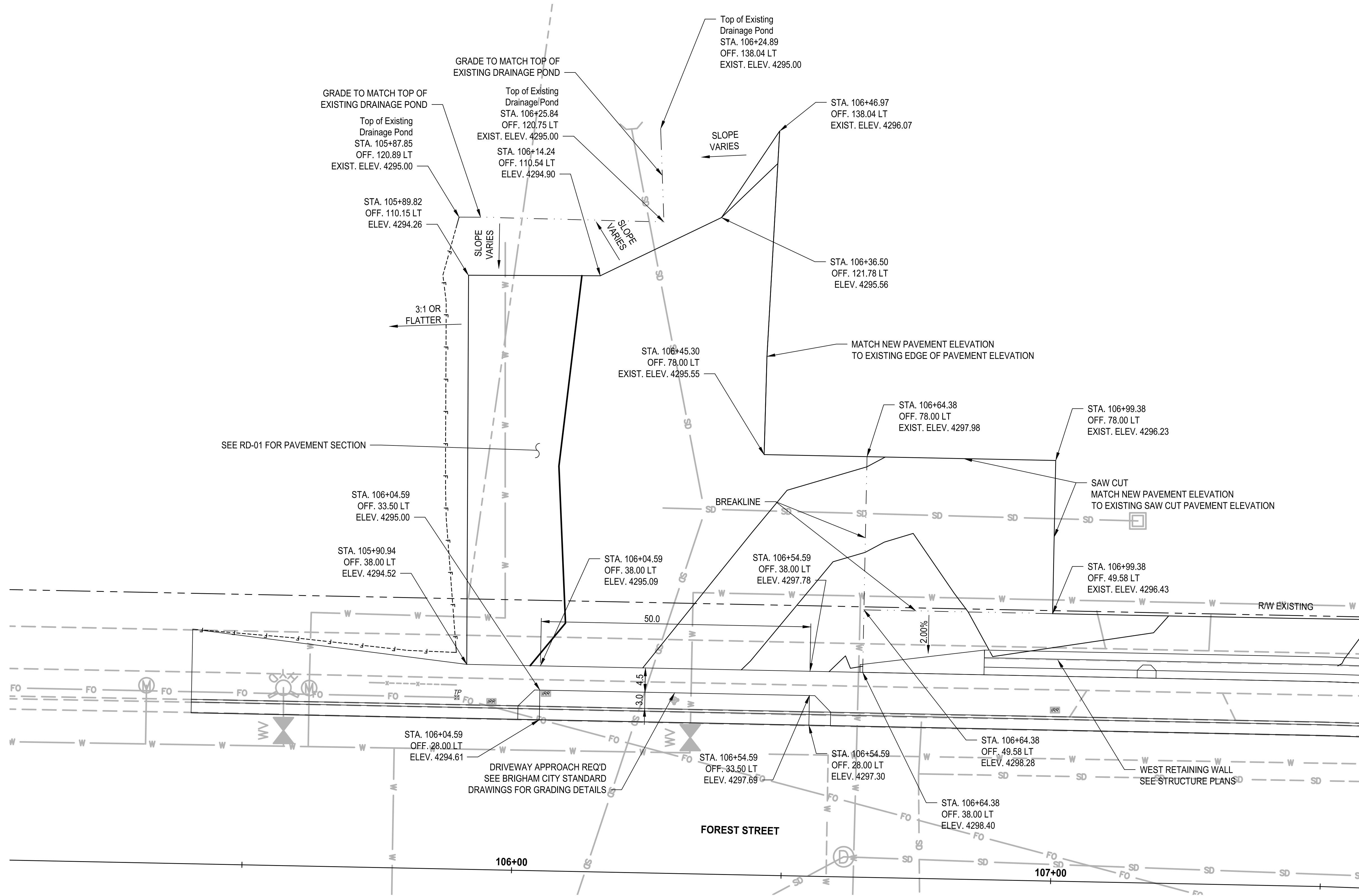
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| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | AP |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

TYPICAL SECTION

LAYOUT: DT-01 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-02 Forest St Final Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:21:13 PM



**FOREST STREET
DRIVEWAY DETAIL 1 OF 5**
SCALE 1" = 10'



| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

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| DESIGNED | DRAWN | CHECKED | APPROVED |
| 03/20/2024 | BKP | BKP | AP |
| JOB No. 344-8541-002 | | | |

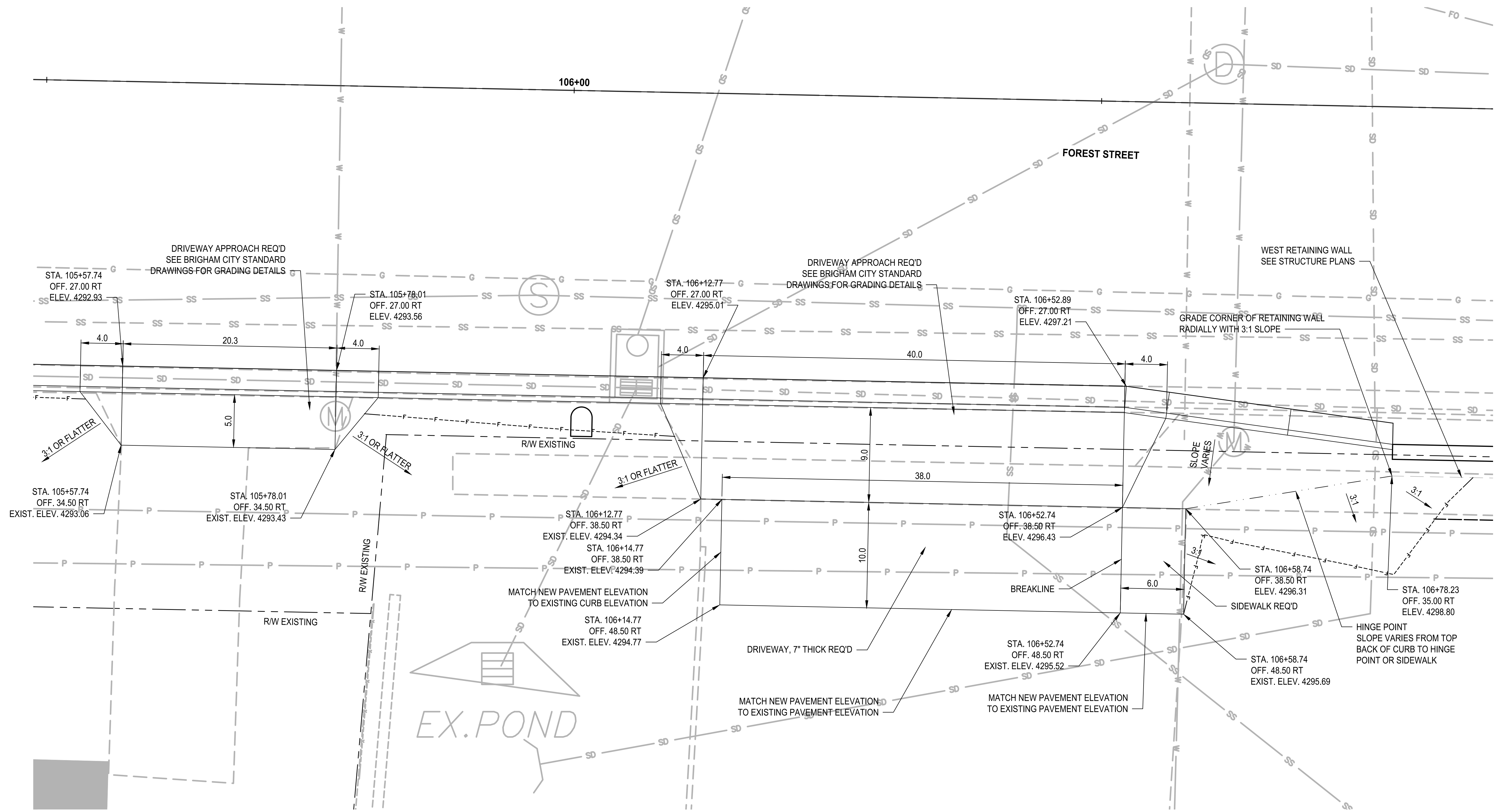


PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

DETAIL

DRAWING NO.
11 OF 63
DT-01

LAYOUT: DT-02 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002_Forest St.Final Design\995svcs\CADD\DWG\Civil PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 12:21:28 PM



FOREST STREET
DRIVEWAY DETAIL 2 OF 5
 SCALE 1" = 10'

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: BKP
 DRAWN BY: BKP
 CHECKED BY: AP
 APPROVED BY: AP


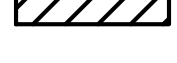
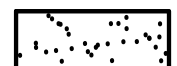





DATE: 03/20/2024
 JOB No.: 344-8541-002

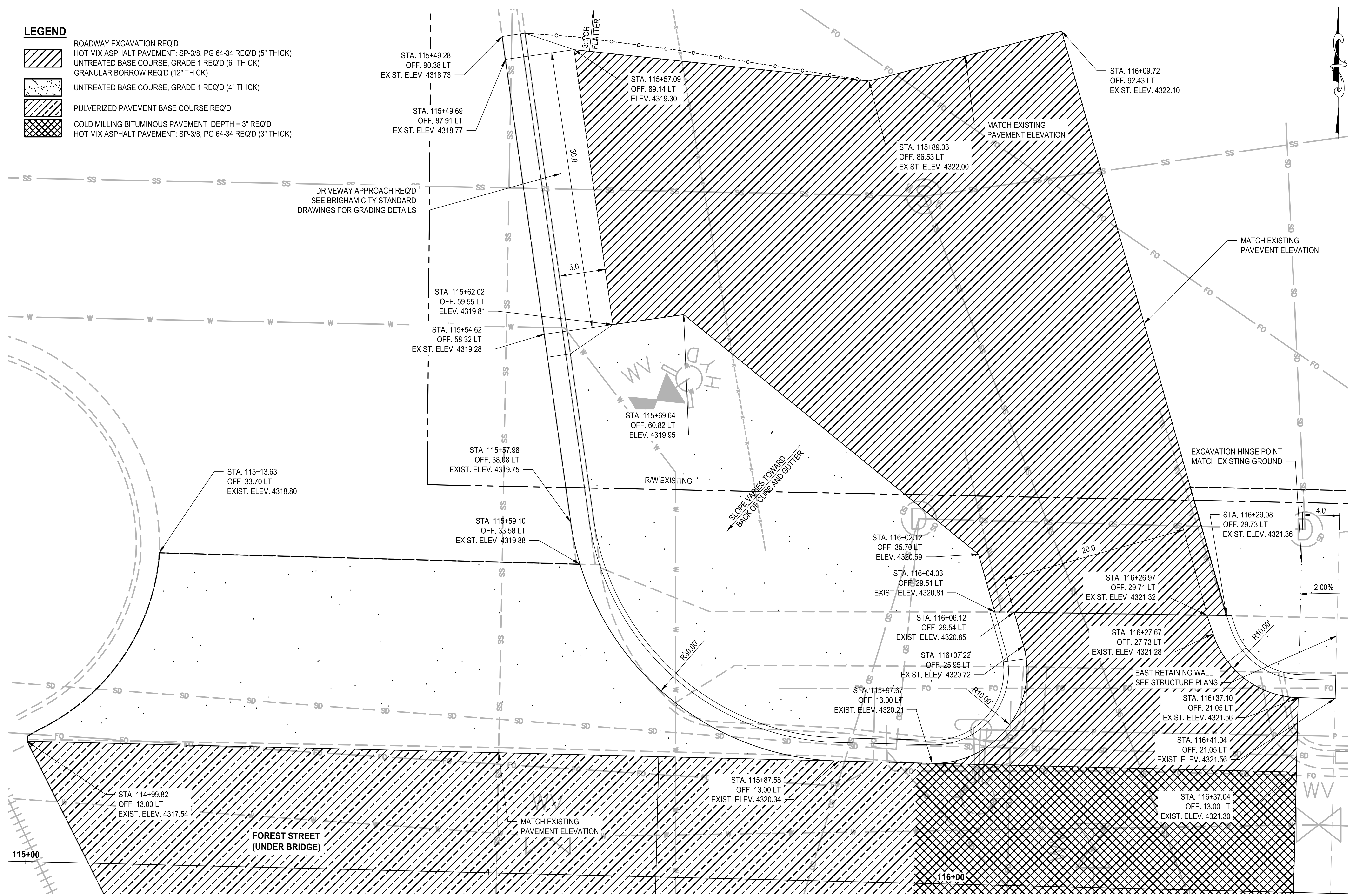


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

LAYOUT: DT-03
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 PLOTTED BY: OliveStg DATE: Friday, March 22, 2024 12:21:41 PM

- LEGEND**
-  ROADWAY EXCAVATION REQ'D
 -  HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (5" THICK)
 -  UNTREATED BASE COURSE, GRADE 1 REQ'D (6" THICK)
 -  GRANULAR BORROW REQ'D (12" THICK)
 -  UNTREATED BASE COURSE, GRADE 1 REQ'D (4" THICK)
 -  PULVERIZED PAVEMENT BASE COURSE REQ'D
 -  COLD MILLING BITUMINOUS PAVEMENT, DEPTH = 3" REQ'D
 -  HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (3" THICK)



**FOREST STREET
 DRIVEWAY DETAIL 3 OF 5**
 SCALE 1" = 5'

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: BKP
 DRAWN BY: BKP
 CHECKED BY: AP
 APPROVED BY: AP

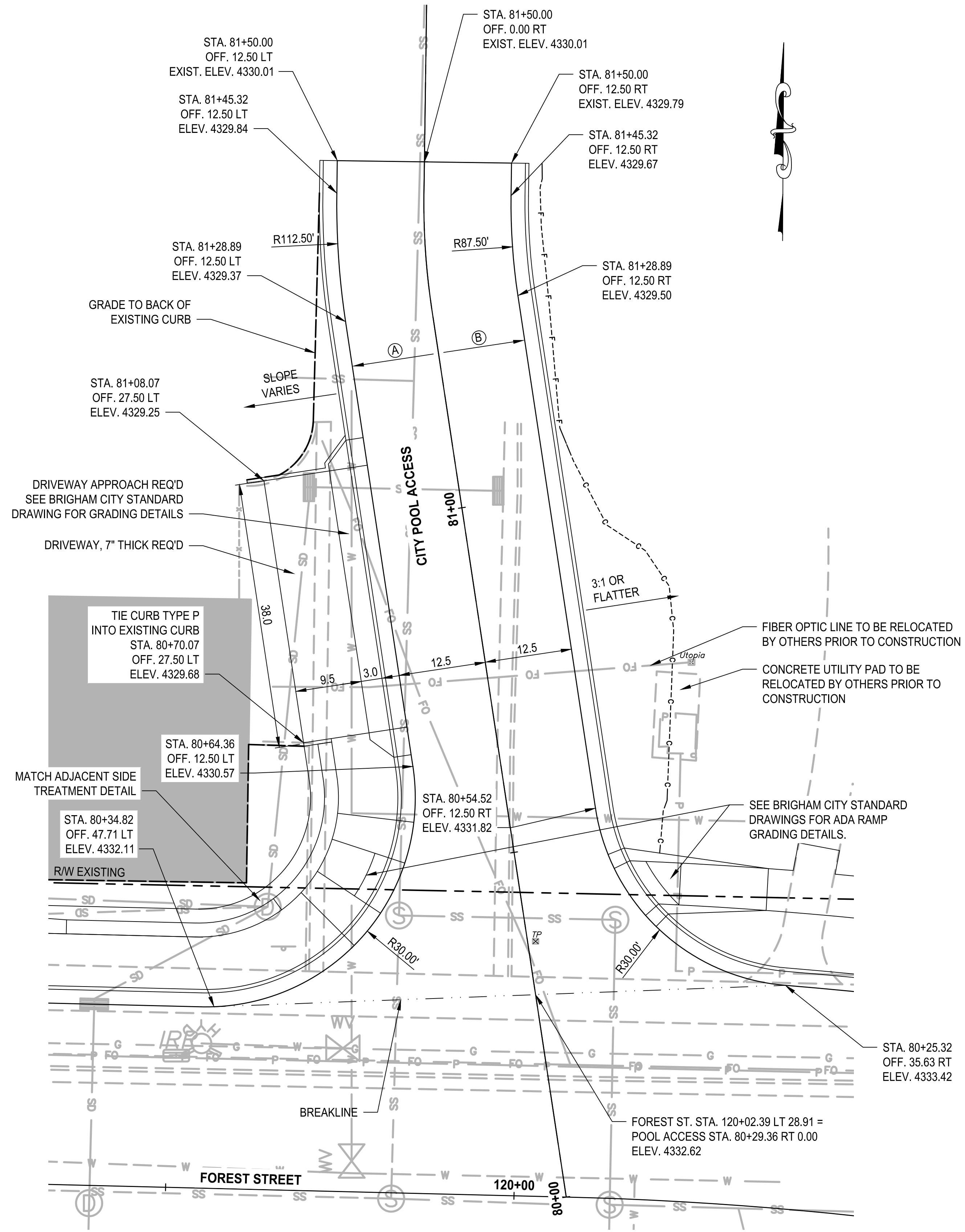
DATE: 03/20/2024
 JOB No.: 344-8541-002



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

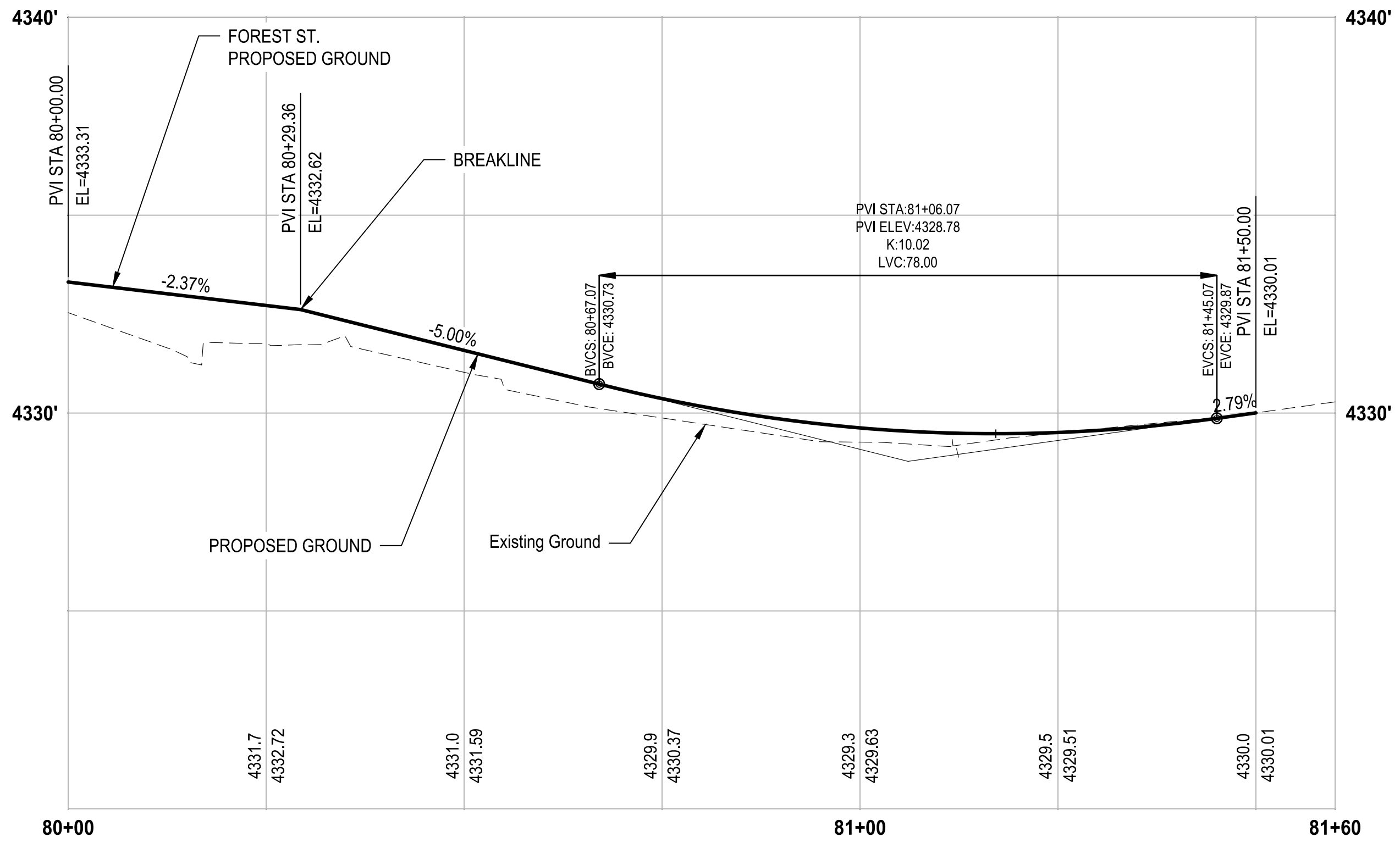
DETAIL

LAYOUT: DT-04
 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002_Forest St.Final Design\995svca\CADD\DWG\Civil
 PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 12:22:01 PM



SUPERELEVATION TABLE

| STATION | (A) | (B) |
|----------|------------|--------|
| 80+25.32 | FOREST ST. | 1.91% |
| 80+34.82 | -0.49% | 2.48% |
| 80+60.00 | -2.09% | 4.00% |
| 80+90.00 | -4.00% | 2.08% |
| 81+50.00 | 0.00% | -1.75% |



**FOREST STREET
 DRIVEWAY DETAIL 4 OF 5**
 SCALE 1" = 10'

REVISIONS

| NO. | DATE | BY |
|-----|------|----|
| | | |

ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY.

Parametrix

DESIGNED BY: BKP
 DRAWN BY: BKP
 CHECKED BY: AP
 APPROVED BY: AP

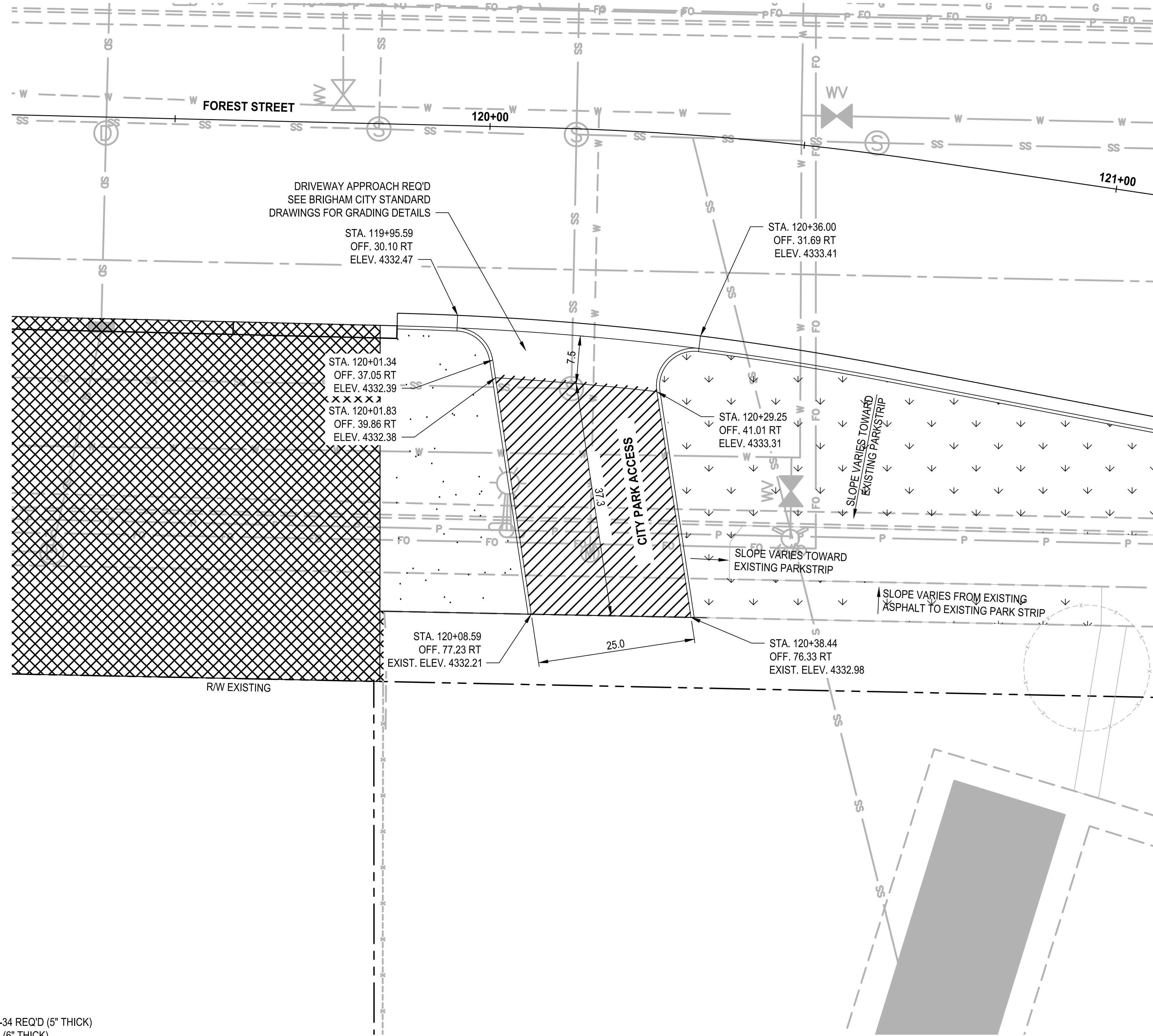
DATE: 03/20/2024
 JOB No.: 344-8541-002



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

DETAIL

LAYOUT: DT-05 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995\ca\ADD\DWG\Civil PLOTTED BY: OliveStg DATE: Friday, March 22, 2024 12:22:17 PM



LEGEND

- ROADWAY EXCAVATION REQ'D
- HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (5" THICK)
- UNTREATED BASE COURSE, GRADE 1 REQ'D (6" THICK)
- GRANULAR BORROW REQ'D (12" THICK)
- UNTREATED BASE COURSE, GRADE 1 REQ'D (4" THICK)
- COLD MILLING BITUMINOUS PAVEMENT, DEPTH = 3" REQ'D
- HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (3" THICK)
- IRRIGATION SYSTEM AND LANDSCAPE RESTORATION REQ'D

**FOREST STREET
DRIVEWAY DETAIL 5 OF 5**
SCALE 1" = 10'

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY.

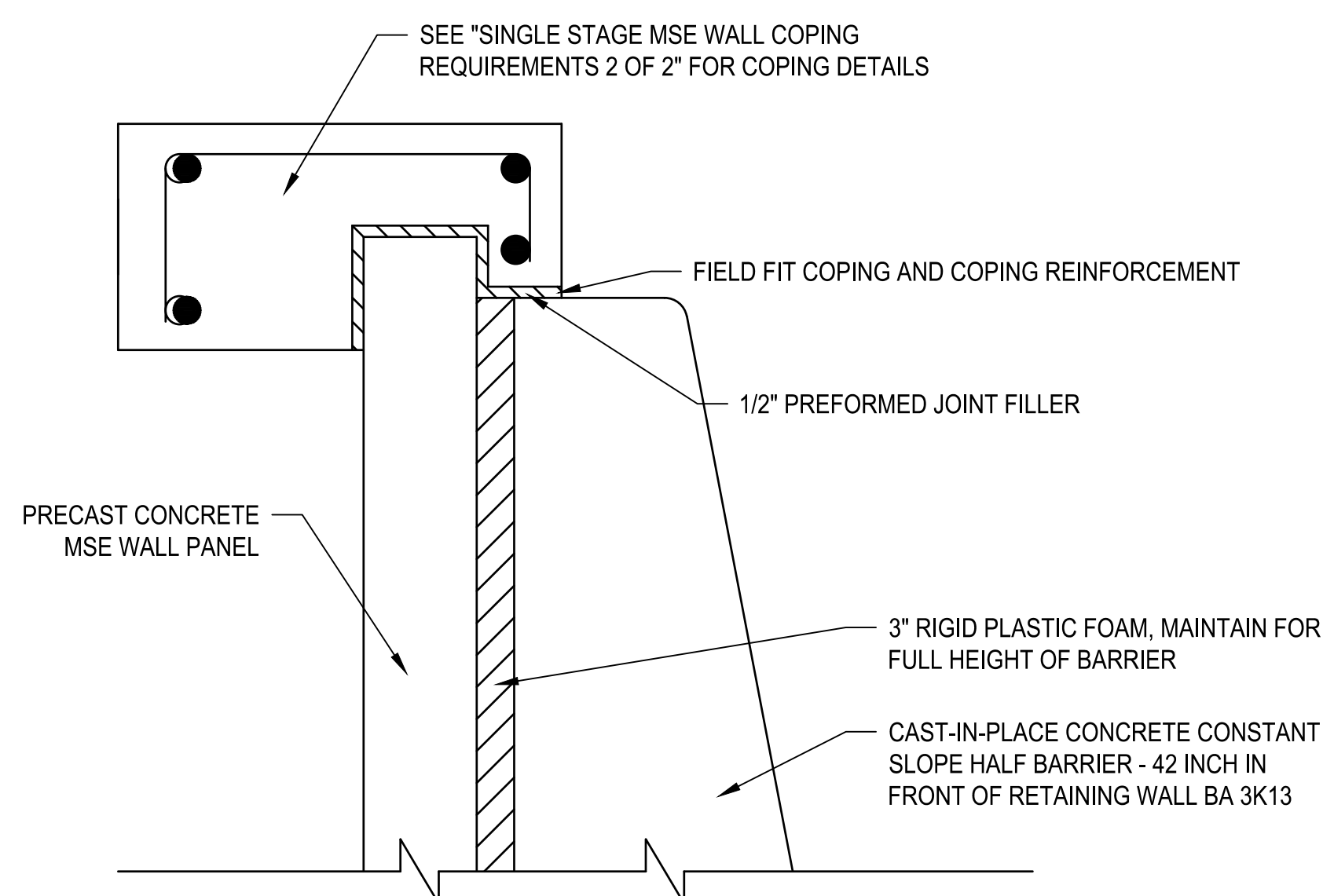
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| Parametrix | DESIGNED BKP | DRAWN BKP | CHECKED AP | APPROVED AP |
| DATE: 03/20/2024 | JOB No.: 344-8541-002 | DRAWN: BKP | CHECKED: AP | APPROVED: AP |



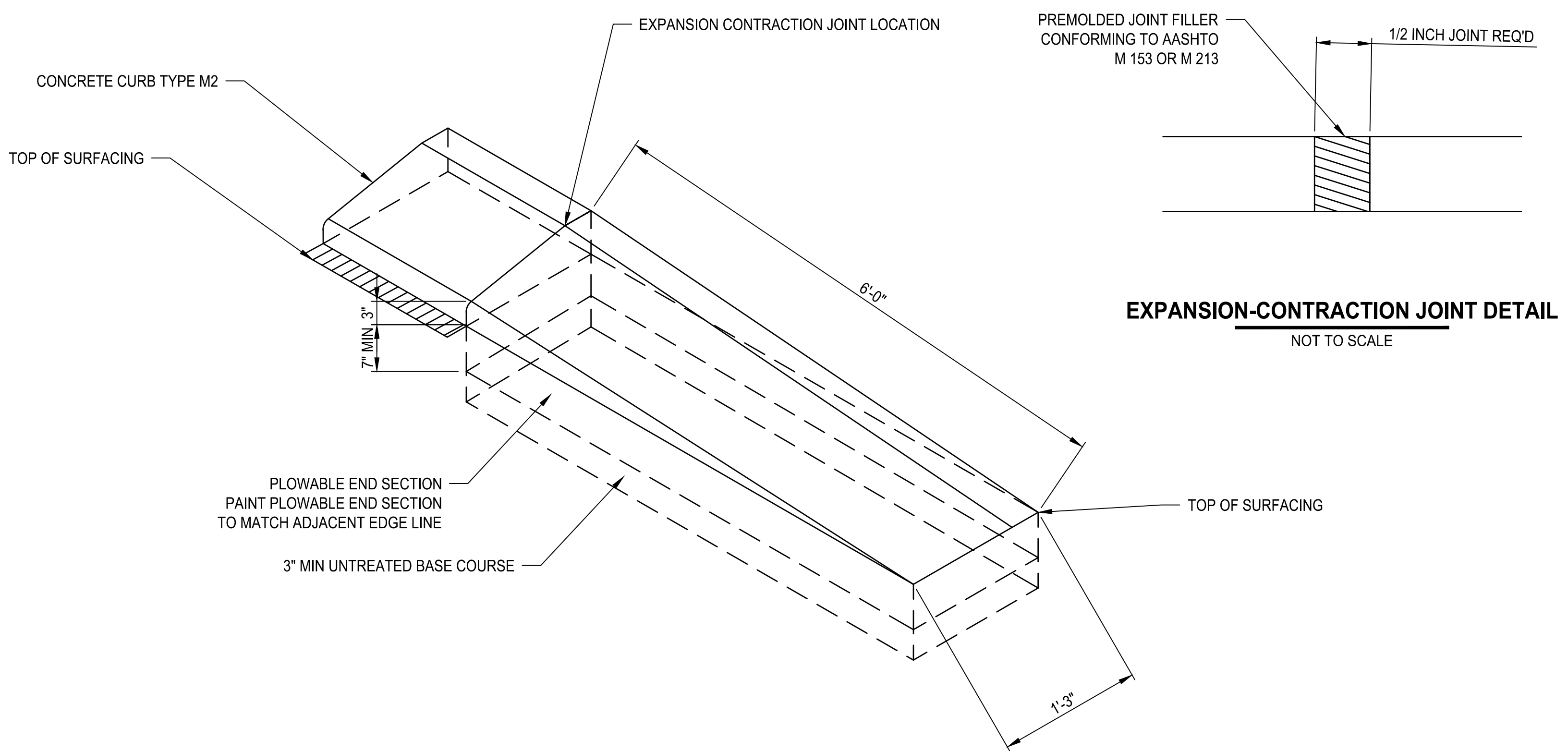
PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

DETAIL

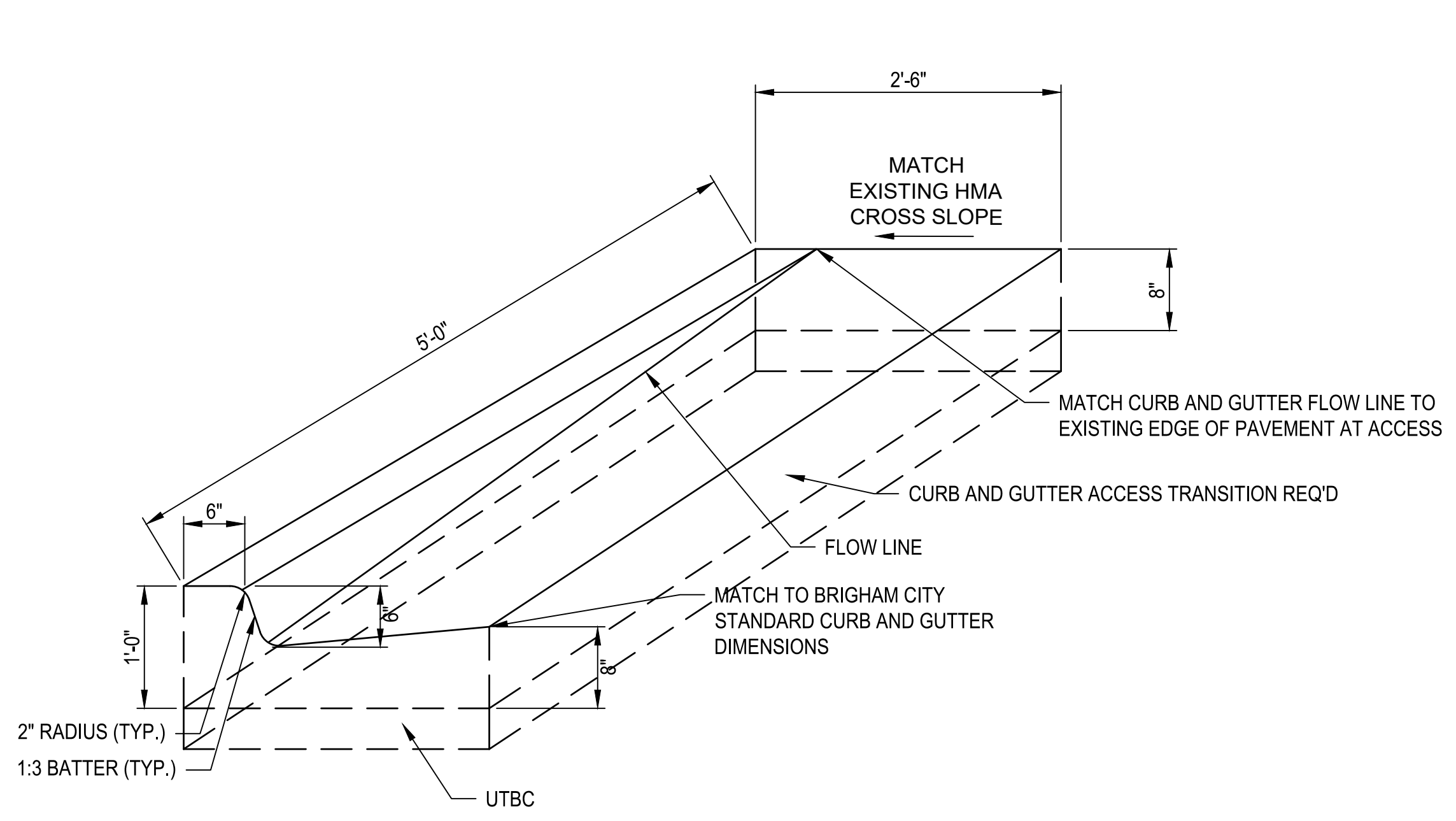
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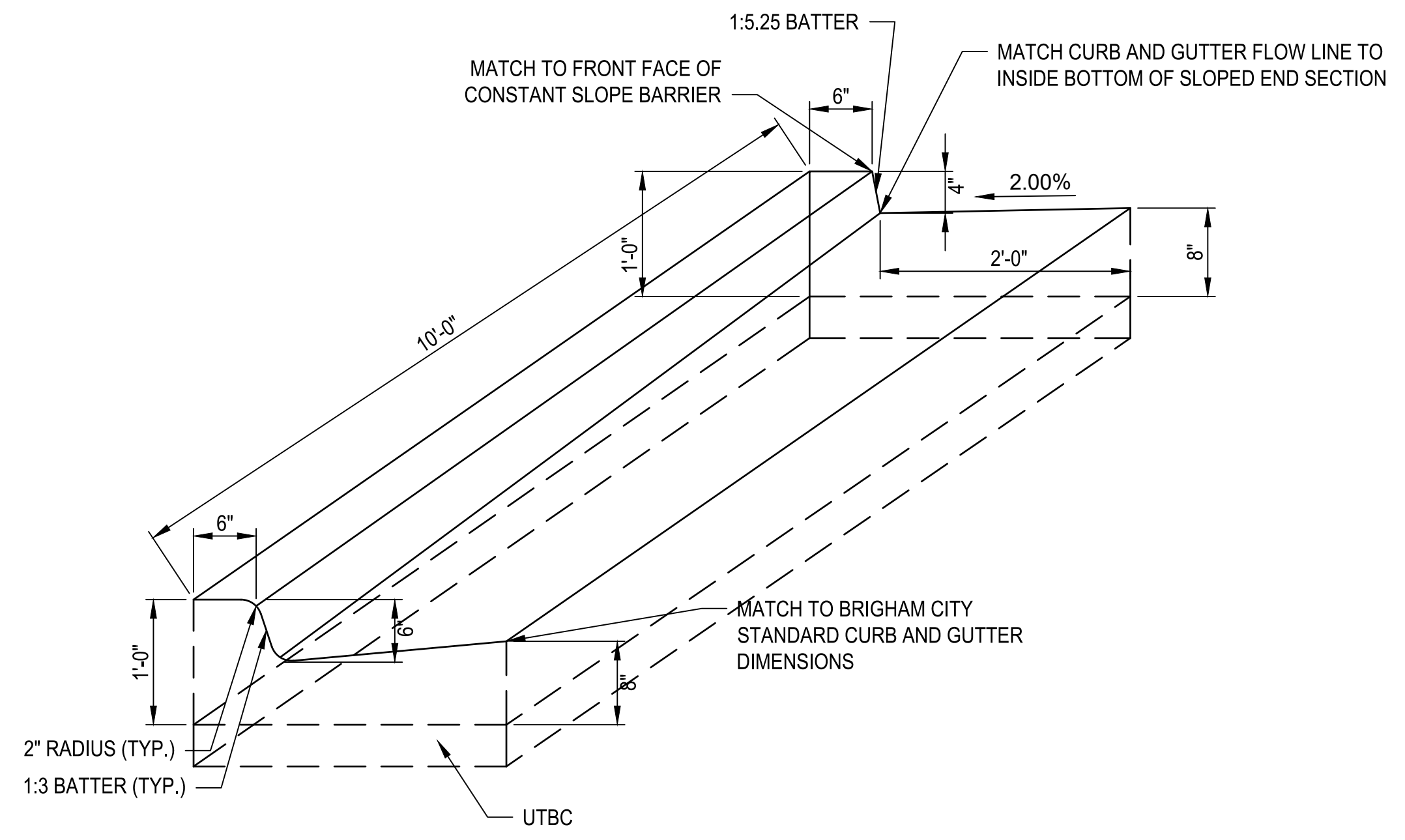
COPING DETAIL AT RETAINING WALL TERMINUS
NOT TO SCALE



CONCRETE CURB TYPE M2 PLOWABLE END SECTION DETAIL
NOT TO SCALE



CONCRETE CURB AND GUTTER ACCESS TRANSITION
NOT TO SCALE



CONCRETE CURB AND GUTTER TRANSITION
NOT TO SCALE

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
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1
 ONE INCH
 AT FULL
 SCALE. IF
 NOT SCALE
 ACCORDINGLY

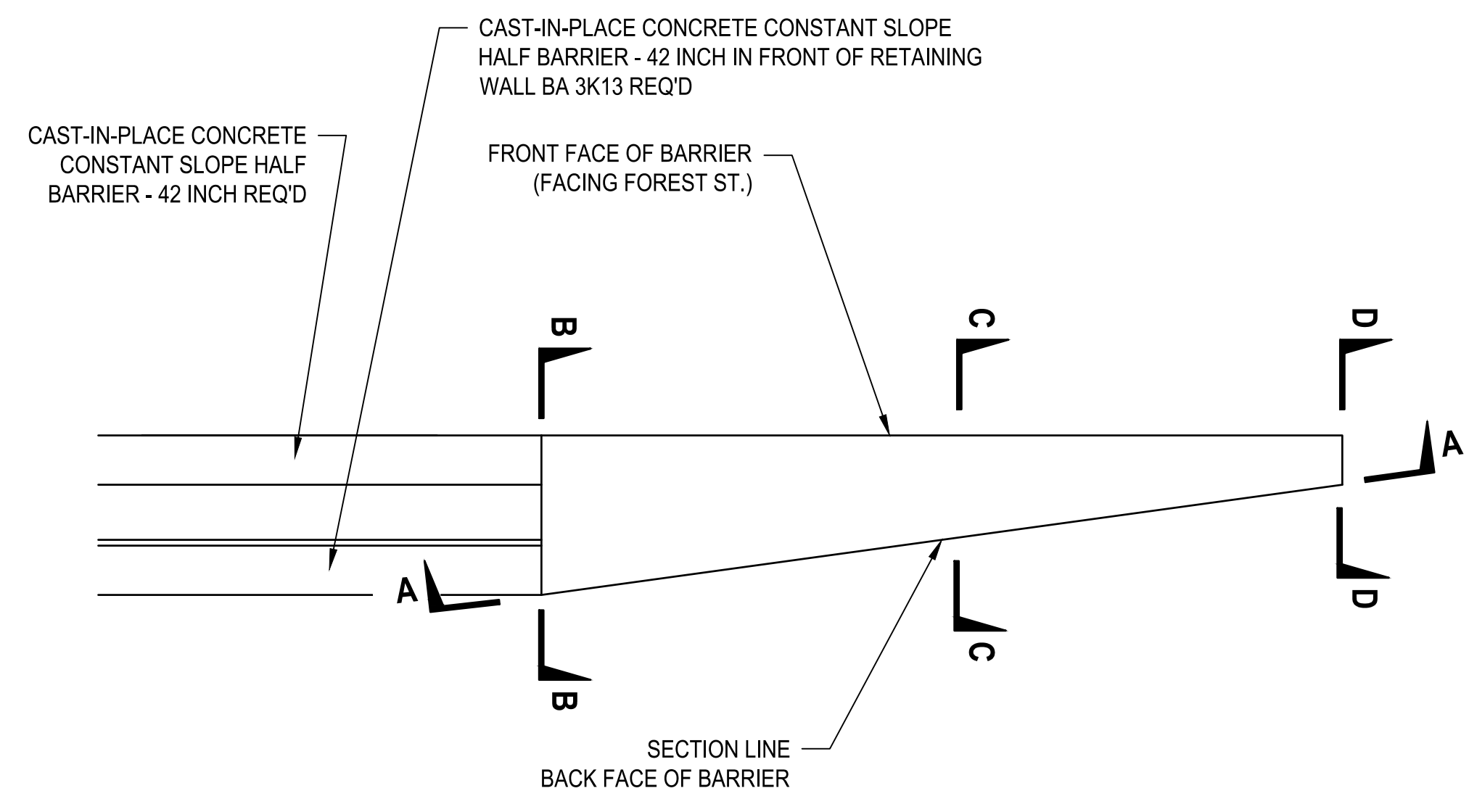
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|-------------------|----------------------|-------|---------|----------|
| Parametrix | DESIGNED | DRAWN | CHECKED | APPROVED |
| | 03/20/2024 | BKP | BKP | AP |
| | JOB No. 344-8541-002 | | | |
| | | | | |



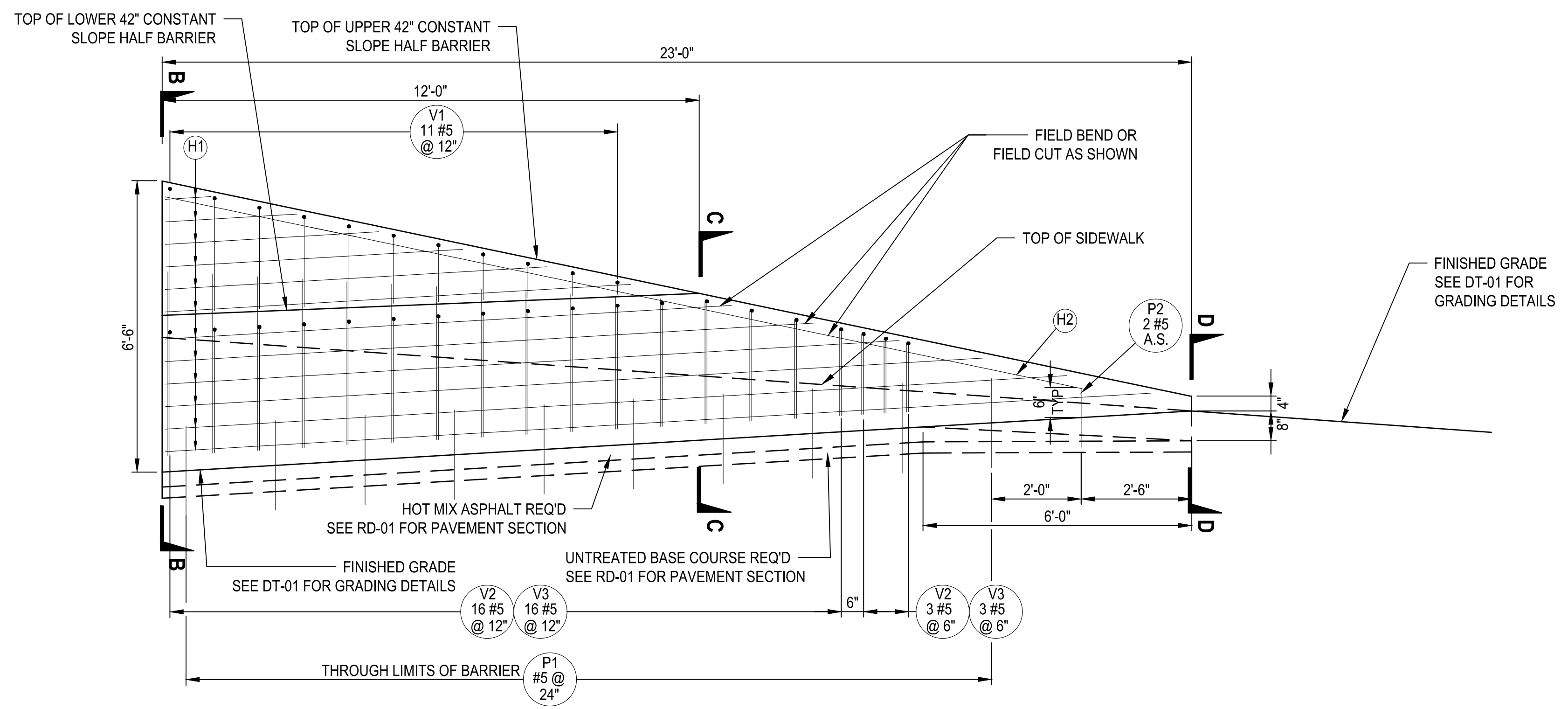
PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

DETAIL

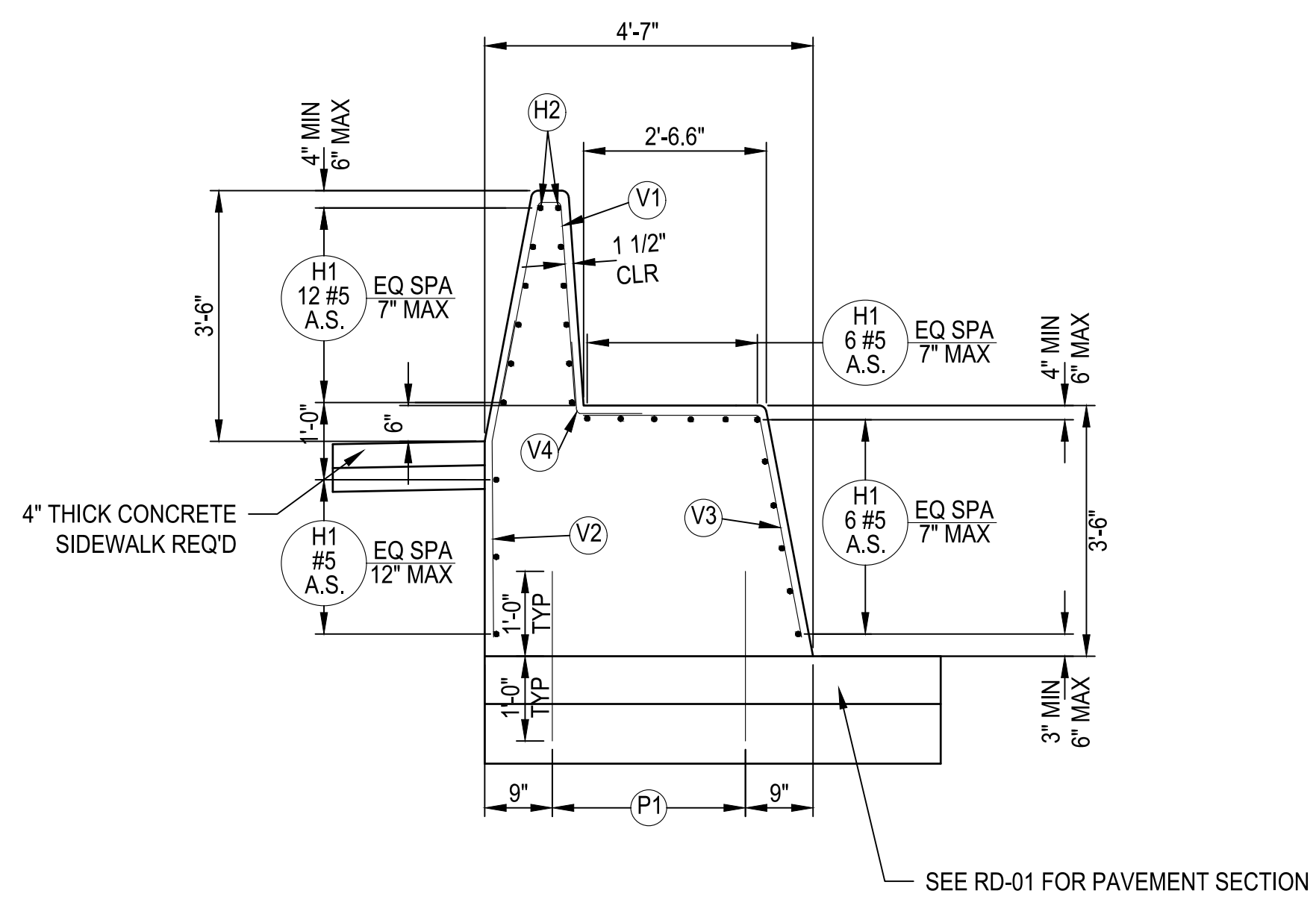
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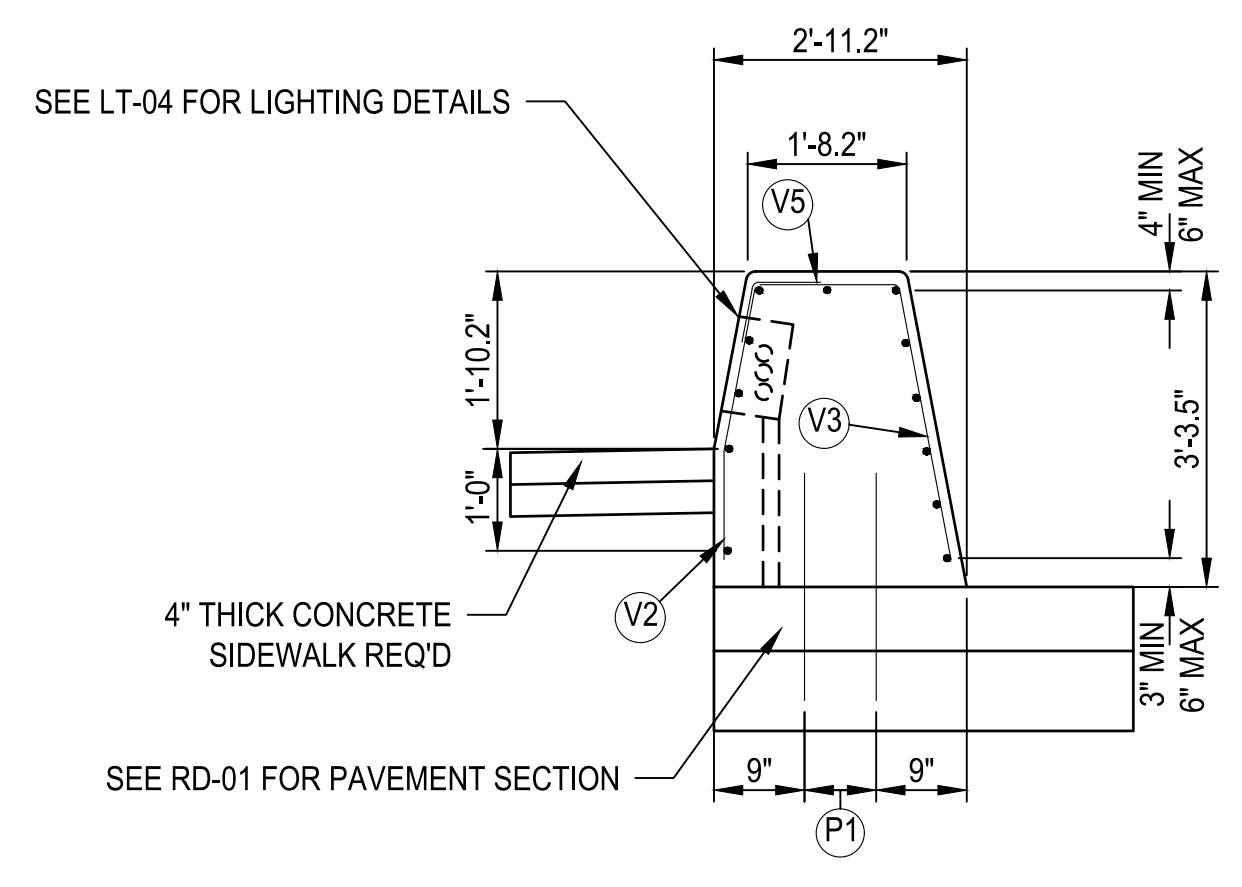
PLAN VIEW



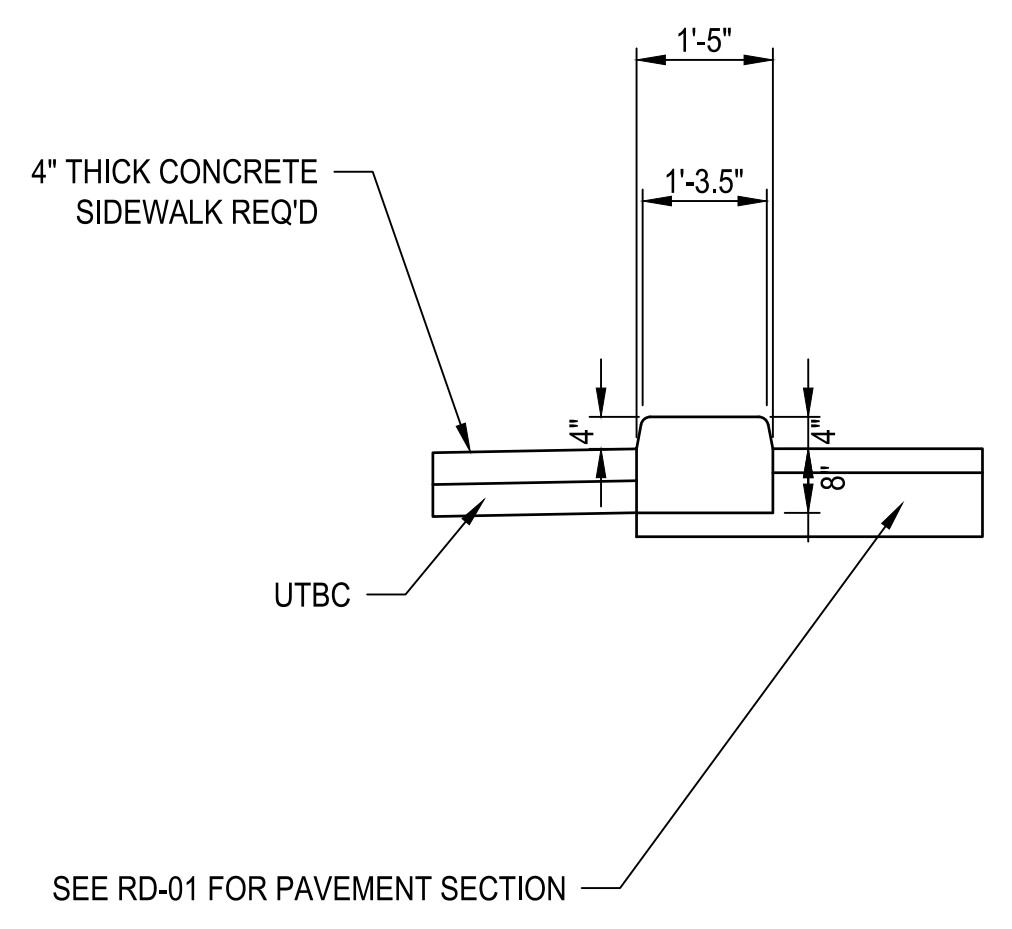
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

MODIFIED SLOPED END SECTION (NORTHWEST END)

NOT TO SCALE

- NOTES:**
1. SEE UDOT STD DWG BA 1A1 FOR GENERAL NOTES.
 2. SEE UDOT STD DWG BA 3A11 FOR NOTES.
 3. CHAMFER EXPOSED CORNERS WITH 3/4" CHAMFER.

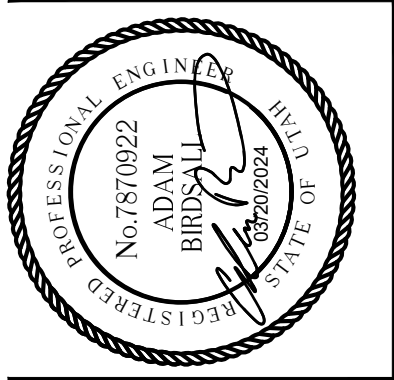
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Parametrix

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 DRAWN BY: BKP
 CHECKED BY: AP
 APPROVED BY: AP

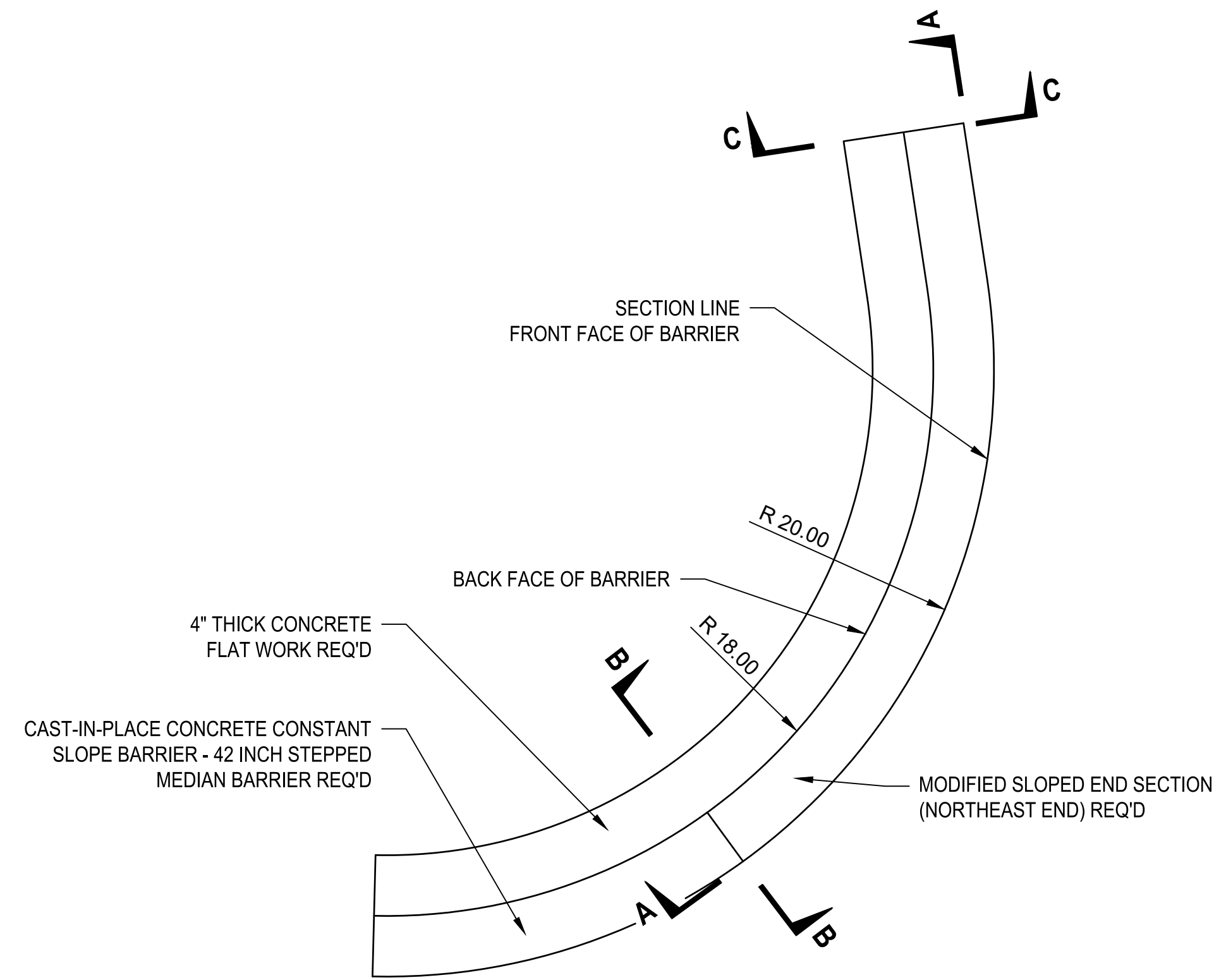
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 JOB No.: 344-8541-002



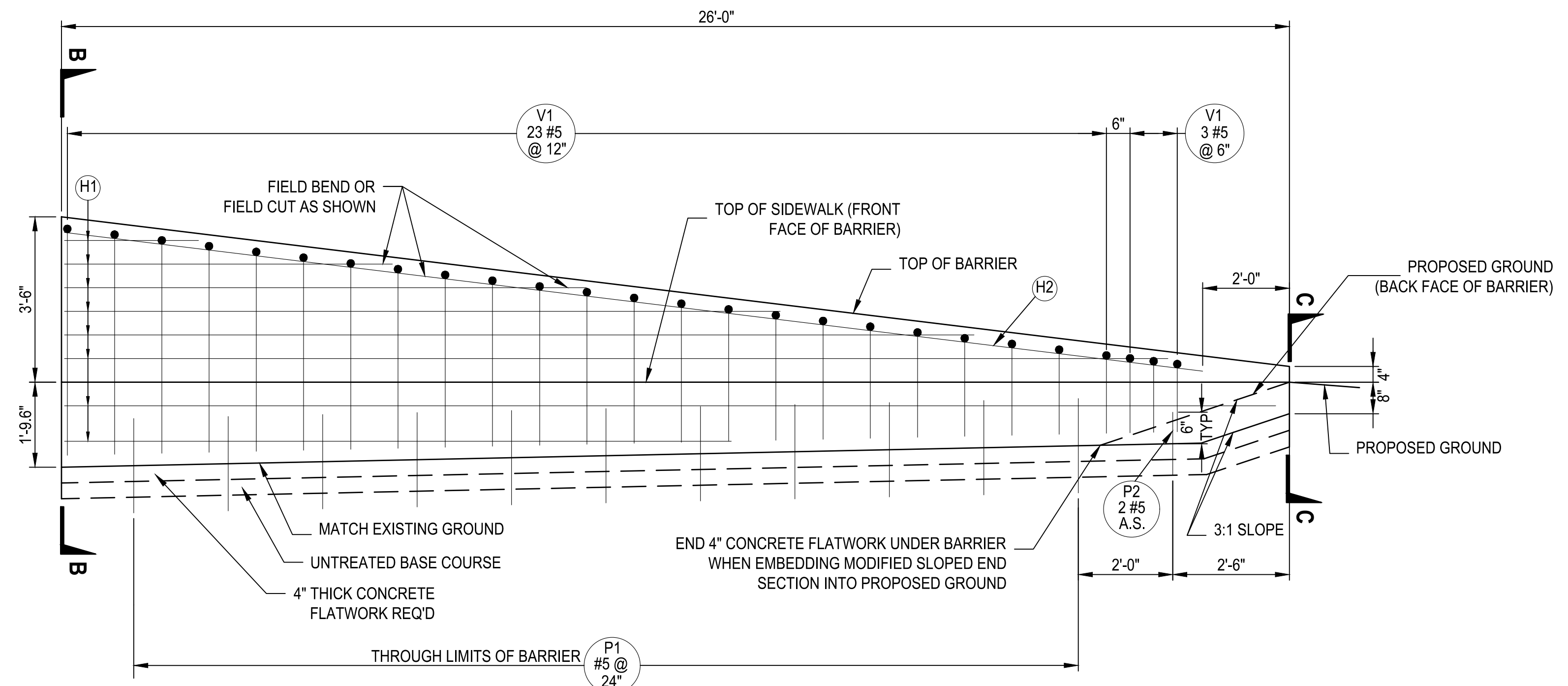
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

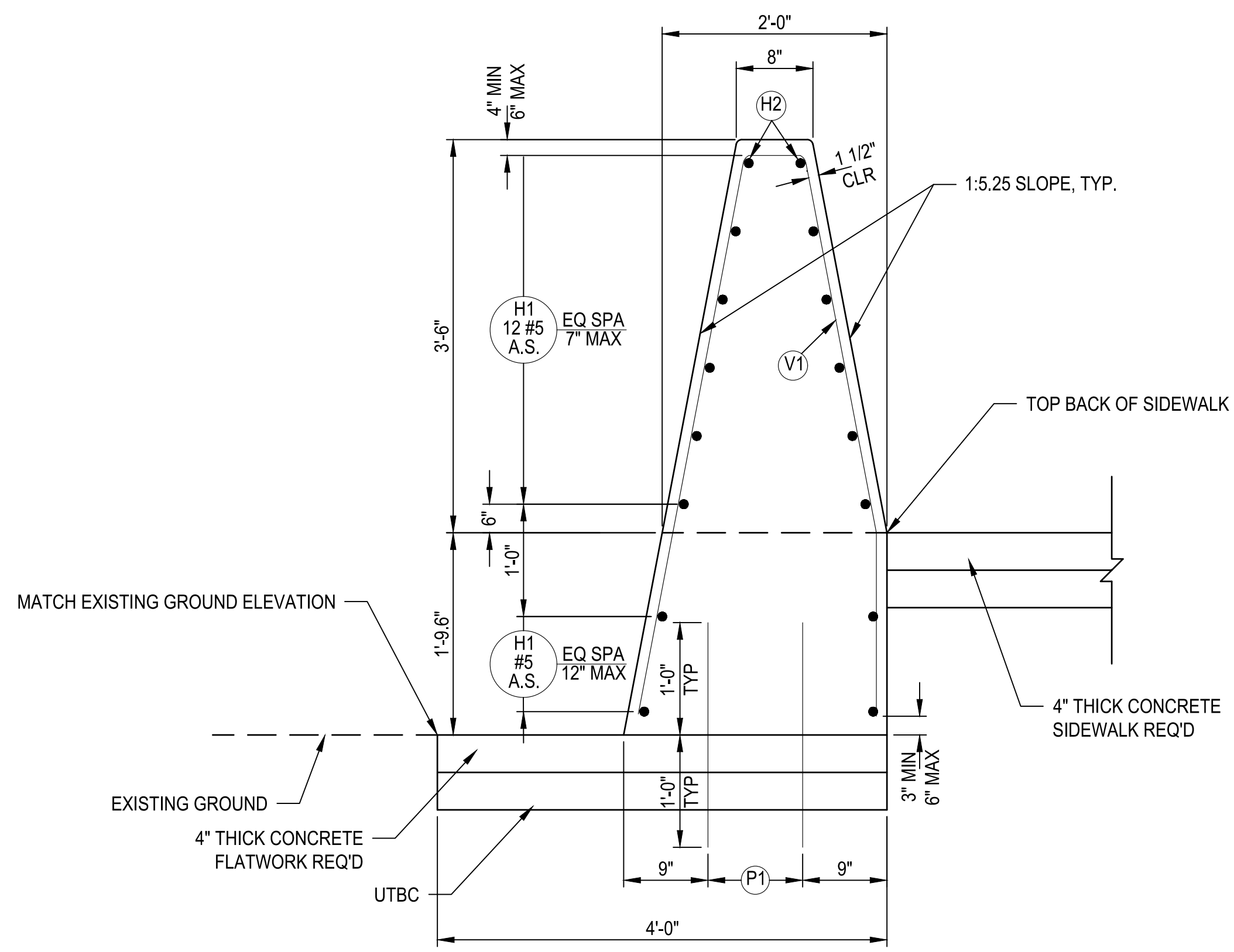
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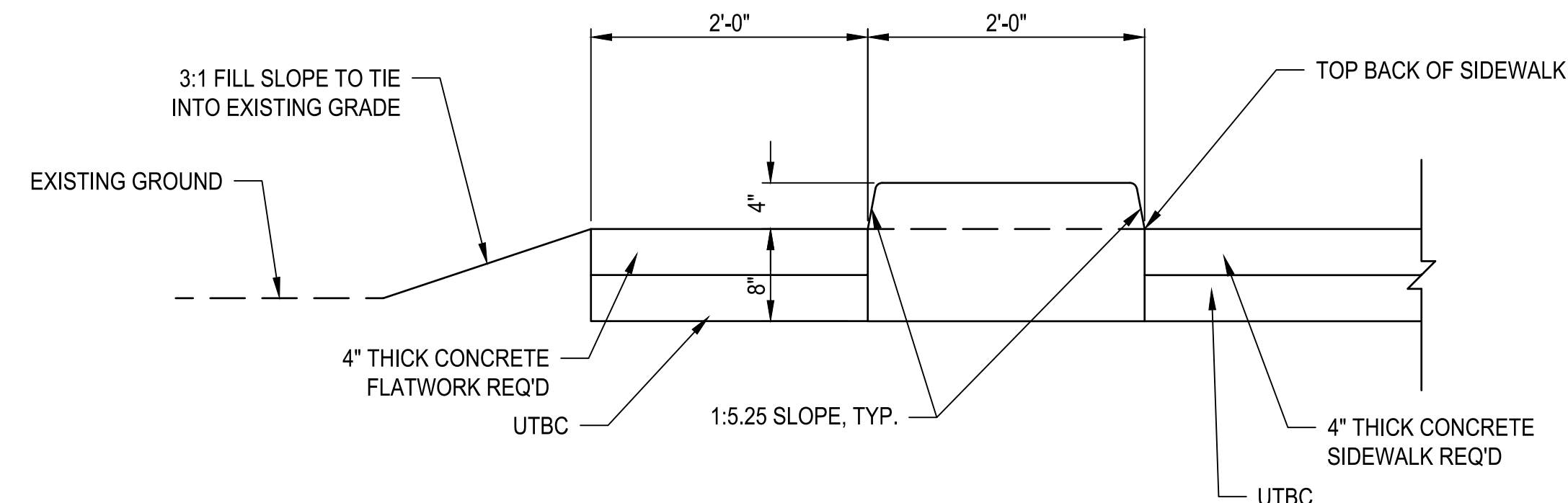
PLAN VIEW



SECTION A-A



SECTION B-B



SECTION C-C

MODIFIED SLOPED END SECTION (NORTHEAST END)

NOT TO SCALE

- NOTES:**
- SEE UDOT STD DWG BA 1A1 FOR GENERAL NOTES.
 - SEE UDOT STD DWG BA 3A11 FOR NOTES.
 - CHAMFER EXPOSED CORNERS WITH 3/4" CHAMFER.

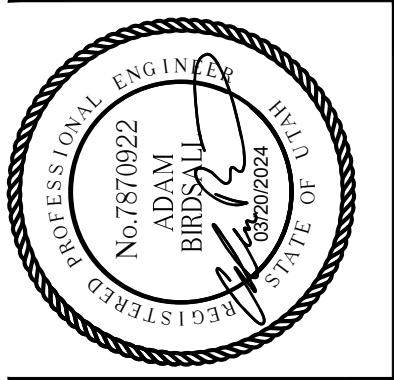
| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: BKP
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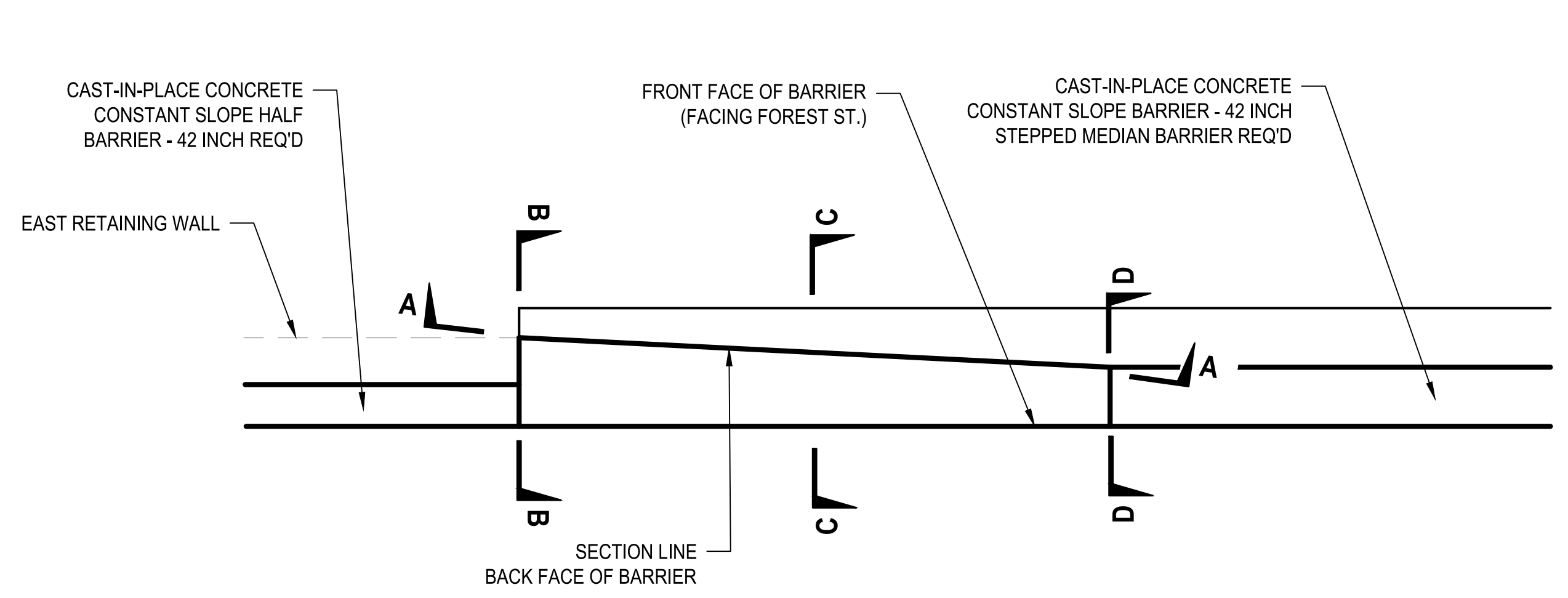
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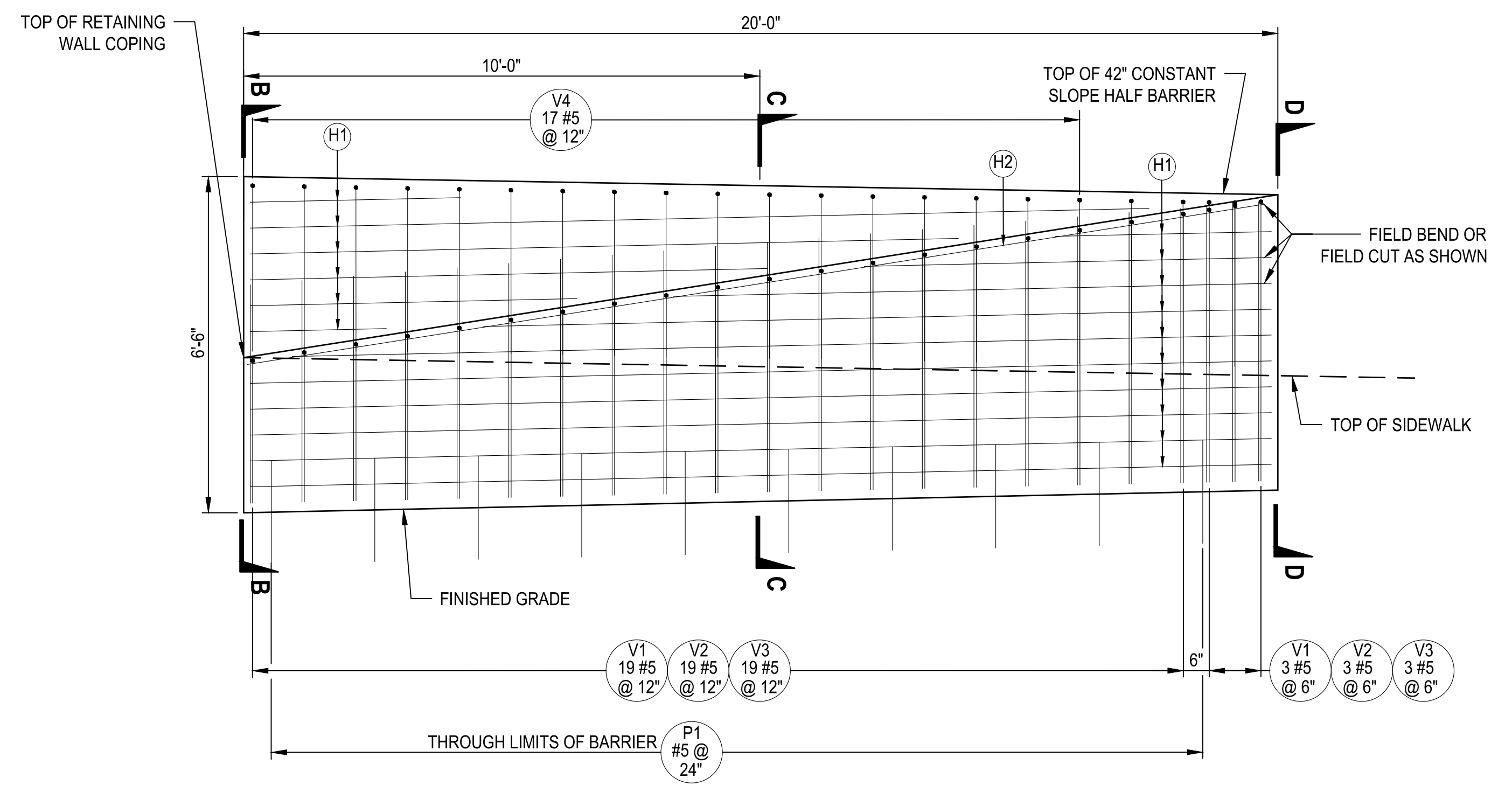
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DETAIL

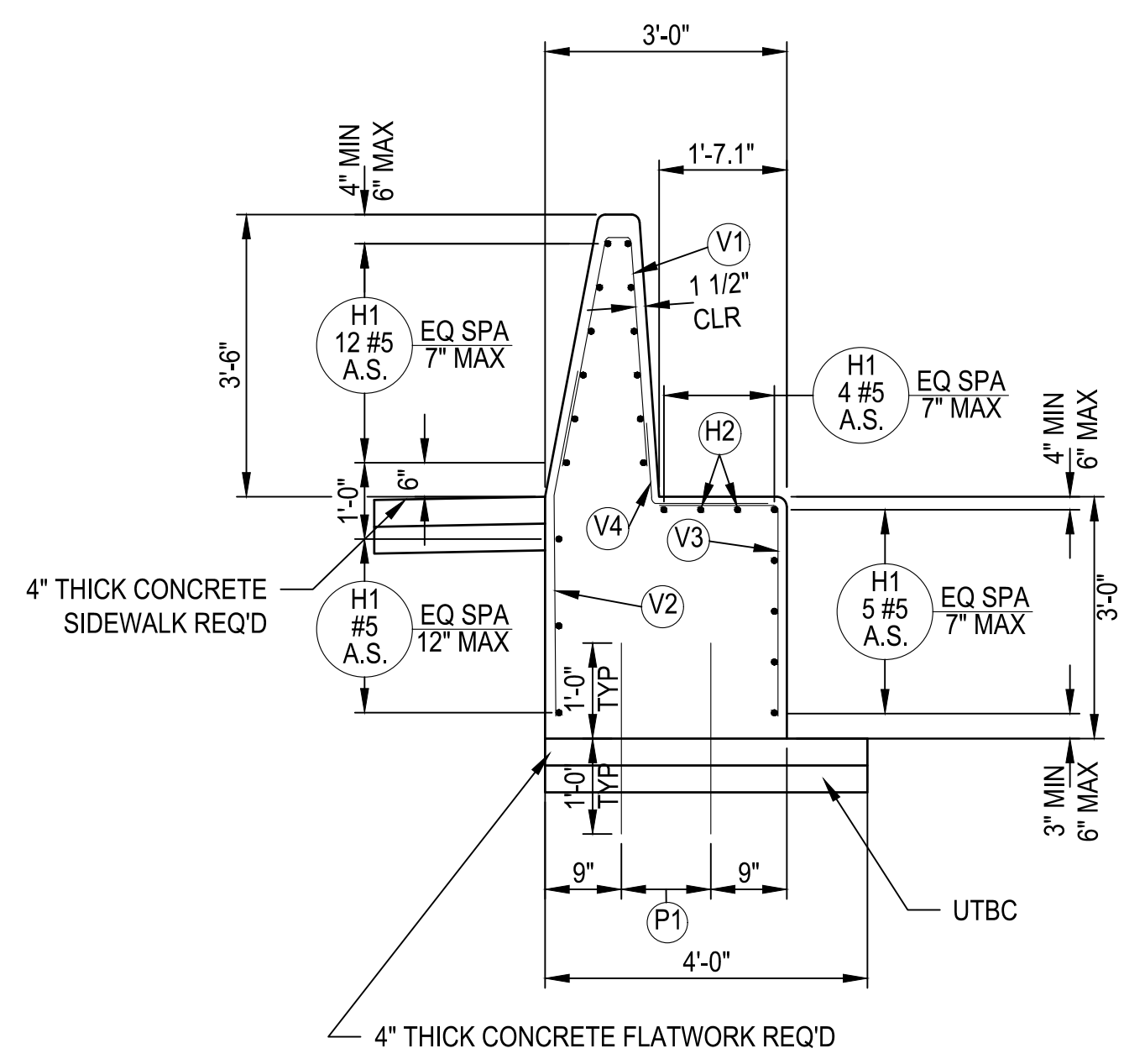
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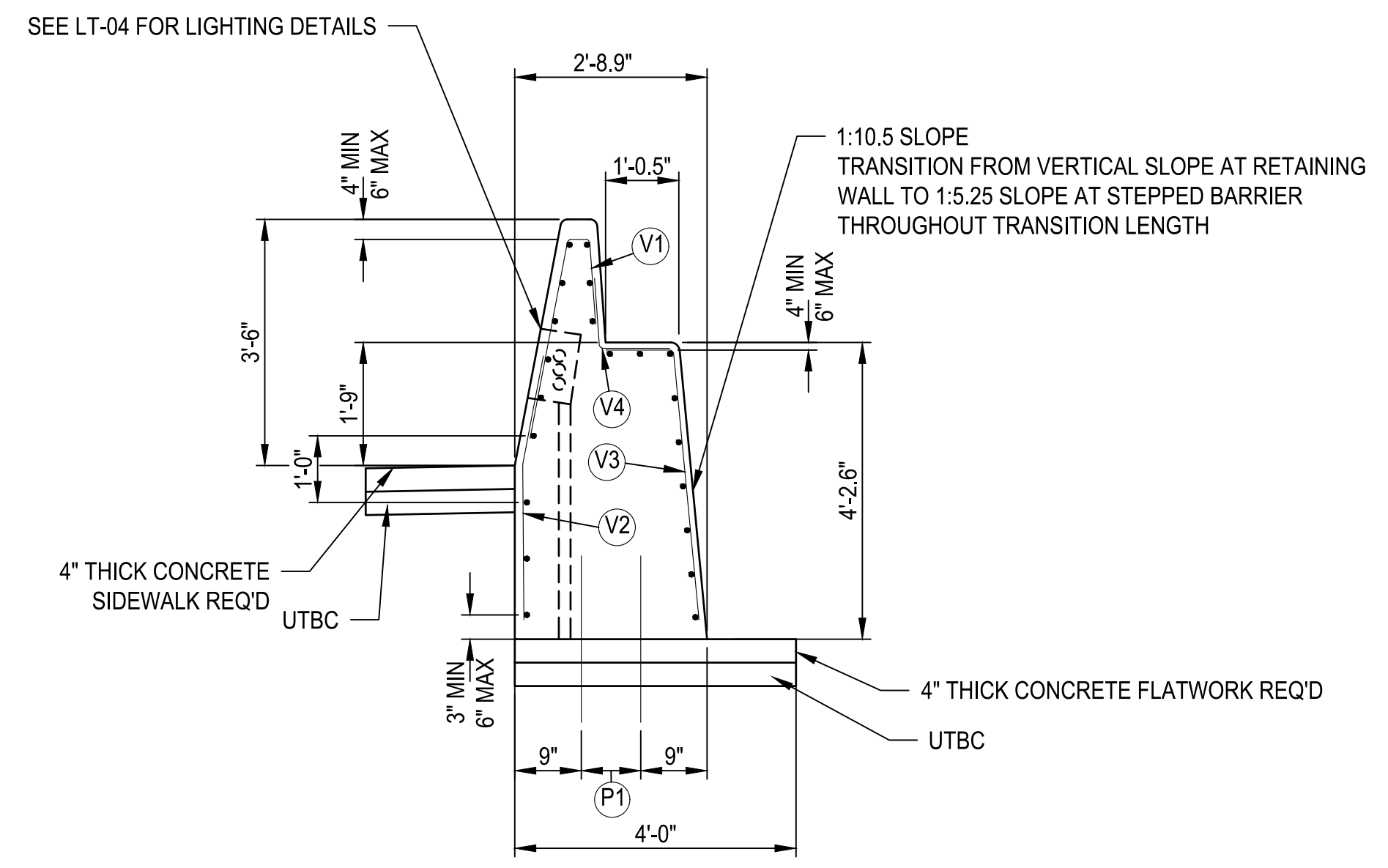
PLAN VIEW



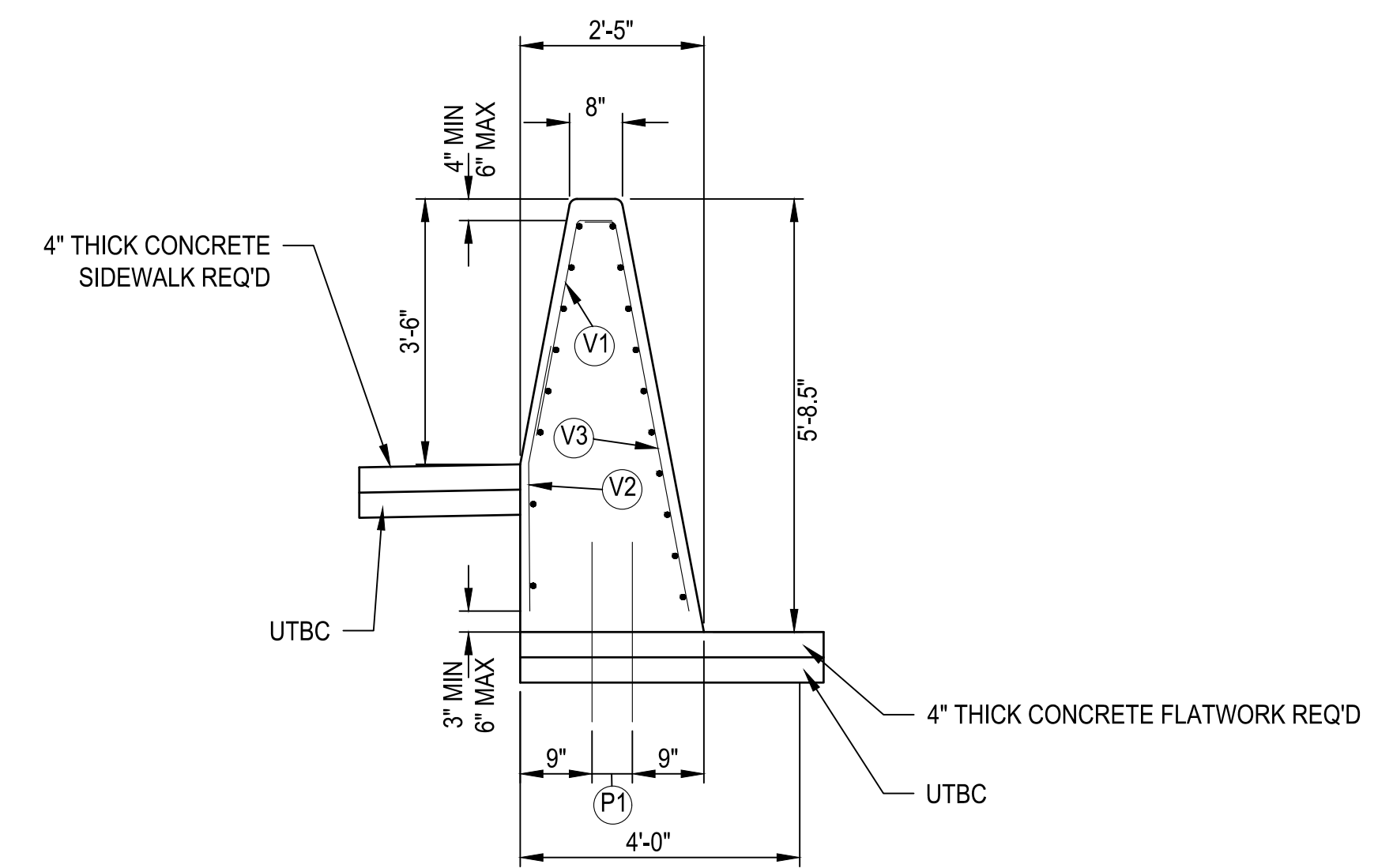
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (NORTHEAST)

NOT TO SCALE

- NOTES:**
- SEE UDOT STD DWG BA 1A1 FOR GENERAL NOTES.
 - SEE UDOT STD DWG BA 3A11 FOR NOTES.
 - CHAMFER EXPOSED CORNERS WITH 3/4" CHAMFER.

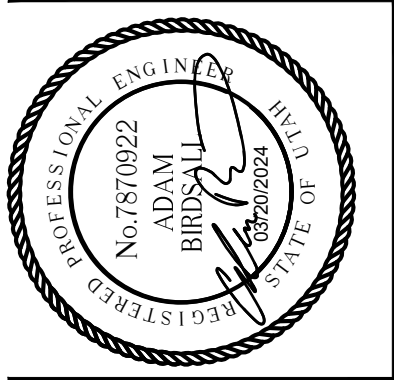
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| DATE | |
| REVISIONS | |

ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: BKP
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 CHECKED BY: AP
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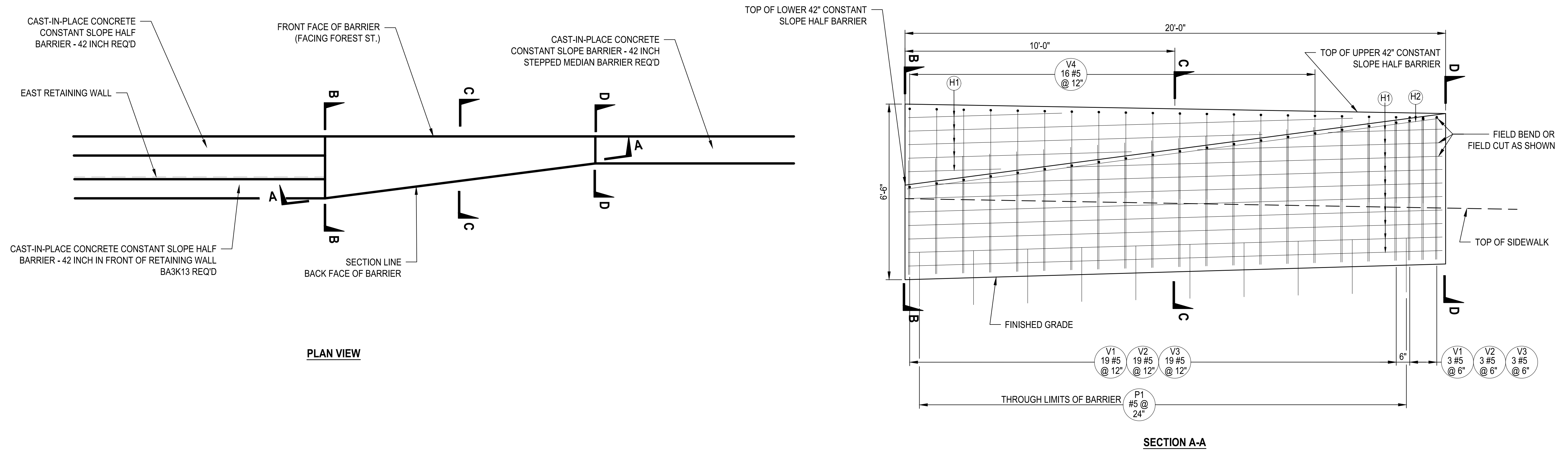
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PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

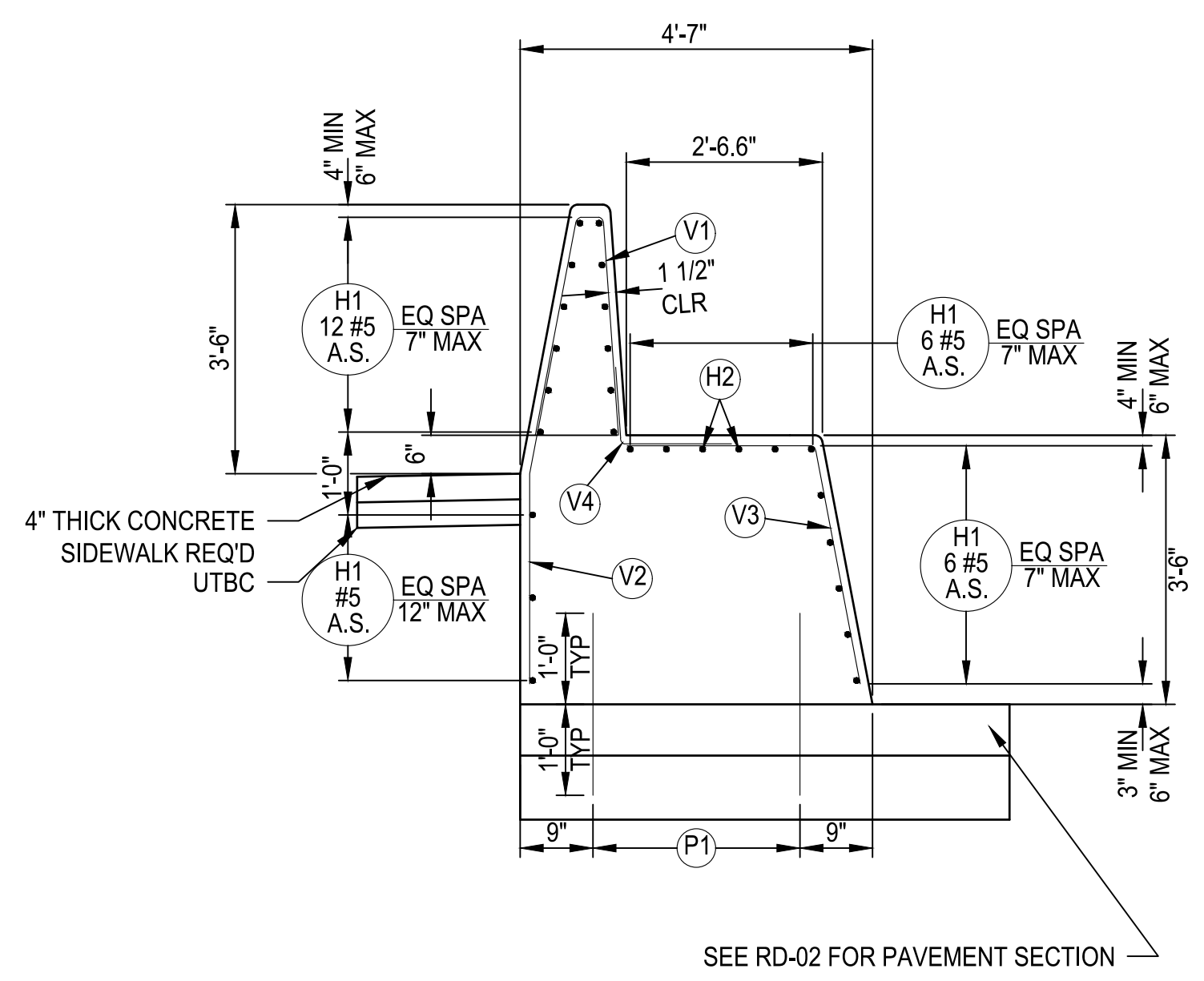
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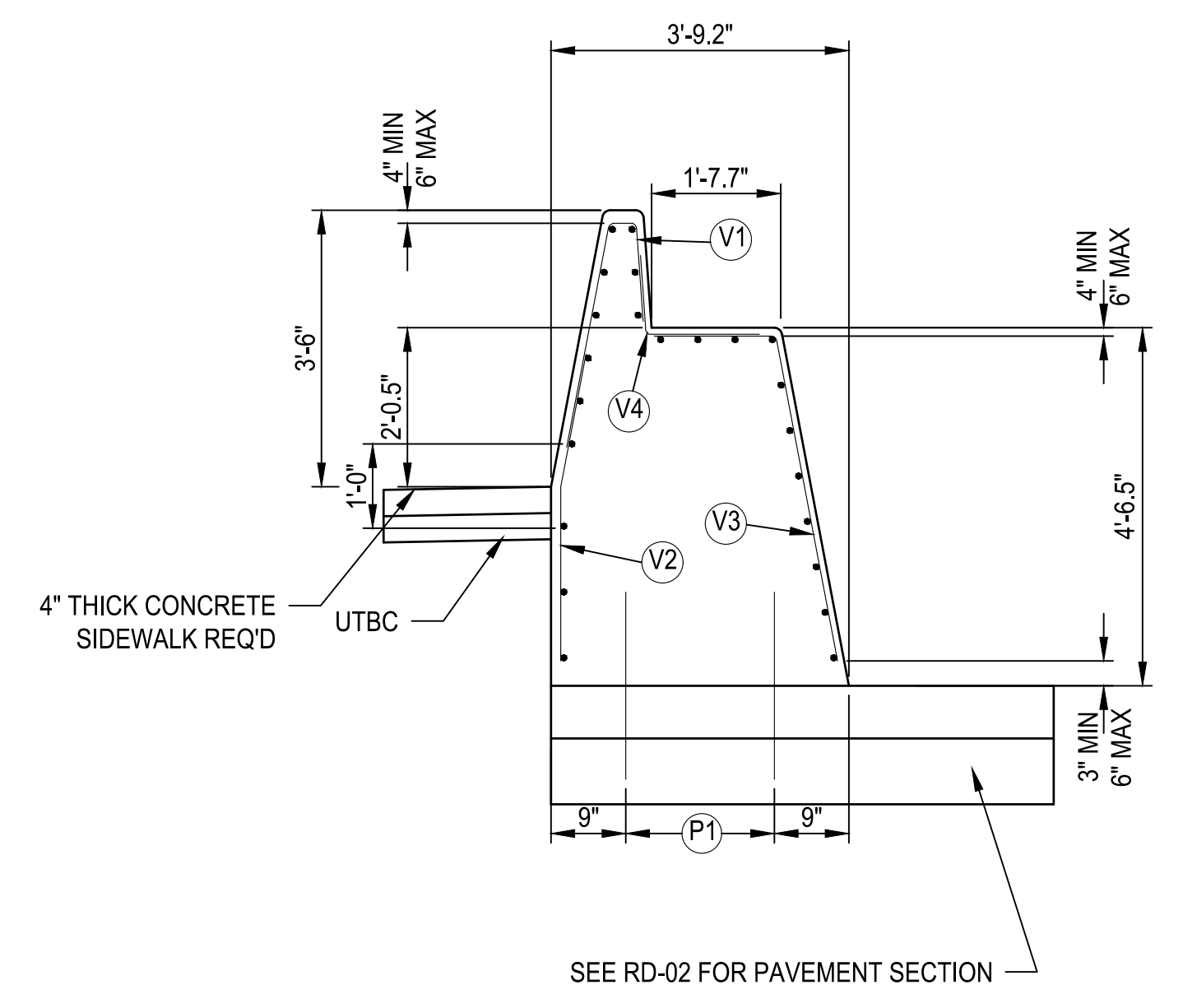
PLAN VIEW

SECTION A-A



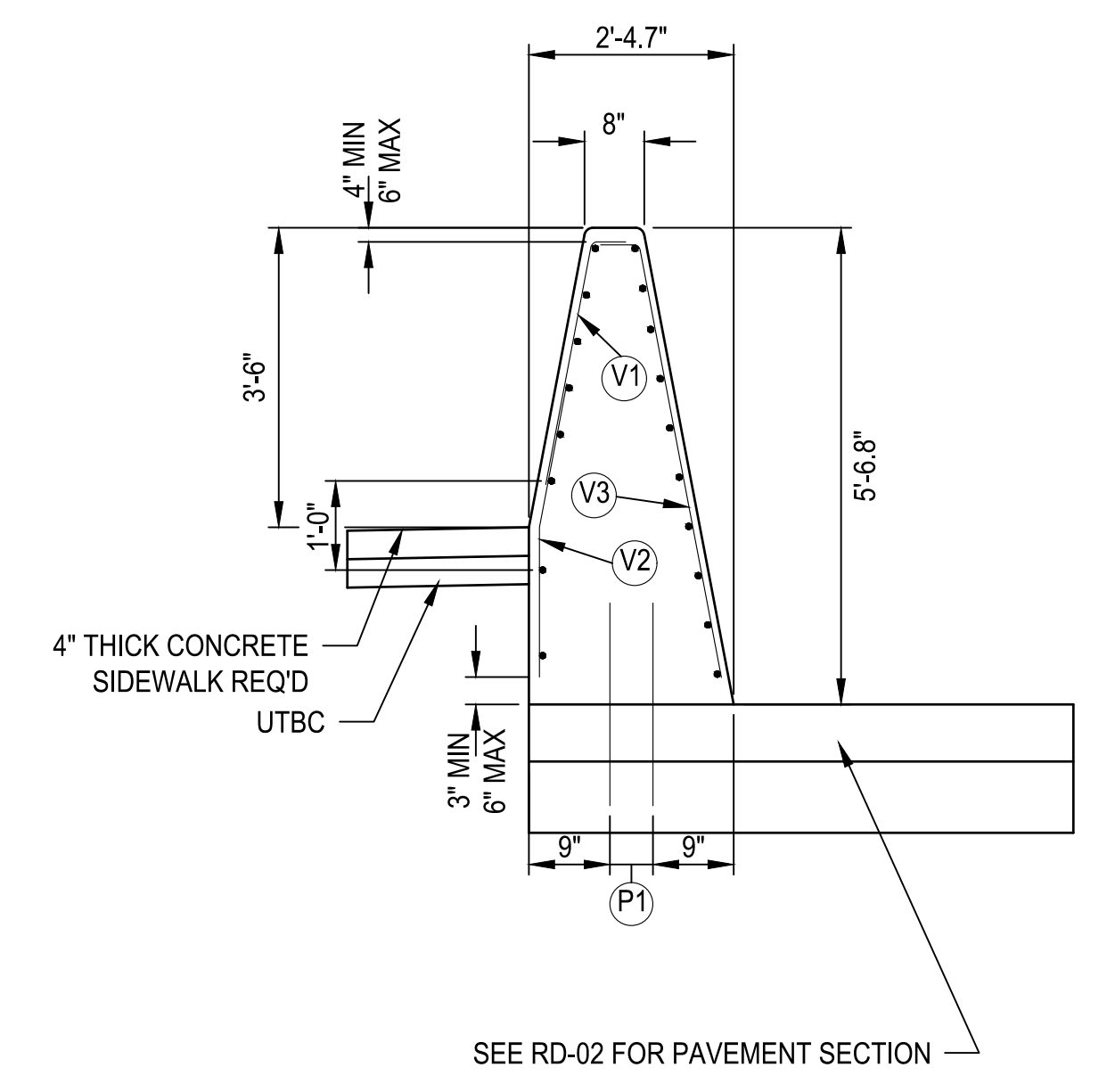
SECTION B-B

SEE RD-02 FOR PAVEMENT SECTION



SECTION C-C

SEE RD-02 FOR PAVEMENT SECTION



SECTION D-D

SEE RD-02 FOR PAVEMENT SECTION

RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (SOUTHEAST)

NOT TO SCALE

- NOTES:**
- SEE UDOT STD DWG BA 1A1 FOR GENERAL NOTES.
 - SEE UDOT STD DWG BA 3A11 FOR NOTES.
 - CHAMFER EXPOSED CORNERS WITH 3/4" CHAMFER.

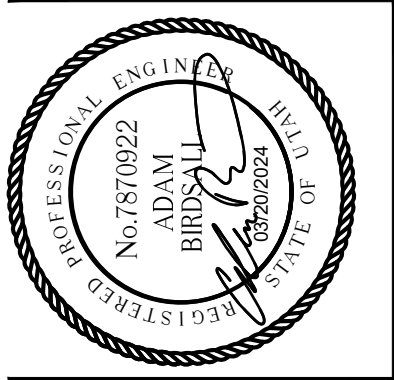
| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY

Parametrix

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 DRAWN BY: BKP
 CHECKED BY: AP
 APPROVED BY: AP

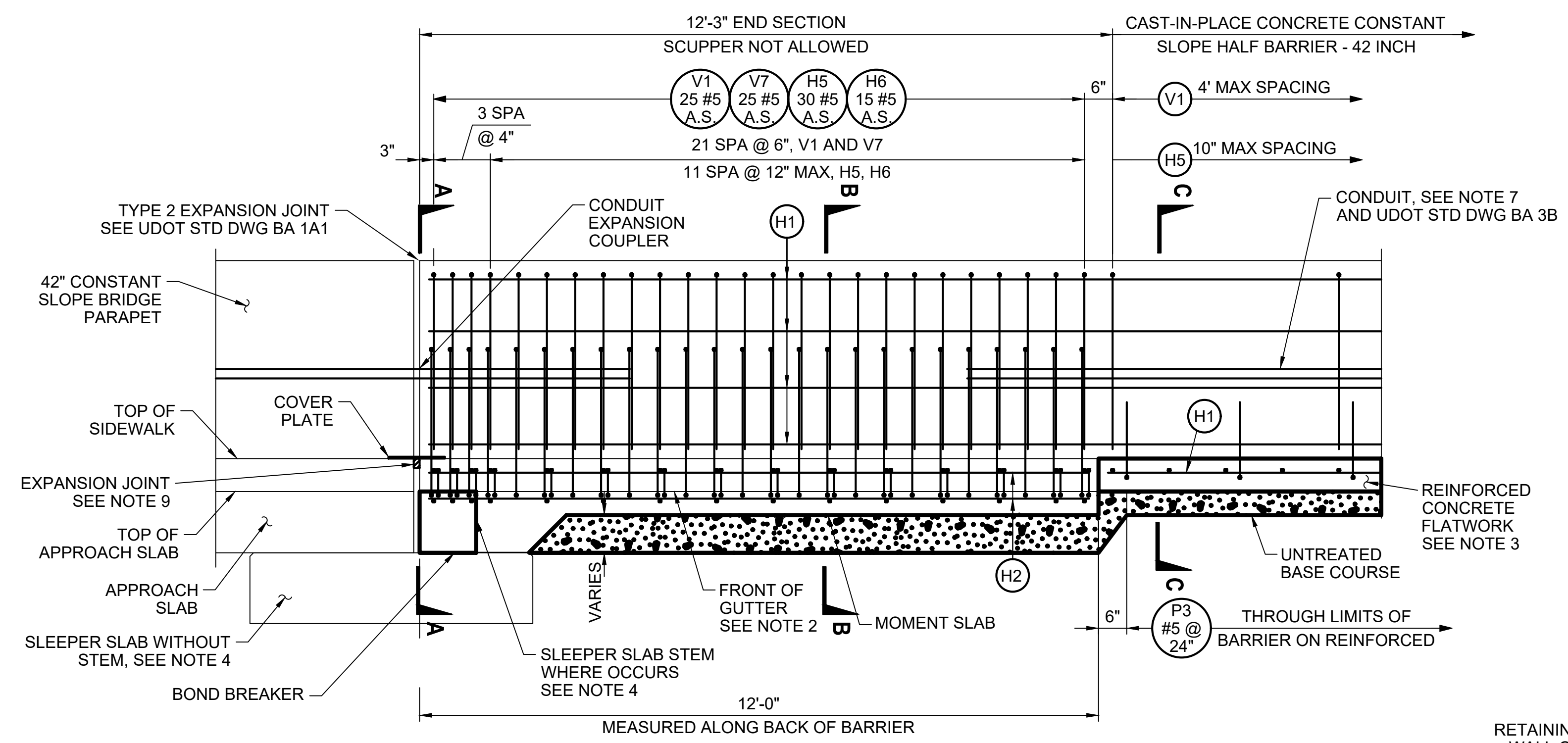
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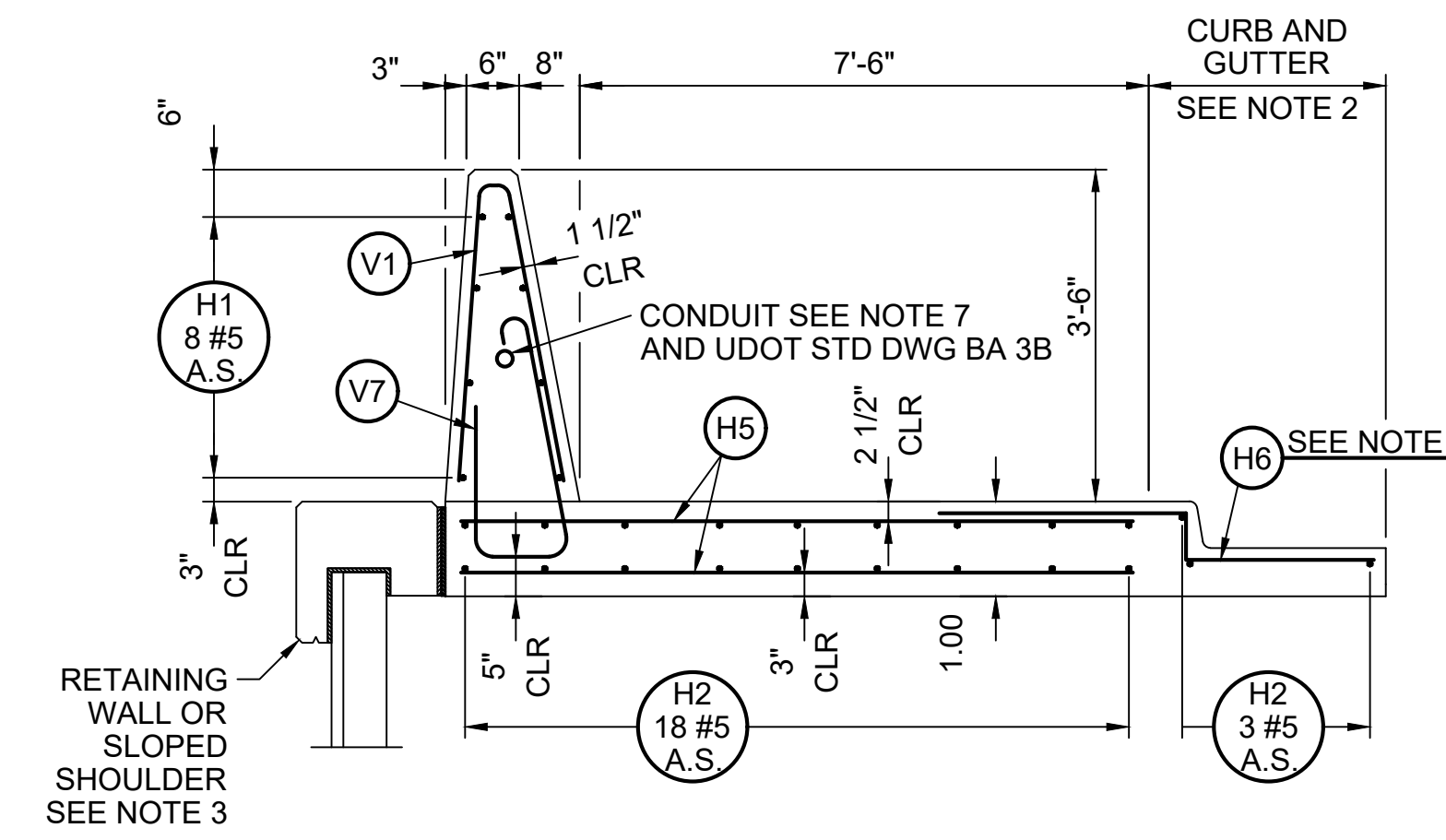
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BRIGHAM CITY CONNECTION PROJECT

DETAIL

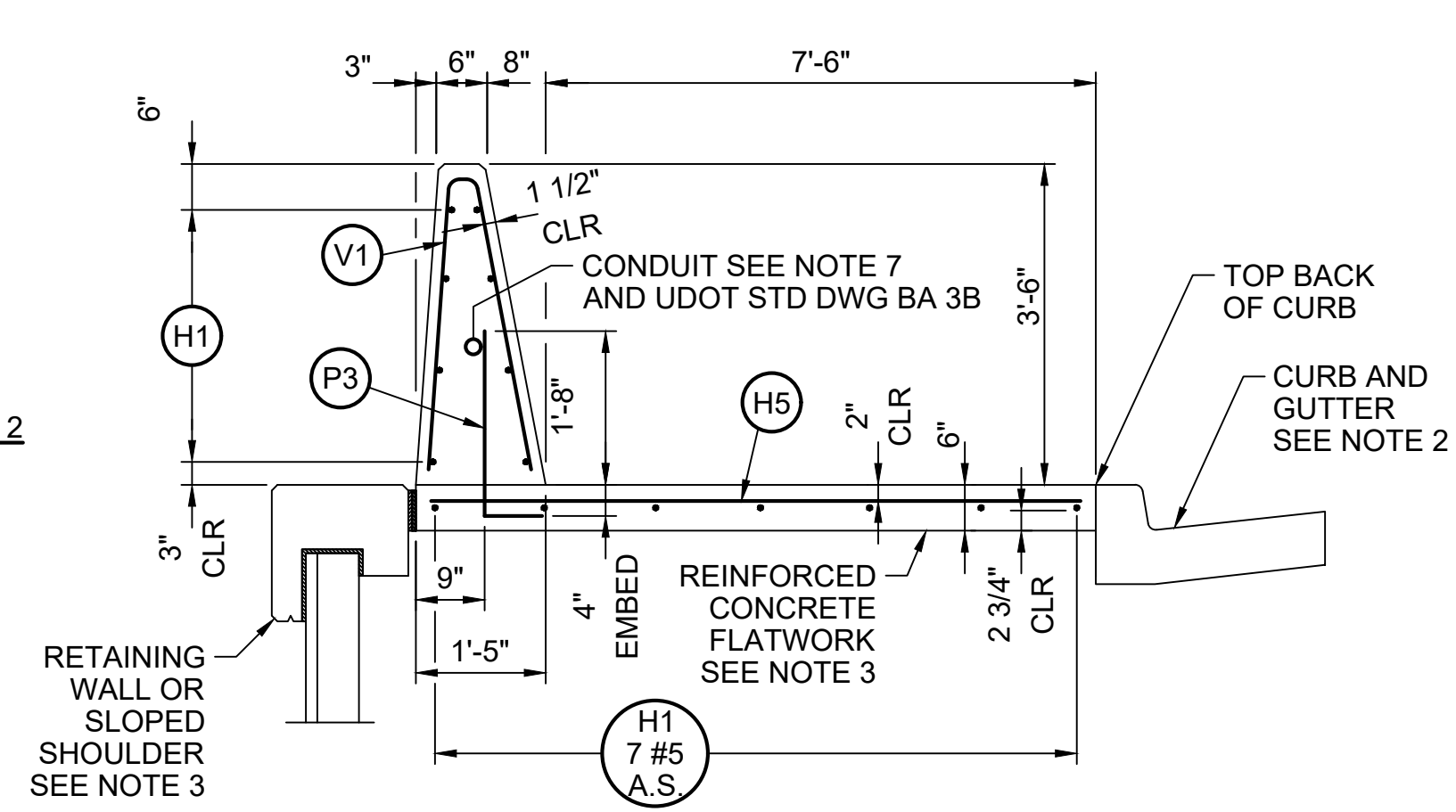
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ELEVATION
 42 INCH CONSTANT SLOPE HALF BARRIER TO
 42 INCH CONSTANT SLOPE BRIDGE PARAPET

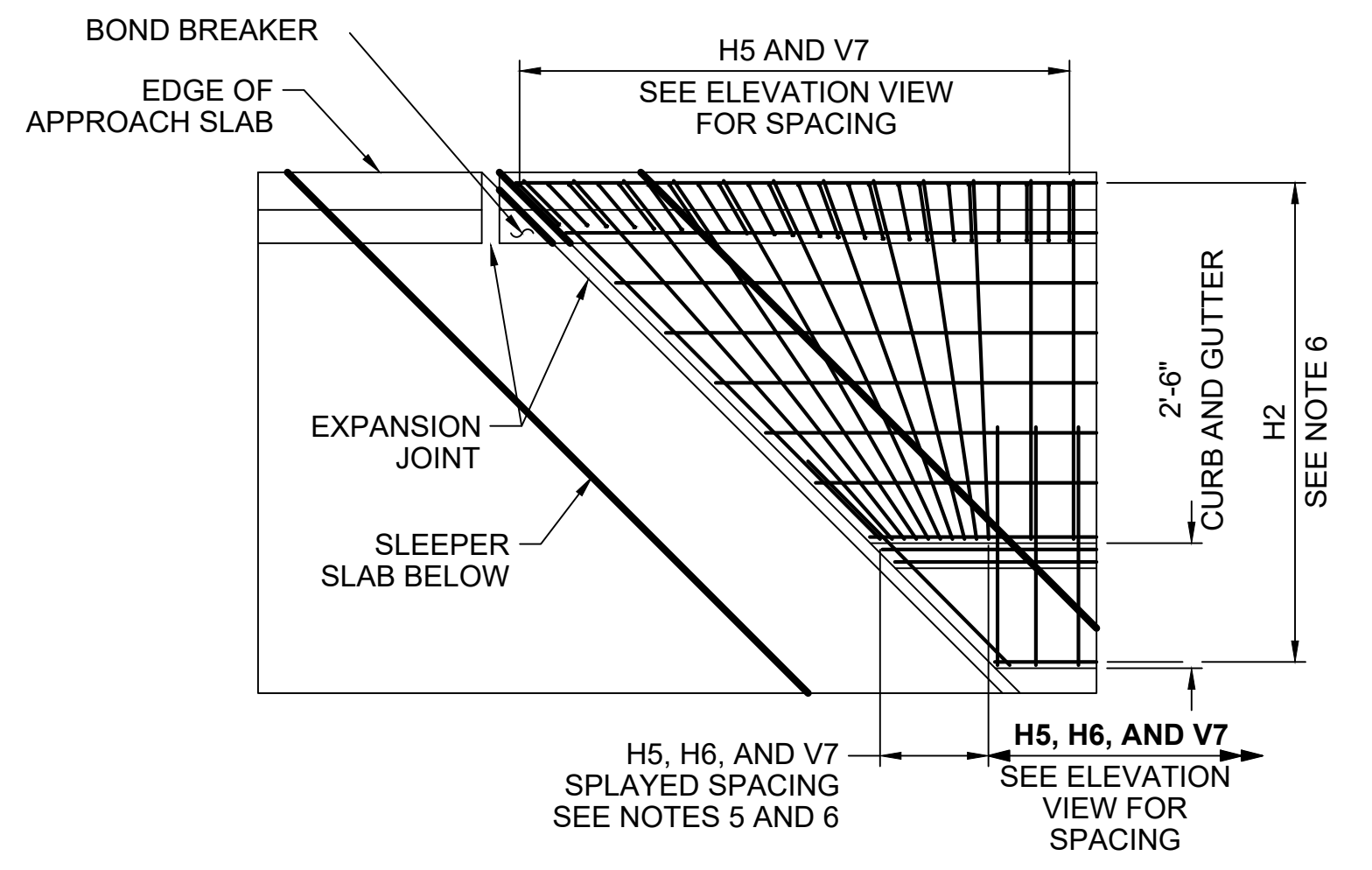


SECTION B-B



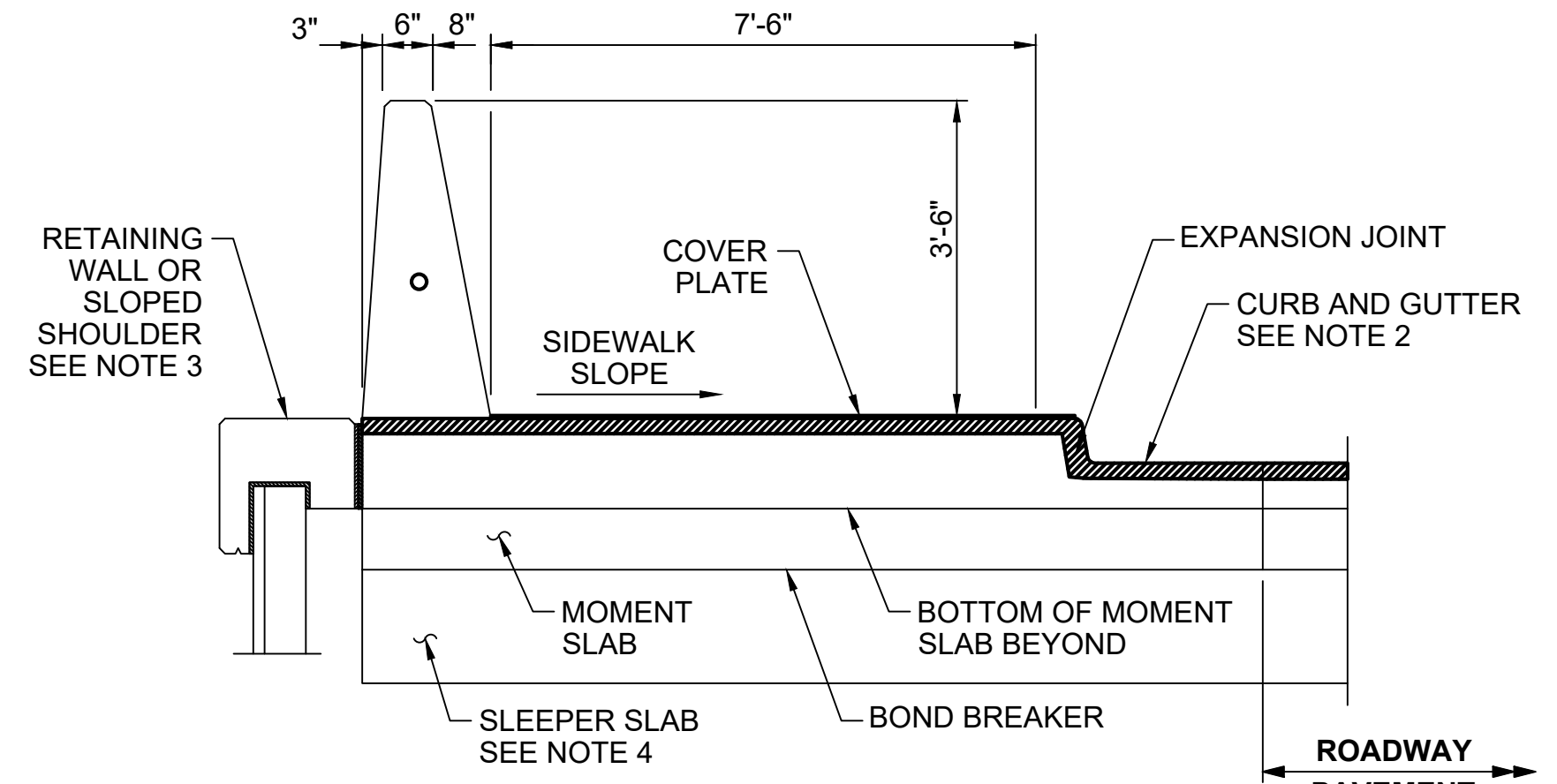
SECTION C-C

BRIDGE PARAPET END SECTION WITH MOMENT SLAB



**PARTIAL PLAN AT TYPICAL SKEWED
 APPROACH SLAB**

BARRIER REINFORCEMENT AND COVER PLATE NOT SHOWN FOR CLARITY
 BRIDGE SKEW OF 45 DEGREES SHOWN, SEE NOTES 5 AND 6
 DO NOT SPLAY BARRIER REINFORCEMENT



VIEW A-A

SLEEPER SLAB WITHOUT STEM SHOWN,
 SEE NOTE 4 IF PRESENT

NOTES

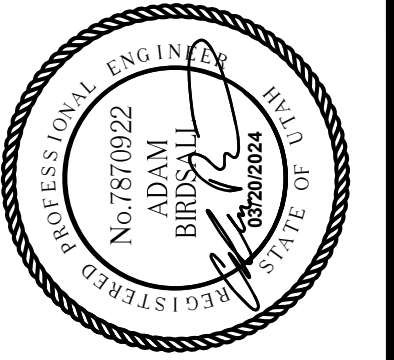
- SEE UDOT STD DWG BA 1A1 FOR GENERAL NOTES.
- CAST CURB AND GUTTER WITH THE MOMENT SLAB. THE STANDARD LAYOUT ASSUMES A 2'-6" WIDE CURB AND GUTTER. GRADUALLY TRANSITION GUTTER SLOPE THROUGH LIMITS OF END SECTION TO ENSURE UNINTERRUPTED DRAINAGE BETWEEN ROADWAY PAVEMENT AND BRIDGE APPROACH SLAB. ADJUST H6 BAR TO MATCH SLOPE AND WIDTH OF GUTTER AS NEEDED.
- PLACE BARRIER ADJACENT TO SLOPED SHOULDER OR RETAINING WALL. SEE BA 1C FOR BARRIER OFFSET REQUIREMENTS. CONTINUE REINFORCED CONCRETE FLATWORK UNTIL THE REQUIREMENTS OF THE BA 1B AND BA 1C DRAWINGS ARE MET.
- IF SLEEPER SLAB STEM IS PRESENT, BLOCK OUT THE CONFLICTING MOMENT SLAB AND BUNDLE DISPLACED H5 AND H6 BARS WITH NEAREST BARS, SHIFT V7 BARS UP TO PROVIDE 1 INCH CLEAR BETWEEN TOP OF SLEEPER SLAB STEM, AND FIELD TRIM H2 BARS TO FIT. USE TWO BARS MAX PER H5 AND H6 BUNDLE. PLACE BOND BREAKER BETWEEN FRESH CONCRETE AND HARDENED CONCRETE SURFACES OF SLEEPER SLAB AND SLEEPER SLAB STEM.
- SPLAY H5 BARS WHEN MOMENT SLAB MEETS SKEWED APPROACH OR SLEEPER SLAB. USE SPACING OF 3 INCH MINIMUM AND 18 INCH MAXIMUM TO ACCOMMODATE VARYING SKEWS. PROVIDE ONE H6 BAR AS SHOWN WITHIN SKEWED ZONE.
- H2 AND H5 BAR LENGTHS PROVIDED ARE BASED UPON A MOMENT SLAB WITH NO SKEW. INCREASE OR DECREASE H2 AND H5 BAR LENGTHS AS NEEDED TO PROVIDE 2 INCH CLEAR COVER AT END OF BAR.
- FIELD VERIFY CONDUIT IN ADJACENT PARAPET AND ADJUST SIZE AND QUANTITY IN END SECTION AS NEEDED.
- SEE UDOT STD DWG BA 3K15 FOR REINFORCING STEEL SCHEDULE AND NOTES.
- WHEN NO SLEEPER SLAB STEM IS PRESENT, MATCH STRUCTURE EXPANSION JOINT SIZE BY ALIGNING FACE OF MOMENT SLAB WITH ADJACENT PCCP. SEE STRUCTURE PLANS FOR SIZE OF EXPANSION JOINT.

| REVISIONS | DATE | BY |
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ONE INCH
 AT FULL
 SCALE IF
 NOT
 OTHERWISE
 ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

DETAIL

MODIFIED SLOPED END SECTION (NORTHWEST END) DT-07 REINFORCING SCHEDULE

| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|----------|----------|-----------|--|---|----|----|-----|-----|---------|----|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|--------|---------|---|---------|---------|---|--------|---------|---|---------|---------|---|---------|---------|---|
| P1 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| P2 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H1 | #5 | AS NEEDED | HORIZONTAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H2 | #5 | 2 | DIAGONAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V1 | #5 | 11 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1" style="font-size: 8px;"> <thead> <tr><th>H</th><th>W</th><th>QTY</th></tr> </thead> <tbody> <tr><td>32"</td><td>15 3/8"</td><td>1</td></tr> <tr><td>29"</td><td>14"</td><td>1</td></tr> <tr><td>26"</td><td>12 1/2"</td><td>1</td></tr> <tr><td>23"</td><td>11"</td><td>1</td></tr> <tr><td>20"</td><td>9 5/8"</td><td>1</td></tr> <tr><td>17"</td><td>8 1/4"</td><td>1</td></tr> <tr><td>14"</td><td>6 3/4"</td><td>1</td></tr> <tr><td>11"</td><td>5 3/8"</td><td>1</td></tr> <tr><td>8"</td><td>3 7/8"</td><td>1</td></tr> <tr><td>5"</td><td>2 3/8"</td><td>1</td></tr> <tr><td>2"</td><td>1"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 32" | 15 3/8" | 1 | 29" | 14" | 1 | 26" | 12 1/2" | 1 | 23" | 11" | 1 | 20" | 9 5/8" | 1 | 17" | 8 1/4" | 1 | 14" | 6 3/4" | 1 | 11" | 5 3/8" | 1 | 8" | 3 7/8" | 1 | 5" | 2 3/8" | 1 | 2" | 1" | 1 | | | | | | | | | | | | | | | | | | | | | | | | |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 32" | 15 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29" | 14" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26" | 12 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23" | 11" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20" | 9 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17" | 8 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14" | 6 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11" | 5 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8" | 3 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5" | 2 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2" | 1" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V2 | #5 | 19 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1" style="font-size: 8px;"> <thead> <tr><th>H1</th><th>H2</th><th>QTY</th></tr> </thead> <tbody> <tr><td>30"</td><td>42 1/8"</td><td>1</td></tr> <tr><td>28 1/2"</td><td>40"</td><td>1</td></tr> <tr><td>26 7/8"</td><td>37 7/8"</td><td>1</td></tr> <tr><td>25 3/8"</td><td>35 3/4"</td><td>1</td></tr> <tr><td>23 3/4"</td><td>33 5/8"</td><td>1</td></tr> <tr><td>22 1/4"</td><td>31 1/2"</td><td>1</td></tr> <tr><td>20 5/8"</td><td>29 3/8"</td><td>1</td></tr> <tr><td>19"</td><td>27 1/8"</td><td>1</td></tr> <tr><td>17 1/2"</td><td>25"</td><td>1</td></tr> <tr><td>16"</td><td>23"</td><td>1</td></tr> <tr><td>14 3/8"</td><td>20 3/4"</td><td>1</td></tr> <tr><td>12 7/8"</td><td>18 5/8"</td><td>1</td></tr> <tr><td>11 1/4"</td><td>16 1/2"</td><td>1</td></tr> <tr><td>9 3/4"</td><td>14 3/4"</td><td>1</td></tr> <tr><td>8 1/8"</td><td>13"</td><td>1</td></tr> <tr><td>6 1/2"</td><td>11 3/8"</td><td>1</td></tr> <tr><td>5 3/4"</td><td>10 1/2"</td><td>1</td></tr> <tr><td>5"</td><td>9 5/8"</td><td>1</td></tr> <tr><td>4 1/4"</td><td>8 3/4"</td><td>1</td></tr> </tbody> </table> | H1 | H2 | QTY | 30" | 42 1/8" | 1 | 28 1/2" | 40" | 1 | 26 7/8" | 37 7/8" | 1 | 25 3/8" | 35 3/4" | 1 | 23 3/4" | 33 5/8" | 1 | 22 1/4" | 31 1/2" | 1 | 20 5/8" | 29 3/8" | 1 | 19" | 27 1/8" | 1 | 17 1/2" | 25" | 1 | 16" | 23" | 1 | 14 3/8" | 20 3/4" | 1 | 12 7/8" | 18 5/8" | 1 | 11 1/4" | 16 1/2" | 1 | 9 3/4" | 14 3/4" | 1 | 8 1/8" | 13" | 1 | 6 1/2" | 11 3/8" | 1 | 5 3/4" | 10 1/2" | 1 | 5" | 9 5/8" | 1 | 4 1/4" | 8 3/4" | 1 |
| H1 | H2 | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 42 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/2" | 40" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 7/8" | 37 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 3/8" | 35 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 3/4" | 33 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 1/4" | 31 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 5/8" | 29 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19" | 27 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17 1/2" | 25" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16" | 23" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 3/8" | 20 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12 7/8" | 18 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 1/4" | 16 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9 3/4" | 14 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8 1/8" | 13" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6 1/2" | 11 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 3/4" | 10 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5" | 9 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 1/4" | 8 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V3 | #5 | 19 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1" style="font-size: 8px;"> <thead> <tr><th>H</th><th>W</th><th>QTY</th></tr> </thead> <tbody> <tr><td>30"</td><td>29"</td><td>1</td></tr> <tr><td>29 3/4"</td><td>28 1/8"</td><td>1</td></tr> <tr><td>29 5/8"</td><td>27 1/8"</td><td>1</td></tr> <tr><td>29 3/8"</td><td>26 1/8"</td><td>1</td></tr> <tr><td>29 1/8"</td><td>25 1/8"</td><td>1</td></tr> <tr><td>29"</td><td>24 1/8"</td><td>1</td></tr> <tr><td>28 3/4"</td><td>23 1/8"</td><td>1</td></tr> <tr><td>28 1/2"</td><td>22 1/8"</td><td>1</td></tr> <tr><td>28 3/8"</td><td>21 1/8"</td><td>1</td></tr> <tr><td>28 1/8"</td><td>20 1/8"</td><td>1</td></tr> <tr><td>27 7/8"</td><td>19 1/8"</td><td>1</td></tr> <tr><td>27 5/8"</td><td>18 1/8"</td><td>1</td></tr> <tr><td>27 1/2"</td><td>17 1/8"</td><td>1</td></tr> <tr><td>24 1/4"</td><td>16 3/4"</td><td>1</td></tr> <tr><td>21"</td><td>16 3/8"</td><td>1</td></tr> <tr><td>17 3/4"</td><td>15 7/8"</td><td>1</td></tr> <tr><td>16"</td><td>15 5/8"</td><td>1</td></tr> <tr><td>14 1/2"</td><td>15 1/2"</td><td>1</td></tr> <tr><td>12 7/8"</td><td>15 1/4"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 30" | 29" | 1 | 29 3/4" | 28 1/8" | 1 | 29 5/8" | 27 1/8" | 1 | 29 3/8" | 26 1/8" | 1 | 29 1/8" | 25 1/8" | 1 | 29" | 24 1/8" | 1 | 28 3/4" | 23 1/8" | 1 | 28 1/2" | 22 1/8" | 1 | 28 3/8" | 21 1/8" | 1 | 28 1/8" | 20 1/8" | 1 | 27 7/8" | 19 1/8" | 1 | 27 5/8" | 18 1/8" | 1 | 27 1/2" | 17 1/8" | 1 | 24 1/4" | 16 3/4" | 1 | 21" | 16 3/8" | 1 | 17 3/4" | 15 7/8" | 1 | 16" | 15 5/8" | 1 | 14 1/2" | 15 1/2" | 1 | 12 7/8" | 15 1/4" | 1 |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 29" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 3/4" | 28 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 5/8" | 27 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 3/8" | 26 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 1/8" | 25 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29" | 24 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 3/4" | 23 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/2" | 22 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 3/8" | 21 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/8" | 20 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 7/8" | 19 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 5/8" | 18 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 1/2" | 17 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 1/4" | 16 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21" | 16 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17 3/4" | 15 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16" | 15 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 1/2" | 15 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12 7/8" | 15 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V4 | #5 | 11 | VERTICAL IN TRAILING SLOPED END SECTION SPLICE FOR V1 AND V3 | <table border="1" style="font-size: 8px;"> <thead> <tr><th>H</th><th>W</th><th>QTY</th></tr> </thead> <tbody> <tr><td>18"</td><td>18"</td><td>5</td></tr> <tr><td>17"</td><td>18"</td><td>1</td></tr> <tr><td>14"</td><td>18"</td><td>1</td></tr> <tr><td>11"</td><td>18"</td><td>1</td></tr> <tr><td>8"</td><td>18"</td><td>1</td></tr> <tr><td>5"</td><td>18"</td><td>1</td></tr> <tr><td>2"</td><td>18"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 18" | 18" | 5 | 17" | 18" | 1 | 14" | 18" | 1 | 11" | 18" | 1 | 8" | 18" | 1 | 5" | 18" | 1 | 2" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 18" | 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2" | 18" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V5 | #5 | 19 | VERTICAL IN TRAILING SLOPED END SECTION SPLICE FOR V2 AND V3 | <table border="1" style="font-size: 8px;"> <thead> <tr><th>H</th><th>W</th><th>QTY</th></tr> </thead> <tbody> <tr><td>18"</td><td>18"</td><td>12</td></tr> <tr><td>16 1/2"</td><td>17 1/8"</td><td>1</td></tr> <tr><td>14 3/4"</td><td>16 3/4"</td><td>1</td></tr> <tr><td>13"</td><td>16 3/8"</td><td>1</td></tr> <tr><td>11 3/8"</td><td>15 7/8"</td><td>1</td></tr> <tr><td>10 1/2"</td><td>15 5/8"</td><td>1</td></tr> <tr><td>9 5/8"</td><td>15 1/2"</td><td>1</td></tr> <tr><td>8 3/4"</td><td>15 1/4"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 18" | 18" | 12 | 16 1/2" | 17 1/8" | 1 | 14 3/4" | 16 3/4" | 1 | 13" | 16 3/8" | 1 | 11 3/8" | 15 7/8" | 1 | 10 1/2" | 15 5/8" | 1 | 9 5/8" | 15 1/2" | 1 | 8 3/4" | 15 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 18" | 12 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16 1/2" | 17 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 3/4" | 16 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13" | 16 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 3/8" | 15 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10 1/2" | 15 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9 5/8" | 15 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8 3/4" | 15 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

LAYOUT: DT-12 PATH: U:\S&T\Projects\Clients\8541-Brigham City\344-8541-02-Forest St-Final-Design\995sect\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:23:45 PM

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| REVISIONS | DATE | BY | | | |
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| BRIGHAM CITY CONNECTION PROJECT | | | | | |
| DETAIL | | | | | |
| DRAWING NO. 22 OF 63 DT-12 | | | | | |

MODIFIED SLOPED END SECTION (NORTHEAST END) DT-08 REINFORCING SCHEDULE

| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|----------|----------|-----------|--|---|---|-----|----------|---|----------|---------|----------|---------|---------|---|----------|---------|----------|---------|----------|---------|----------|---------|----------|---|----------|---------|----------|---------|----------|---------|---------|---------|----------|---|----------|---------|---------|---------|---|---------|---------|---------|---------|---|---------|---------|---------|---------|---|---------|---------|---------|---------|---|---------|---------|---------|---------|---|-----|---------|---------|---------|---|---------|---------|---------|---------|---|---------|---------|-----|---------|---|---------|-----|---------|---------|---|---------|---------|---------|---------|---|---------|---------|---------|---------|---|---------|---------|-----|-----|---|-----|---------|---------|-----|---|---------|--------|---------|-----|---|---------|--------|---------|---------|---|---------|--------|-----|---------|---|---------|--------|---------|---------|---|---------|--------|---------|---------|---|---------|--------|---------|---------|---|---------|--------|---------|---------|---|---------|--------|---------|---------|---|
| P1 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| P2 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H1 | #5 | AS NEEDED | HORIZONTAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | <table border="1"> <thead> <tr> <th>R</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>215 3/8"</td><td>1</td></tr> <tr><td>217 5/8"</td><td>1</td></tr> <tr><td>219 7/8"</td><td>1</td></tr> <tr><td>221"</td><td>1</td></tr> <tr><td>222 1/8"</td><td>1</td></tr> <tr><td>223 3/8"</td><td>1</td></tr> <tr><td>224 1/2"</td><td>1</td></tr> <tr><td>225 5/8"</td><td>1</td></tr> <tr><td>230 3/8"</td><td>1</td></tr> <tr><td>231 1/2"</td><td>1</td></tr> <tr><td>232 5/8"</td><td>1</td></tr> <tr><td>233 7/8"</td><td>1</td></tr> <tr><td>235"</td><td>1</td></tr> <tr><td>236 1/8"</td><td>1</td></tr> <tr><td>237 1/4"</td><td>1</td></tr> </tbody> </table> | R | QTY | 215 3/8" | 1 | 217 5/8" | 1 | 219 7/8" | 1 | 221" | 1 | 222 1/8" | 1 | 223 3/8" | 1 | 224 1/2" | 1 | 225 5/8" | 1 | 230 3/8" | 1 | 231 1/2" | 1 | 232 5/8" | 1 | 233 7/8" | 1 | 235" | 1 | 236 1/8" | 1 | 237 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| R | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 215 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 217 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 219 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 221" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 222 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 223 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 224 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 225 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 230 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 231 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 232 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 233 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 235" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 236 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 237 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H2 | #5 | 2 | DIAGONAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | <table border="1"> <thead> <tr> <th>R</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>225 5/8"</td><td>1</td></tr> <tr><td>230 3/8"</td><td>1</td></tr> </tbody> </table> | R | QTY | 225 5/8" | 1 | 230 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| R | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 225 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 230 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V1 | #5 | 26 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>H1</th> <th>H2</th> <th>W</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>54 1/8"</td><td>36"</td><td>18 7/8"</td><td>23 7/8"</td><td>1</td></tr> <tr><td>52 3/8"</td><td>34 1/2"</td><td>17 7/8"</td><td>23 3/4"</td><td>1</td></tr> <tr><td>50 5/8"</td><td>33 1/8"</td><td>17 5/8"</td><td>23 3/4"</td><td>1</td></tr> <tr><td>49"</td><td>31 5/8"</td><td>17 3/8"</td><td>23 5/8"</td><td>1</td></tr> <tr><td>47 1/4"</td><td>30 1/8"</td><td>17 1/8"</td><td>23 5/8"</td><td>1</td></tr> <tr><td>45 1/2"</td><td>28 3/4"</td><td>16 7/8"</td><td>23 5/8"</td><td>1</td></tr> <tr><td>43 3/4"</td><td>27 1/4"</td><td>16 5/8"</td><td>23 1/2"</td><td>1</td></tr> <tr><td>42 1/8"</td><td>25 3/4"</td><td>16 3/8"</td><td>23 1/2"</td><td>1</td></tr> <tr><td>40 3/8"</td><td>24 1/4"</td><td>16 1/8"</td><td>23 3/8"</td><td>1</td></tr> <tr><td>38 5/8"</td><td>22 7/8"</td><td>15 7/8"</td><td>23 3/8"</td><td>1</td></tr> <tr><td>37"</td><td>21 3/8"</td><td>15 1/2"</td><td>23 3/8"</td><td>1</td></tr> <tr><td>35 1/4"</td><td>19 7/8"</td><td>15 1/4"</td><td>23 1/4"</td><td>1</td></tr> <tr><td>33 1/2"</td><td>18 1/2"</td><td>15"</td><td>23 1/4"</td><td>1</td></tr> <tr><td>31 3/4"</td><td>17"</td><td>14 3/4"</td><td>23 1/4"</td><td>1</td></tr> <tr><td>30 1/8"</td><td>15 1/2"</td><td>14 1/2"</td><td>23 1/8"</td><td>1</td></tr> <tr><td>28 3/8"</td><td>14 1/8"</td><td>14 1/4"</td><td>23 1/8"</td><td>1</td></tr> <tr><td>26 5/8"</td><td>12 5/8"</td><td>14"</td><td>23"</td><td>1</td></tr> <tr><td>25"</td><td>11 1/8"</td><td>13 3/4"</td><td>23"</td><td>1</td></tr> <tr><td>23 1/4"</td><td>9 3/4"</td><td>13 1/2"</td><td>23"</td><td>1</td></tr> <tr><td>21 1/2"</td><td>8 1/4"</td><td>13 1/4"</td><td>22 7/8"</td><td>1</td></tr> <tr><td>19 3/4"</td><td>6 3/4"</td><td>13"</td><td>22 7/8"</td><td>1</td></tr> <tr><td>18 1/8"</td><td>5 1/4"</td><td>12 3/4"</td><td>22 3/4"</td><td>1</td></tr> <tr><td>16 3/8"</td><td>3 7/8"</td><td>12 1/2"</td><td>22 3/4"</td><td>1</td></tr> <tr><td>15 1/2"</td><td>3 1/8"</td><td>12 3/8"</td><td>22 3/4"</td><td>1</td></tr> <tr><td>14 5/8"</td><td>2 3/8"</td><td>12 1/4"</td><td>22 5/8"</td><td>1</td></tr> <tr><td>13 3/4"</td><td>1 5/8"</td><td>12 1/8"</td><td>22 5/8"</td><td>1</td></tr> </tbody> </table> <p align="center">* VARIES WITH H</p> | H | H1 | H2 | W | QTY | 54 1/8" | 36" | 18 7/8" | 23 7/8" | 1 | 52 3/8" | 34 1/2" | 17 7/8" | 23 3/4" | 1 | 50 5/8" | 33 1/8" | 17 5/8" | 23 3/4" | 1 | 49" | 31 5/8" | 17 3/8" | 23 5/8" | 1 | 47 1/4" | 30 1/8" | 17 1/8" | 23 5/8" | 1 | 45 1/2" | 28 3/4" | 16 7/8" | 23 5/8" | 1 | 43 3/4" | 27 1/4" | 16 5/8" | 23 1/2" | 1 | 42 1/8" | 25 3/4" | 16 3/8" | 23 1/2" | 1 | 40 3/8" | 24 1/4" | 16 1/8" | 23 3/8" | 1 | 38 5/8" | 22 7/8" | 15 7/8" | 23 3/8" | 1 | 37" | 21 3/8" | 15 1/2" | 23 3/8" | 1 | 35 1/4" | 19 7/8" | 15 1/4" | 23 1/4" | 1 | 33 1/2" | 18 1/2" | 15" | 23 1/4" | 1 | 31 3/4" | 17" | 14 3/4" | 23 1/4" | 1 | 30 1/8" | 15 1/2" | 14 1/2" | 23 1/8" | 1 | 28 3/8" | 14 1/8" | 14 1/4" | 23 1/8" | 1 | 26 5/8" | 12 5/8" | 14" | 23" | 1 | 25" | 11 1/8" | 13 3/4" | 23" | 1 | 23 1/4" | 9 3/4" | 13 1/2" | 23" | 1 | 21 1/2" | 8 1/4" | 13 1/4" | 22 7/8" | 1 | 19 3/4" | 6 3/4" | 13" | 22 7/8" | 1 | 18 1/8" | 5 1/4" | 12 3/4" | 22 3/4" | 1 | 16 3/8" | 3 7/8" | 12 1/2" | 22 3/4" | 1 | 15 1/2" | 3 1/8" | 12 3/8" | 22 3/4" | 1 | 14 5/8" | 2 3/8" | 12 1/4" | 22 5/8" | 1 | 13 3/4" | 1 5/8" | 12 1/8" | 22 5/8" | 1 |
| H | H1 | H2 | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54 1/8" | 36" | 18 7/8" | 23 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 52 3/8" | 34 1/2" | 17 7/8" | 23 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 50 5/8" | 33 1/8" | 17 5/8" | 23 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 49" | 31 5/8" | 17 3/8" | 23 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 47 1/4" | 30 1/8" | 17 1/8" | 23 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 45 1/2" | 28 3/4" | 16 7/8" | 23 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 43 3/4" | 27 1/4" | 16 5/8" | 23 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42 1/8" | 25 3/4" | 16 3/8" | 23 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 40 3/8" | 24 1/4" | 16 1/8" | 23 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 38 5/8" | 22 7/8" | 15 7/8" | 23 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 37" | 21 3/8" | 15 1/2" | 23 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 35 1/4" | 19 7/8" | 15 1/4" | 23 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 33 1/2" | 18 1/2" | 15" | 23 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 31 3/4" | 17" | 14 3/4" | 23 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30 1/8" | 15 1/2" | 14 1/2" | 23 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 3/8" | 14 1/8" | 14 1/4" | 23 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 5/8" | 12 5/8" | 14" | 23" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25" | 11 1/8" | 13 3/4" | 23" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 1/4" | 9 3/4" | 13 1/2" | 23" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21 1/2" | 8 1/4" | 13 1/4" | 22 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 3/4" | 6 3/4" | 13" | 22 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18 1/8" | 5 1/4" | 12 3/4" | 22 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16 3/8" | 3 7/8" | 12 1/2" | 22 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 15 1/2" | 3 1/8" | 12 3/8" | 22 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 5/8" | 2 3/8" | 12 1/4" | 22 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13 3/4" | 1 5/8" | 12 1/8" | 22 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

LAYOUT: DT-13 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002-Forest St-Final Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:23:50 PM

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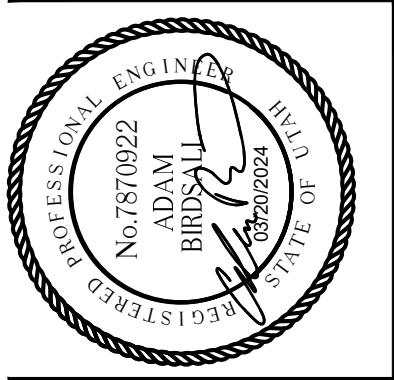
ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: JAT
 DRAWN: JAT

CHECKED: JWP
 APPROVED: AJB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

LAYOUT: DT-14 PATH: U:\Satt\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\cadd\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:23:55 PM

| RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (NORTHEAST END) DT-09 REINFORCING SCHEDULE | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| P1 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H1 | #5 | AS NEEDED | HORIZONTAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H2 | #5 | 2 | DIAGONAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V1 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>30"</td><td>1</td></tr> <tr><td>28 1/2"</td><td>1</td></tr> <tr><td>26 7/8"</td><td>1</td></tr> <tr><td>25 3/8"</td><td>1</td></tr> <tr><td>23 7/8"</td><td>1</td></tr> <tr><td>22 1/4"</td><td>1</td></tr> <tr><td>20 3/4"</td><td>1</td></tr> <tr><td>19 1/4"</td><td>1</td></tr> <tr><td>17 3/4"</td><td>1</td></tr> <tr><td>16 1/8"</td><td>1</td></tr> <tr><td>14 5/8"</td><td>1</td></tr> <tr><td>13 1/8"</td><td>1</td></tr> <tr><td>11 1/2"</td><td>1</td></tr> <tr><td>10"</td><td>1</td></tr> <tr><td>8 1/2"</td><td>1</td></tr> <tr><td>6 7/8"</td><td>1</td></tr> <tr><td>5 3/8"</td><td>1</td></tr> <tr><td>3 7/8"</td><td>1</td></tr> <tr><td>2 1/4"</td><td>1</td></tr> <tr><td>1 1/2"</td><td>1</td></tr> <tr><td>3/4"</td><td>1</td></tr> <tr><td>0"</td><td>1</td></tr> </tbody> </table> | H | QTY | 30" | 1 | 28 1/2" | 1 | 26 7/8" | 1 | 25 3/8" | 1 | 23 7/8" | 1 | 22 1/4" | 1 | 20 3/4" | 1 | 19 1/4" | 1 | 17 3/4" | 1 | 16 1/8" | 1 | 14 5/8" | 1 | 13 1/8" | 1 | 11 1/2" | 1 | 10" | 1 | 8 1/2" | 1 | 6 7/8" | 1 | 5 3/8" | 1 | 3 7/8" | 1 | 2 1/4" | 1 | 1 1/2" | 1 | 3/4" | 1 | 0" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 0" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V2 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>30"</td><td>1</td></tr> <tr><td>29 1/2"</td><td>1</td></tr> <tr><td>29"</td><td>1</td></tr> <tr><td>28 1/2"</td><td>1</td></tr> <tr><td>28"</td><td>1</td></tr> <tr><td>27 5/8"</td><td>1</td></tr> <tr><td>27 1/8"</td><td>1</td></tr> <tr><td>26 5/8"</td><td>1</td></tr> <tr><td>26 1/8"</td><td>1</td></tr> <tr><td>25 5/8"</td><td>1</td></tr> <tr><td>25 1/8"</td><td>1</td></tr> <tr><td>24 5/8"</td><td>1</td></tr> <tr><td>24 1/8"</td><td>1</td></tr> <tr><td>23 5/8"</td><td>1</td></tr> <tr><td>23 1/4"</td><td>1</td></tr> <tr><td>22 3/4"</td><td>1</td></tr> <tr><td>22 1/4"</td><td>1</td></tr> <tr><td>21 3/4"</td><td>1</td></tr> <tr><td>21 1/4"</td><td>1</td></tr> <tr><td>21"</td><td>1</td></tr> <tr><td>20 3/4"</td><td>1</td></tr> <tr><td>20 1/2"</td><td>1</td></tr> </tbody> </table> | H | QTY | 30" | 1 | 29 1/2" | 1 | 29" | 1 | 28 1/2" | 1 | 28" | 1 | 27 5/8" | 1 | 27 1/8" | 1 | 26 5/8" | 1 | 26 1/8" | 1 | 25 5/8" | 1 | 25 1/8" | 1 | 24 5/8" | 1 | 24 1/8" | 1 | 23 5/8" | 1 | 23 1/4" | 1 | 22 3/4" | 1 | 22 1/4" | 1 | 21 3/4" | 1 | 21 1/4" | 1 | 21" | 1 | 20 3/4" | 1 | 20 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V3 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>W</th> <th>A</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>24"</td><td>17 5/8"</td><td>90°</td><td>1</td></tr> <tr><td>25 5/8"</td><td>16 7/8"</td><td>91°</td><td>1</td></tr> <tr><td>27 3/8"</td><td>16 1/4"</td><td>91°</td><td>1</td></tr> <tr><td>29"</td><td>15 5/8"</td><td>92°</td><td>1</td></tr> <tr><td>30 5/8"</td><td>15"</td><td>92°</td><td>1</td></tr> <tr><td>32 3/8"</td><td>14 1/4"</td><td>93°</td><td>1</td></tr> <tr><td>34"</td><td>13 5/8"</td><td>93°</td><td>1</td></tr> <tr><td>35 5/8"</td><td>13"</td><td>94°</td><td>1</td></tr> <tr><td>37 3/8"</td><td>12 1/4"</td><td>95°</td><td>1</td></tr> <tr><td>39"</td><td>11 5/8"</td><td>95°</td><td>1</td></tr> <tr><td>40 5/8"</td><td>11"</td><td>96°</td><td>1</td></tr> <tr><td>42 3/8"</td><td>10 3/8"</td><td>96°</td><td>1</td></tr> <tr><td>44"</td><td>9 5/8"</td><td>97°</td><td>1</td></tr> <tr><td>45 5/8"</td><td>9"</td><td>97°</td><td>1</td></tr> <tr><td>47 3/8"</td><td>8 3/8"</td><td>98°</td><td>1</td></tr> <tr><td>49"</td><td>7 5/8"</td><td>98°</td><td>1</td></tr> <tr><td>50 5/8"</td><td>7"</td><td>99°</td><td>1</td></tr> <tr><td>52 3/8"</td><td>6 3/8"</td><td>100°</td><td>1</td></tr> <tr><td>54"</td><td>5 3/4"</td><td>100°</td><td>1</td></tr> <tr><td>54 7/8"</td><td>5 3/8"</td><td>100°</td><td>1</td></tr> <tr><td>55 5/8"</td><td>5"</td><td>101°</td><td>1</td></tr> <tr><td>56 1/2"</td><td>4 3/4"</td><td>101°</td><td>1</td></tr> </tbody> </table> | H | W | A | QTY | 24" | 17 5/8" | 90° | 1 | 25 5/8" | 16 7/8" | 91° | 1 | 27 3/8" | 16 1/4" | 91° | 1 | 29" | 15 5/8" | 92° | 1 | 30 5/8" | 15" | 92° | 1 | 32 3/8" | 14 1/4" | 93° | 1 | 34" | 13 5/8" | 93° | 1 | 35 5/8" | 13" | 94° | 1 | 37 3/8" | 12 1/4" | 95° | 1 | 39" | 11 5/8" | 95° | 1 | 40 5/8" | 11" | 96° | 1 | 42 3/8" | 10 3/8" | 96° | 1 | 44" | 9 5/8" | 97° | 1 | 45 5/8" | 9" | 97° | 1 | 47 3/8" | 8 3/8" | 98° | 1 | 49" | 7 5/8" | 98° | 1 | 50 5/8" | 7" | 99° | 1 | 52 3/8" | 6 3/8" | 100° | 1 | 54" | 5 3/4" | 100° | 1 | 54 7/8" | 5 3/8" | 100° | 1 | 55 5/8" | 5" | 101° | 1 | 56 1/2" | 4 3/4" | 101° | 1 |
| H | W | A | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24" | 17 5/8" | 90° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 5/8" | 16 7/8" | 91° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 3/8" | 16 1/4" | 91° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29" | 15 5/8" | 92° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30 5/8" | 15" | 92° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 32 3/8" | 14 1/4" | 93° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 34" | 13 5/8" | 93° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 35 5/8" | 13" | 94° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 37 3/8" | 12 1/4" | 95° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 39" | 11 5/8" | 95° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 40 5/8" | 11" | 96° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42 3/8" | 10 3/8" | 96° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 44" | 9 5/8" | 97° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 45 5/8" | 9" | 97° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 47 3/8" | 8 3/8" | 98° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 49" | 7 5/8" | 98° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 50 5/8" | 7" | 99° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 52 3/8" | 6 3/8" | 100° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54" | 5 3/4" | 100° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54 7/8" | 5 3/8" | 100° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 55 5/8" | 5" | 101° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 56 1/2" | 4 3/4" | 101° | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V4 | #5 | 17 | VERTICAL IN TRAILING SLOPED END SECTION SPLICE FOR V1 AND V3 | <table border="1"> <thead> <tr> <th>H</th> <th>W</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>18"</td><td>17 5/8"</td><td>1</td></tr> <tr><td>18"</td><td>16 7/8"</td><td>1</td></tr> <tr><td>18"</td><td>16 1/4"</td><td>1</td></tr> <tr><td>18"</td><td>15 5/8"</td><td>1</td></tr> <tr><td>18"</td><td>15"</td><td>1</td></tr> <tr><td>18"</td><td>14 1/4"</td><td>1</td></tr> <tr><td>18"</td><td>13 5/8"</td><td>1</td></tr> <tr><td>18"</td><td>13"</td><td>1</td></tr> <tr><td>18"</td><td>12 1/4"</td><td>1</td></tr> <tr><td>17 1/8"</td><td>11 5/8"</td><td>1</td></tr> <tr><td>15"</td><td>11"</td><td>1</td></tr> <tr><td>12 7/8"</td><td>10 3/8"</td><td>1</td></tr> <tr><td>10 3/4"</td><td>9 5/8"</td><td>1</td></tr> <tr><td>8 3/4"</td><td>9"</td><td>1</td></tr> <tr><td>6 5/8"</td><td>8 3/8"</td><td>1</td></tr> <tr><td>4 1/2"</td><td>7 5/8"</td><td>1</td></tr> <tr><td>2 3/8"</td><td>7"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 18" | 17 5/8" | 1 | 18" | 16 7/8" | 1 | 18" | 16 1/4" | 1 | 18" | 15 5/8" | 1 | 18" | 15" | 1 | 18" | 14 1/4" | 1 | 18" | 13 5/8" | 1 | 18" | 13" | 1 | 18" | 12 1/4" | 1 | 17 1/8" | 11 5/8" | 1 | 15" | 11" | 1 | 12 7/8" | 10 3/8" | 1 | 10 3/4" | 9 5/8" | 1 | 8 3/4" | 9" | 1 | 6 5/8" | 8 3/8" | 1 | 4 1/2" | 7 5/8" | 1 | 2 3/8" | 7" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 17 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 16 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 16 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 15 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 15" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 14 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 13 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 13" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 12 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17 1/8" | 11 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 15" | 11" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12 7/8" | 10 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10 3/4" | 9 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8 3/4" | 9" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6 5/8" | 8 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 1/2" | 7 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 3/8" | 7" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

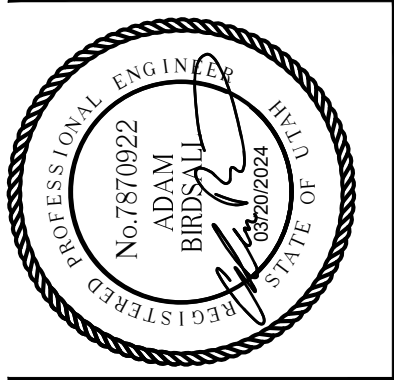
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ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED: JAT
 DRAWN: JAT
 CHECKED: JWP
 APPROVED: AJB

DATE: 03/20/2024
 JOB No.: 344-8541-002



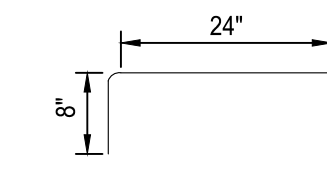
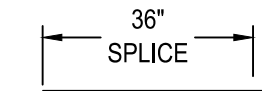
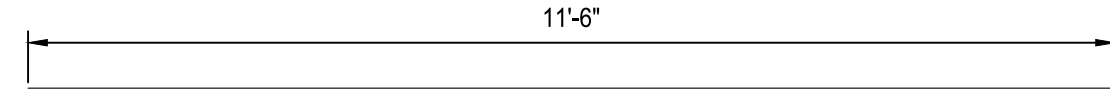
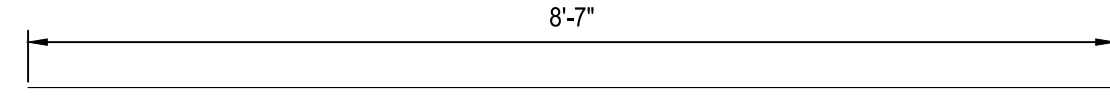
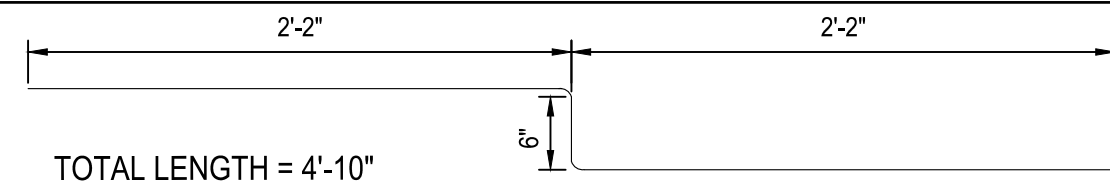
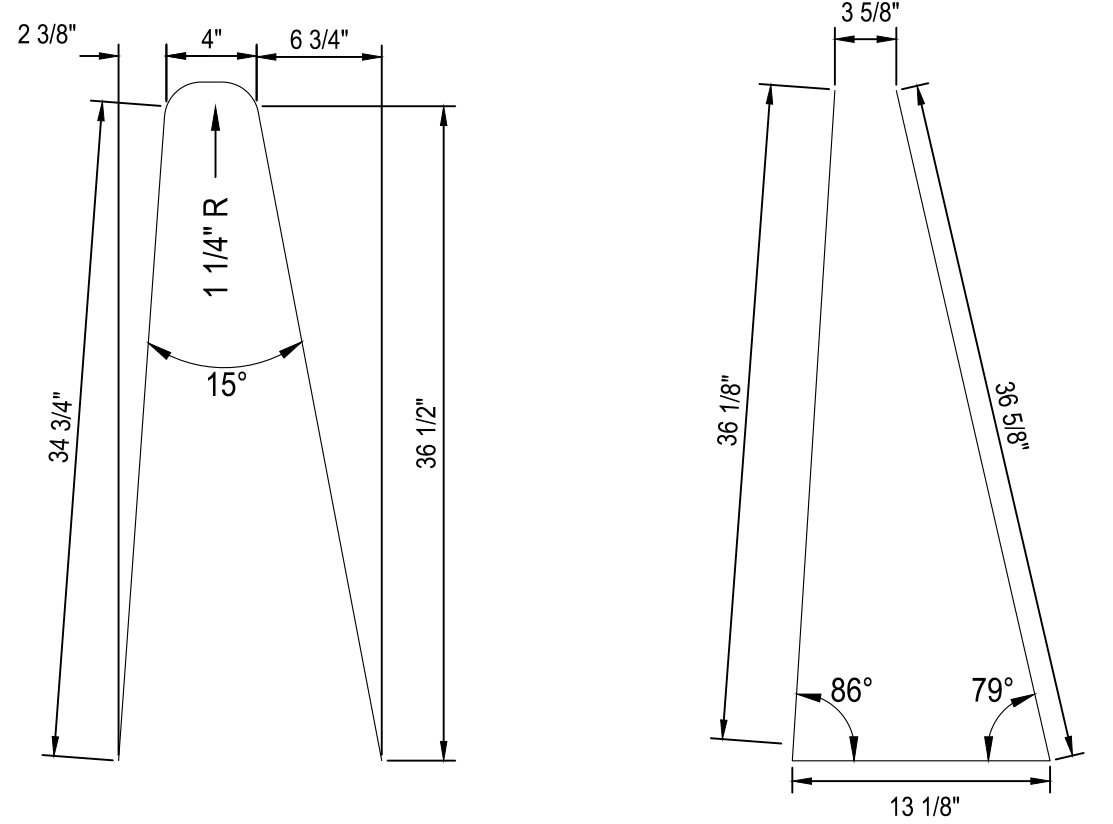
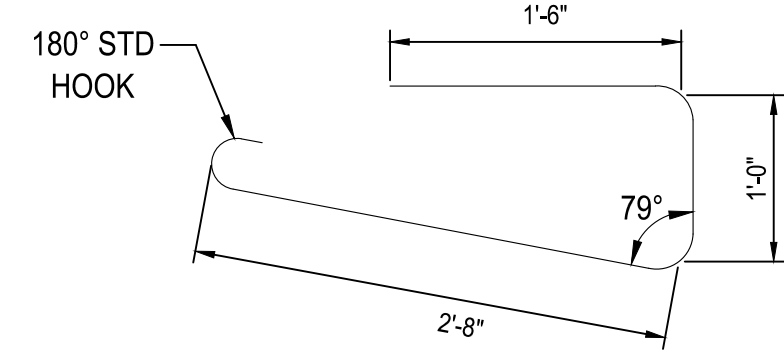
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

LAYOUT: DT-15 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002-Forest St-Final-Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:24:00 PM

| RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (SOUTHEAST END) DT-10 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|----------|-----------|--|---|---|-----|-----|-----|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---------|---------|---------|---|---------|---------|---|---------|---------|---|---------|---------|---|---------|--------|---|---------|--------|---|---------|--------|---|---------|--------|---|
| REINFORCING SCHEDULE | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| P1 | #5 | AS NEEDED | PAVEMENT TO BARRIER THROUGH LIMITS OF BARRIER (VERTICAL) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H1 | #5 | AS NEEDED | HORIZONTAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| H2 | #5 | 2 | DIAGONAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V1 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>30"</td><td>1</td></tr> <tr><td>28 1/2"</td><td>1</td></tr> <tr><td>26 7/8"</td><td>1</td></tr> <tr><td>25 3/8"</td><td>1</td></tr> <tr><td>23 7/8"</td><td>1</td></tr> <tr><td>22 1/4"</td><td>1</td></tr> <tr><td>20 3/4"</td><td>1</td></tr> <tr><td>19 1/4"</td><td>1</td></tr> <tr><td>17 3/4"</td><td>1</td></tr> <tr><td>16 1/8"</td><td>1</td></tr> <tr><td>14 5/8"</td><td>1</td></tr> <tr><td>13 1/8"</td><td>1</td></tr> <tr><td>11 1/2"</td><td>1</td></tr> <tr><td>10"</td><td>1</td></tr> <tr><td>8 1/2"</td><td>1</td></tr> <tr><td>6 7/8"</td><td>1</td></tr> <tr><td>5 3/8"</td><td>1</td></tr> <tr><td>3 7/8"</td><td>1</td></tr> <tr><td>2 1/4"</td><td>1</td></tr> <tr><td>1 1/2"</td><td>1</td></tr> <tr><td>3/4"</td><td>1</td></tr> <tr><td>0"</td><td>1</td></tr> </tbody> </table> | H | QTY | 30" | 1 | 28 1/2" | 1 | 26 7/8" | 1 | 25 3/8" | 1 | 23 7/8" | 1 | 22 1/4" | 1 | 20 3/4" | 1 | 19 1/4" | 1 | 17 3/4" | 1 | 16 1/8" | 1 | 14 5/8" | 1 | 13 1/8" | 1 | 11 1/2" | 1 | 10" | 1 | 8 1/2" | 1 | 6 7/8" | 1 | 5 3/8" | 1 | 3 7/8" | 1 | 2 1/4" | 1 | 1 1/2" | 1 | 3/4" | 1 | 0" | 1 | | | | | | | | | | | | | | | | | | | | | | | |
| H | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 14 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 0" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V2 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>30"</td><td>1</td></tr> <tr><td>29 3/8"</td><td>1</td></tr> <tr><td>28 7/8"</td><td>1</td></tr> <tr><td>28 1/4"</td><td>1</td></tr> <tr><td>27 3/4"</td><td>1</td></tr> <tr><td>27 1/8"</td><td>1</td></tr> <tr><td>26 5/8"</td><td>1</td></tr> <tr><td>26"</td><td>1</td></tr> <tr><td>25 3/8"</td><td>1</td></tr> <tr><td>24 7/8"</td><td>1</td></tr> <tr><td>24 1/4"</td><td>1</td></tr> <tr><td>23 3/4"</td><td>1</td></tr> <tr><td>23 1/8"</td><td>1</td></tr> <tr><td>22 1/2"</td><td>1</td></tr> <tr><td>22"</td><td>1</td></tr> <tr><td>21 3/8"</td><td>1</td></tr> <tr><td>20 7/8"</td><td>1</td></tr> <tr><td>20 1/4"</td><td>1</td></tr> <tr><td>19 3/4"</td><td>1</td></tr> <tr><td>19 3/8"</td><td>1</td></tr> <tr><td>19 1/8"</td><td>1</td></tr> <tr><td>18 7/8"</td><td>1</td></tr> </tbody> </table> | H | QTY | 30" | 1 | 29 3/8" | 1 | 28 7/8" | 1 | 28 1/4" | 1 | 27 3/4" | 1 | 27 1/8" | 1 | 26 5/8" | 1 | 26" | 1 | 25 3/8" | 1 | 24 7/8" | 1 | 24 1/4" | 1 | 23 3/4" | 1 | 23 1/8" | 1 | 22 1/2" | 1 | 22" | 1 | 21 3/8" | 1 | 20 7/8" | 1 | 20 1/4" | 1 | 19 3/4" | 1 | 19 3/8" | 1 | 19 1/8" | 1 | 18 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | |
| H | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 28 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 27 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 21 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 19 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V3 | #5 | 22 | VERTICAL IN TRAILING SLOPED END SECTION | <table border="1"> <thead> <tr> <th>H</th> <th>W</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>30"</td><td>29 1/8"</td><td>1</td></tr> <tr><td>31 1/4"</td><td>28"</td><td>1</td></tr> <tr><td>32 1/2"</td><td>26 7/8"</td><td>1</td></tr> <tr><td>33 7/8"</td><td>25 7/8"</td><td>1</td></tr> <tr><td>35 1/8"</td><td>24 3/4"</td><td>1</td></tr> <tr><td>36 3/8"</td><td>23 5/8"</td><td>1</td></tr> <tr><td>37 5/8"</td><td>22 1/2"</td><td>1</td></tr> <tr><td>38 7/8"</td><td>21 1/2"</td><td>1</td></tr> <tr><td>40 1/4"</td><td>20 3/8"</td><td>1</td></tr> <tr><td>41 1/2"</td><td>19 1/4"</td><td>1</td></tr> <tr><td>42 3/4"</td><td>18 1/8"</td><td>1</td></tr> <tr><td>44"</td><td>17 3/8"</td><td>1</td></tr> <tr><td>45 1/4"</td><td>16"</td><td>1</td></tr> <tr><td>46 1/2"</td><td>14 7/8"</td><td>1</td></tr> <tr><td>47 7/8"</td><td>13 3/4"</td><td>1</td></tr> <tr><td>49 1/8"</td><td>12 3/4"</td><td>1</td></tr> <tr><td>50 3/8"</td><td>11 5/8"</td><td>1</td></tr> <tr><td>51 5/8"</td><td>10 1/2"</td><td>1</td></tr> <tr><td>52 7/8"</td><td>9 1/2"</td><td>1</td></tr> <tr><td>53 5/8"</td><td>8 7/8"</td><td>1</td></tr> <tr><td>54 1/4"</td><td>8 3/8"</td><td>1</td></tr> <tr><td>54 7/8"</td><td>7 3/4"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 30" | 29 1/8" | 1 | 31 1/4" | 28" | 1 | 32 1/2" | 26 7/8" | 1 | 33 7/8" | 25 7/8" | 1 | 35 1/8" | 24 3/4" | 1 | 36 3/8" | 23 5/8" | 1 | 37 5/8" | 22 1/2" | 1 | 38 7/8" | 21 1/2" | 1 | 40 1/4" | 20 3/8" | 1 | 41 1/2" | 19 1/4" | 1 | 42 3/4" | 18 1/8" | 1 | 44" | 17 3/8" | 1 | 45 1/4" | 16" | 1 | 46 1/2" | 14 7/8" | 1 | 47 7/8" | 13 3/4" | 1 | 49 1/8" | 12 3/4" | 1 | 50 3/8" | 11 5/8" | 1 | 51 5/8" | 10 1/2" | 1 | 52 7/8" | 9 1/2" | 1 | 53 5/8" | 8 7/8" | 1 | 54 1/4" | 8 3/8" | 1 | 54 7/8" | 7 3/4" | 1 |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 30" | 29 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 31 1/4" | 28" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 32 1/2" | 26 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 33 7/8" | 25 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 35 1/8" | 24 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 36 3/8" | 23 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 37 5/8" | 22 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 38 7/8" | 21 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 40 1/4" | 20 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 41 1/2" | 19 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 42 3/4" | 18 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 44" | 17 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 45 1/4" | 16" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 46 1/2" | 14 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 47 7/8" | 13 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 49 1/8" | 12 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 50 3/8" | 11 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 51 5/8" | 10 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 52 7/8" | 9 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 53 5/8" | 8 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54 1/4" | 8 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 54 7/8" | 7 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| V4 | #5 | 16 | VERTICAL IN TRAILING SLOPED END SECTION SPLICE FOR V1 AND V3 | <table border="1"> <thead> <tr> <th>H</th> <th>W</th> <th>QTY</th> </tr> </thead> <tbody> <tr><td>18"</td><td>17 5/8"</td><td>1</td></tr> <tr><td>18"</td><td>16 1/2"</td><td>1</td></tr> <tr><td>18"</td><td>15 3/8"</td><td>1</td></tr> <tr><td>18"</td><td>14 1/4"</td><td>1</td></tr> <tr><td>18"</td><td>13 1/4"</td><td>1</td></tr> <tr><td>18"</td><td>12 1/8"</td><td>1</td></tr> <tr><td>18"</td><td>11"</td><td>1</td></tr> <tr><td>17"</td><td>9 7/8"</td><td>1</td></tr> <tr><td>15 1/4"</td><td>8 7/8"</td><td>1</td></tr> <tr><td>13 3/8"</td><td>7 3/4"</td><td>1</td></tr> <tr><td>11 1/2"</td><td>6 5/8"</td><td>1</td></tr> <tr><td>9 5/8"</td><td>5 5/8"</td><td>1</td></tr> <tr><td>7 3/4"</td><td>4 1/2"</td><td>1</td></tr> <tr><td>6"</td><td>3 3/8"</td><td>1</td></tr> <tr><td>4 1/8"</td><td>2 1/4"</td><td>1</td></tr> <tr><td>2 1/4"</td><td>1 1/4"</td><td>1</td></tr> </tbody> </table> | H | W | QTY | 18" | 17 5/8" | 1 | 18" | 16 1/2" | 1 | 18" | 15 3/8" | 1 | 18" | 14 1/4" | 1 | 18" | 13 1/4" | 1 | 18" | 12 1/8" | 1 | 18" | 11" | 1 | 17" | 9 7/8" | 1 | 15 1/4" | 8 7/8" | 1 | 13 3/8" | 7 3/4" | 1 | 11 1/2" | 6 5/8" | 1 | 9 5/8" | 5 5/8" | 1 | 7 3/4" | 4 1/2" | 1 | 6" | 3 3/8" | 1 | 4 1/8" | 2 1/4" | 1 | 2 1/4" | 1 1/4" | 1 | | | | | | | | | | | | | | | | | | |
| H | W | QTY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 17 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 16 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 15 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 14 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 13 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 12 1/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 18" | 11" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 17" | 9 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 15 1/4" | 8 7/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13 3/8" | 7 3/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 1/2" | 6 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9 5/8" | 5 5/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 3/4" | 4 1/2" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6" | 3 3/8" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 1/8" | 2 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 1/4" | 1 1/4" | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

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| BY | |
| DATE | |
| REVISIONS | |
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| | |
| BRIGHAM CITY CONNECTION PROJECT | |
| DETAIL | |
| DRAWING NO. 25 OF 63 DT-15 | |

| BRIDGE PARAPET END SECTION WITH MOMENT SLAB DT-11 REINFORCING SCHEDULE | | | | |
|--|----------|-----------|--|--|
| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH |
| P3 | #5 | AS NEEDED | CONCRETE FLATWORK TO BARRIER |  |
| H1 | #5 | AS NEEDED | HORIZONTAL IN BARRIER CONTINUOUS THROUGH LENGTH OF BARRIER |  |
| H2 | #5 | AS NEEDED | HORIZONTAL IN FOUNDATION OR MOMENT SLAB |  |
| H5 | #5 | 30 | HORIZONTAL IN MOMENT SLAB |  |
| H6 | #5 | 15 | HORIZONTAL IN CURB AND GUTTER |  |
| V1 | #5 | AS NEEDED | VERTICAL IN BARRIER |  <p>OPTION 1 TOTAL LENGTH = 6'-4"</p> <p>OPTION 2 TOTAL LENGTH = 7'-1 7/8"</p> |
| V7 | #5 | 25 | VERTICAL IN MOMENT SLAB |  <p>TOTAL LENGTH = 5'-9"</p> |

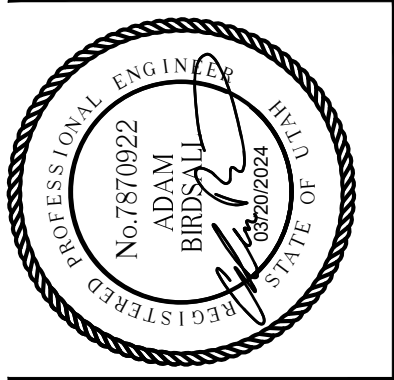
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|-----------|------|----|
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ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: IAT
 DRAWN BY: IAT
 CHECKED BY: JWP
 APPROVED BY: AJB

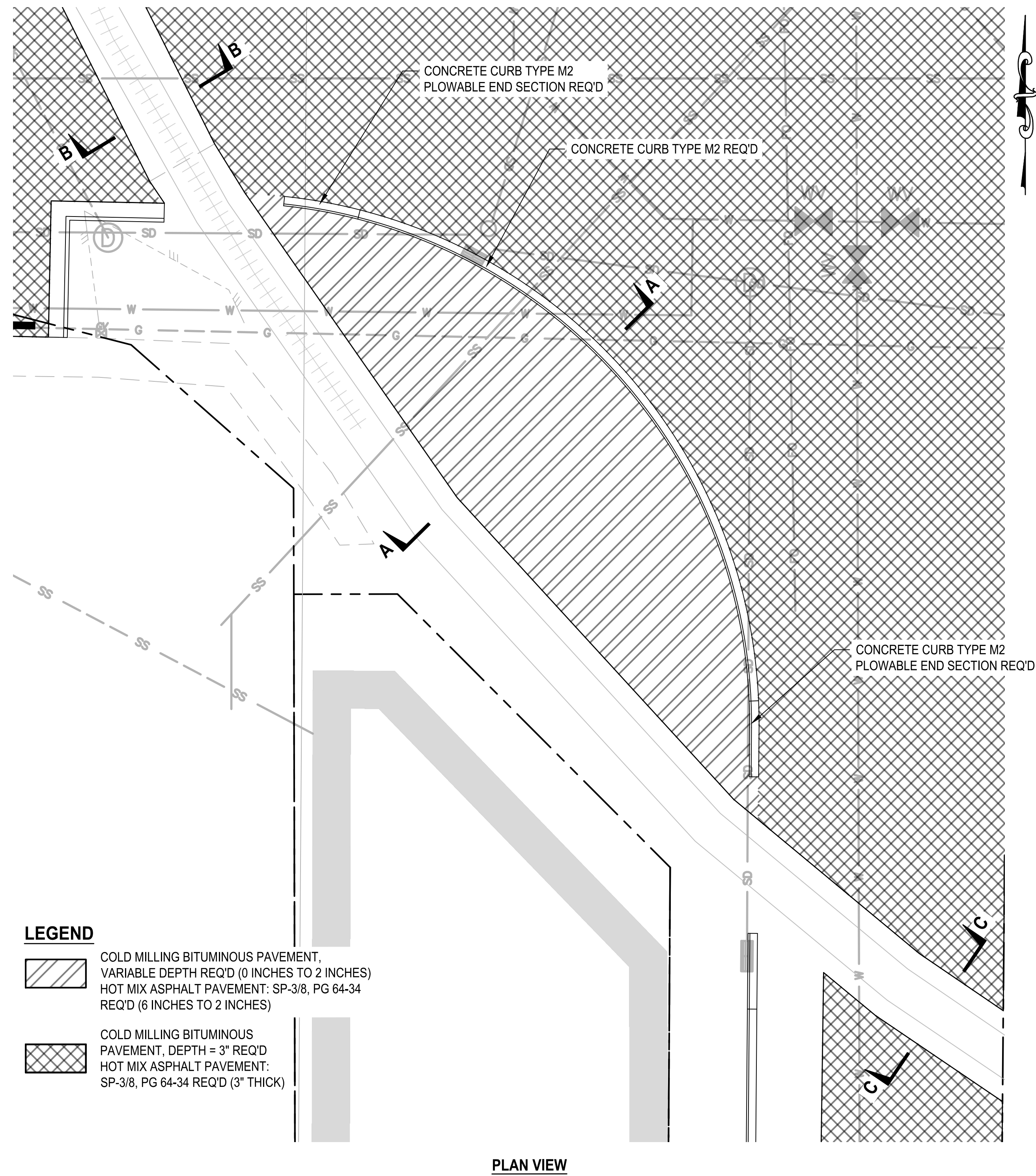
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PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

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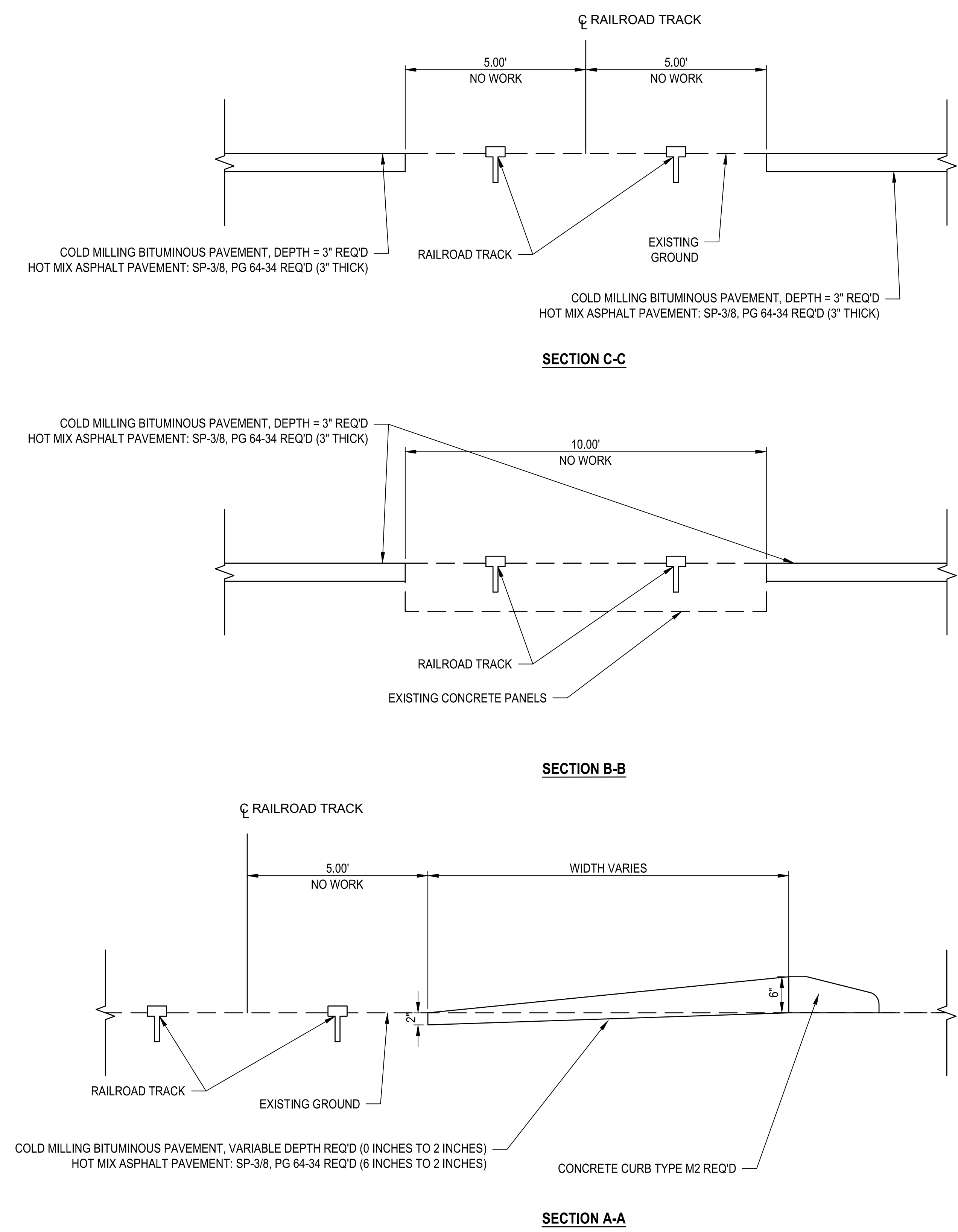


PLAN VIEW

LEGEND

- COLD MILLING BITUMINOUS PAVEMENT,
VARIABLE DEPTH REQ'D (0 INCHES TO 2 INCHES)
HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34
REQ'D (6 INCHES TO 2 INCHES)
- COLD MILLING BITUMINOUS
PAVEMENT, DEPTH = 3" REQ'D
HOT MIX ASPHALT PAVEMENT:
SP-3/8, PG 64-34 REQ'D (3" THICK)

OVERLAY DETAIL

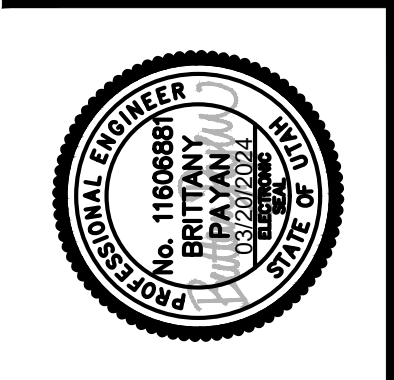


| REVISIONS | DATE | BY |
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ONE INCH
AT FULL
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NOT
SPECIFIED
OTHERWISE
ACCORDINGLY

Parametrix

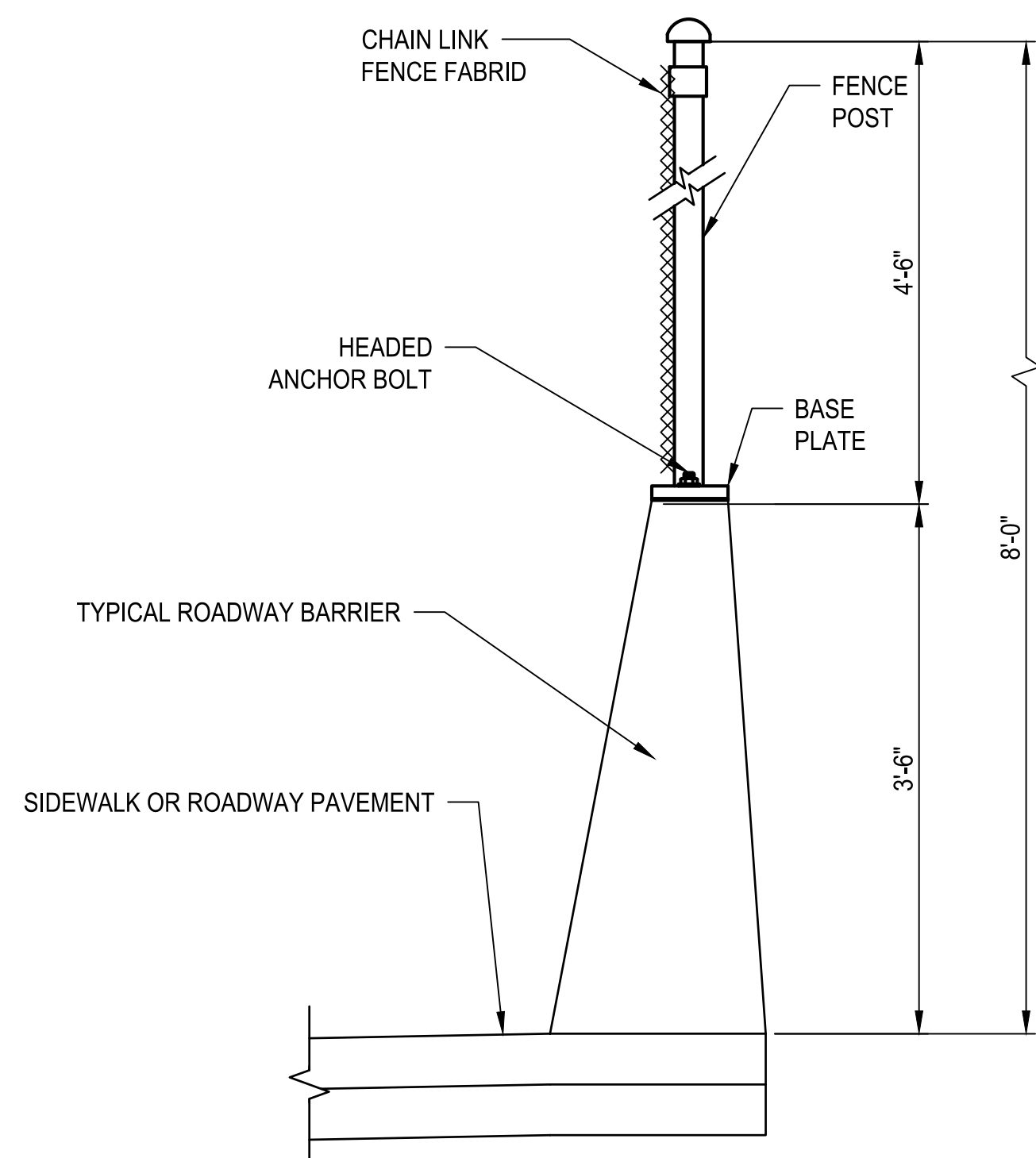
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| 03/20/2024 | BKP | CCS |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



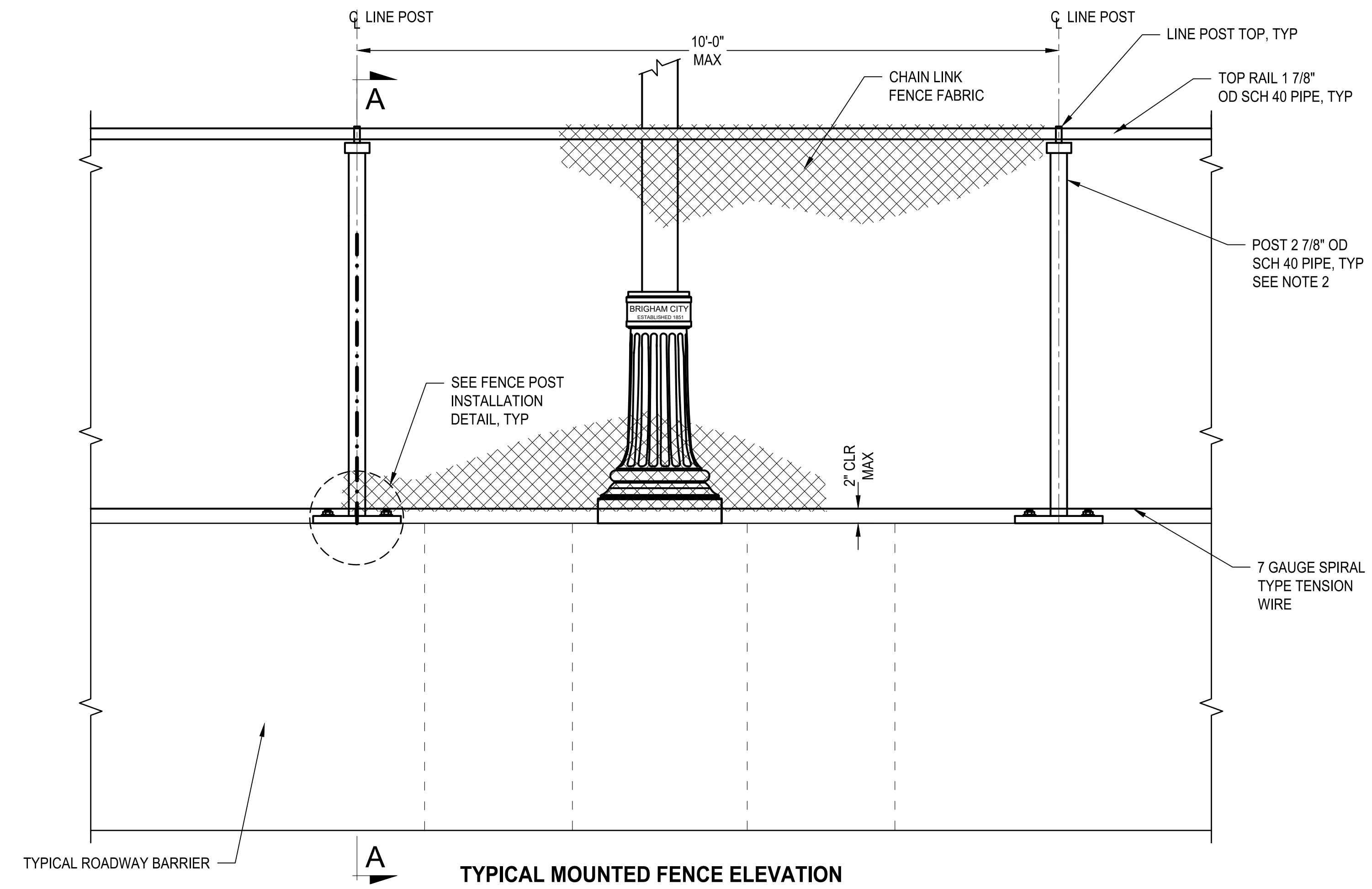
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**BRIGHAM CITY
CONNECTION PROJECT**

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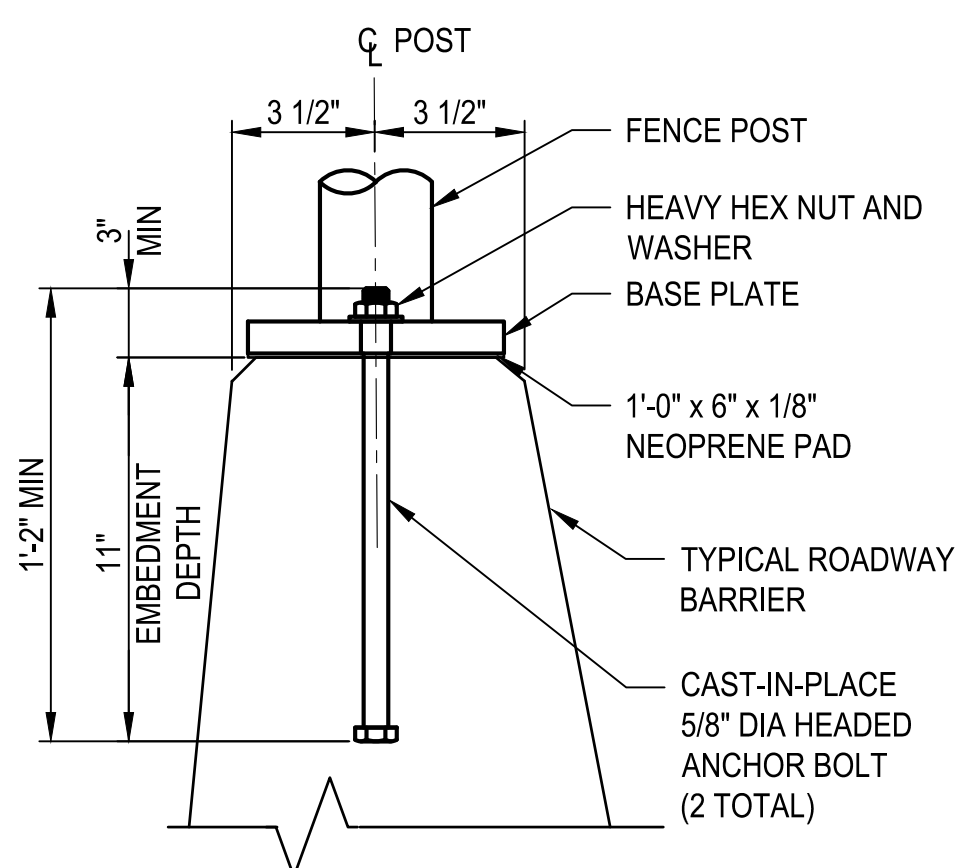
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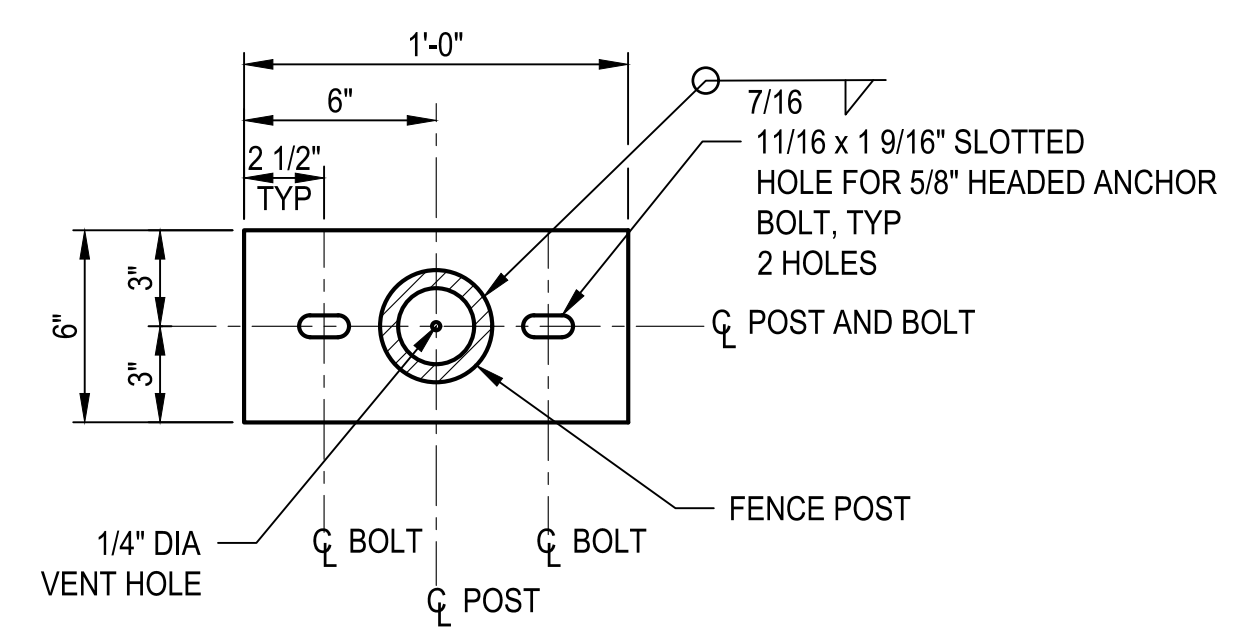
SECTION A-A



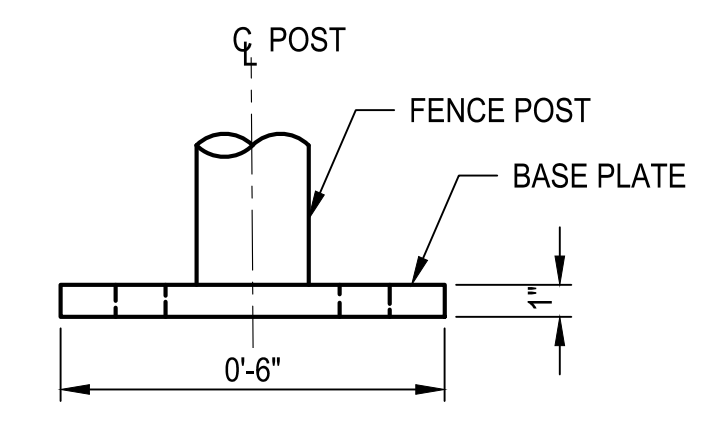
TYPICAL MOUNTED FENCE ELEVATION



FENCE POST INSTALLATION DETAIL



BASE PLATE PLAN



BASE PLATE ELEVATION

FENCING DETAIL 1 OF 2

NOTES

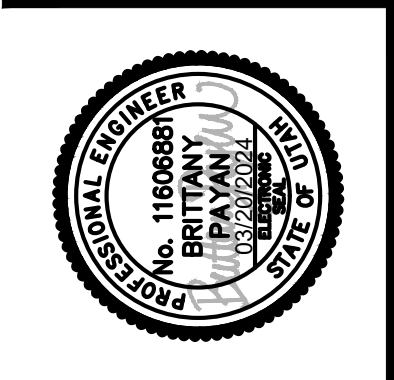
1. PLACE FENCE FABRIC ON TRAFFIC SIDE OF POST.
2. FENCE POSTS ARE PLUMB.
3. FOR DETAILS NOT SHOWN SEE UDOT STD DWG FG 6.
4. CONTRACTOR IS RESPONSIBLE FOR LOCATION AND PLUMBNESS OF ANCHOR BOLTS.
5. ADJUST ANCHOR BOLT LOCATIONS TO AVOID BARRIER REINFORCING STEEL. MAINTAIN MAXIMUM POST SPACING.
6. LOCATE TERMINAL POSTS AT DISCONTINUOUS ENDS OF FENCE AND AT LOCATIONS OF DISCRETE ANGLE CHANGE GREATER THAN 15° HORIZONTALLY OR VERTICALLY. PROVIDE BRACE PANELS ON BOTH SIDES OF TERMINAL POST AS APPLICABLE.
7. INSTALL NUTS FOR EXPANSION RAILS FINGER-TIGHT. NUTS FULLY ENGAGE BOLT WITH A MINIMUM OF ONE BOLT THREAD EXTENDING BEYOND THE NUT. DISTORT THE FIRST THREAD ON THE OUTSIDE OF THE NUT TO PREVENT LOOSENING.
8. FENCE POST CAN NOT BE INSTALLED IN FRONT OF LIGHT FIXTURE.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

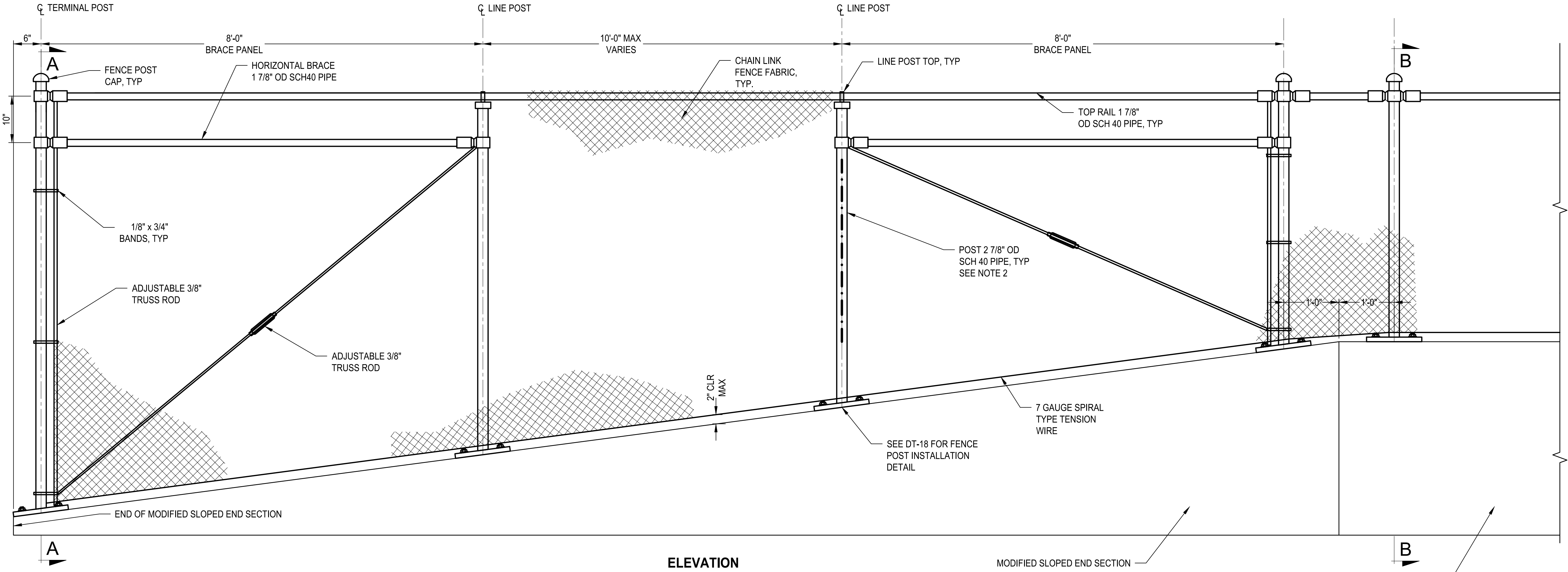
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| 03/20/2024 | BKP | AP |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



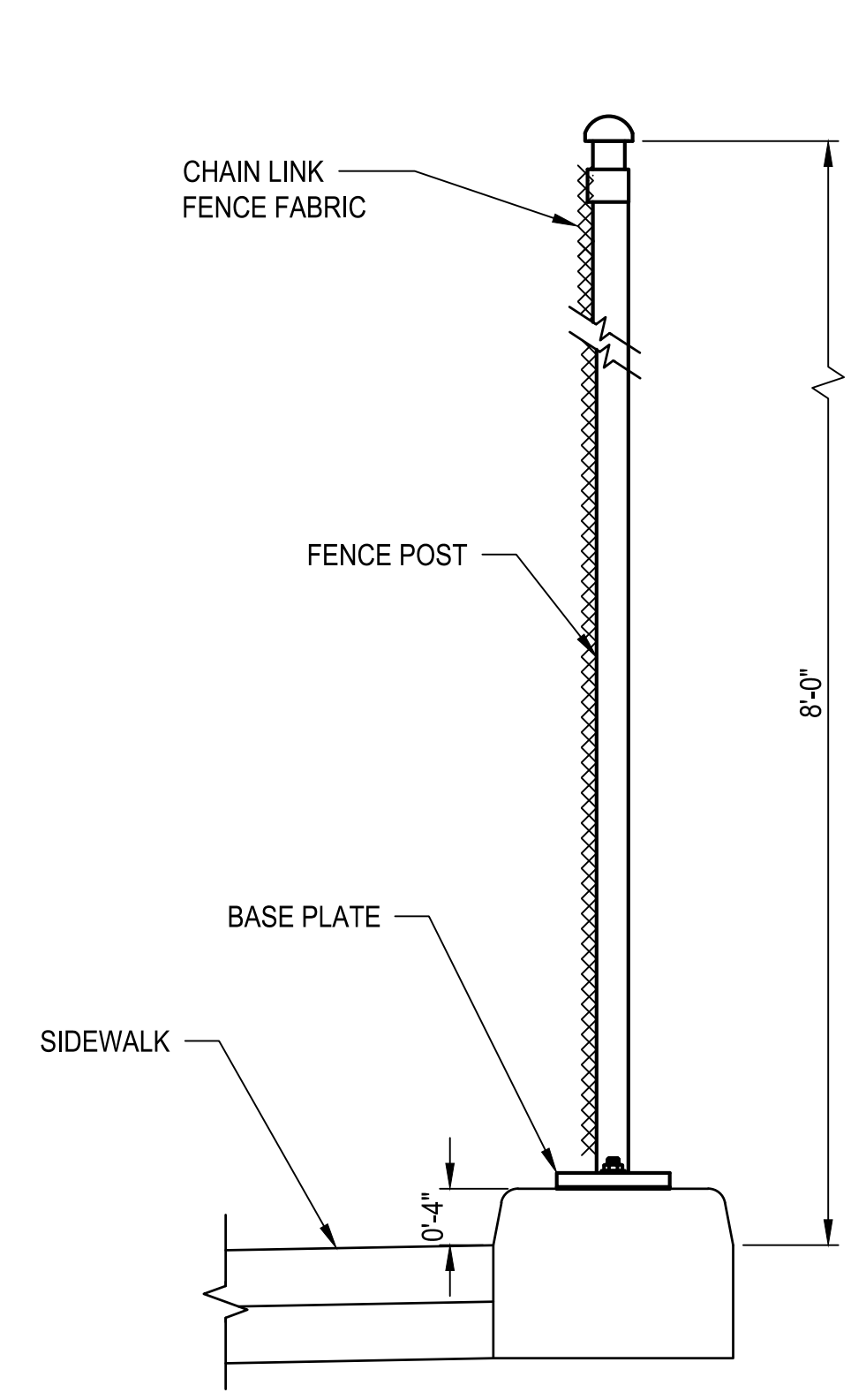
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DETAIL

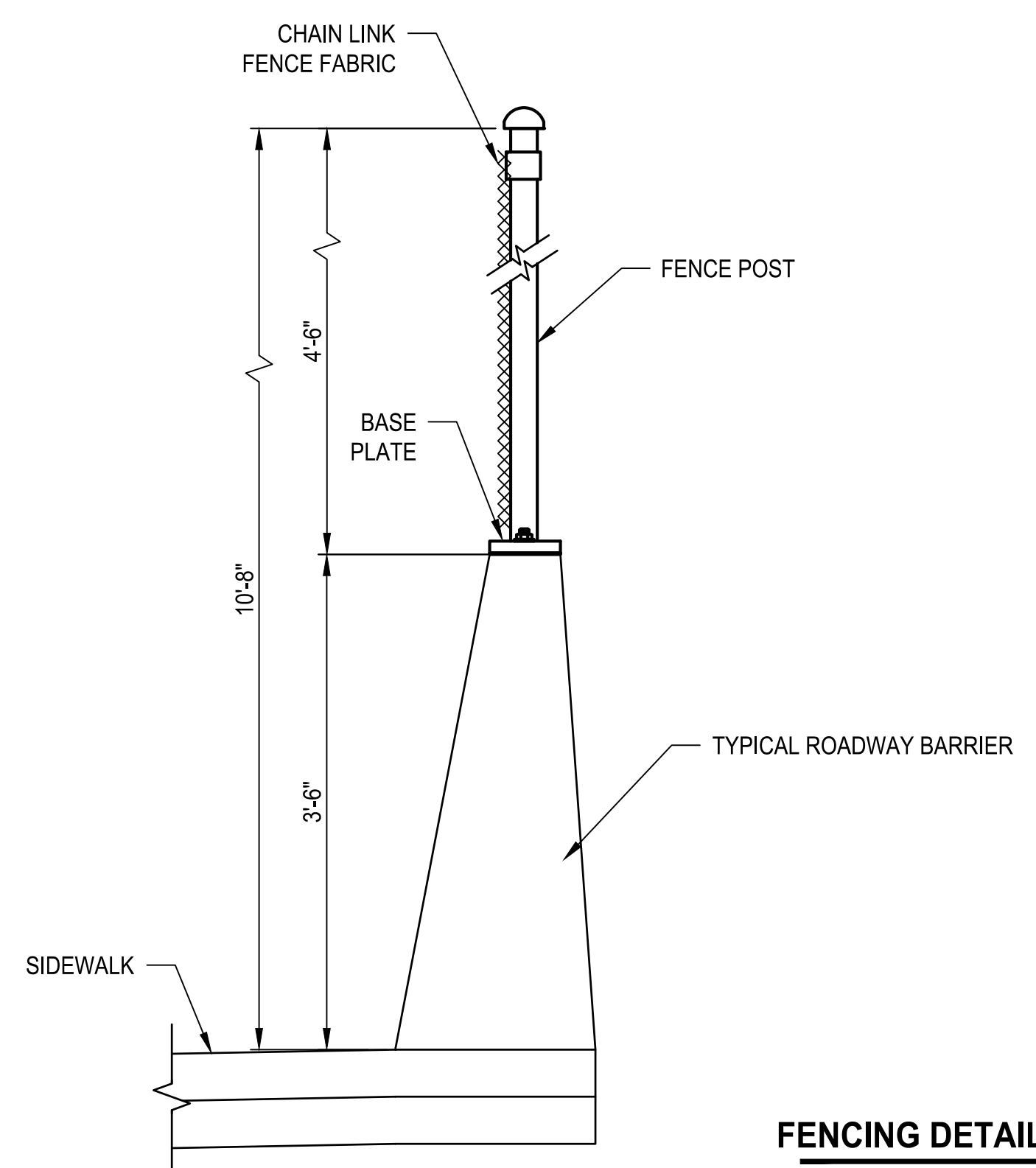
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ELEVATION



SECTION A-A



SECTION B-B

FENCING DETAIL 2 OF 2

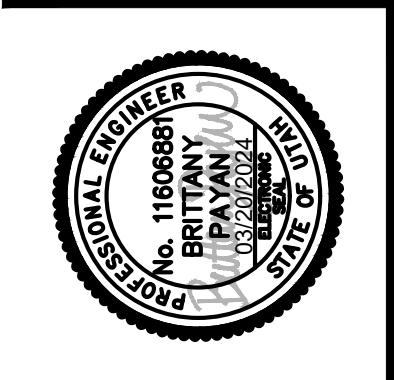
NOTES

1. PLACE FENCE FABRIC ON TRAFFIC SIDE OF POST.
2. FENCE POSTS ARE PLUMB.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATION AND PLUMBNESS OF ANCHOR BOLTS.
4. ADJUST ANCHOR BOLT LOCATIONS TO AVOID BARRIER REINFORCING STEEL. MAINTAIN MAXIMUM POST SPACING.

| REVISIONS | DATE | BY |
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ONE INCH
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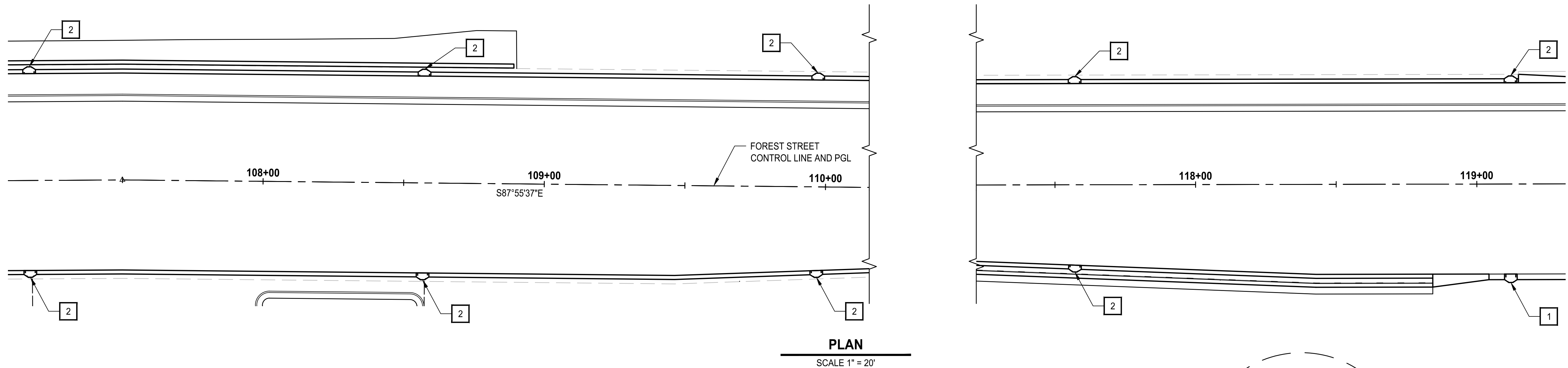
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| | JOB No. | DRAWN | APPROVED |
| | 344-8541-002 | BKP | AP |



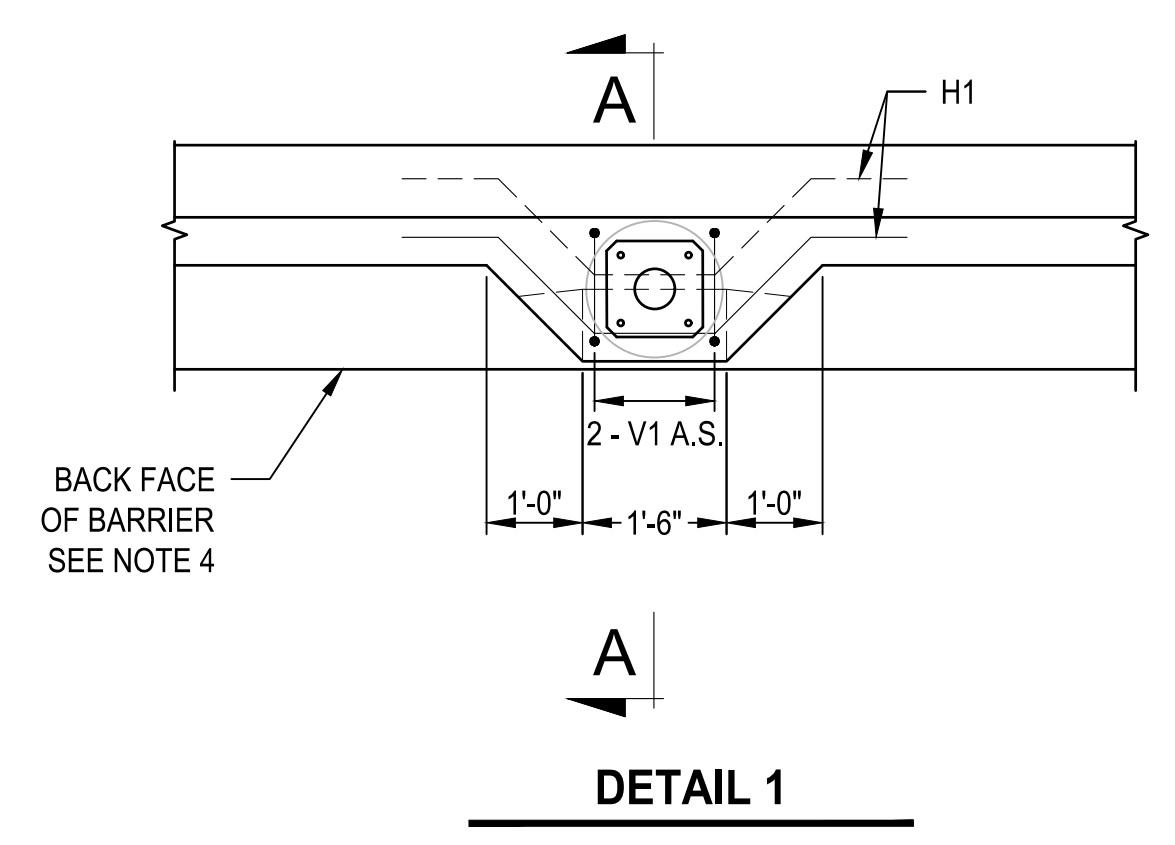
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**BRIGHAM CITY
 CONNECTION PROJECT**

DETAIL

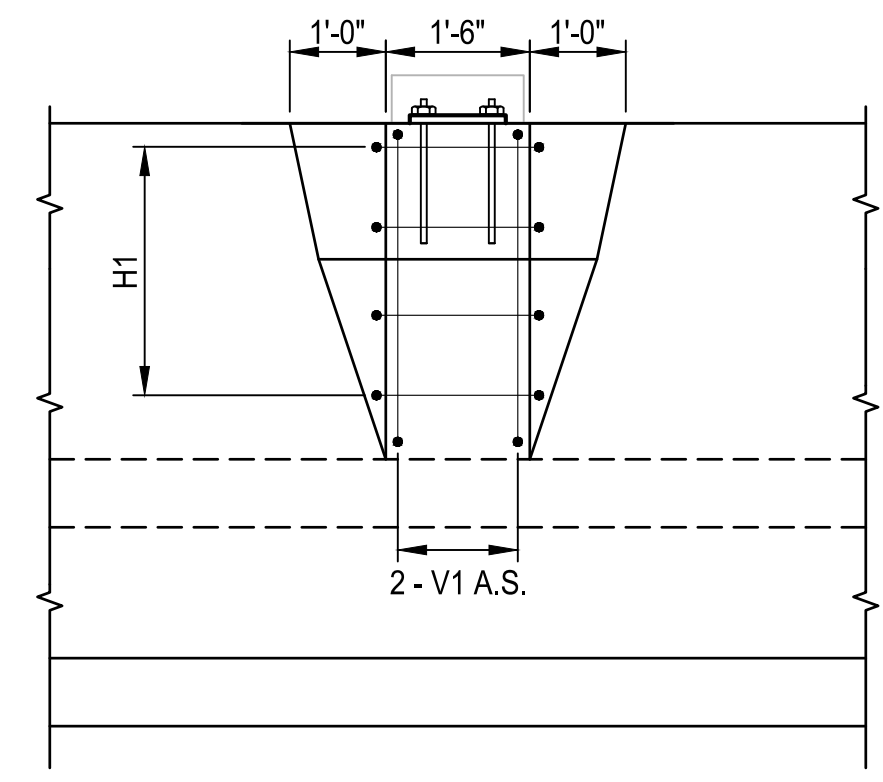
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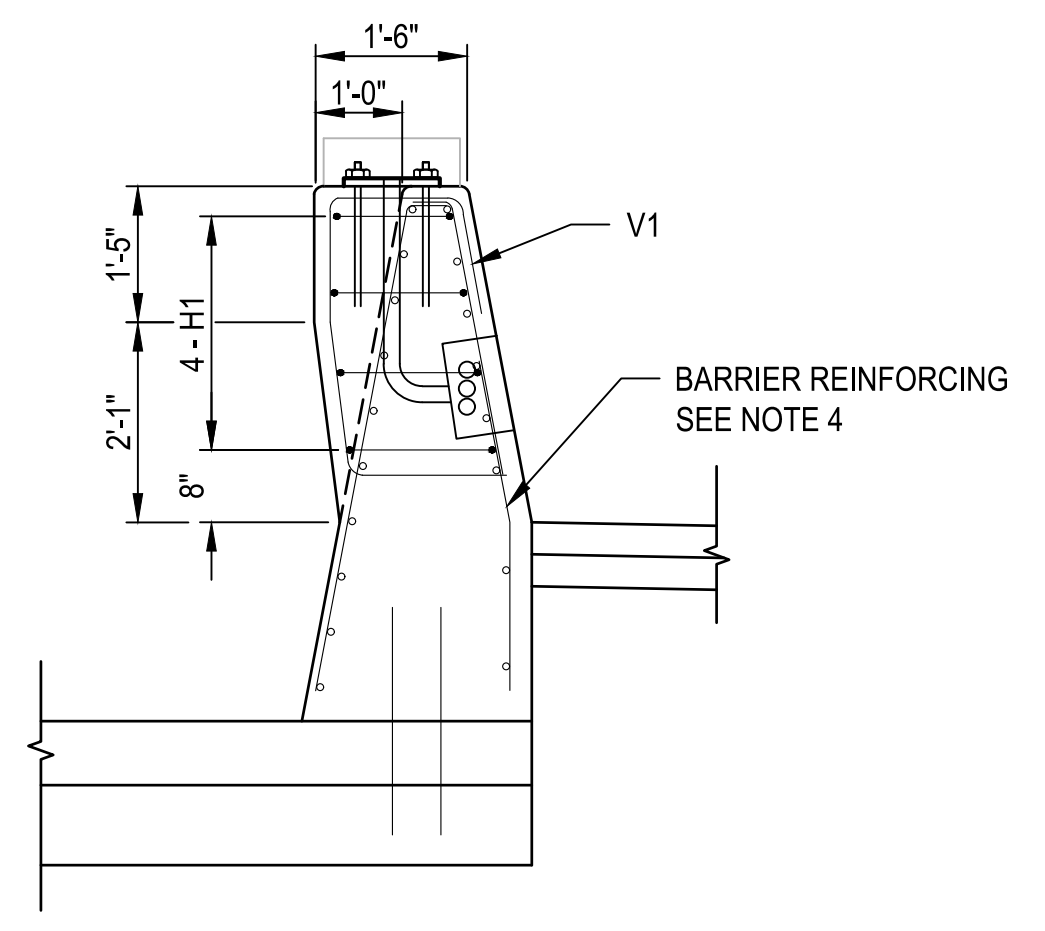
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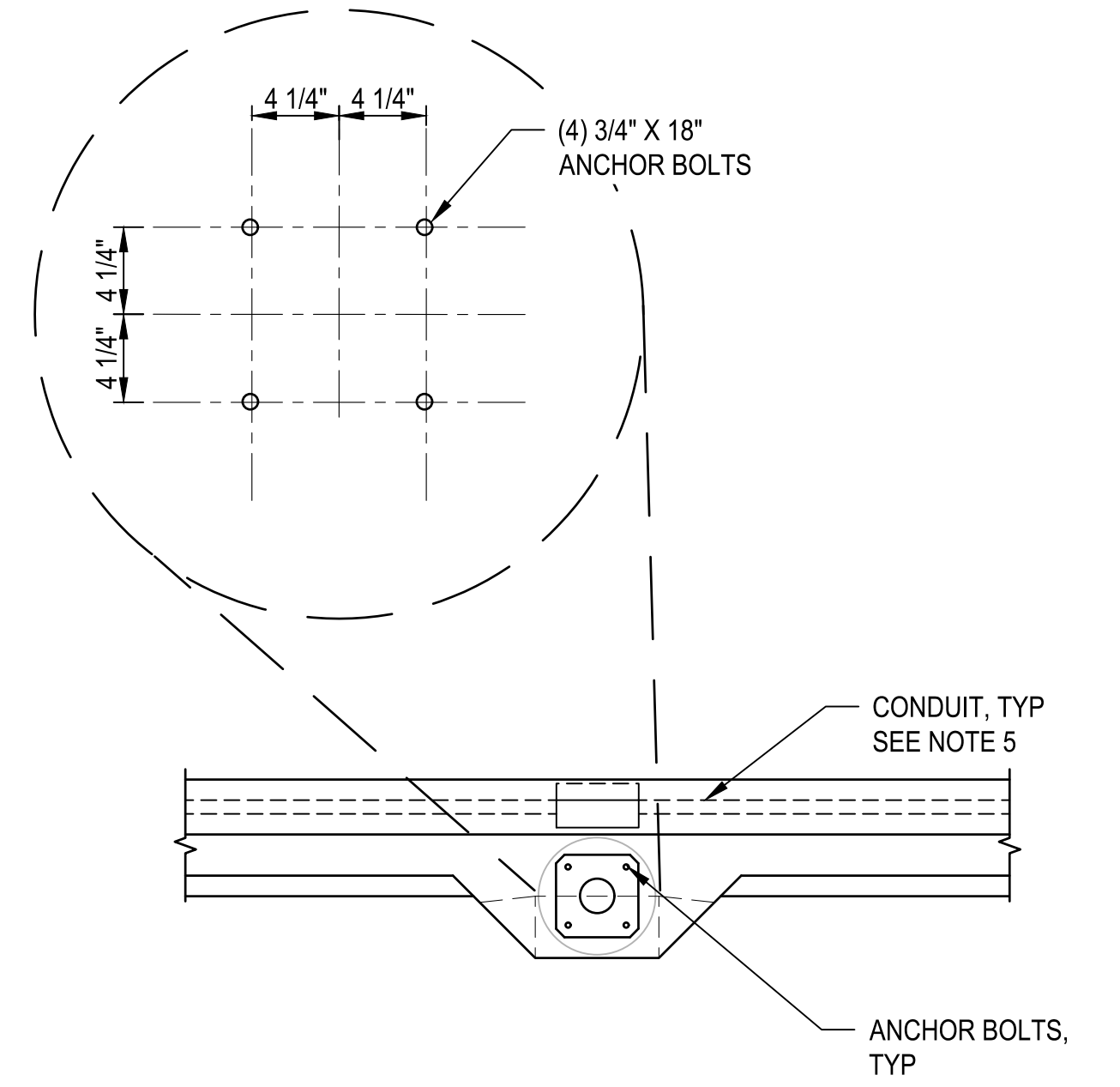
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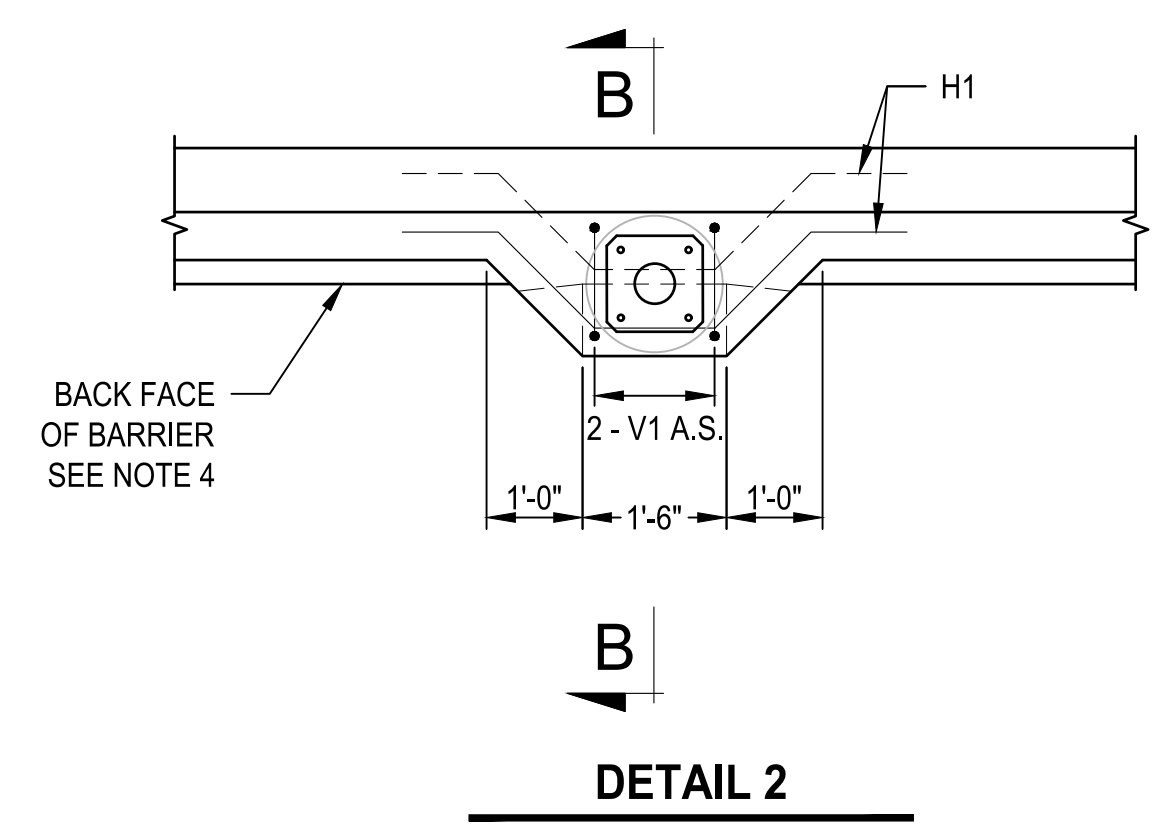
ELEVATION 1



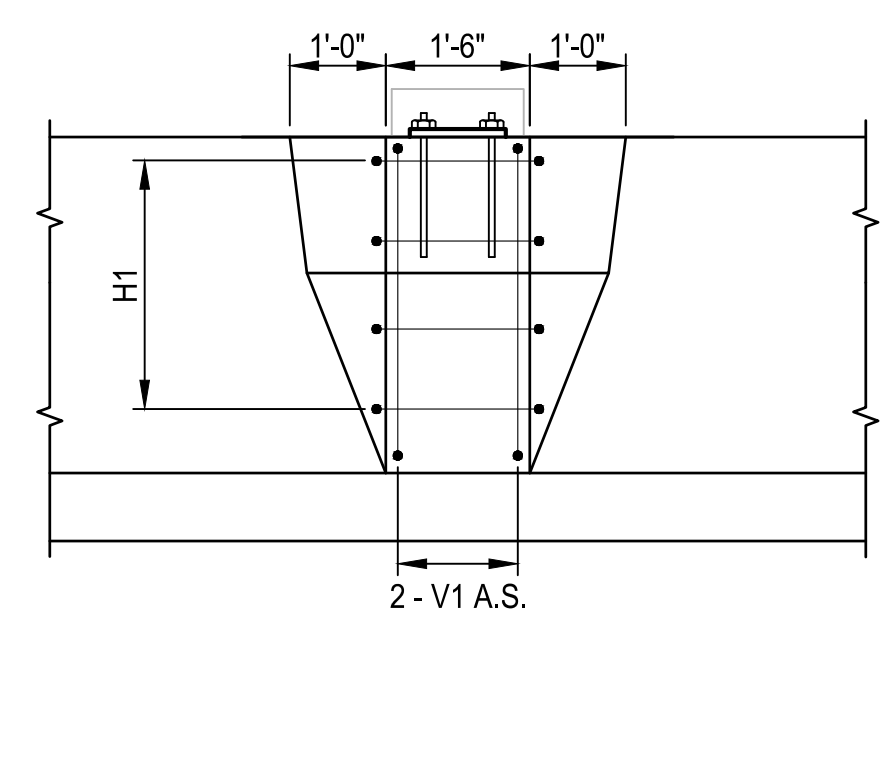
SECTION A-A



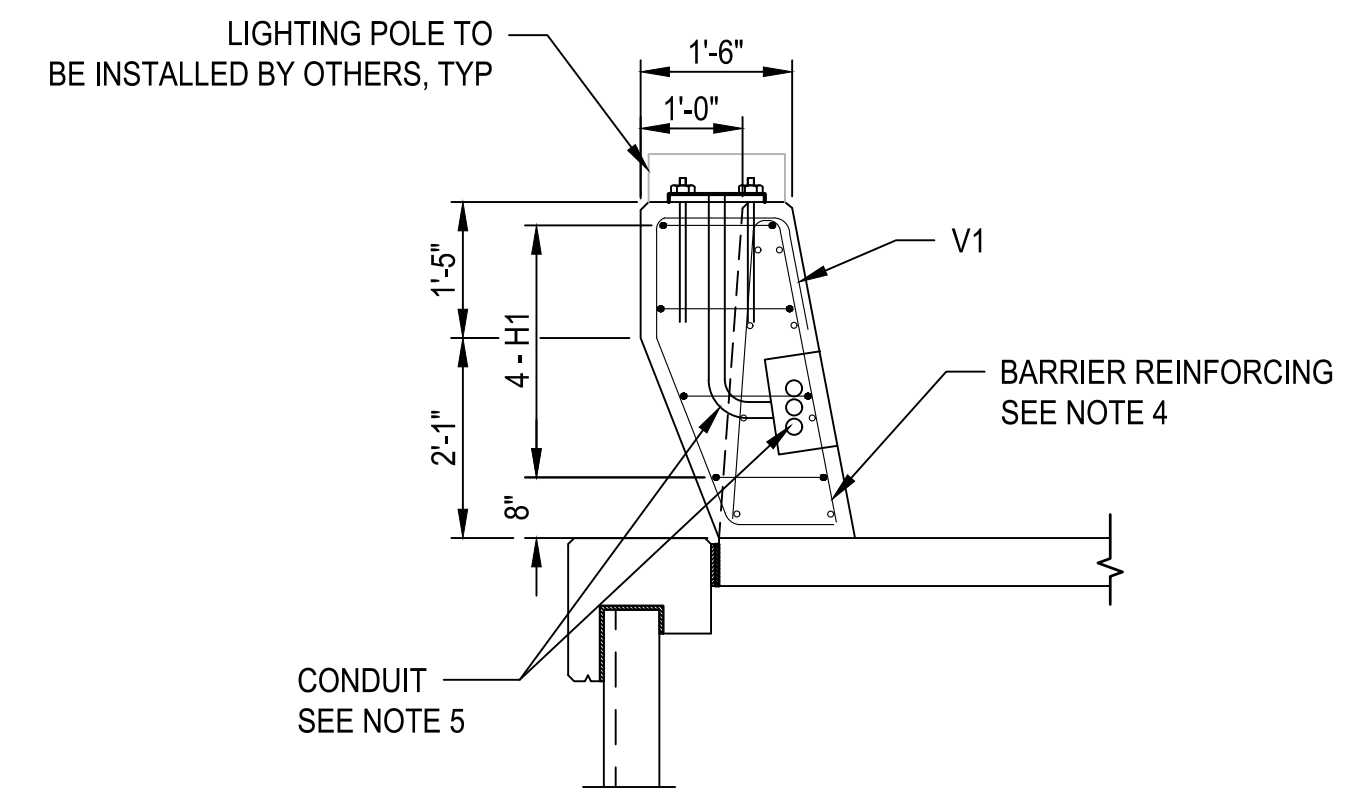
ANCHOR DETAIL



DETAIL 2



ELEVATION 2



SECTION B-B

NOTES

- ILLUMINATION FIXTURES, WIRING, AND OTHER APPURTENANCES RELATED TO LIGHTING SERVICE AND CONNECTION TO BE INSTALLED BY OTHERS.
- THE CONDUIT LAYOUT SHOWN IS SCHEMATIC. CONTRACTOR SHALL COORDINATE LAYOUT WITH BRIGHAM CITY PUBLIC POWER DIRECTOR PRIOR TO INSTALLATION.
- REFER TO APWA STANDARD PLANS AND SPECIFICATIONS FOR CONDUIT CONSTRUCTION DETAILS AND INSTALLATION REQUIREMENTS.
- SEE "DT-21" SHEET FOR BARRIER REINFORCING DETAILS.
- SEE "ELECTRICAL AND STRUCTURE NUMBER DETAILS" SHEET FOR ADDITIONAL CONDUIT AND JUNCTION BOX DETAILS.

CAST-IN-PLACE LIGHTING FOUNDATION DETAILS

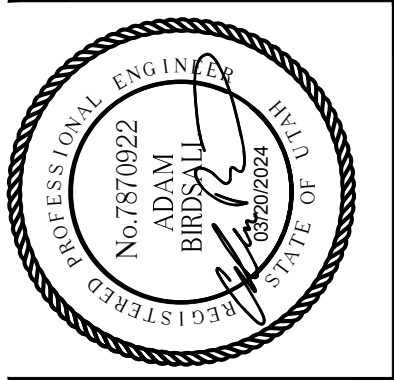
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ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED: JLB / 20/2024
 DRAWN: JAT
 CHECKED: AJB
 APPROVED: AJB

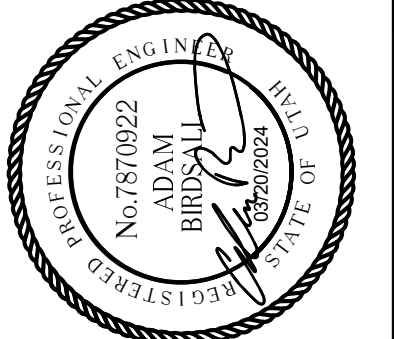
JOB No. 344-8541-002



BRIGHAM CITY CONNECTION PROJECT

DETAIL

| CAST-IN-PLACE LIGHTING FOUNDATION DT-20 REINFORCING SCHEDULE | | | | | | | | | | |
|--|----------|----------|-----------------------|--------|--|--|--|---|---|-----|
| BAR MARK | BAR SIZE | NO. BARS | LOCATION | SKETCH | | | | | | |
| | | | | | | | | <table border="1"> <thead> <tr> <th>A</th> <th>QTY</th> </tr> </thead> <tbody> <tr> <td>159°</td> <td>18</td> </tr> <tr> <td>173°</td> <td>2</td> </tr> </tbody> </table> | A | QTY |
| A | QTY | | | | | | | | | |
| 159° | 18 | | | | | | | | | |
| 173° | 2 | | | | | | | | | |
| V1 | #4 | 20 | VERTICAL IN BARRIER | | | | | | | |
| H1 | #4 | 40 | HORIZONTAL IN BARRIER | | | | | | | |



Parametrix

| | | |
|-------------------------|-----------------|-----------------|
| DATE 03/20/2024 | DESIGNED JAT | CHECKED JMP |
| JOB No. 344-8541-002 | DRAWN JAT | APPROVED AJB |

**ONE INCH
AT FULL
SCALE. IF
NOT SCALE
ACCORDINGLY**

| REVISIONS | DATE | BY |
|-----------|------|----|
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LAYOUT: SM-01 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:27:51 PM

| SUMMARY OF ITEMS | | | | |
|------------------|---|----------|----------|-----------------|
| BID ITEM NO. | BID ITEM DESCRIPTION | QUANTITY | UNIT | REFERENCE SHEET |
| MP 01 | MOBILIZATION | 1 | LUMP SUM | SM-02 |
| MP 02 | UPDES STORM WATER REGULATIONS COMPLIANCE (OVER ONE ACRE) | 1 | LUMP SUM | SM-02 |
| MP 03 | TRAFFIC CONTROL | 1 | LUMP SUM | SM-02 |
| MP 04 | SURVEY | 1 | LUMP SUM | SM-02 |
| MP 05 | REMOVE EXISTING ASPHALT | 84,152 | SQ FT | SM-03 |
| MP 06 | REMOVE EXISTING FENCE | 619 | LIN FT | SM-03 |
| MP 07 | REMOVE MAILBOX | 1 | EACH | SM-03 |
| MP 08 | REMOVE CONCRETE SIDEWALK | 10,339 | SQ FT | SM-03 |
| MP 09 | REMOVE CONCRETE DRIVEWAY | 3,251 | SQ FT | SM-03 |
| MP 10 | REMOVE CURB | 15 | LIN FT | SM-03 |
| MP 11 | REMOVE CURB AND GUTTER | 2,551 | LIN FT | SM-03 |
| MP 12 | REMOVE PIPE | 143 | LIN FT | SM-03 |
| MP 13 | REMOVE CATCH BASIN | 5 | EACH | SM-03 |
| MP 14 | BORROW | 5,444 | CU YD | SM-04 |
| MP 15 | GRANULAR BORROW | 5,153 | TON | SM-02 |
| MP 16 | ROADWAY EXCAVATION | 8,861 | CU YD | SM-04 |
| MP 17 | RELOCATE MAILBOX | 3 | EACH | SM-02 |
| MP 18 | COLD MILLING BITUMINOUS PAVEMENT, DEPTH = 3" | 3,950 | SQ YD | SM-04 |
| MP 19 | COLD MILLING BITUMINOUS PAVEMENT, VARIABLE DEPTH | 205 | SQ YD | SM-04 |
| MP 20 | UNTREATED BASE COURSE, GRADE 1 | 3,275 | TON | SM-02 |
| MP 21 | PULVERIZED PAVEMENT BASE COURSE | 785 | SQ YD | SM-02 |
| MP 22 | SEAL COAT | 14,441 | SQ YD | SM-02 |
| MP 23 | TACK COAT | 14,441 | SQ YD | SM-02 |
| MP 24 | PRIME COAT | 10,287 | SQ YD | SM-02 |
| MP 25 | HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 | 3,420 | TON | SM-02 |
| MP 26 | CONCRETE CURB AND GUTTER, CITY STANDARD | 1,911 | LIN FT | SM-05 TO SM-06 |
| MP 27 | CONCRETE CURB, TYPE P | 118 | LIN FT | SM-05 TO SM-06 |
| MP 28 | CONCRETE CURB TYPE M2 | 170 | LIN FT | SM-05 TO SM-06 |
| MP 29 | CONCRETE CURB AND GUTTER TRANSITION | 2 | EACH | SM-05 TO SM-06 |
| MP 30 | CONCRETE CURB AND GUTTER ACCESS TRANSITION | 2 | EACH | SM-05 TO SM-06 |
| MP 31 | CONCRETE CURB TYPE M2 PLOWABLE END SECTION | 3 | EACH | SM-05 TO SM-06 |
| MP 32 | 6-FT PRECAST CONCRETE PARKING STOP BLOCK | 16 | EACH | SM-05 TO SM-06 |
| MP 33 | 4" THICK CONCRETE FLATWORK | 301 | SQ FT | SM-05 TO SM-06 |
| MP 34 | 4" THICK CONCRETE SIDEWALK | 7,974 | SQ FT | SM-05 TO SM-06 |
| MP 35 | 6" THICK CONCRETE SIDEWALK | 225 | SQ FT | SM-05 TO SM-06 |
| MP 36 | DRIVEWAY APPROACH | 1,188 | SQ FT | SM-04 |
| MP 37 | DRIVEWAY, 7" THICK | 741 | SQ FT | SM-04 |
| MP 38 | CONCRETE PEDESTRIAN (ADA) ACCESS RAMP | 2 | EACH | SM-05 TO SM-06 |
| MP 39 | 8-FT BLACK CHAIN LINK FENCE, TYPE III | 96 | LIN FT | SM-07 |
| MP 40 | 6-FT BLACK CHAIN LINK FENCE, TYPE III | 26 | LIN FT | SM-07 |
| MP 41 | 4.5-FT BLACK CHAIN LINK BARRIER MOUNTED FENCE, TYPE III | 1,462 | LIN FT | SM-07 |
| MP 42 | 8-FT TO 4.5-FT BLACK CHAIN LINK BARRIER MOUNTED FENCE TRANSITION, TYPE III | 48 | LIN FT | SM-07 |
| MP 43 | 6-FT GALVANIZED CHAIN LINK FENCE, TYPE III | 414 | LIN FT | SM-07 |
| MP 44 | RIGHT-OF-WAY FENCE, TYPE B (METAL POST) | 534 | LIN FT | SM-07 |
| MP 45 | 8-FT X 4.5-FT WIDE BLACK CHAIN LINK MAN GATE, TYPE III | 1 | EACH | SM-07 |
| MP 46 | 8-FT X 8-FT WIDE BLACK CHAIN LINK MAN GATE, TYPE III | 1 | EACH | SM-07 |
| MP 47 | 6-FT X 12-FT WIDE GALVANIZED CHAIN LINK GATE, TYPE III | 2 | EACH | SM-07 |
| MP 48 | 6-FT X 16-FT WIDE GALVANIZED CHAIN LINK GATE, TYPE III | 2 | EACH | SM-07 |
| MP 49 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH STEPPED MEDIAN BARRIER | 84 | LIN FT | SM-08 |
| MP 50 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER 42 INCH | 1,202 | LIN FT | SM-08 |
| MP 51 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH IN FRONT OF RETAINING WALL BA 3K13 | 499 | LIN FT | SM-08 |
| MP 52 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH TRAILING SLOPED END SECTION | 1 | EACH | SM-08 |
| MP 53 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH TRAILING SLOPED END SECTION | 1 | EACH | SM-08 |
| MP 54 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH TO 42 INCH BRIDGE PARAPET END SECTION WITH MOMENT SLAB BA 3K14 | 2 | EACH | SM-08 |
| MP 55 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH, FULL HEIGHT END SECTION WITH MOMENT SLAB FOUNDATION BA 3K2 | 2 | EACH | SM-08 |
| MP 56 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH, LIGHTING FOUNDATION | 9 | EACH | SM-08 |
| MP 57 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE STEPPED MEDIAN BARRIER - 42 INCH LIGHTING FOUNDATION | 1 | EACH | SM-08 |
| MP 58 | MODIFIED SLOPED END SECTION (NORTHWEST END) | 1 | EACH | SM-08 |

| SUMMARY OF ITEMS | | | | |
|------------------|--|----------|----------|----------------------|
| BID ITEM NO. | BID ITEM DESCRIPTION | QUANTITY | UNIT | REFERENCE SHEET |
| MP 59 | MODIFIED SLOPED END SECTION (NORTHEAST END) | 1 | EACH | SM-08 |
| MP 60 | RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (NORTHEAST) | 1 | EACH | SM-08 |
| MP 61 | RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (SOUTHEAST) | 1 | EACH | SM-08 |
| MP 62 | CHECK DAM - FIBER ROLL | 53 | LIN FT | SM-09 |
| MP 63 | SILT FENCE | 193 | LIN FT | SM-09 |
| MP 64 | DROP-INLET BARRIER - FIBER ROLL | 456 | LIN FT | SM-09 |
| MP 65 | GRANULAR BACKFILL BORROW (PLAN QUANTITY) | 2,311 | CU YD | SEE STRUCTURE SHEETS |
| MP 66 | TEMPORARY RETAINING WALL | 1 | LUMP SUM | SEE STRUCTURE SHEETS |
| MP 67 | PILE DRIVING EQUIPMENT | 1 | LUMP SUM | SEE STRUCTURE SHEETS |
| MP 68 | DRIVEN PILES, 16 INCH | 18,558 | LIN FT | SEE STRUCTURE SHEETS |
| MP 69 | 7 FT CHAIN LINK FENCE, TYPE III | 1,126 | LIN FT | SEE STRUCTURE SHEETS |
| MP 70 | REINFORCING STEEL - UNCOATED CM (PLAN QUANTITY) | 70,660 | LBS | SEE STRUCTURE SHEETS |
| MP 71 | REINFORCING STEEL - UNCOATED CS (PLAN QUANTITY) | 182,404 | LBS | SEE STRUCTURE SHEETS |
| MP 72 | REINFORCING STEEL - COATED (PLAN QUANTITY) | 411,548 | LBS | SEE STRUCTURE SHEETS |
| MP 73 | STRUCTURAL CONCRETE | 1,390 | CU YD | SEE STRUCTURE SHEETS |
| MP 74 | STRUCTURAL CONCRETE - LOW SHRINKAGE FIBER | 1,548 | CU YD | SEE STRUCTURE SHEETS |
| MP 75 | PARTIAL DEPTH PRECAST CONCRETE DECK PANEL | 20,178 | SQ FT | SEE STRUCTURE SHEETS |
| MP 76 | THIN BONDED POLYMER OVERLAY, TYPE I | 30,402 | SQ FT | SEE STRUCTURE SHEETS |
| MP 77 | CONCRETE COATING PARAPET | 1,126 | LIN FT | SEE STRUCTURE SHEETS |
| MP 78 | STRUCTURAL STEEL | 1,872 | LBS | SEE STRUCTURE SHEETS |
| MP 79 | PRESTRESSED CONCRETE MEMBER, 109 FT 0 INCH TYPE UBT58 | 21 | EACH | SEE STRUCTURE SHEETS |
| MP 80 | PRESTRESSED CONCRETE MEMBER, 90 FT 4 INCH TYPE UBT58 | 14 | EACH | SEE STRUCTURE SHEETS |
| MP 81 | COMPRESSION SEAL JOINT (TYPE A) | 130 | LIN FT | SEE STRUCTURE SHEETS |
| MP 82 | CONCRETE COATING (PLAN QUANTITY) | 23,438 | SQ FT | SEE STRUCTURE SHEETS |
| MP 83 | ELECTRICAL WORK BRIDGES | 1 | LUMP SUM | SEE STRUCTURE SHEETS |
| MP 84 | MSE RETAINING WALL | 26,759 | SQ FT | SEE STRUCTURE SHEETS |
| MP 85 | WEED BARRIER GEOTEXTILE | 158 | SQ YD | SM-09 |
| MP 86 | CLEAR AND GRUB SITE | 1 | ACRE | SM-02 |
| MP 87 | REMOVE TREE | 31 | EACH | SM-03 |
| MP 88 | GREEN VASE ZELKOVA TREE, 2" CALIPER | 9 | EACH | SM-09 |
| MP 89 | IRRIGATION SYSTEM AND LANDSCAPE RESTORATION | 1 | LUMP SUM | SM-09 |
| MP 90 | DECORATIVE ROCK MULCH | 158 | SQ YD | SM-09 |
| MP 91 | REMOVE SIGN | 7 | EACH | SM-10 |
| MP 92 | REGULATORY SIGN, POST, AND BASE | 4 | EACH | SM-10 |
| MP 93 | WARNING SIGN, POST, AND BASE | 2 | EACH | SM-10 |
| MP 94 | SIGN RELOCATION | 2 | EACH | SM-10 |
| MP 95 | REMOVE PAVEMENT STRIPING | 1,254 | LIN FT | SM-10 |
| MP 96 | REMOVE PAVEMENT SYMBOL | 1 | EACH | SM-10 |
| MP 97 | PAVEMENT MARKING PAINT (4 INCH) | 12,475 | LIN FT | SM-11 TO SM-12 |
| MP 98 | PAVEMENT MARKING PAINT (8 INCH) | 532 | LIN FT | SM-11 TO SM-12 |
| MP 99 | PAVEMENT MARKING PAINT (12 INCH) | 245 | LIN FT | SM-11 TO SM-12 |
| MP 100 | PAVEMENT MARKING PAINT (24 INCH) | 138 | LIN FT | SM-11 TO SM-12 |
| MP 101 | PAVEMENT SYMBOL PAINT | 31 | EACH | SM-11 TO SM-12 |
| MP 102 | ROADWAY ELECTRICAL WORK | 1 | LUMP SUM | SM-02 AND LT-01 |
| MP 103 | CONNECT NEW STORM DRAIN TO EXISTING STRUCTURE | 3 | EACH | SM-13 |
| MP 104 | 15" REINFORCED CONCRETE PIPE, LEAK RESISTANT | 730 | LIN FT | SM-13 |
| MP 105 | 18" REINFORCED CONCRETE PIPE, LEAK RESISTANT | 305 | LIN FT | SM-13 |
| MP 106 | PRECAST MANHOLE - 341.2 - A | 2 | EACH | SM-13 |
| MP 107 | 30" FRAME AND COVER - 302 | 2 | EACH | SM-13 |
| MP 108 | 44" FRAME AND COVER - 303 | 1 | EACH | SM-13 |
| MP 109 | CLEANOUT BOX 331.1 - 305 | 8 | EACH | SM-13 |
| MP 110 | COLLAR COVER - 362 | 25 | EACH | SM-13 |
| MP 111 | 48" GRID GRATE AND FRAME - 310 | 8 | EACH | SM-13 |
| MP 112 | RAISE FRAME TO GRADE - 360.1 | 14 | EACH | SM-13 |
| MP 113 | PRECAST BOX - 332 | 1 | EACH | SM-13 |

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| BY | |
| DATE | |
| REVISIONS | |
| ONE INCH AT FULL SCALE IF NECESSARY | |
| Parametrix | |
| DATE: 03/20/2024 | CHECKED: CCS |
| JOB No.: 344-8541-002 | APPROVED: AP |
| DESIGNED: BKP | DRAWN: BKP |
| | |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | |
| SUMMARY | |
| DRAWING NO. 32 OF 63 | SM-01 |

| MISCELLANEOUS SUMMARY | | | | | | | | |
|-----------------------|-----------|-----------|--------------|--|-----------------|----------|-------------------------|---------|
| LINE | STATION | | MOBILIZATION | UPDES STORM WATER REGULATIONS COMPLIANCE (OVER ONE ACRE) | TRAFFIC CONTROL | SURVEY | ROADWAY ELECTRICAL WORK | REMARKS |
| | FROM | TO | LUMP SUM | LUMP SUM | LUMP SUM | LUMP SUM | LUMP SUM | |
| FOREST ST. | 105+00.00 | 125+00.00 | 1 | 1 | 1 | 1 | 1 | |
| TOTAL | | | 1 | 1 | 1 | 1 | 1 | |

| SURFACING SUMMARY | | | | | | | | | | | | | | | | |
|-------------------|-----------|-----------|--|--------|---------|-----------|-----------|------------|-----------------|-------|---------|--------------------------------|-------|---------|---------------------------------|--|
| LINE | STATION | | HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 | | | SEAL COAT | TACK COAT | PRIME COAT | BASE COURSES | | | | | | PULVERIZED PAVEMENT BASE COURSE | REMARKS |
| | | | | | | | | | GRANULAR BORROW | | | UNTREATED BASE COURSE, GRADE 1 | | | | |
| | FROM | TO | 148 LB/CU FT | | | SQ YD | SQ YD | SQ YD | 126 LB/CU FT | | | 128 LB/CU FT | | | SQ YD | |
| | | | AREA | DEPTH | TON | | | | AREA | DEPTH | TON | AREA | DEPTH | TON | | |
| | | SQ FT | IN | | | | | SQ FT | IN | | SQ FT | IN | | | | |
| FOREST ST. | 105+40.00 | 111+10.54 | 33527.2 | 5 | 1,033.8 | 3,725.2 | 3,725.2 | 3,725.2 | | | | | | | | FOREST ST. RECONSTRUCTION |
| FOREST ST. | 105+90.94 | 108+89.80 | 8256.7 | 3 | 152.7 | 917.4 | 917.4 | 917.4 | | | | | | | | STORAGE FACILITY ACCESS |
| FOREST ST. | 106+52.74 | 111+86.82 | | | | | | | | | | | | | | UTBC ADJACENT TO WEST RETAINING WALL |
| FOREST ST. | 113+79.90 | 115+10.50 | | | | | | | | | | | | 523.6 | | UNDER FOREST ST. BRIDGE |
| FOREST ST. | 113+80.70 | 115+25.44 | 5354.2 | 3 | 99.1 | 594.9 | 594.9 | | | | | | | | | EXISTING DEPOT PARKING LOT |
| FOREST ST. | 113+82.26 | 114+13.47 | 800.0 | 4 | 19.7 | 88.9 | 88.9 | 88.9 | 800.0 | 8 | 33.5 | 800.0 | 6 | 25.6 | | PROPOSED ADDITION TO DEPOT PARKING LOT |
| FOREST ST. | 113+84.92 | 114+41.08 | 4832.1 | 4 | 119.2 | 536.9 | 536.9 | 536.9 | 4832.1 | 8 | 202.3 | 4832.1 | 6 | 154.7 | | PROPOSED ADDITION TO DEPOT PARKING LOT |
| FOREST ST. | 114+99.82 | 115+68.11 | | | | | | | | | | | | | 261.3 | UNDER FOREST ST. BRIDGE |
| FOREST ST. | 114+99.82 | 115+87.58 | | | | | | | | | | 1186.3 | 4 | 25.3 | | EXISTING UPRR ACCESS |
| FOREST ST. | 115+21.69 | 119+84.66 | 28543.2 | 3 | 528.0 | 3,171.5 | 3,171.5 | | | | | | | | | UNDER FOREST ST. BRIDGE AND SOUTH OF FOREST ST. |
| FOREST ST. | 115+41.18 | 116+04.49 | 1841.5 | 6 TO 2 | 51.4 | 204.6 | 204.6 | | | | | | | | | 800 WEST INTERSECTION, SOUTHWEST CORNER TO RAILROAD TRACKS SEE DT-17 FOR DETAILS. |
| FOREST ST. | 115+57.09 | 116+37.04 | 3092.7 | 5 | 95.4 | 343.6 | 343.6 | 343.6 | 3092.7 | 12 | 194.2 | 3092.7 | 6 | 99.0 | | PROPOSED UPRR ACCESS / SECONDARY POOL ACCESS |
| FOREST ST. | 115+57.49 | 116+05.16 | | | | | | | | | | 1381.0 | 4 | 29.5 | | UTBC BETWEEN EXISTING AND PROPOSED UPRR ACCESS |
| FOREST ST. | 116+09.84 | 116+39.90 | 1643.6 | 3 | 30.4 | 182.6 | 182.6 | | | | | | | | | 800 WEST, SOUTH OF RAILROAD |
| FOREST ST. | 116+24.82 | 119+69.44 | | | | | | | | | | 2613.5 | 4 | 55.8 | | UTBC ADJACENT TO EAST RETAINING WALL |
| FOREST ST. | 116+37.04 | 118+84.27 | 1189.7 | 4 | 29.3 | 132.2 | 132.2 | 132.2 | 1189.7 | 8 | 49.8 | 1189.7 | 6 | 38.1 | | NEW PAVEMENT ADJACENT TO EAST RETAINING WALL |
| FOREST ST. | 116+73.54 | 122+63.17 | 36544.8 | 5 | 1,126.8 | 4,060.5 | 4,060.5 | 4,060.5 | 36544.8 | 12 | 2,295.0 | 36544.8 | 6 | 1,170.3 | | FOREST ST. RECONSTRUCTION |
| FOREST ST. | 119+84.40 | 120+08.00 | | | | | | | | | | 891.2 | 4 | 19.0 | | UTBC WEST OF CITY PARK ACCESS |
| FOREST ST. | 120+01.83 | 120+38.44 | 927.5 | 5 | 28.6 | 103.1 | 103.1 | 103.1 | 927.5 | 12 | 58.2 | 927.5 | 6 | 29.7 | | CITY PARK ACCESS |
| CITY POOL ACCESS | 80+29.36 | 81+50.00 | 3408.7 | 5 | 105.1 | 378.7 | 378.7 | 378.7 | 3408.7 | 12 | 214.1 | 3408.7 | 6 | 109.2 | | CITY POOL ACCESS RECONSTRUCTION |
| CITY POOL ACCESS | 81+08.57 | 81+45.32 | | | | | | | | | | 86.2 | 4 | 1.8 | | UTBC WEST OF CITY POOL ACCESS |
| TOTAL | | | | | 3,420 | 14,441 | 14,441 | 10,287 | | | 5,153 | | | 3,275 | 785 | |

| RELOCATE SUMMARY SHEET | | | | | | |
|------------------------|-----------|----------|-----------|-----------|-------------------|---------|
| LINE | FROM | | TO | | RELOCATE MAIL BOX | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | EACH | |
| FOREST ST. | 106+01.32 | 31.00 RT | 106+01.32 | 31.00 RT | 1 | |
| FOREST ST. | 107+38.01 | 29.43 RT | 107+98.00 | 139.50 RT | 1 | |
| FOREST ST. | 113+50.94 | 25.38 RT | 114+40.52 | 82.16 RT | 1 | |
| TOTAL | | | | | 3 | |

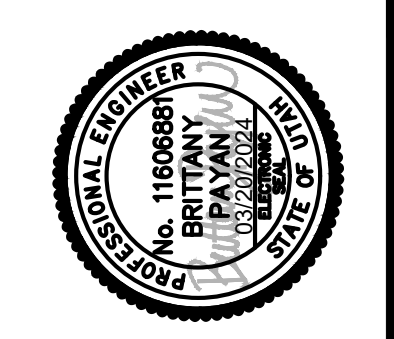
| CLEAR AND GRUB SUMMARY SHEET | | | | | | |
|------------------------------|-----------|--------|-----------|--------|---------------------|---------|
| LINE | FROM | | TO | | CLEAR AND GRUB SITE | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | ACRE | |
| FOREST ST. | 105+40.00 | LT | 122+63.17 | LT | 0.58 | |
| FOREST ST. | 105+40.00 | RT | 122+63.17 | RT | 0.10 | |
| TOTAL | | | | | 1.0 | |

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-02 PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St. Final Design\995\cvs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:27:57 PM

LAYOUT: SM-03 PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:02 PM

| REMOVAL SUMMARY SHEET | | | | | | | | | | | | | | | |
|-----------------------|-----------|-----------|-----------|-----------|-------------------------|-----------------------|-----------------|--------------------------|--------------------------|-------------|------------------------|-------------|-------------|--------------------|---------|
| LINE | FROM | | TO | | REMOVE EXISTING ASPHALT | REMOVE EXISTING FENCE | REMOVE MAIL BOX | REMOVE CONCRETE SIDEWALK | REMOVE CONCRETE DRIVEWAY | REMOVE CURB | REMOVE CURB AND GUTTER | REMOVE TREE | REMOVE PIPE | REMOVE CATCH BASIN | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | SQ FT | LIN FEET | EACH | SQ FT | SQ FT | LIN FEET | LIN FEET | EACH | LIN FEET | EACH | |
| FOREST ST. | 105+40.00 | 0.00 RT | 111+47.03 | 0.00 RT | 34299.2 | | | | | | | | | | |
| FOREST ST. | 106+45.30 | 78.00 LT | 106+99.38 | 78.00 LT | 1510.1 | | | | | | | | | | |
| FOREST ST. | 107+07.59 | 50.10 LT | 107+28.66 | 50.14 LT | 137.8 | | | | | | | | | | |
| FOREST ST. | 112+52.25 | 0.00 RT | 113+85.21 | 0.00 RT | 5955.8 | | | | | | | | | | |
| FOREST ST. | 115+89.03 | 86.53 LT | 116+09.72 | 92.43 LT | 1358.7 | | | | | | | | | | |
| FOREST ST. | 116+37.04 | 0.00 RT | 122+63.17 | 0.00 RT | 37893.6 | | | | | | | | | | |
| FOREST ST. | 121+11.58 | 96.20 RT | 121+11.84 | 68.45 RT | 108.5 | | | | | | | | | | |
| FOREST ST. | 105+92.03 | 50.58 LT | 108+90.61 | 53.35 LT | | 299.4 | | | | | | | | | |
| FOREST ST. | 109+53.78 | 39.98 RT | 111+64.89 | 33.94 RT | | 247.0 | | | | | | | | | |
| FOREST ST. | 115+65.20 | 106.21 LT | 115+79.09 | 35.56 LT | | 72.0 | | | | | | | | | |
| FOREST ST. | 111+38.61 | 24.72 RT | | | | | 1 | | | | | | | | |
| FOREST ST. | 105+40.00 | 43.36 LT | 111+84.14 | 51.58 RT | | | | 5053.2 | | | | | | | |
| FOREST ST. | 106+12.78 | 38.50 RT | 106+58.84 | 38.54 RT | | | | 183.8 | | | | | | | |
| FOREST ST. | 115+61.30 | 33.24 LT | 122+63.17 | 54.00 LT | | | | 4263.9 | | | | | | | |
| FOREST ST. | 119+85.07 | 77.34 RT | 121+07.84 | 68.89 RT | | | | 702.9 | | | | | | | |
| FOREST ST. | 120+30.76 | 32.04 LT | 120+36.63 | 52.52 LT | | | | 135.0 | | | | | | | |
| FOREST ST. | 105+57.77 | 34.56 RT | 105+78.06 | 34.35 RT | | | | | 109.7 | | | | | | |
| FOREST ST. | 106+11.68 | 34.58 RT | 106+53.86 | 34.57 RT | | | | | 231.6 | | | | | | |
| FOREST ST. | 106+14.77 | 48.50 RT | 106+58.74 | 48.50 RT | | | | | 440.4 | | | | | | |
| FOREST ST. | 107+06.02 | 35.49 LT | 107+31.18 | 35.48 LT | | | | | 139.8 | | | | | | |
| FOREST ST. | 110+22.03 | 39.75 LT | 110+30.75 | 40.06 LT | | | | | 61.6 | | | | | | |
| FOREST ST. | 111+27.28 | 40.61 LT | 111+40.57 | 40.93 LT | | | | | 79.1 | | | | | | |
| FOREST ST. | 111+43.58 | 29.69 RT | 111+63.49 | 29.20 RT | | | | | 114.5 | | | | | | |
| FOREST ST. | 115+13.60 | 33.70 LT | 115+61.25 | 33.57 LT | | | | | 1121.3 | | | | | | |
| FOREST ST. | 116+09.80 | 23.69 LT | 116+32.82 | 23.71 LT | | | | | 270.9 | | | | | | |
| FOREST ST. | 119+66.31 | 25.13 LT | 119+95.55 | 25.23 LT | | | | | 290.1 | | | | | | |
| FOREST ST. | 120+17.36 | 70.96 RT | 120+45.54 | 69.70 RT | | | | | 213.2 | | | | | | |
| FOREST ST. | 105+40.00 | 29.00 RT | 107+97.78 | 47.33 RT | | | | | | 292.2 | | | | | |
| FOREST ST. | 105+40.00 | 30.00 LT | 111+84.89 | 64.73 LT | | | | | | 689.6 | | | | | |
| FOREST ST. | 108+57.20 | 45.18 RT | 111+47.03 | 23.96 RT | | | | | | 299.8 | | | | | |
| FOREST ST. | 115+51.26 | 90.71 LT | 116+07.82 | 23.69 LT | | | | | | 121.8 | | | | | |
| FOREST ST. | 116+34.86 | 23.61 LT | 122+63.17 | 38.94 LT | | | | | | 632.6 | | | | | |
| FOREST ST. | 119+81.01 | 62.02 RT | 122+63.17 | 39.51 RT | | | | | | 281.1 | | | | | |
| FOREST ST. | 105+93.31 | 51.56 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 106+09.93 | 52.57 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 106+25.67 | 34.31 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 106+28.84 | 51.85 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 106+55.63 | 33.95 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 107+74.91 | 34.78 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 108+37.56 | 36.10 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 108+63.77 | 36.22 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 108+89.66 | 36.83 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 109+18.09 | 37.79 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 109+44.33 | 33.73 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 110+41.76 | 39.07 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 111+48.79 | 39.36 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 115+79.35 | 84.04 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 115+87.50 | 52.38 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 117+28.56 | 41.63 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 117+48.19 | 36.21 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 117+65.85 | 23.29 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 117+99.94 | 38.57 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 118+06.27 | 23.30 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 118+62.89 | 36.82 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 118+87.04 | 42.65 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 119+87.17 | 66.36 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 120+18.17 | 23.52 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 120+95.44 | 30.13 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 121+34.26 | 35.46 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 122+10.39 | 43.79 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 122+18.06 | 44.16 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 122+45.47 | 44.42 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 122+61.83 | 43.57 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 122+82.65 | 44.32 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 119+12.58 | 41.28 LT | 119+12.69 | 37.70 LT | | | | | | | 4 | | | | |
| FOREST ST. | 119+12.69 | 37.70 LT | 119+63.66 | 40.58 LT | | | | | | | 51 | | | | |
| FOREST ST. | 119+63.66 | 40.58 LT | 119+68.11 | 100.89 LT | | | | | | | 60 | | | | |
| FOREST ST. | 119+68.11 | 100.89 LT | 119+95.28 | 101.05 LT | | | | | | | 27 | | | | |
| FOREST ST. | 119+12.58 | 41.28 LT | | | | | | | | | | 1 | | | |
| FOREST ST. | 119+12.69 | 37.70 LT | | | | | | | | | | 1 | | | |
| FOREST ST. | 119+63.66 | 40.58 LT | | | | | | | | | | 1 | | | |
| FOREST ST. | 119+68.11 | 100.89 LT | | | | | | | | | | 1 | | | |
| FOREST ST. | 119+95.28 | 101.05 LT | | | | | | | | | | 1 | | | |
| CITY POOL ACCESS | 80+35.06 | 17.59 LT | 81+50.00 | 0.00 RT | 2887.4 | | | | | | | | | | |
| CITY POOL ACCESS | 80+70.08 | 27.50 LT | 81+07.57 | 27.50 LT | | | | 178.7 | | | | | | | |
| CITY POOL ACCESS | 81+07.71 | 29.99 LT | 81+15.23 | 18.75 LT | | | | | 15.0 | | | | | | |
| CITY POOL ACCESS | 80+32.57 | 3.51 LT | 81+50.00 | 13.31 RT | | | | | | 116.6 | | | | | |
| CITY POOL ACCESS | 80+37.55 | 31.63 LT | 81+50.00 | 14.87 LT | | | | | | 116.3 | | | | | |
| TOTAL | | | | | 84,152 | 619 | 1 | 10,339 | 3,251 | 15 | 2,551 | 31 | 143 | 5 | |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
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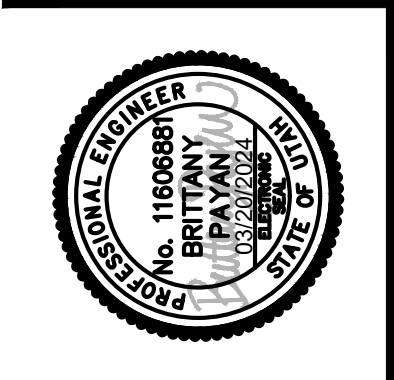
ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: BKP
 DRAWN: BKP

CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-04 PATH: U:\Sat1\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Civil PLOTTED BY: OliveStg DATE: Friday, March 22, 2024 12:28:07 PM

| EARTHWORK SUMMARY | | | | | | |
|--------------------------------|-----------|-----------|--------------------|------------------------|--------------|--------------------------------------|
| LINE | STATION | | ROADWAY EXCAVATION | NATIVE FILL MATERIAL * | BORROW | REMARKS |
| | FROM | TO | | X0.87 | | |
| | | | CU YD | CU YD | CU YD | |
| FOREST ST. | 105+40.00 | 111+43.03 | 4444.7 | 3866.8 | 8055.6 | |
| FOREST ST. | 105+90.94 | 108+89.80 | 280.3 | 243.9 | | STORAGE FACILITY ACCESS |
| FOREST ST. | 106+52.74 | 111+86.82 | 52.1 | | | UTBC EXCAVATION |
| FOREST ST. | 111+43.03 | 111+47.03 | 28.1 | 24.5 | 30.0 | WEST WALL EXCAVATION UNDER STRUCTURE |
| FOREST ST. | 113+82.26 | 114+13.47 | 44.4 | 38.7 | | PARKING LOT EXCAVATION |
| FOREST ST. | 113+84.92 | 114+41.08 | 268.5 | 233.6 | | PARKING LOT EXCAVATION |
| FOREST ST. | 115+45.54 | 116+41.03 | 14.4 | | | UTBC EXCAVATION |
| FOREST ST. | 115+57.09 | 116+02.12 | 97.7 | 85.0 | | UPRR ACCESS |
| FOREST ST. | 115+89.03 | 116+37.04 | 104.4 | 90.8 | | CITY POOL WEST ACCESS |
| FOREST ST. | 116+24.22 | 119+69.44 | 32.4 | | | UTBC EXCAVATION |
| FOREST ST. | 116+37.04 | 116+41.04 | 29.1 | 25.3 | 16.9 | EAST WALL EXCAVATION UNDER STRUCTURE |
| FOREST ST. | 116+41.04 | 122+63.17 | 3265.2 | 2840.7 | 4941.0 | |
| CITY POOL ACCESS | 80+29.36 | 81+50.00 | 198.4 | 172.6 | 22.5 | |
| CITY POOL ACCESS | 81+08.57 | 81+45.32 | 1.1 | | | UTBC EXCAVATION |
| SUB-TOTAL | | | 8,861 | | 13,066 | |
| SUB-TOTAL NATIVE FILL MATERIAL | | | | 7,622 | (7,622) | |
| TOTAL | | | 8,861 | 7,622 * | 5,444 | |

*QUANTITY FOR INFORMATION ONLY. ASSUMED 87% OF NATIVE EARTHWORK WILL BE SUITABLE FOR USE OF FILL MATERIAL.

| ROTOMILLING SUMMARY SHEET | | | | | |
|---------------------------|-----------|-----------|----------------------------------|----------------|--|
| LINE | STATION | | COLD MILLING BITUMINOUS PAVEMENT | | REMARKS |
| | FROM | TO | DEPTH = 3" | VARIABLE DEPTH | |
| | | | SQ YD | SQ YD | |
| FOREST ST. | 113+80.70 | 115+10.50 | 594.9 | | PARKING LOT |
| FOREST ST. | 115+21.69 | 119+84.66 | 3171.5 | | |
| FOREST ST. | 115+41.18 | 116+04.49 | | 204.6 | 800 WEST INTERSECTION, SOUTHWEST CORNER TO RAILROAD TRACKS |
| FOREST ST. | 116+14.95 | 116+38.98 | 182.6 | | 800 WEST, SOUTH OF RAILROAD |
| TOTAL | | | 3950 | 205 | |

| DRIVEWAY SUMMARY | | | | | | | |
|------------------|-----------|----------|-----------|----------|-------------------|--------------------|---------|
| LINE | FROM | | TO | | DRIVEWAY APPROACH | DRIVEWAY, 7" THICK | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | SQ FT | SQ FT | |
| FOREST ST. | 105+53.74 | 29.50 RT | 105+82.01 | 29.50 RT | 121.4 | | |
| FOREST ST. | 106+00.59 | 30.50 LT | 106+58.59 | 30.50 LT | 165.0 | | |
| FOREST ST. | 106+08.77 | 29.50 RT | 106+56.69 | 29.97 RT | 395.5 | | |
| FOREST ST. | 106+14.77 | 48.50 RT | 106+52.74 | 48.50 RT | | 380.0 | |
| FOREST ST. | 115+51.75 | 90.79 LT | 115+57.49 | 56.26 LT | 162.5 | | |
| FOREST ST. | 119+95.54 | 32.60 RT | 120+35.88 | 34.19 RT | 214.2 | | |
| CITY POOL ACCESS | 80+66.07 | 15.00 LT | 81+12.07 | 15.00 LT | 129.0 | | |
| CITY POOL ACCESS | 80+70.07 | 27.50 LT | 81+08.07 | 27.50 LT | | 361.0 | |
| TOTAL | | | | | 1188 | 741 | |

| | |
|---|--|
| BY | |
| DATE | |
| REVISIONS | |
| ONE INCH AT FULL SCALE IF NOT ACCORDINGLY | |
| Parametrix | CHECKED: CCS APPROVED: AP DESIGNED: BKP DRAWN: BKP DATE: 03/20/2024 JOB No.: 344-8541-002 |
| | |
| PROJECT NAME | BRIGHAM CITY CONNECTION PROJECT |
| SUMMARY | |
| DRAWING NO. | 35 OF 63 |
| SM-04 | |

LAYOUT: SM-05 PATH: U:\Sat\Projects\Clients\Brigham City\344-8541-002 Forest St Final Design\995\cs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:12 PM

CONCRETE SUMMARY SHEET (1 OF 2)

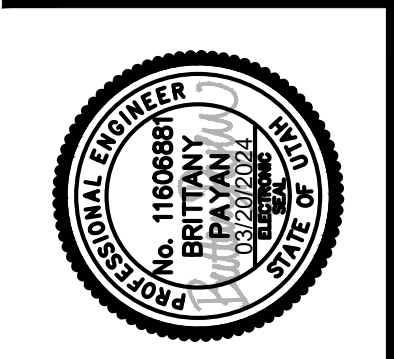
| LINE | FROM | | TO | | CONCRETE CURB AND GUTTER, CITY STANDARD | CONCRETE CURB, TYPE P | CONCRETE CURB TYPE M2 | CONCRETE CURB AND GUTTER TRANSITION | CONCRETE CURB AND GUTTER ACCESS TRANSITION | CONCRETE CURB TYPE M2 PLOWABLE END SECTION | 6-FT PRECAST CONCRETE PARKING STOP BLOCK | 4" THICK CONCRETE FLATWORK | 4" THICK CONCRETE SIDEWALK | 6" THICK CONCRETE SIDEWALK | CONCRETE PEDESTRIAN (ADA) ACCESS RAMP | REMARKS |
|---------------------------------|-----------|-----------|-----------|-----------|---|-----------------------|-----------------------|-------------------------------------|--|--|--|----------------------------|----------------------------|----------------------------|---------------------------------------|---------|
| | STATION | OFFSET | STATION | OFFSET | LIN FEET | LIN FEET | LIN FEET | EACH | EACH | EACH | EACH | SQ FT | SQ FT | SQ FT | EACH | |
| FOREST ST. | 105+40.00 | 29.00 RT | 106+52.77 | 29.00 RT | 112.8 | | | | | | | | | | | |
| FOREST ST. | 105+40.00 | 30.00 LT | 110+41.21 | 30.00 LT | 501.7 | | | | | | | | | | | |
| FOREST ST. | 106+52.77 | 29.00 RT | 106+68.30 | 30.83 RT | 15.6 | | | | | | | | | | | |
| FOREST ST. | 107+98.22 | 44.09 RT | 108+02.72 | 39.50 RT | 3.2 | | | | | | | | | | | |
| FOREST ST. | 107+98.28 | 47.32 RT | 107+98.22 | 44.09 RT | 7.2 | | | | | | | | | | | |
| FOREST ST. | 108+02.72 | 39.50 RT | 108+52.68 | 39.50 RT | 50.0 | | | | | | | | | | | |
| FOREST ST. | 108+52.68 | 39.50 RT | 108+57.18 | 43.92 RT | 7.0 | | | | | | | | | | | |
| FOREST ST. | 108+57.18 | 43.92 RT | 108+57.20 | 45.18 RT | 1.3 | | | | | | | | | | | |
| FOREST ST. | 110+41.21 | 30.00 LT | 110+91.28 | 28.00 LT | 49.8 | | | | | | | | | | | |
| FOREST ST. | 110+91.28 | 28.00 LT | 110+98.29 | 28.00 LT | 7.0 | | | | | | | | | | | |
| FOREST ST. | 115+12.49 | 57.15 RT | 115+25.44 | 57.11 RT | 16.0 | | | | | | | | | | | |
| FOREST ST. | 115+12.55 | 73.15 RT | 115+12.49 | 57.15 RT | 13.0 | | | | | | | | | | | |
| FOREST ST. | 115+51.26 | 90.71 LT | 115+59.96 | 38.41 LT | 53.0 | | | | | | | | | | | |
| FOREST ST. | 115+59.96 | 38.41 LT | 115+87.58 | 15.00 LT | 39.4 | | | | | | | | | | | |
| FOREST ST. | 115+87.58 | 15.00 LT | 115+97.67 | 15.00 LT | 10.1 | | | | | | | | | | | |
| FOREST ST. | 115+97.67 | 15.00 LT | 116+05.57 | 24.23 LT | 13.8 | | | | | | | | | | | |
| FOREST ST. | 116+30.90 | 25.99 LT | 116+37.10 | 23.05 LT | 7.1 | | | | | | | | | | | |
| FOREST ST. | 116+37.10 | 23.05 LT | 116+41.04 | 23.05 LT | 3.9 | | | | | | | | | | | |
| FOREST ST. | 116+85.79 | 28.00 LT | 119+54.46 | 28.00 LT | 268.7 | | | | | | | | | | | |
| FOREST ST. | 119+54.46 | 28.00 LT | 119+74.75 | 36.71 LT | 22.7 | | | | | | | | | | | |
| FOREST ST. | 119+95.55 | 32.10 RT | 120+68.62 | 35.07 RT | 69.1 | | | | | | | | | | | |
| FOREST ST. | 120+19.81 | 40.74 LT | 120+36.38 | 34.00 LT | 19.4 | | | | | | | | | | | |
| FOREST ST. | 120+36.38 | 34.00 LT | 120+58.26 | 34.00 LT | 23.3 | | | | | | | | | | | |
| FOREST ST. | 120+58.26 | 34.00 LT | 121+69.16 | 36.55 LT | 111.8 | | | | | | | | | | | |
| FOREST ST. | 120+68.62 | 35.07 RT | 121+61.23 | 38.91 RT | 92.6 | | | | | | | | | | | |
| FOREST ST. | 121+61.23 | 38.91 RT | 122+49.47 | 39.70 RT | 93.3 | | | | | | | | | | | |
| FOREST ST. | 121+69.16 | 36.55 LT | 122+20.62 | 38.88 LT | 48.6 | | | | | | | | | | | |
| FOREST ST. | 122+20.62 | 38.88 LT | 122+63.17 | 39.50 LT | 40.5 | | | | | | | | | | | |
| FOREST ST. | 122+49.47 | 39.70 RT | 122+63.17 | 39.70 RT | 13.7 | | | | | | | | | | | |
| FOREST ST. | 119+95.55 | 32.10 RT | 120+01.34 | 37.05 RT | | 8.3 | | | | | | | | | | |
| FOREST ST. | 120+01.34 | 37.05 RT | 120+08.59 | 77.24 RT | | 40.8 | | | | | | | | | | |
| FOREST ST. | 120+29.25 | 41.01 RT | 120+38.44 | 76.33 RT | | 36.2 | | | | | | | | | | |
| FOREST ST. | 120+35.90 | 33.69 RT | 120+29.25 | 41.01 RT | | 11.1 | | | | | | | | | | |
| FOREST ST. | 115+51.08 | 56.34 RT | 116+04.49 | 118.94 RT | | | 88.2 | | | | | | | | | |
| FOREST ST. | 116+05.25 | 159.61 RT | 116+06.59 | 241.11 RT | | | 81.5 | | | | | | | | | |
| FOREST ST. | 106+68.30 | 30.83 RT | 106+78.23 | 32.00 RT | | | | 1 | | | | | | | | |
| FOREST ST. | 119+86.02 | 32.00 RT | 119+95.55 | 32.10 RT | | | | 1 | | | | | | | | |
| FOREST ST. | 116+05.57 | 24.23 LT | 116+04.03 | 29.51 LT | | | | | 1 | | | | | | | |
| FOREST ST. | 116+29.08 | 29.73 LT | 116+30.90 | 25.99 LT | | | | | 1 | | | | | | | |
| FOREST ST. | 115+41.20 | 54.81 RT | 115+51.08 | 56.34 RT | | | | | | 1 | | | | | | |
| FOREST ST. | 116+04.49 | 118.94 RT | 116+04.74 | 128.94 RT | | | | | | 1 | | | | | | |
| FOREST ST. | 116+05.08 | 149.61 RT | 116+05.25 | 159.61 RT | | | | | | 1 | | | | | | |
| FOREST ST. | 114+37.27 | 92.67 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+37.50 | 101.66 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+37.73 | 110.66 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+37.95 | 119.65 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+38.18 | 128.65 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+38.40 | 137.64 RT | | | | | | | | | 1 | | | | | |
| SUB-TOTAL (SHEET 1 OF 2) | | | | | 1715.5 | 96.4 | 169.7 | 2 | 2 | 3 | 6 | 0.0 | 0.0 | 0.0 | 0 | |

| | | |
|-----------|----|------|
| REVISIONS | BY | DATE |
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-06 PATH: U:\Salt\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995\cs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:17 PM

CONCRETE SUMMARY SHEET (2 OF 2)

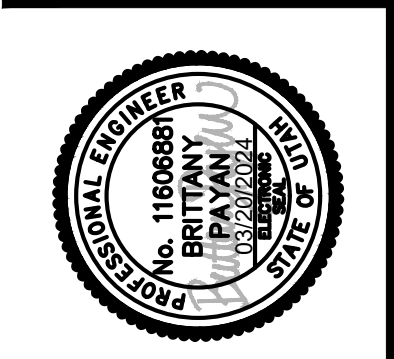
| LINE | FROM | | TO | | CONCRETE CURB AND GUTTER, CITY STANDARD | CONCRETE CURB, TYPE P | CONCRETE CURB TYPE M2 | CONCRETE CURB AND GUTTER TRANSITION | CONCRETE CURB AND GUTTER ACCESS TRANSITION | CONCRETE CURB TYPE M2 PLOWABLE END SECTION | 6-FT PRECAST CONCRETE PARKING STOP BLOCK | 4" THICK CONCRETE FLATWORK | 4" THICK CONCRETE SIDEWALK | 6" THICK CONCRETE SIDEWALK | CONCRETE PEDESTRIAN (ADA) ACCESS RAMP | REMARKS |
|---------------------------------|-----------|-----------|-----------|----------|---|-----------------------|-----------------------|-------------------------------------|--|--|--|----------------------------|----------------------------|----------------------------|---------------------------------------|---------|
| | STATION | OFFSET | STATION | OFFSET | LIN FEET | LIN FEET | LIN FEET | EACH | EACH | EACH | EACH | SQ FT | SQ FT | SQ FT | EACH | |
| FOREST ST. | 114+38.63 | 146.64 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+38.85 | 155.63 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+43.06 | 71.46 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+52.06 | 71.42 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+61.06 | 71.39 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+70.05 | 71.36 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+79.05 | 71.32 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+88.05 | 71.29 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 114+97.05 | 71.26 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 115+06.05 | 71.22 RT | | | | | | | | | 1 | | | | | |
| FOREST ST. | 119+14.87 | 40.00 LT | 119+53.44 | 40.00 LT | | | | | | | | 300.1 | | | | |
| FOREST ST. | 119+53.44 | 40.00 LT | 119+69.20 | 58.78 LT | | | | | | | | | | | | |
| FOREST ST. | 119+69.20 | 58.78 LT | 119+68.32 | 63.73 LT | | | | | | | | | | | | |
| FOREST ST. | 105+40.00 | 43.50 LT | 105+90.94 | 38.00 LT | | | | | | | | | 617.0 | | | |
| FOREST ST. | 105+90.94 | 38.00 LT | 106+04.59 | 38.00 LT | | | | | | | | | | | | |
| FOREST ST. | 106+54.59 | 38.00 LT | 106+86.88 | 38.00 LT | | | | | | | | | | | | |
| FOREST ST. | 106+86.88 | 38.00 LT | 106+86.88 | 39.42 LT | | | | | | | | | | | | |
| FOREST ST. | 106+86.88 | 39.42 LT | 110+41.36 | 39.42 LT | | | | | | | | | | | | |
| FOREST ST. | 110+41.36 | 39.42 LT | 110+91.48 | 37.42 LT | | | | | | | | | 3,321.6 | | | |
| FOREST ST. | 110+91.48 | 37.42 LT | 110+98.29 | 37.42 LT | | | | | | | | | | | | |
| FOREST ST. | 106+58.74 | 48.50 RT | 106+58.74 | 38.50 RT | | | | | | | | | 60.0 | | | |
| FOREST ST. | 113+82.44 | 85.59 RT | 113+85.82 | 85.48 RT | | | | | | | | | | | | |
| FOREST ST. | 113+85.82 | 85.48 RT | 114+00.23 | 82.27 RT | | | | | | | | | | | | |
| FOREST ST. | 114+00.23 | 82.27 RT | 114+13.50 | 79.52 RT | | | | | | | | | 220.3 | | | |
| FOREST ST. | 114+13.50 | 79.52 RT | 114+18.26 | 79.50 RT | | | | | | | | | | | | |
| FOREST ST. | 116+85.79 | 37.42 LT | 119+14.87 | 37.42 LT | | | | | | | | | | | | |
| FOREST ST. | 119+14.87 | 37.42 LT | 119+14.87 | 36.00 LT | | | | | | | | | | | | |
| FOREST ST. | 119+14.87 | 36.00 LT | 119+53.44 | 36.00 LT | | | | | | | | | | | | |
| FOREST ST. | 119+53.44 | 36.00 LT | 119+68.41 | 42.74 LT | | | | | | | | | | | | |
| FOREST ST. | 120+32.94 | 48.50 LT | 120+58.28 | 48.50 LT | | | | | | | | | | | | |
| FOREST ST. | 120+58.28 | 48.50 LT | 121+68.81 | 51.05 LT | | | | | | | | | | | | |
| FOREST ST. | 121+68.81 | 51.05 LT | 122+19.79 | 53.36 LT | | | | | | | | | | | | |
| FOREST ST. | 122+19.79 | 53.36 LT | 122+63.17 | 54.00 LT | | | | | | | | | | | | |
| FOREST ST. | 120+36.20 | 48.50 LT | 120+36.58 | 52.53 LT | | | | | | | | | | | | |
| FOREST ST. | 121+11.47 | 96.04 RT | 121+11.84 | 68.36 RT | | | | | | | | | | | | |
| FOREST ST. | 106+04.59 | 38.00 LT | 106+54.59 | 38.00 LT | | | | | | | | | | | | |
| FOREST ST. | 119+77.67 | 45.45 LT | | | | | | | | | | | | 225.0 | | |
| FOREST ST. | 120+21.47 | 45.50 LT | | | | | | | | | | | | | 1 | |
| CITY POOL ACCESS | 80+54.53 | 14.50 RT | 80+48.14 | 15.24 RT | 6.4 | | | | | | | | | | | |
| CITY POOL ACCESS | 80+56.95 | 15.50 LT | 80+64.36 | 14.50 LT | 7.5 | | | | | | | | | | | |
| CITY POOL ACCESS | 80+64.36 | 14.50 LT | 81+50.00 | 14.50 LT | 88.0 | | | | | | | | | | | |
| CITY POOL ACCESS | 81+50.00 | 14.50 RT | 80+54.53 | 14.50 RT | 93.1 | | | | | | | | | | | |
| CITY POOL ACCESS | 80+69.57 | 27.50 LT | 80+69.57 | 23.50 LT | | 4.0 | | | | | | | | | | |
| CITY POOL ACCESS | 81+08.57 | 29.90 LT | 81+08.57 | 18.50 LT | | 11.4 | | | | | | | | | | |
| CITY POOL ACCESS | 81+08.57 | 18.50 LT | 81+09.28 | 18.50 LT | | 0.7 | | | | | | | | | | |
| CITY POOL ACCESS | 81+09.28 | 18.50 LT | 81+12.78 | 15.00 LT | | 4.9 | | | | | | | | | | |
| CITY POOL ACCESS | 80+59.33 | 24.19 LT | 80+64.54 | 23.50 LT | | | | | | | | | | | | |
| CITY POOL ACCESS | 80+64.54 | 23.50 LT | 80+70.07 | 23.50 LT | | | | | | | | | 94.0 | | | |
| SUB-TOTAL (SHEET 1 OF 2) | | | | | 1715.5 | 96.4 | 169.7 | 2 | 2 | 3 | 6 | 0.0 | 0.0 | 0.0 | 0 | |
| SUB-TOTAL (SHEET 2 OF 2) | | | | | 195.0 | 21.0 | 0.0 | 0 | 0 | 0 | 10 | 300.1 | 7973.4 | 225.0 | 2 | |
| TOTAL | | | | | 1911 | 118 | 170 | 2 | 2 | 3 | 16 | 301 | 7974 | 225 | 2 | |

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|-----------|--|
| BY | |
| DATE | |
| REVISIONS | |

ONE INCH AT FULL SCALE, IF NECESSARY, ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME: **BRIGHAM CITY CONNECTION PROJECT**

SUMMARY

LAYOUT: SM-07 PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995\cs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:22 PM

FENCING AND GATE SUMMARY

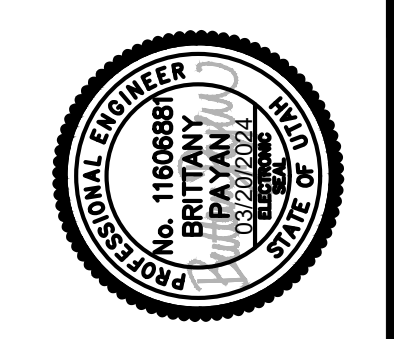
| LINE | FROM | | TO | | BLACK CHAIN LINK FENCE, TYPE III | | | | 6-FT GALVANIZED CHAIN LINK FENCE, TYPE III | RIGHT-OF-WAY FENCE, TYPE B (METAL POST) | 8-FT X 4.5-FT WIDE BLACK CHAIN LINK MAN GATE, TYPE III | 8-FT X 8-FT WIDE BLACK CHAIN LINK MAN GATE, TYPE III | 6-FT X 12-FT WIDE GALVANIZED CHAIN LINK GATE, TYPE III | 6-FT X 16-FT WIDE GALVANIZED CHAIN LINK GATE, TYPE III | REMARKS |
|--------------|-----------|-----------|-----------|-----------|----------------------------------|-----------|------------------------|---|--|---|--|--|--|--|---------|
| | STATION | OFFSET | STATION | OFFSET | 8-FT | 6-FT | 4.5-FT BARRIER MOUNTED | 8-FT TO 4.5-FT BARRIER MOUNTED TRANSITION | | | | | | | |
| | FEET | FEET | FEET | FEET | FEET | FEET | FEET | FEET | | | | | | | |
| FOREST ST. | 105+89.82 | 110.15 LT | 106+14.24 | 110.54 LT | 24.4 | | | | | | | | | | |
| FOREST ST. | 106+14.24 | 110.54 LT | 106+21.67 | 103.11 LT | 10.5 | | | | | | | | | | |
| FOREST ST. | 106+38.62 | 86.12 LT | 106+46.05 | 78.69 LT | 10.5 | | | | | | | | | | |
| FOREST ST. | 106+46.05 | 78.69 LT | 106+64.38 | 78.67 LT | 18.3 | | | | | | | | | | |
| FOREST ST. | 106+64.38 | 52.41 LT | 106+64.38 | 38.71 LT | 13.7 | | | | | | | | | | |
| FOREST ST. | 106+64.38 | 78.67 LT | 106+64.38 | 76.41 LT | 2.3 | | | | | | | | | | |
| FOREST ST. | 108+90.61 | 53.35 LT | 108+90.80 | 41.00 LT | 12.4 | | | | | | | | | | |
| FOREST ST. | 119+68.37 | 63.23 LT | 119+71.65 | 63.81 LT | 3.3 | | | | | | | | | | |
| FOREST ST. | 107+17.78 | 46.07 RT | 107+17.78 | 35.00 RT | | 11.1 | | | | | | | | | |
| FOREST ST. | 107+93.19 | 49.31 RT | 107+92.96 | 35.00 RT | | 14.3 | | | | | | | | | |
| FOREST ST. | 106+78.23 | 32.71 RT | 109+46.65 | 32.71 RT | | | 267.9 | | | | | | | | |
| FOREST ST. | 106+86.88 | 38.71 LT | 110+41.35 | 38.71 LT | | | 355.1 | | | | | | | | |
| FOREST ST. | 109+46.65 | 32.71 RT | 110+91.24 | 26.71 RT | | | 145.0 | | | | | | | | |
| FOREST ST. | 110+41.35 | 38.71 LT | 110+91.47 | 36.71 LT | | | 49.7 | | | | | | | | |
| FOREST ST. | 110+91.24 | 26.71 RT | 111+10.54 | 26.71 RT | | | 19.3 | | | | | | | | |
| FOREST ST. | 110+91.47 | 36.71 LT | 111+10.54 | 36.71 LT | | | 19.1 | | | | | | | | |
| FOREST ST. | 116+73.54 | 26.71 RT | 116+92.79 | 26.71 RT | | | 19.2 | | | | | | | | |
| FOREST ST. | 116+73.54 | 36.71 LT | 119+53.44 | 36.71 LT | | | 279.9 | | | | | | | | |
| FOREST ST. | 116+92.79 | 26.71 RT | 118+42.79 | 32.71 RT | | | 150.1 | | | | | | | | |
| FOREST ST. | 118+42.79 | 32.71 RT | 119+86.02 | 32.71 RT | | | 143.2 | | | | | | | | |
| FOREST ST. | 119+53.44 | 36.71 LT | 119+65.25 | 40.75 LT | | | 12.7 | | | | | | | | |
| FOREST ST. | 106+64.38 | 38.71 LT | 106+86.88 | 38.71 LT | | | | 22.5 | | | | | | | |
| FOREST ST. | 119+65.25 | 40.75 LT | 119+71.65 | 63.81 LT | | | | 25.5 | | | | | | | |
| FOREST ST. | 113+53.28 | 43.31 RT | 113+80.68 | 27.00 RT | | | | | 31.9 | | | | | | |
| FOREST ST. | 113+80.68 | 27.00 RT | 115+03.56 | 27.00 RT | | | | | 122.9 | | | | | | |
| FOREST ST. | 115+27.56 | 27.00 RT | 115+67.11 | 27.00 RT | | | | | 39.6 | | | | | | |
| FOREST ST. | 115+61.86 | 90.95 LT | 115+65.20 | 106.21 LT | | | | | 15.6 | | | | | | |
| FOREST ST. | 115+67.11 | 27.00 RT | 115+67.11 | 59.38 LT | | | | | 86.4 | | | | | | |
| FOREST ST. | 119+60.02 | 34.50 RT | 119+83.90 | 34.50 RT | | | | | 23.9 | | | | | | |
| FOREST ST. | 119+83.90 | 34.50 RT | 119+86.08 | 127.34 RT | | | | | 92.9 | | | | | | |
| FOREST ST. | 112+14.75 | 500.00 LT | 112+37.77 | 50.00 LT | | | | | 450.0 | | | | | | |
| FOREST ST. | 112+37.77 | 50.00 LT | 112+52.34 | 40.00 LT | | | | | 17.7 | | | | | | |
| FOREST ST. | 112+52.34 | 40.00 LT | 112+52.34 | 25.73 RT | | | | | 65.7 | | | | | | |
| FOREST ST. | 116+58.18 | 39.00 LT | | | | | | | | 1 | | | | | |
| FOREST ST. | 119+68.37 | 63.23 LT | | | | | | | | | 1 | | | | |
| FOREST ST. | 115+03.56 | 27.00 RT | | | | | | | | | | 1 | | | |
| FOREST ST. | 115+27.56 | 27.00 RT | | | | | | | | | | 1 | | | |
| FOREST ST. | 115+61.86 | 90.95 LT | | | | | | | | | | | 1 | | |
| FOREST ST. | 115+67.11 | 59.38 LT | | | | | | | | | | | | 1 | |
| TOTAL | | | | | 96 | 26 | 1,462 | 48 | 414 | 534 | 1 | 1 | 2 | 2 | |

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| REVISIONS | BY | DATE |
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ONE INCH AT FULL SCALE. IF NOT SHOWN, SCALE ACCORDINGLY.

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-08
 PATH: U:\Salt\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995\cs\CADD\DWG\Civil
 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:27 PM

BARRIER SUMMARY SHEET

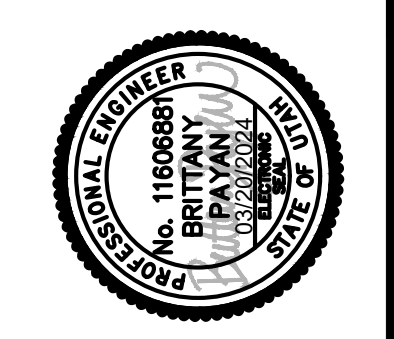
| LINE | FROM | | TO | | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH STEPPED MEDIAN BARRIER | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER 42 INCH | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH IN FRONT OF RETAINING WALL BA 3K13 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH TRAILING SLOPED END SECTION | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH TRAILING SLOPED END SECTION | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH TO 42 INCH BRIDGE PARAPET END SECTION WITH MOMENT SLAB BA 3K14 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH, FULL HEIGHT END SECTION WITH MOMENT SLAB FOUNDATION BA 3K2 | CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH, LIGHTING FOUNDATION | CAST-IN-PLACE CONCRETE CONSTANT SLOPE STEPPED MEDIAN BARRIER - 42 INCH LIGHTING FOUNDATION | MODIFIED SLOPED END SECTION (NORTHWEST END) | MODIFIED SLOPED END SECTION (NORTHEAST END) | RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (NORTHEAST) | RETAINING WALL TO RETAINING STEPPED BARRIER TRANSITION (SOUTHEAST) |
|--------------|-----------|----------|-----------|----------|--|--|---|---|--|---|--|--|--|---|---|--|--|
| | STATION | OFFSET | STATION | OFFSET | FEET | FEET | FEET | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH |
| FOREST ST. | 119+04.27 | 32.00 RT | 119+10.29 | 32.00 RT | 6.0 | | | | | | | | | | | | |
| FOREST ST. | 119+13.79 | 32.00 RT | 119+60.02 | 32.00 RT | 46.2 | | | | | | | | | | | | |
| FOREST ST. | 119+34.87 | 36.00 LT | 119+53.44 | 36.00 LT | 18.6 | | | | | | | | | | | | |
| FOREST ST. | 119+53.44 | 36.00 LT | 119+65.68 | 40.18 LT | 12.5 | | | | | | | | | | | | |
| FOREST ST. | 106+86.88 | 38.00 LT | 107+15.29 | 38.00 LT | | 28.4 | | | | | | | | | | | |
| FOREST ST. | 107+04.23 | 32.00 RT | 107+15.29 | 32.00 RT | | 11.1 | | | | | | | | | | | |
| FOREST ST. | 107+18.79 | 38.00 LT | 108+55.29 | 38.00 LT | | 137.1 | | | | | | | | | | | |
| FOREST ST. | 107+18.79 | 32.00 RT | 108+55.29 | 32.00 RT | | 136.0 | | | | | | | | | | | |
| FOREST ST. | 108+58.79 | 32.00 RT | 109+46.64 | 32.00 RT | | 87.9 | | | | | | | | | | | |
| FOREST ST. | 108+58.79 | 38.00 LT | 109+95.29 | 38.00 LT | | 136.5 | | | | | | | | | | | |
| FOREST ST. | 109+46.64 | 32.00 RT | 109+95.25 | 29.82 RT | | 48.7 | | | | | | | | | | | |
| FOREST ST. | 109+98.74 | 29.67 RT | 110+91.23 | 26.00 RT | | 92.9 | | | | | | | | | | | |
| FOREST ST. | 109+98.79 | 38.00 LT | 110+41.33 | 38.00 LT | | 42.6 | | | | | | | | | | | |
| FOREST ST. | 110+41.33 | 38.00 LT | 110+91.45 | 36.00 LT | | 49.7 | | | | | | | | | | | |
| FOREST ST. | 110+91.23 | 26.00 RT | 110+98.29 | 26.00 RT | | 7.0 | | | | | | | | | | | |
| FOREST ST. | 110+91.45 | 36.00 LT | 110+98.29 | 36.00 LT | | 6.8 | | | | | | | | | | | |
| FOREST ST. | 116+85.79 | 26.00 RT | 116+92.80 | 26.00 RT | | 7.0 | | | | | | | | | | | |
| FOREST ST. | 116+85.79 | 36.00 LT | 117+55.29 | 36.00 LT | | 69.5 | | | | | | | | | | | |
| FOREST ST. | 116+92.80 | 26.00 RT | 117+55.33 | 28.50 RT | | 62.6 | | | | | | | | | | | |
| FOREST ST. | 117+58.79 | 36.00 LT | 119+10.29 | 36.00 LT | | 151.5 | | | | | | | | | | | |
| FOREST ST. | 117+58.83 | 28.64 RT | 118+42.80 | 32.00 RT | | 84.1 | | | | | | | | | | | |
| FOREST ST. | 118+42.80 | 32.00 RT | 118+84.27 | 32.00 RT | | 41.5 | | | | | | | | | | | |
| FOREST ST. | 119+13.79 | 36.00 LT | 119+14.87 | 36.00 LT | | 1.1 | | | | | | | | | | | |
| FOREST ST. | 106+86.88 | 42.58 LT | 108+88.77 | 42.58 LT | | | 202.6 | | | | | | | | | | |
| FOREST ST. | 116+39.45 | 20.05 LT | 116+39.45 | 30.58 RT | | | 50.6 | | | | | | | | | | |
| FOREST ST. | 116+39.45 | 30.58 RT | 116+92.71 | 30.58 RT | | | 53.3 | | | | | | | | | | |
| FOREST ST. | 116+92.71 | 30.58 RT | 118+42.71 | 36.58 RT | | | 150.1 | | | | | | | | | | |
| FOREST ST. | 118+42.71 | 36.58 RT | 118+84.27 | 36.58 RT | | | 41.6 | | | | | | | | | | |
| FOREST ST. | 119+60.02 | 32.00 RT | 119+86.02 | 32.00 RT | | | | 1 | | | | | | | | | |
| FOREST ST. | 106+78.23 | 32.00 RT | 107+04.23 | 32.00 RT | | | | | 1 | | | | | | | | |
| FOREST ST. | 110+98.29 | 26.00 LT | 111+10.54 | 26.00 LT | | | | | 1 | | | | | | | | |
| FOREST ST. | 116+73.54 | 26.00 LT | 116+85.79 | 26.00 LT | | | | | 1 | | | | | | | | |
| FOREST ST. | 110+98.29 | 26.00 RT | 111+10.54 | 26.00 RT | | | | | | 1 | | | | | | | |
| FOREST ST. | 116+73.54 | 26.00 RT | 116+85.79 | 26.00 RT | | | | | | 1 | | | | | | | |
| FOREST ST. | 107+15.29 | 38.00 LT | 107+18.79 | 38.00 LT | | | | | | | 1 | | | | | | |
| FOREST ST. | 107+15.29 | 32.00 RT | 107+18.79 | 32.00 RT | | | | | | | 1 | | | | | | |
| FOREST ST. | 108+55.29 | 38.00 LT | 108+58.79 | 38.00 LT | | | | | | | 1 | | | | | | |
| FOREST ST. | 108+55.29 | 32.00 RT | 108+58.79 | 32.00 RT | | | | | | | 1 | | | | | | |
| FOREST ST. | 109+95.25 | 29.82 RT | 109+98.74 | 29.67 RT | | | | | | | 1 | | | | | | |
| FOREST ST. | 109+95.29 | 38.00 LT | 109+98.79 | 38.00 LT | | | | | | | 1 | | | | | | |
| FOREST ST. | 117+55.29 | 36.00 LT | 117+58.79 | 36.00 LT | | | | | | | 1 | | | | | | |
| FOREST ST. | 117+55.33 | 28.50 RT | 117+58.83 | 28.64 RT | | | | | | | 1 | | | | | | |
| FOREST ST. | 119+10.29 | 36.00 LT | 119+13.79 | 36.00 LT | | | | | | | 1 | | | | | | |
| FOREST ST. | 119+10.29 | 32.00 RT | 119+13.79 | 32.00 RT | | | | | | | | 1 | | | | | |
| FOREST ST. | 106+63.88 | 38.00 LT | 106+86.88 | 38.00 LT | | | | | | | | | 1 | | | | |
| FOREST ST. | 119+65.68 | 40.18 LT | 119+72.26 | 64.43 LT | | | | | | | | | | 1 | | | |
| FOREST ST. | 119+14.87 | 36.00 LT | 119+34.87 | 36.00 LT | | | | | | | | | | | 1 | | |
| FOREST ST. | 118+84.27 | 32.00 RT | 119+04.27 | 32.00 RT | | | | | | | | | | | | 1 | |
| TOTAL | | | | | 84 | 1,202 | 499 | 1 | 1 | 2 | 2 | 9 | 1 | 1 | 1 | 1 | 1 |

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| BY | |
| DATE | |
| REVISIONS | |
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ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-09 PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995\cs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:32 PM

| TEMPORARY EROSION CONTROL SUMMARY | | | | | | | | |
|-----------------------------------|-----------|-----------|-----------|-----------|------------------------|------------|---------------------------------|---------|
| LINE | FROM | | TO | | CHECK DAM - FIBER ROLL | SILT FENCE | DROP-INLET BARRIER - FIBER ROLL | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | LIN FEET | LIN FEET | LIN FEET | |
| FOREST ST. | 116+46.38 | 39.00 LT | 116+46.38 | 43.55 LT | 4.6 | | | |
| FOREST ST. | 116+55.48 | 39.00 LT | 116+55.48 | 43.57 LT | 4.6 | | | |
| FOREST ST. | 116+79.24 | 39.00 LT | 116+79.24 | 43.61 LT | 4.6 | | | |
| FOREST ST. | 117+05.99 | 39.00 LT | 117+05.99 | 43.66 LT | 4.7 | | | |
| FOREST ST. | 117+42.24 | 39.00 LT | 117+42.24 | 48.98 LT | 10.0 | | | |
| FOREST ST. | 118+26.43 | 39.00 LT | 118+26.43 | 44.36 LT | 5.4 | | | |
| FOREST ST. | 118+95.73 | 39.00 LT | 118+95.73 | 53.69 LT | 14.7 | | | |
| FOREST ST. | 119+22.23 | 40.00 LT | 119+22.23 | 43.70 LT | 3.7 | | | |
| FOREST ST. | 105+39.93 | 34.99 RT | 105+54.26 | 35.19 RT | | 14.3 | | |
| FOREST ST. | 105+40.33 | 50.00 LT | 105+80.99 | 50.00 LT | | 40.7 | | |
| FOREST ST. | 105+80.44 | 36.05 RT | 106+09.99 | 37.61 RT | | 29.6 | | |
| FOREST ST. | 105+80.99 | 50.00 LT | 105+79.82 | 122.07 LT | | 72.1 | | |
| FOREST ST. | 106+58.74 | 49.52 RT | 106+80.85 | 49.86 RT | | 22.1 | | |
| FOREST ST. | 106+80.85 | 49.86 RT | 106+89.10 | 39.00 RT | | 13.6 | | |
| FOREST ST. | 106+06.46 | 26.06 RT | | | | | 26.0 | |
| FOREST ST. | 106+33.97 | 67.66 LT | | | | | 26.0 | |
| FOREST ST. | 107+14.83 | 67.10 LT | | | | | 22.0 | |
| FOREST ST. | 108+01.31 | 48.94 RT | | | | | 30.0 | |
| FOREST ST. | 108+54.14 | 47.80 RT | | | | | 30.0 | |
| FOREST ST. | 111+17.53 | 26.92 LT | | | | | 17.3 | CB 1-1 |
| FOREST ST. | 111+17.53 | 24.92 RT | | | | | 17.3 | CB 1-2 |
| FOREST ST. | 111+72.58 | 63.26 LT | | | | | 30.0 | |
| FOREST ST. | 111+74.26 | 44.08 RT | | | | | 30.0 | |
| FOREST ST. | 112+32.07 | 57.87 LT | | | | | 17.0 | |
| FOREST ST. | 115+67.38 | 59.20 RT | | | | | 30.0 | |
| FOREST ST. | 116+04.77 | 152.64 RT | | | | | 17.0 | |
| FOREST ST. | 116+66.53 | 24.92 RT | | | | | 17.3 | CB 2-2 |
| FOREST ST. | 116+66.54 | 26.92 LT | | | | | 17.3 | CB 2-1 |
| FOREST ST. | 119+39.90 | 32.75 RT | | | | | 17.3 | CB 2-4 |
| FOREST ST. | 119+39.94 | 26.75 LT | | | | | 17.3 | CB 2-3 |
| FOREST ST. | 119+75.19 | 30.85 LT | | | | | 24.7 | MH 2-4 |
| FOREST ST. | 120+09.71 | 75.46 RT | | | | | 17.3 | CB 2-5 |
| FOREST ST. | 120+69.80 | 33.92 RT | | | | | 17.3 | CB 2-6 |
| CITY POOL ACCESS | 81+08.95 | 13.08 LT | | | | | 17.3 | CB 2-7 |
| CITY POOL ACCESS | 81+24.71 | 13.08 RT | | | | | 17.3 | CB 2-8 |
| TOTAL | | | | | 53 | 193 | 456 | |

| LANDSCAPING SUMMARY SHEET | | | | | | | | | |
|---------------------------|-----------|----------|-----------|----------|-------------------------|-----------------------|-------------------------------------|--|---------|
| LINE | FROM | | TO | | WEED BARRIER GEOTEXTILE | DECORATIVE ROCK MULCH | GREEN VASE ZELKOVA TREE, 2" CALIPER | *IRRIGATION SYSTEM AND LANDSCAPE RESTORATION | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | SQ YD | SQ YD | EACH | *SQ YD | |
| FOREST ST. | 107+17.78 | 35.00 RT | 108+57.70 | 35.00 RT | 157.8 | 157.8 | | | |
| FOREST ST. | 120+54.22 | 38.50 LT | | | | | 1 | | |
| FOREST ST. | 120+93.33 | 38.96 LT | | | | | 1 | | |
| FOREST ST. | 121+33.67 | 39.63 LT | | | | | 1 | | |
| FOREST ST. | 121+73.98 | 40.53 LT | | | | | 1 | | |
| FOREST ST. | 122+10.54 | 46.03 RT | | | | | 1 | | |
| FOREST ST. | 122+18.14 | 42.76 LT | | | | | 1 | | |
| FOREST ST. | 122+45.47 | 44.50 RT | | | | | 1 | | |
| FOREST ST. | 122+74.76 | 43.50 LT | | | | | 1 | | |
| FOREST ST. | 123+05.14 | 44.50 RT | | | | | 1 | | |
| FOREST ST. | 115+51.75 | LT | 116+58.18 | LT | | | | 545.5 | |
| FOREST ST. | 120+20.12 | LT | 122+63.17 | LT | | | | 596.3 | |
| FOREST ST. | 120+29.78 | RT | 122+63.17 | RT | | | | 418.1 | |
| CITY POOL ACCESS | 80+48.25 | RT | 81+50.00 | RT | | | | 142.6 | |
| TOTAL | | | | | 158 | 158 | 9 | LUMP SUM | |

*FOR CONTRACTOR INFORMATION ONLY

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| BY | |
| DATE | |
| REVISIONS | |

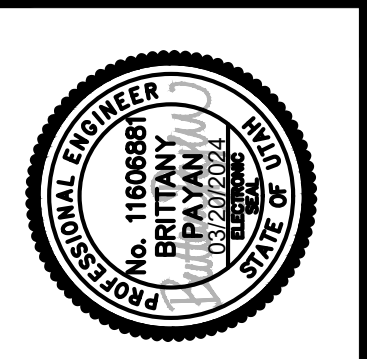
ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: BKP
 DRAWN: BKP

CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-10 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:37 PM

| SIGN SCHEDULE SUMMARY | | | | | | | | | | | | | | | | | | |
|-----------------------|----------|-----------|----------|-----------|----------|---------------------------------------|-----------|-----------|---------|---------|----------------------------|---------------------|------------|---------------------------------|----------|----|---|---------|
| LINE | SIGN NO. | FROM | | TO | | SIGN DESCRIPTION | SIGN CODE | SIGN SIZE | | | BRIGHAM CITY STANDARD POST | SLIP BASE SIGN BASE | SIGN COLOR | REGULATORY SIGN, POST, AND BASE | | | | REMARKS |
| | | STATION | OFFSET | STATION | OFFSET | | | 24 X 18 | 30 X 36 | 36 X 36 | | | | EA | EA | EA | EA | |
| FOREST ST. | 1.1 | 105+30.00 | 34.00 LT | | | R2-1; SPEED LIMIT 40 | N | | | | | W/BK | 1 | | | | | |
| FOREST ST. | 1.2 | 105+30.00 | 33.00 RT | | | R2-1; SPEED LIMIT 35 | N | | | | | W/BK | 1 | | | | | |
| FOREST ST. | 1.3 | 109+77.75 | 28.33 RT | | | W10-1; GRADE CROSSING ADVANCE WARNING | X | | | | | Y/BK | | | | 1 | | |
| FOREST ST. | 1.4 | 109+86.78 | 37.84 LT | | | R2-1; SPEED LIMIT 40 | X | | | | | W/BK | | | | 1 | | |
| FOREST ST. | 1.5 | 110+19.08 | 25.93 RT | | | R2-1; SPEED LIMIT 35 | X | | | | | W/BK | | | | 1 | | |
| FOREST ST. | 2.1 | 118+24.66 | 21.63 LT | | | W10-1; GRADE CROSSING ADVANCE WARNING | X | | | | | Y/BK | | | | 1 | | |
| FOREST ST. | 2.2 | 119+13.52 | 63.63 RT | | | R2-1; SPEED LIMIT 35 | X | | | | | W/BK | | | | 1 | | |
| FOREST ST. | 2.3 | 119+62.50 | 35.19 LT | 119+78.67 | 54.74 LT | R1-1; STOP | R | | | | | W/R | | | 1 | | SIGN BASE TO BE INSTALLED ATOP NEW SIDEWALK. | |
| FOREST ST. | 2.4 | 119+96.66 | 91.96 LT | 120+09.31 | 94.25 LT | W13-1; ADVISORY SPEED (PLAQUE) | R | | | | | Y/BK | | | 1 | | | |
| FOREST ST. | 2.5 | 121+67.37 | 40.51 LT | | | R3-7R; RIGHT LANE MUST TURN RIGHT | N | | | | | W/BK | 1 | | | | | |
| FOREST ST. | 2.6 | 121+87.39 | 40.95 LT | | | W4-2R; LANE ENDS (RIGHT) | X | | | | | Y/BK | | | | 1 | | |
| FOREST ST. | 2.7 | 123+93.24 | 44.33 LT | | | W9-1R; (RIGHT) LANE ENDS | X | | | | | Y/BK | | | | 1 | SIGN BASE AND SIGN POST TO REMAIN PROTECTED IN PLACE. | |
| FOREST ST. | 2.8 | 123+93.24 | 44.33 LT | | | R3-7R; RIGHT LANE MUST TURN RIGHT | N | | | | | W/BK | 1 | | | | INSTALL ON EXISTING POST AND BASE FROM SIGN 2.7. | |
| FOREST ST. | 2.9 | | | | | W9-1R; (RIGHT) LANE ENDS | N | | | | | Y/BK | | 1 | | | SEE SS-02 FOR SIGN LOCATION. | |
| FOREST ST. | 2.10 | | | | | W16-2P; (600) FEET | N | | | | | Y/BK | | 1 | | | INSTALL ON SAME POST AS 2.9. | |
| TOTAL | | | | | | | | | | | | 4 | 2 | 2 | 7 | | | |

SIGNING KEY

| | |
|-------------------|------------------|
| SIGN COLOR | SIGN CODE |
| W = WHITE | R = RELOCATION |
| BK = BLACK | N = NEW SIGN |
| Y = YELLOW | X = REMOVE SIGN |
| R = RED | |

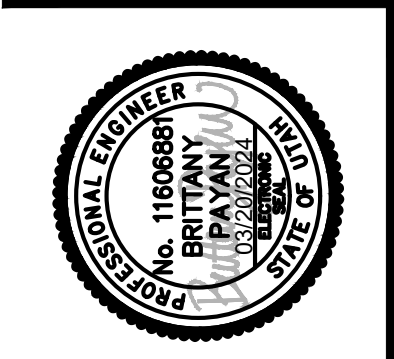
| PAVEMENT MARKING REMOVAL SUMMARY SHEET | | | | | | | |
|--|-----------|----------|-----------|----------|--------------------------|------------------------|---------|
| LINE | FROM | | TO | | REMOVE PAVEMENT STRIPING | REMOVE PAVEMENT SYMBOL | REMARKS |
| | STATION | OFFSET | STATION | OFFSET | LIN FEET | EACH | |
| FOREST ST. | 122+63.17 | 31.00 LT | 124+51.73 | 31.00 LT | 188.6 | | |
| FOREST ST. | 122+63.17 | 19.00 LT | 124+51.51 | 19.00 LT | 188.3 | | |
| FOREST ST. | 122+63.17 | 7.00 LT | 124+51.29 | 7.00 LT | 188.1 | | |
| FOREST ST. | 122+63.17 | 3.00 RT | 123+73.10 | 3.00 RT | 109.9 | | |
| FOREST ST. | 122+63.17 | 15.00 RT | 124+50.88 | 15.00 RT | 187.7 | | |
| FOREST ST. | 122+63.17 | 27.00 RT | 124+50.65 | 27.00 RT | 187.5 | | |
| FOREST ST. | 124+11.10 | 3.00 RT | 124+51.10 | 3.00 RT | 40.0 | | |
| FOREST ST. | 124+50.46 | 37.62 RT | 124+51.89 | 39.31 LT | 76.9 | | |
| FOREST ST. | 124+60.34 | 44.01 RT | 124+61.96 | 42.88 LT | 86.9 | | |
| FOREST ST. | 124+20.00 | 2.00 LT | | | | 1 | |
| TOTAL | | | | | 1,254 | 1 | |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

PAVEMENT MARKING PAINT SUMMARY SHEET (1 OF 2)

| LINE | FROM | | TO | | 4" WHITE | | 4" YELLOW | | | 8" WHITE | | 12" WHITE | | 24" WHITE | PAVEMENT SYMBOL PAINT | REMARKS | |
|---------------------------------|-----------|-----------|-----------|-----------|---------------|--------------|--------------|------------|------------|------------|------------|------------|------------|------------|-----------------------|------------------------|------|
| | STATION | OFFSET | STATION | OFFSET | FEET | FEET* | FEET | FEET | FEET* | FEET | FEET* | FEET | FEET | FEET | | | EACH |
| | | | | | | | | | | | | | | | | | |
| FOREST ST. | 106+52.89 | 27.00 RT | 106+74.32 | 18.00 RT | 23.9 | | | | | | | | | | | | |
| FOREST ST. | 106+54.59 | 28.00 LT | 106+76.96 | 18.00 LT | 25.2 | | | | | | | | | | | | |
| FOREST ST. | 106+74.32 | 18.00 RT | 109+91.03 | 18.00 RT | 316.4 | | | | | | | | | | | | |
| FOREST ST. | 106+76.96 | 18.00 LT | 110+41.03 | 18.00 LT | 364.4 | | | | | | | | | | | | |
| FOREST ST. | 109+91.03 | 18.00 RT | 110+91.03 | 14.00 RT | 100.3 | | | | | | | | | | | | |
| FOREST ST. | 110+41.03 | 18.00 LT | 110+91.03 | 16.00 LT | 49.8 | | | | | | | | | | | | |
| FOREST ST. | 110+91.03 | 16.00 LT | 119+50.22 | 16.00 LT | 859.2 | | | | | | | | | | | | |
| FOREST ST. | 110+91.03 | 14.00 RT | 116+93.04 | 14.00 RT | 602.0 | | | | | | | | | | | | |
| FOREST ST. | 115+22.59 | 29.70 RT | 115+40.41 | 29.40 RT | 17.8 | | | | | | | | | | | | |
| FOREST ST. | 115+40.41 | 29.40 RT | 116+42.12 | 128.38 RT | 157.8 | | | | | | | | | | | | |
| FOREST ST. | 116+93.04 | 14.00 RT | 118+43.04 | 20.00 RT | 150.1 | | | | | | | | | | | | |
| FOREST ST. | 118+43.04 | 20.00 RT | 119+98.02 | 20.00 RT | 155.0 | | | | | | | | | | | | |
| FOREST ST. | 119+50.22 | 16.00 LT | 119+79.44 | 39.40 LT | 40.4 | | | | | | | | | | | | |
| FOREST ST. | 119+98.02 | 20.00 RT | 120+81.49 | 21.59 RT | 80.9 | | | | | | | | | | | | |
| FOREST ST. | 120+17.73 | 40.11 LT | 120+39.31 | 28.00 LT | 26.9 | | | | | | | | | | | | |
| FOREST ST. | 120+39.31 | 28.00 LT | 120+58.26 | 28.00 LT | 20.0 | | | | | | | | | | | | |
| FOREST ST. | 120+58.26 | 28.00 LT | 121+94.21 | 31.00 LT | 135.9 | | | | | | | | | | | | |
| FOREST ST. | 120+81.49 | 21.59 RT | 121+69.62 | 25.25 RT | 88.2 | | | | | | | | | | | | |
| FOREST ST. | 121+69.62 | 25.25 RT | 122+57.00 | 27.00 RT | 90.8 | | | | | | | | | | | | |
| FOREST ST. | 121+94.21 | 31.00 LT | 124+51.73 | 31.00 LT | 254.4 | | | | | | | | | | | | |
| FOREST ST. | 122+57.00 | 27.00 RT | 124+50.88 | 27.00 RT | 193.6 | | | | | | | | | | | | |
| FOREST ST. | 124+10.88 | 15.00 RT | 124+50.88 | 15.00 RT | 40.0 | | | | | | | | | | | | |
| FOREST ST. | 122+57.00 | 15.00 RT | 124+10.88 | 15.00 RT | | 153.9 | | | | | | | | | | | |
| FOREST ST. | 114+20.87 | 73.49 RT | 114+38.57 | 73.42 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+20.87 | 73.49 RT | 114+38.57 | 73.42 RT | | | 79.6 | | | | | | | | | PARKING STALL HATCHING | |
| FOREST ST. | 114+20.89 | 79.49 RT | 114+21.12 | 88.57 RT | | | 9.1 | | | | | | | | | | |
| FOREST ST. | 114+21.12 | 88.57 RT | 114+39.11 | 88.12 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+21.12 | 88.57 RT | 114+39.11 | 88.12 RT | | | 80.4 | | | | | | | | | PARKING STALL HATCHING | |
| FOREST ST. | 114+21.35 | 97.57 RT | 114+39.34 | 97.11 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+21.57 | 106.56 RT | 114+39.56 | 106.11 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+21.80 | 115.56 RT | 114+39.79 | 115.11 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+22.02 | 124.55 RT | 114+40.01 | 124.10 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+22.25 | 133.55 RT | 114+40.24 | 133.10 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+22.48 | 142.54 RT | 114+40.47 | 142.09 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+22.70 | 151.54 RT | 114+40.69 | 151.09 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+22.93 | 160.53 RT | 114+23.04 | 166.79 RT | | | 6.3 | | | | | | | | | | |
| FOREST ST. | 114+23.04 | 166.79 RT | 114+41.08 | 166.48 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+23.04 | 166.79 RT | 114+41.08 | 166.48 RT | | | 57.6 | | | | | | | | | PARKING STALL HATCHING | |
| FOREST ST. | 114+38.50 | 55.43 RT | 114+20.87 | 73.49 RT | | | 25.2 | | | | | | | | | | |
| FOREST ST. | 114+38.57 | 73.42 RT | 114+38.50 | 55.43 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+38.89 | 79.42 RT | 114+20.89 | 79.49 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+39.11 | 88.12 RT | 114+38.89 | 79.42 RT | | | 8.7 | | | | | | | | | | |
| FOREST ST. | 114+40.92 | 160.08 RT | 114+22.93 | 160.53 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+41.08 | 166.48 RT | 114+40.92 | 160.08 RT | | | 6.4 | | | | | | | | | | |
| FOREST ST. | 114+47.57 | 73.39 RT | 114+47.50 | 55.39 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+56.56 | 73.36 RT | 114+56.50 | 55.36 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+65.56 | 73.32 RT | 114+65.49 | 55.33 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+74.56 | 73.29 RT | 114+74.49 | 55.29 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+83.56 | 73.26 RT | 114+83.49 | 55.26 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 114+92.56 | 73.22 RT | 114+92.49 | 55.23 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 115+01.55 | 73.19 RT | 115+01.49 | 55.19 RT | | | 18.0 | | | | | | | | | | |
| FOREST ST. | 115+13.05 | 73.15 RT | 115+34.53 | 73.42 RT | | | 21.5 | | | | | | | | | | |
| FOREST ST. | 115+13.05 | 73.15 RT | 115+34.53 | 73.42 RT | | | 135.2 | | | | | | | | | PARKING STALL HATCHING | |
| SUB-TOTAL (SHEET 1 OF 2) | | | | | 3793.0 | 153.9 | 790.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0 | | |

*LINEAL FEET QUANTITY INCLUDES GAPS IN PAINT

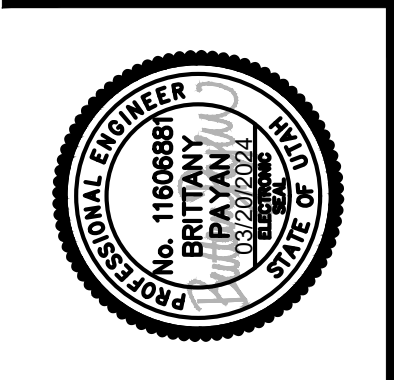
LAYOUT: SM-11
 PATH: U:\Soil\Projects\Clients\8541--Brigham City\344--8541--002 Forest St Final Design\995svcs\CADD\DWG\Civil
 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:42 PM

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE IF NOT INDICATED OTHERWISE ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY

LAYOUT: SM-12 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:47 PM

| PAVEMENT MARKING PAINT SUMMARY SHEET (2 OF 2) | | | | | | | | | | | | | | | | | |
|---|-----------|-----------|-----------|-----------|---------------|--------------|--------------|-------------------|----------------------|--------------|-----------------|----------------|----------------------|----------------|-----------------------|--|------|
| LINE | FROM | | TO | | 4" WHITE | | 4" YELLOW | | | 8" WHITE | | 12" WHITE | | 24" WHITE | PAVEMENT SYMBOL PAINT | REMARKS | |
| | STATION | OFFSET | STATION | OFFSET | FEET | FEET* | FEET | DOUBLE SOLID FEET | SOLID & BROKEN FEET* | FEET | LANE DROP FEET* | STOP LINE FEET | CROSS WALK LINE FEET | STOP LINE FEET | | | EACH |
| | | | | | | | | | | | | | | | | | |
| FOREST ST. | 115+34.53 | 73.42 RT | 115+26.45 | 57.98 RT | | | 17.4 | | | | | | | | | | |
| FOREST ST. | 109+91.03 | 6.00 LT | 110+41.03 | 6.00 LT | | | | 100.0 | | | | | | | | | |
| FOREST ST. | 109+91.03 | 6.00 RT | 110+91.03 | 2.00 RT | | | | 200.2 | | | | | | | | | |
| FOREST ST. | 110+41.03 | 6.00 LT | 110+91.03 | 4.00 LT | | | | 100.0 | | | | | | | | | |
| FOREST ST. | 110+91.03 | 4.00 LT | 118+43.04 | 4.00 LT | | | | 1504.0 | | | | | | | | | |
| FOREST ST. | 110+91.03 | 2.00 RT | 116+93.04 | 2.00 RT | | | | 1204.0 | | | | | | | | | |
| FOREST ST. | 115+43.79 | 42.13 RT | 116+28.20 | 135.59 RT | | | | 275.4 | | | | | | | | | |
| FOREST ST. | 116+28.20 | 135.59 RT | 116+28.32 | 148.15 RT | | | | 25.2 | | | | | | | | | |
| FOREST ST. | 116+28.52 | 168.19 RT | 116+29.46 | 260.95 RT | | | | 185.6 | | | | | | | | | |
| FOREST ST. | 116+93.04 | 2.00 RT | 118+43.04 | 8.00 RT | | | | 300.2 | | | | | | | | | |
| FOREST ST. | 122+46.20 | 7.00 LT | 124+51.29 | 7.00 LT | | | | 410.2 | | | | | | | | | |
| FOREST ST. | 105+40.00 | 6.00 LT | 109+91.03 | 6.00 LT | | | | | 902.3 | | | | | | | | |
| FOREST ST. | 105+40.00 | 6.00 RT | 109+91.03 | 6.00 RT | | | | | 901.9 | | | | | | | | |
| FOREST ST. | 118+43.04 | 4.00 LT | 120+58.18 | 4.00 LT | | | | | 431.1 | | | | | | | | |
| FOREST ST. | 118+43.04 | 8.00 RT | 120+51.06 | 8.00 RT | | | | | 414.7 | | | | | | | | |
| FOREST ST. | 120+51.06 | 8.00 RT | 122+01.33 | 3.00 RT | | | | | 300.4 | | | | | | | | |
| FOREST ST. | 120+58.18 | 4.00 LT | 121+94.21 | 7.00 LT | | | | | 272.0 | | | | | | | | |
| FOREST ST. | 121+94.21 | 7.00 LT | 122+46.20 | 7.00 LT | | | | | 102.6 | | | | | | | | |
| FOREST ST. | 122+01.33 | 3.00 RT | 122+46.21 | 3.00 RT | | | | | 90.3 | | | | | | | | |
| FOREST ST. | 120+19.59 | 16.00 LT | 120+58.18 | 16.00 LT | | | | | 39.8 | | | | | | | | |
| FOREST ST. | 120+58.18 | 16.00 LT | 121+67.96 | 18.52 LT | | | | | 110.2 | | | | | | | | |
| FOREST ST. | 123+51.10 | 3.00 RT | 124+51.10 | 3.00 RT | | | | | 100.0 | | | | | | | | |
| FOREST ST. | 121+67.96 | 18.52 LT | 121+94.21 | 19.00 LT | | | | | | 25.8 | | | | | | | |
| FOREST ST. | 121+94.21 | 19.00 LT | 124+51.51 | 19.00 LT | | | | | | 255.3 | | | | | | | |
| FOREST ST. | 119+84.35 | 53.48 LT | 119+97.83 | 53.68 LT | | | | | | | 13.5 | | | | | | |
| FOREST ST. | 119+79.44 | 39.40 LT | 120+17.73 | 40.11 LT | | | | | | | | 39.1 | | | | | |
| FOREST ST. | 119+83.74 | 49.47 LT | 120+11.63 | 49.92 LT | | | | | | | | 28.3 | | | | | |
| FOREST ST. | 124+50.46 | 37.62 RT | 124+51.89 | 39.31 LT | | | | | | | | 76.9 | | | | | |
| FOREST ST. | 124+60.34 | 44.01 RT | 124+61.96 | 42.88 LT | | | | | | | | 86.9 | | | | | |
| FOREST ST. | 115+03.35 | 54.04 RT | 115+03.14 | 42.04 RT | | | | | | | | | 12.0 | | | | |
| FOREST ST. | 115+43.79 | 42.13 RT | 115+44.03 | 29.41 RT | | | | | | | | | 12.7 | | | | |
| FOREST ST. | 115+49.94 | 55.07 RT | 115+52.35 | 43.49 RT | | | | | | | | | 11.8 | | | | |
| FOREST ST. | 115+86.36 | 75.35 RT | 115+96.50 | 65.70 RT | | | | | | | | | 14.0 | | | | |
| FOREST ST. | 116+05.32 | 116.95 RT | 116+26.11 | 116.74 RT | | | | | | | | | 20.8 | | | | |
| FOREST ST. | 116+11.56 | 82.83 RT | 116+25.18 | 73.69 RT | | | | | | | | | 16.4 | | | | |
| FOREST ST. | 116+27.94 | 129.44 RT | 116+42.15 | 128.38 RT | | | | | | | | | 14.3 | | | | |
| FOREST ST. | 116+28.75 | 187.01 RT | 116+40.75 | 186.89 RT | | | | | | | | | 12.0 | | | | |
| FOREST ST. | 116+29.76 | 287.01 RT | 116+41.76 | 286.89 RT | | | | | | | | | 12.0 | | | | |
| FOREST ST. | 116+30.26 | 337.01 RT | 116+42.26 | 336.89 RT | | | | | | | | | 12.0 | | | | |
| FOREST ST. | 114+25.61 | 93.11 RT | | | | | | | | | | | | 1 | | ADA PARKING SYMBOL | |
| FOREST ST. | 115+72.91 | 57.05 RT | | | | | | | | | | | | 4 | | RAILROAD CROSSING (INCLUDES 2 CROSSBARS AND 2 "R" SYMBOLS) | |
| FOREST ST. | 116+30.35 | 102.78 RT | | | | | | | | | | | | 4 | | RAILROAD CROSSING (INCLUDES 2 CROSSBARS AND 2 "R" SYMBOLS) | |
| FOREST ST. | 116+36.01 | 311.95 RT | | | | | | | | | | | | 4 | | RAILROAD CROSSING (INCLUDES 2 CROSSBARS AND 2 "R" SYMBOLS) | |
| FOREST ST. | 120+38.78 | 22.00 LT | | | | | | | | | | | | 1 | | RIGHT TURN ARROW | |
| FOREST ST. | 121+02.67 | 23.00 LT | | | | | | | | | | | | 4 | | ONLY (1 EACH PER LETTER) | |
| FOREST ST. | 121+67.51 | 24.50 LT | | | | | | | | | | | | 1 | | RIGHT TURN ARROW | |
| FOREST ST. | 122+33.88 | 25.00 LT | | | | | | | | | | | | 4 | | ONLY (1 EACH PER LETTER) | |
| FOREST ST. | 122+99.46 | 25.00 LT | | | | | | | | | | | | 1 | | RIGHT TURN ARROW | |
| FOREST ST. | 123+64.44 | 25.00 LT | | | | | | | | | | | | 4 | | ONLY (1 EACH PER LETTER) | |
| FOREST ST. | 123+73.09 | 2.00 LT | | | | | | | | | | | | 1 | | LEFT TURN ARROW | |
| FOREST ST. | 124+29.45 | 25.00 LT | | | | | | | | | | | | 1 | | RIGHT TURN ARROW | |
| FOREST ST. | 124+33.09 | 2.00 LT | | | | | | | | | | | | 1 | | LEFT TURN ARROW | |
| SUB-TOTAL (SHEET 1 OF 2) | | | | | 3793.0 | 153.9 | 790.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0 | | | |
| SUB-TOTAL (SHEET 2 OF 2) | | | | | 0.0 | 0.0 | 17.4 | 4304.8 | 3415.3 | 250.0 | 281.1 | 13.5 | 231.3 | 138.0 | 31 | | |
| TOTAL | | | | | 12475 | | | | | 532 | | 245 | | 138 | 31 | | |

*LINEAL FEET QUANTITY INCLUDES GAPS IN PAINT

| | | | |
|---|------|------------------------------|-----------------------------|
| REVISIONS | DATE | BY | |
| △ | | | |
| ONE INCH AT FULL SCALE, IF NECESSARY, ACCORDINGLY | | | |
| | | CHECKED: CCS APPROVED: AP | DESIGNED: BKP DRAWN: BKP |
| DATE: 03/20/2024 JOB No.: 344-8541-002 | | | |
| | | | |
| BRIGHAM CITY CONNECTION PROJECT | | | |
| SUMMARY | | | |
| DRAWING NO. 43 OF 63 | | | |
| SM-12 | | | |

LAYOUT: SM-13 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:28:56 PM

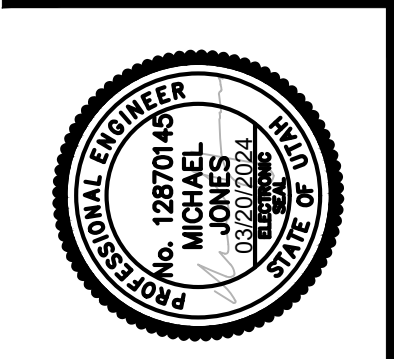
| DRAINAGE SYSTEMS | | | | | | | | | | | | | | | |
|------------------|-----------|-----------|-----------|-----------|--------|-------------------------------------|----------------------------|---|---|---|---|--------------------|-------------------------|-----|-----|
| LINE | FROM | | TO | | ID # | DRAINAGE PIPE | | SMALL STRUCTURES | | | | | REMARKS | | |
| | STATION | OFFSET | STATION | OFFSET | | REINFORCED CONCRETE, LEAK-RESISTANT | RAISE FRAME TO GRADE 360.1 | CONNECT NEW STORM DRAIN TO EXISTING STRUCTURE | PRECAST MANHOLE 341.2 - A & 30" FRAME AND COVER - 302 | CLEANOUT BOX 331.1 - 305 & 48" GRID GRATE AND FRAME - 310 | PRECAST BOX 332 & 44" FRAME AND COVER - 303 | COVER COLLAR - 362 | | | |
| | | | | | | | | | | | | | | 15" | 18" |
| | | | | | | | | | | | | | | FT | FT |
| FOREST ST. | 105+97.05 | 19.49 RT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 106+06.49 | 28.08 RT | | | | | | | | | | 1 | STORM DRAIN COMBO BOX | | |
| FOREST ST. | 106+61.55 | 3.73 LT | | | | | | | | | | 1 | STORM DRAIN MANHOLE | | |
| FOREST ST. | 106+63.18 | 32.03 RT | | | | | | | | | | 1 | WATER METER | | |
| FOREST ST. | 107+97.55 | 2.25 LT | | | MH 1-1 | | | 1 | | | | 1 | STORM DRAIN MANHOLE | | |
| FOREST ST. | 108+22.15 | 19.76 RT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 111+01.87 | 0.00 RT | | | MH 1-2 | | | | 1 | | | 1 | | | |
| FOREST ST. | 111+17.54 | 26.75 LT | | | CB 1-1 | | | | | 1 | | 1 | | | |
| FOREST ST. | 111+17.53 | 24.93 RT | | | CB 1-2 | | | | | 1 | | 1 | | | |
| FOREST ST. | 111+01.87 | 0.00 RT | 111+17.54 | 26.75 LT | P 1-2 | 31.0 | | | | | | | | | |
| FOREST ST. | 111+01.87 | 0.00 RT | 111+17.53 | 24.93 RT | P1-3 | 29.4 | | | | | | | | | |
| FOREST ST. | 107+97.55 | 2.25 LT | 111+01.87 | 0.00 RT | P1-1 | | 304.3 | | | | | | | | |
| FOREST ST. | 116+82.20 | 0.00 RT | | | MH 2-1 | | | | 1 | | | 1 | | | |
| FOREST ST. | 119+32.11 | 69.24 RT | | | MH 2-3 | | | | | 2 | | | | | |
| FOREST ST. | 119+75.12 | 33.85 LT | | | MH 2-4 | | | | | | | 1 | SEE DR-03 FOR DETAILS | | |
| FOREST ST. | 116+66.54 | 26.75 LT | | | CB 2-1 | | | | | | | 1 | | | |
| FOREST ST. | 116+66.53 | 24.93 RT | | | CB 2-2 | | | | | 1 | | 1 | | | |
| FOREST ST. | 119+75.67 | 105.02 LT | | | CB 2-7 | | | | | 1 | | 1 | | | |
| FOREST ST. | 119+98.71 | 125.08 LT | | | CB 2-8 | | | | | 1 | | 1 | | | |
| FOREST ST. | 120+09.71 | 75.46 RT | | | CB 2-5 | | | | | 1 | | 1 | | | |
| FOREST ST. | 120+69.80 | 33.91 RT | | | CB 2-6 | | | | | 1 | | 1 | | | |
| FOREST ST. | 116+66.54 | 26.75 LT | 116+82.20 | 0.00 RT | P 2-1 | 31.0 | | | | | | | | | |
| FOREST ST. | 116+66.53 | 24.73 RT | 116+82.20 | 0.00 RT | P 2-2 | 29.3 | | | | | | | | | |
| FOREST ST. | 116+82.20 | 0.00 RT | 119+39.90 | 0.00 RT | P 2-3 | 257.7 | | | | | | | | | |
| FOREST ST. | 119+98.71 | 125.08 LT | 119+75.67 | 105.02 LT | P 2-11 | 30.5 | | | | | | | | | |
| FOREST ST. | 119+75.67 | 105.02 LT | 119+75.12 | 33.85 LT | P 2-10 | 71.2 | | | | | | | | | |
| FOREST ST. | 119+12.58 | 41.28 LT | 119+75.19 | 37.36 LT | P 2-12 | 62.7 | | | | | | | | | |
| FOREST ST. | 119+75.17 | 31.64 LT | 119+39.93 | 26.75 LT | P 2-9 | 35.6 | | | | | | | | | |
| FOREST ST. | 119+32.11 | 69.24 RT | 120+09.71 | 75.46 RT | P 2-7 | 77.8 | | | | | | | | | |
| FOREST ST. | 120+09.71 | 75.46 RT | 120+69.80 | 33.91 RT | P 2-8 | 73.1 | | | | | | | | | |
| FOREST ST. | 119+39.90 | 0.00 RT | | | MH 2-2 | | | | | | | 1 | STORM DRAIN MANHOLE | | |
| FOREST ST. | 119+39.90 | 32.75 RT | | | CB 2-4 | | | | | | | 1 | STORM DRAIN CATCH BASIN | | |
| FOREST ST. | 119+39.93 | 26.75 LT | | | CB 2-3 | | | | | | | 1 | STORM DRAIN CATCH BASIN | | |
| FOREST ST. | 119+82.52 | 39.89 LT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 120+13.12 | 39.96 LT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 120+13.76 | 0.91 RT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 120+14.50 | 41.24 RT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| FOREST ST. | 120+61.41 | 1.72 LT | | | | | | | | | | 1 | SEWER MANHOLE | | |
| TOTAL | | | | | | 730 | 305 | 14 | 3 | 2 | 8 | 1 | 25 | | |

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| BY | |
| DATE | |
| REVISIONS | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: MCJ
 DRAWN: MCJ
 CHECKED: AP
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SUMMARY



- ① M4-10L
48 IN X 18 IN
- ② M4-10R
48 IN X 18 IN
- ③ R11-2
48 IN X 30 IN
- ④ R11-4
60 IN X 30 IN
- ⑤ W20-2
36 IN X 36 IN
- ⑥ W20-3
36 IN X 36 IN
- ⑦ M4-8A
24 IN X 18 IN
- ⑧ R3-1
36 IN X 36 IN
- ⑨ R3-2
36 IN X 36 IN
- ⑩ CUSTOM M4-9L
30 IN X 36 IN
- ⑪ CUSTOM M4-9R
30 IN X 36 IN
- ⑫ CUSTOM M4-9L
30 IN X 42 IN
- ⑬ CUSTOM M4-9R
30 IN X 42 IN

- LEGEND :**
- TYPE 3 BARRICADE
 - TEMPORARY TRAFFIC CONTROL SIGN
 - DETOUR ROUTE
 - LOCAL ACCESS ONLY

DETOUR PLAN

NOTES :

1. SIGN LAYOUT NOT TO SCALE. SIGN LOCATIONS TO BE PLACED ACCORDING TO THE UTAH MUTCD. SIGN LOCATIONS MAY BE ADJUSTED IN THE FIELD AS NECESSARY.
2. NOTIFY BRIGHAM CITY PUBLIC WORKS DIRECTOR, TYLER PUGSLEY, AT 435-226-1437 IF ADDITIONAL POLICE SURVEILLANCE IS NEEDED FOR MONITORING TRAFFIC FLOWS OR TRAFFIC SIGN COMPLIANCE AT 400 SOUTH AT-GRADE RAILROAD CROSSING.
3. PROVIDE 2 VMS SIGNS ON I-15 FOR ALTERNATE ROUTES FOR TRUCK TRAFFIC, CONTINGENT UPON COORDINATION WITH UDOT. PROVIDE 2 ALTERNATIVE VMS SIGNS ON FOREST STREET, WEST OF 1200 WEST, DIRECTING TRUCK TRAFFIC TO USE 600 NORTH. COORDINATE WITH BRIGHAM CITY ON MESSAGING FOR VMS SIGNS.

FOR INFORMATION ONLY

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY

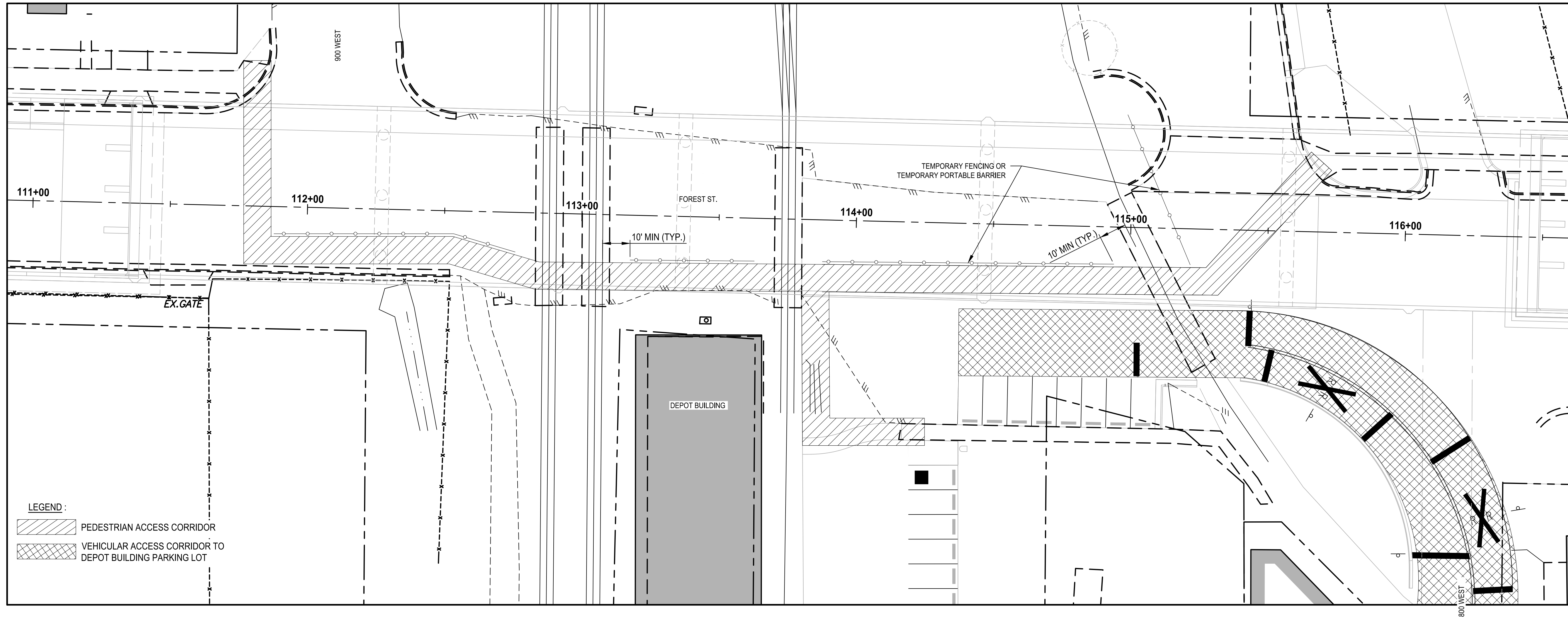
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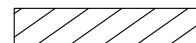

| | | | |
|-------------------------|-----------------|----------------|----------------|
| DATE 03/20/2024 | DESIGNED BKP | CHECKED CCS | APPROVED AP |
| JOB No. 344-8541-002 | DRAWN BKP | | |

PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

**MAINTENANCE
OF TRAFFIC**

LAYOUT: MOTZ PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Sves\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:29:22 PM



LEGEND :
 PEDESTRIAN ACCESS CORRIDOR
 VEHICULAR ACCESS CORRIDOR TO DEPOT BUILDING PARKING LOT

MAINTENANCE OF TRAFFIC AT RAILROAD TRACKS

NOTES:

1. PEDESTRIAN AND VEHICULAR CORRIDORS ON THIS SHEET ARE CONCEPTUAL AND FOR INFORMATION ONLY. CONTRACTOR TO SUBMIT DETAILED TRAFFIC CONTROL PLANS FOR PEDESTRIAN TRAFFIC ALONG FOREST STREET AND VEHICLE TRAFFIC TO DEPOT BUILDING FROM 800 WEST TO UPRR FOR APPROVAL PRIOR TO CONSTRUCTION. PLANS SHALL INCLUDE ALL PHASES OF CONSTRUCTION AND SHOW ALL TRAFFIC CONTROL DEVICES AND SIGNING TO BE USED DURING FULL CLOSURE.
2. ALL TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED WITHIN 10 FT OF NEAREST RAIL.
3. ALL PEDESTRIAN TRAFFIC AT GRADE CROSSINGS SHALL BE RESTRICTED TO CONCRETE PANELS WITHIN UPRR RIGHT-OF-WAY LIMITS. GRADE CROSSING CONCRETE PANELS SHALL REMAIN IN PLACE UNTIL SIDEWALK ON BRIDGE IS FULLY OPERATIONAL AND PEDESTRIAN ACCESS ACROSS TRACKS IS NO LONGER REQUIRED.
4. TRAFFIC CONTROL PLANS SHALL COMPLY WITH THE UTAH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR ALL RELEVANT TRAFFIC CONTROL ELEMENTS.
5. NON-APPLICABLE SIGNING SHALL BE COVERED OR REMOVED, FOR BOTH EXISTING AND WORK ZONE SIGNS.
6. REFER TO SPECIFICATIONS FOR FLAGGING REQUIREMENTS AT OPERATING RAILROAD CROSSINGS.
7. USE EITHER TEMPORARY PORTABLE BARRIER CHANNELIZING DEVICES OR TEMPORARY FENCING TO DELINEATE A 5 FT MINIMUM WALKWAY SECTION ACROSS RAILROAD TRACKS. TEMPORARY WALKWAY SECTION SHALL INCLUDE A FIRM, STABLE, FREE-DRAINING, AND NON-SLIP SURFACE THAT ALLOWS NORMAL USAGE OF WHEELCHAIRS, WALKERS, STROLLERS, AND OTHER MOBILITY DEVICES. ACCEPTABLE SURFACE MATERIALS MAY INCLUDE CONCRETE, HMA, STEEL, RUBBER, WOOD (3/4 INCH OR THICKER), OR PLASTIC. NON-ACCEPTABLE SURFACE MATERIALS INCLUDE GRAVEL, UNTREATED BASE COURSE, AND ANY OTHER UNEVEN SURFACES. PORTABLE SIGNS AND BASES SHALL BE KEPT OUT OF THE PEDESTRIAN WALKWAY SURFACE.
8. PHASE WORK AS NECESSARY TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES.

FOR INFORMATION ONLY

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY


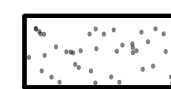
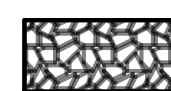




Parametrix

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|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | CCS |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |

PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

MAINTENANCE OF TRAFFIC

LEGEND

-  ASPHALT PAVEMENT (SEE TS SHEETS)
-  UNTREATED BASE COURSE, GRADE 1 REQ'D
-  WEED BARRIER GEOTEXTILE REQ'D
DECORATIVE ROCK MULCH REQ'D
-  SEAL COAT REQ'D
TACK COAT REQ'D
PRIME COAT REQ'D
-  HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (3" THICK)
-  UNTREATED BASE COURSE, GRADE 1 REQ'D (8" THICK)
-  FUTURE 900 / 950 WEST CONNECTION CONSTRUCTION

CONCRETE CURB AND GUTTER, CITY STANDARD REQ'D
FOREST ST.
 105+40.00 LT 29.00 TO 106+57.77 RT 29.00 TO
 106+68.30 RT 30.83 TO 107+98.28 RT 47.32 TO
 107+98.28 RT 47.32 TO 109+02.72 RT 39.50 TO
 109+02.72 RT 39.50 TO 109+57.18 RT 43.92 TO
 109+57.18 RT 43.92 TO 109+57.20 RT 45.18

CONCRETE CURB AND GUTTER TRANSITION REQ'D
FOREST ST.
 106+68.30 RT 30.83 TO 106+78.23 RT 32.00

4" THICK CONCRETE SIDEWALK REQ'D
FOREST ST.
 106+58.74 RT 48.50 TO 106+58.74 RT 38.50

DRIVEWAY APPROACH REQ'D
FOREST ST.
 105+53.74 RT 29.50 TO 105+62.01 RT 29.50
 106+08.77 RT 29.50 TO 106+56.69 RT 29.97

DRIVEWAY 7" THICK REQ'D
FOREST ST.
 106+14.77 RT 48.50 TO 106+52.74 RT 48.50

6-FT BLACK CHAIN LINK FENCE, TYPE III REQ'D
FOREST ST.
 107+17.78 RT 46.07 TO 107+17.78 RT 35.00
 107+93.19 RT 49.31 TO 107+92.96 RT 35.00

4.5-FT BLACK CHAIN LINK BARRIER MOUNTED FENCE, TYPE III REQ'D
FOREST ST.
 106+78.23 RT 32.71 TO 109+46.66 RT 32.71 TO
 110+91.24 RT 26.71 TO 111+10.54 RT 26.71

CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER -42 INCH TRAILING SLOPED END SECTION REQ'D
FOREST ST.
 107+04.23 RT 32.00 TO 107+04.23 RT 32.00 TO
 107+15.29 RT 32.00 TO 107+18.79 RT 32.00 TO
 108+55.29 RT 32.00 TO 108+56.79 RT 32.00 TO
 109+46.64 RT 32.00 TO 109+46.25 RT 29.82
 109+96.74 RT 29.82 TO 110+91.23 RT 26.00 TO
 110+91.23 RT 26.00 TO 111+10.54 RT 26.00

CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER -42 INCH FULL HEIGHT END SECTION WITH MOMENT SLAB FOUNDATION BA. 3K12 REQ'D
FOREST ST.
 110+96.29 RT 26.00 TO 111+10.54 RT 26.00

CAST-IN-PLACE CONCRETE SLOPE BARRIER -42 INCH LIGHTING FOUNDATION REQ'D
FOREST ST.
 107+15.29 RT 32.00 TO 107+18.79 RT 32.00 TO
 108+55.29 RT 32.00 TO 108+56.79 RT 32.00 TO
 109+46.25 RT 29.82 TO 109+46.64 RT 29.82

CONCRETE CURB AND GUTTER, CITY STANDARD REQ'D
FOREST ST.
 110+41.21 LT 30.00 TO 110+91.28 LT 28.00 TO
 110+98.29 LT 28.00

4" THICK CONCRETE SIDEWALK REQ'D
FOREST ST.
 105+40.00 LT 43.50 TO 105+40.94 LT 38.00 TO
 106+04.59 LT 38.00 TO 106+54.59 LT 38.00 TO
 106+68.88 LT 38.00 TO 106+68.88 LT 39.42 TO
 110+41.36 LT 39.42 TO 110+91.48 LT 37.42 TO
 110+98.29 LT 37.42

6" THICK CONCRETE SIDEWALK REQ'D
FOREST ST.
 106+04.59 LT 38.00 TO 106+54.59 LT 38.00

DRIVEWAY APPROACH REQ'D
FOREST ST.
 106+08.77 RT 29.50 TO 106+56.69 RT 29.97

8-FT BLACK CHAIN LINK FENCE, TYPE III REQ'D
FOREST ST.
 105+89.82 LT 110.15 TO 108+14.24 LT 110.54 TO
 108+21.67 LT 103.11 TO 108+36.62 LT 86.12 TO
 108+46.05 LT 78.69 TO 108+64.38 LT 78.67 TO
 108+64.38 LT 76.41 TO 108+64.38 LT 52.41 TO
 108+90.61 LT 53.35 TO 108+90.80 LT 41.00

4.5-FT BLACK CHAIN LINK BARRIER MOUNTED FENCE, TYPE III REQ'D
FOREST ST.
 108+86.88 LT 38.71 TO 110+41.35 LT 38.71 TO
 110+91.47 LT 38.71 TO 111+10.54 LT 38.71

8-FT TO 4.5-FT BLACK CHAIN LINK BARRIER MOUNTED FENCE, TYPE III REQ'D
FOREST ST.
 106+64.38 LT 38.71 TO 106+66.88 LT 38.71

CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER -42 INCH REQ'D
FOREST ST.
 108+86.88 LT 38.00 TO 107+15.29 LT 38.00 TO
 107+18.79 LT 38.00 TO 108+55.29 LT 38.00 TO
 108+56.79 LT 38.00 TO 109+46.29 LT 38.00 TO
 109+46.79 LT 38.00 TO 110+41.33 LT 38.00 TO
 110+91.45 LT 38.00 TO 110+98.29 LT 38.00

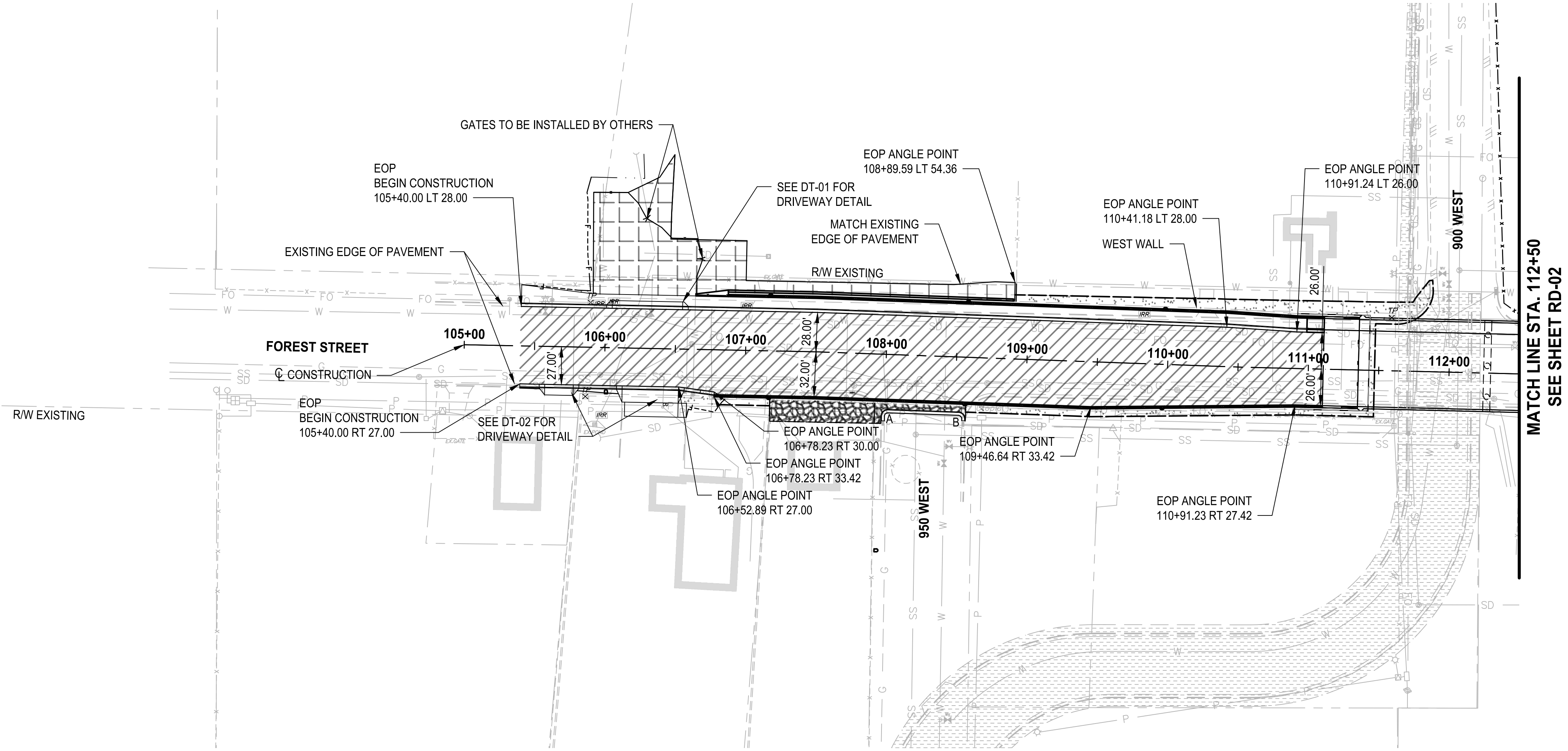
CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER -42 INCH IN FRONT OF RETAINING WALL BA. 3K13 REQ'D
FOREST ST.
 108+86.88 LT 42.58 TO 108+88.77 LT 42.58

CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER -42 INCH TO 42 INCH BRIDGE PARAPET END SECTION WITH MOMENT SLAB BA. 3K14 REQ'D
FOREST ST.
 110+98.29 LT 26.00 TO 111+10.54 LT 26.00

CAST-IN-PLACE CONSTANT SLOPE BARRIER -42 INCH LIGHTING FOUNDATION REQ'D
FOREST ST.
 107+15.29 LT 38.00 TO 107+18.79 LT 38.00 TO
 108+55.29 LT 38.00 TO 108+56.79 LT 38.00 TO
 109+46.29 LT 38.00 TO 109+46.79 LT 38.00

MODIFIED SLOPED END SECTION (NORTHWEST END) REQ'D
FOREST ST.
 106+63.88 LT 38.00 TO 106+66.88 LT 38.00

RIGHT-OF-WAY FENCE, TYPE B (METAL POST) REQ'D
FOREST ST.
 112+14.75 LT 500.00 TO 112+37.77 LT 500.00 TO
 112+52.34 LT 40.00 TO 112+52.34 RT 25.73



FOREST STREET PLAN
 SCALE: 1" = 50'

| CURVE | CURB CURVE DATA* | | | | | | RADIUS |
|-------|------------------|----------|-----------|----------|-----------|----------|--------|
| | PC | | PI | | PT | | |
| | STA | OFF | STA | OFF | STA | OFF | |
| A | 107+98.22 | 44.09 RT | 107+98.13 | 39.50 RT | 108+02.72 | 39.50 RT | 4.50 |
| B | 108+52.68 | 39.50 RT | 108+57.10 | 39.50 RT | 108+57.18 | 43.92 RT | 4.50 |

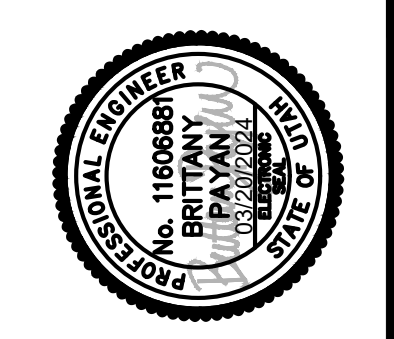
*CURB DATA CALLED OUT FROM FLOW LINE.

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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

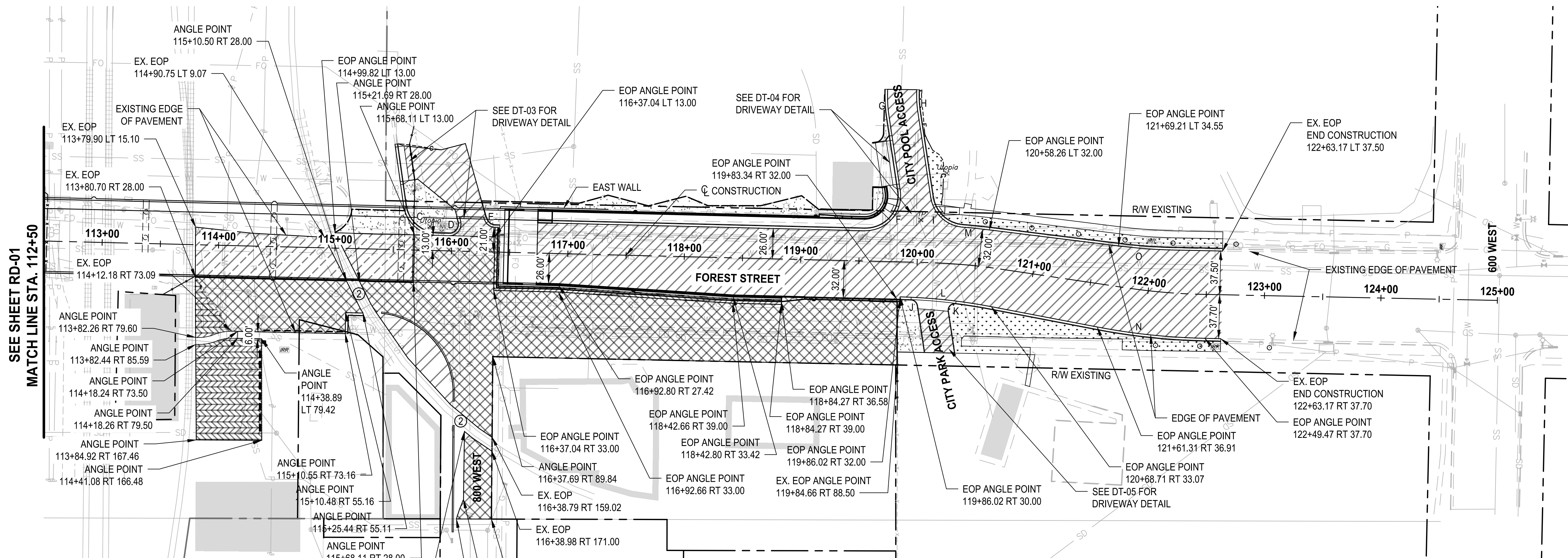
Parametrix

DATE: 03/20/2024
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: AP
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

ROADWAY PLAN



LEGEND

- ASPHALT PAVEMENT (SEE TS SHEETS)
- SEE DT-17
- UNTREATED BASE COURSE, GRADE 1 REQ'D
- IRRIGATION SYSTEM AND LANDSCAPE RESTORATION REQ'D
- SEAL COAT REQ'D
- TACK COAT REQ'D
- PRIME COAT REQ'D
- HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (4" THICK)
- UNTREATED BASE COURSE, GRADE 1 REQ'D (6" THICK)
- GRANULAR BORROW REQ'D (8" THICK)
- PULVERIZED PAVEMENT BASE COURSE REQ'D
- SEE NOTE 3
- COLD MILLING BITUMINOUS PAVEMENT, DEPTH = 3" REQ'D
- HOT MIX ASPHALT PAVEMENT: SP-3/8, PG 64-34 REQ'D (3" THICK)

CONCRETE CURB AND GUTTER, CITY STANDARD

- FOREST ST.**
- 113+53.28 RT 43.31 TO 114+03.56 RT 27.00 TO
- 115+25.44 RT 57.15 TO 115+27.56 RT 27.00 TO
- 115+27.56 RT 27.00 TO 115+67.11 RT 27.00 TO
- 115+67.11 RT 27.00 TO 115+67.11 RT 59.38 TO
- 121+61.23 RT 38.91 TO 119+83.90 RT 34.50 TO
- 122+49.47 RT 39.70 TO 118+88.08 RT 127.34

6-FT GALVANIZED CHAIN LINK FENCE, TYPE III REQ'D

- FOREST ST.**
- 113+53.28 RT 43.31 TO 114+03.56 RT 27.00 TO
- 115+25.44 RT 57.15 TO 115+27.56 RT 27.00 TO
- 115+27.56 RT 27.00 TO 115+67.11 RT 27.00 TO
- 115+67.11 RT 27.00 TO 115+67.11 RT 59.38 TO
- 121+61.23 RT 38.91 TO 119+83.90 RT 34.50 TO
- 122+49.47 RT 39.70 TO 118+88.08 RT 127.34

6-FT X 12-FT WIDE GALVANIZED CHAIN LINK GATE, TYPE III REQ'D

- FOREST ST.**
- 115+03.56 RT 27.00 TO 115+27.56 RT 27.00

CAST-IN-PLACE CONCRETE CONSTANT SLOPE BARRIER - 42 INCH STEPPED MEDIUM BARRIER REQ'D

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CAST-IN-PLACE CONCRETE CONSTANT SLOPE HALF BARRIER - 42 INCH REQ'D

- FOREST ST.**
- 116+85.79 RT 26.00 TO 117+45.33 RT 28.50
- 117+45.33 RT 28.50 TO 117+58.83 RT 28.64 TO
- 118+42.89 RT 32.00 TO 118+84.27 RT 32.00

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 120+01.34 RT 37.05 TO
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+88.02 RT 32.00 TO

CONCRETE CURB, TYPE P

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

CONCRETE CURB AND GUTTER, TYPE III REQ'D

- FOREST ST.**
- 119+96.55 RT 32.10 TO 119+96.55 RT 32.10 TO
- 120+01.34 RT 37.05 TO 120+06.59 RT 37.24
- 120+06.59 RT 37.24 TO 120+35.90 RT 33.69 TO
- 120+35.90 RT 33.69 TO 120+29.25 RT 41.01 TO
- 120+38.44 RT 76.33

CONCRETE CURB, TYPE M2

- FOREST ST.**
- 119+04.27 RT 32.00 TO 119+13.79 RT 32.00 TO
- 119+13.79 RT 32.00 TO 119+06.02 RT 32.00

FOREST STREET PLAN

SCALE: 1" = 50'

CURB CURVE DATA*

| CURVE | STA | PC | OFF | STA | PI | OFF | STA | PT | OFF | RADIUS |
|-------|-----------|--------|-----|-----------|--------|-----|-----------|--------|-----|--------|
| C | 115+59.96 | 38.41 | LT | 115+63.85 | 15.00 | LT | 115+87.58 | 15.00 | LT | 28.00 |
| D | 115+97.67 | 15.00 | LT | 116+08.51 | 15.00 | LT | 116+05.31 | 25.36 | LT | 8.00 |
| E | 116+29.55 | 28.39 | LT | 116+31.44 | 23.05 | LT | 116+37.10 | 23.05 | LT | 8.00 |
| F | 119+54.46 | 28.00 | LT | 119+87.83 | 28.00 | LT | 119+82.03 | 60.87 | LT | 28.00 |
| G | 119+70.82 | 124.41 | LT | 119+69.18 | 133.69 | LT | 119+69.08 | 143.12 | LT | 114.50 |
| H | 119+98.08 | 143.42 | LT | 119+98.15 | 136.38 | LT | 119+99.38 | 129.45 | LT | 85.50 |
| I | 120+11.82 | 56.24 | LT | 120+15.69 | 34.00 | LT | 120+36.38 | 34.00 | LT | 28.00 |
| J | 119+95.55 | 32.10 | RT | 120+00.48 | 32.22 | RT | 120+01.34 | 37.05 | RT | 6.00 |
| K | 120+29.25 | 41.01 | RT | 120+27.44 | 33.33 | RT | 120+35.90 | 33.69 | RT | 6.00 |
| L | 119+86.02 | 32.00 | RT | 120+26.61 | 31.66 | RT | 120+68.62 | 35.07 | RT | 476.00 |
| M | 120+36.38 | 34.00 | LT | 120+47.65 | 34.12 | LT | 120+58.26 | 34.00 | LT | 544.00 |
| N | 121+61.23 | 38.91 | RT | 122+05.99 | 41.47 | RT | 122+49.49 | 39.70 | RT | 552.00 |
| O | 121+69.16 | 36.55 | LT | 121+94.40 | 37.00 | LT | 122+20.62 | 38.88 | LT | 471.00 |

*CURB DATA CALLED OUT FROM FLOW LINE

NOTES:

1. SEE UDOT STD DWG BA 3K13 FOR 42 INCH HALF BARRIER PLACEMENT IN FRONT OF RETAINING WALL.
2. SEE DT-17 FOR SURFACING DETAILS ACROSS RAILROAD TRACKS.
3. VERIFY EXISTING DEPTHS OF HMA AND UTBC FOR PULVERIZED PAVEMENT BASE COURSE. EXISTING HMA DEPTH APPROX. 4.5 INCHES. ASSUMED TOTAL PULVERIZING DEPTH 10.5 INCHES.

Parametrix

PROFESSIONAL ENGINEER
No. 11608860
BRIGHAM CITY, UTAH
STATE OF UTAH

BRIGHAM CITY
CONNECTION PROJECT

ROADWAY PLAN
RD-02

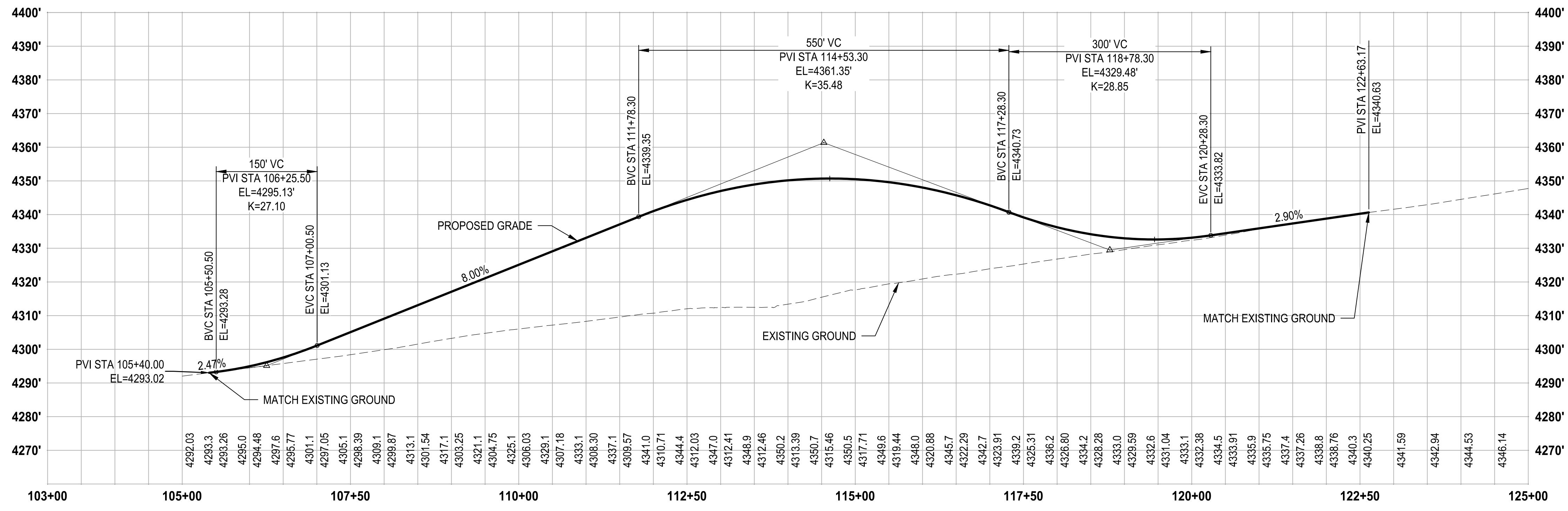
DRAWING NO. 48 OF 63

| DATE | DESIGNED | DRAWN | CHECKED | BY |
|--------------|----------|-------|---------|----|
| 03/20/2024 | BKP | BKP | AP | |
| 344-8541-002 | | | AP | |

REVISIONS

| NO. | DATE | DESCRIPTION |
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ONE INCH AT FULL SCALE IF NOT INDICATED OTHERWISE



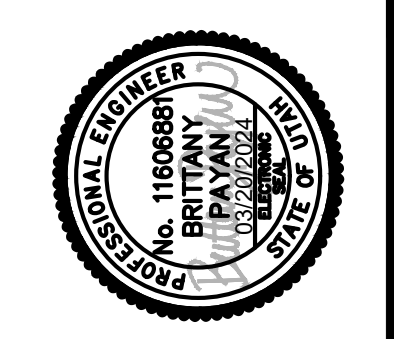
PROFILE
 HORIZ: 1" = 100'
 VERT EXAG: 5:1

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT SHOWN OTHERWISE ACCORDINGLY

Parametrix

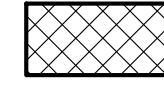
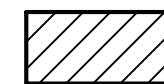
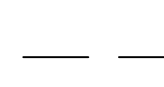
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| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | AP |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

ROADWAY PROFILE

LEGEND

| | |
|---|-------------------------------|
|  | REMOVE EXISTING ASPHALT REQ'D |
|  | CLEAR AND GRUB SITE REQ'D |
|  | REMOVAL ITEMS |

REMOVE EXISTING FENCE REQ'D
 FOREST ST
 109+43.78 RT 39.98 TO 111+64.89 RT 33.94

REMOVE MAIL BOX REQ'D
 FOREST ST
 111+38.81 RT 24.72

REMOVE CONCRETE SIDEWALK REQ'D
 FOREST ST
 106+42.77 RT 38.51 TO 106+58.84 RT 36.54

REMOVE CONCRETE DRIVEWAY REQ'D
 FOREST ST
 105+57.77 RT 34.56 TO 105+78.06 RT 34.35
 106+11.88 RT 34.58 TO 106+53.86 RT 34.57
 106+44.77 RT 48.50 TO 106+58.74 RT 48.50
 111+43.58 RT 29.89 TO 111+63.49 RT 28.20

REMOVE CURB AND GUTTER REQ'D
 FOREST ST
 105+40.00 RT 29.00 TO 107+97.78 RT 47.33
 108+57.20 RT 45.18 TO 111+47.03 RT 23.96

RELOCATE MAIL BOX REQ'D
 FOREST ST
 106+01.32 RT 31.00 TO 106+01.32 RT 31.00
 107+38.01 RT 29.43 TO 107+48.00 RT 138.50

REMOVE TREE REQ'D
 FOREST ST
 109+44.33 RT 33.73

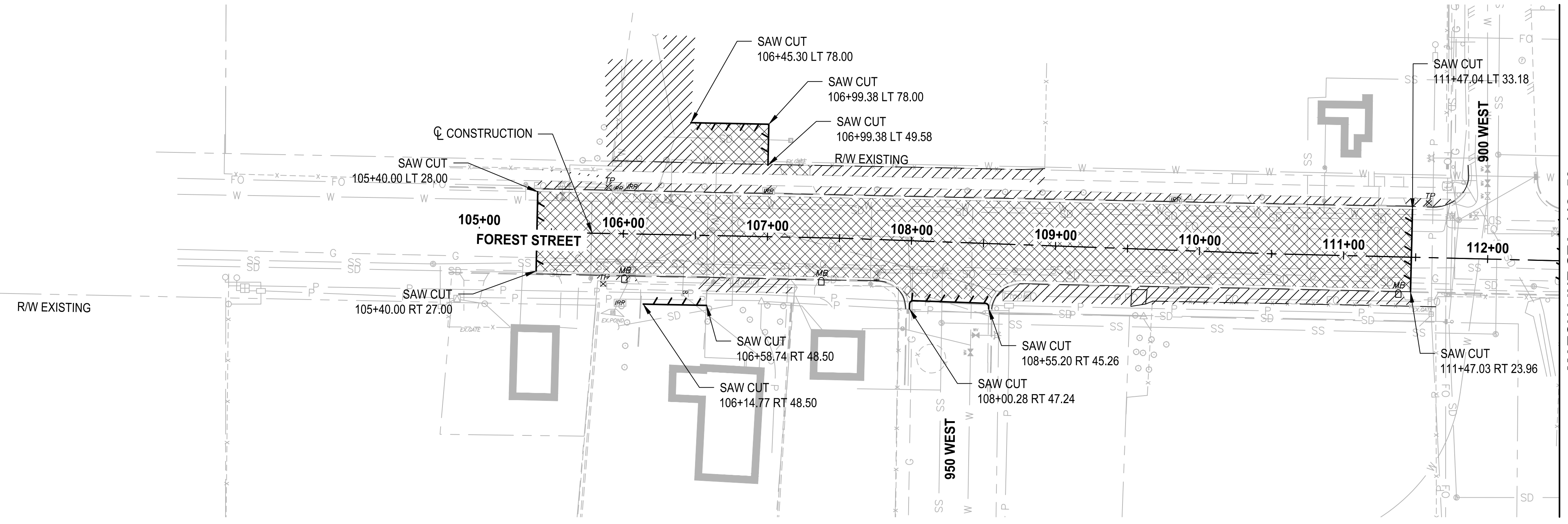
REMOVE EXISTING FENCE REQ'D
 FOREST ST
 105+42.03 LT 59.58 TO 108+90.61 LT 53.35

REMOVE CONCRETE SIDEWALK REQ'D
 FOREST ST
 105+40.00 LT 43.36 TO 111+84.14 LT 51.58

REMOVE CONCRETE DRIVEWAY REQ'D
 FOREST ST
 107+46.02 LT 35.49 TO 107+31.18 LT 35.48
 110+22.03 LT 39.75 TO 110+30.75 LT 40.06
 111+27.28 LT 40.61 TO 111+48.57 LT 40.93

REMOVE CURB AND GUTTER REQ'D
 FOREST ST
 105+40.00 LT 30.00 TO 111+84.89 LT 64.73

REMOVE TREE REQ'D
 FOREST ST
 105+43.31 LT 51.56
 106+49.93 LT 52.57
 106+25.87 LT 34.31
 106+28.84 LT 51.85
 106+55.63 LT 33.95
 107+24.91 LT 34.78
 108+37.56 LT 36.10
 108+63.77 LT 36.22
 108+89.66 LT 36.63
 109+18.09 LT 37.79
 110+41.76 LT 39.07
 111+48.79 LT 39.36



**FOREST STREET
 REMOVAL PLAN**
 SCALE: 1" = 50'

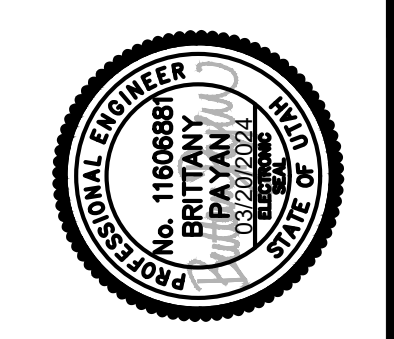
MATCH LINE STA. 112+50
 SEE SHEET RMV-02

- NOTES:**
- SEE DR SHEETS FOR ALL DRAINAGE REMOVALS AND RELOCATIONS.
 - SEE SS SHEETS FOR ALL SIGNING REMOVALS AND RELOCATIONS.

| REVISIONS | DATE | BY |
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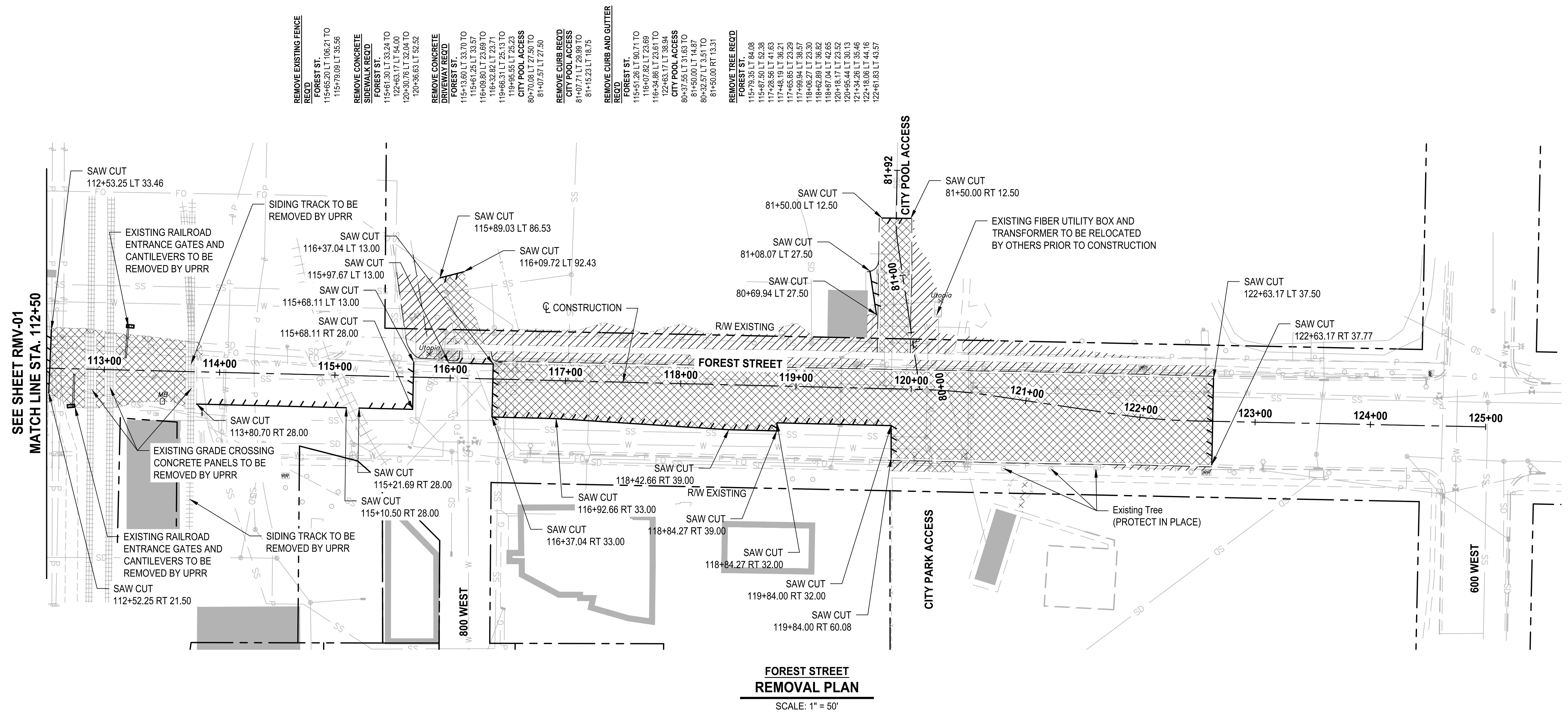
ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

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|-------------------|--------------|-----|----------|----|
| Parametrix | DESIGNED | BKP | CHECKED | AP |
| | DRAWN | BKP | APPROVED | AP |
| DATE | 03/20/2024 | | | |
| JOB No. | 344-8541-002 | | | |



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

REMOVAL



**FOREST STREET
REMOVAL PLAN**
SCALE: 1" = 50'

- LEGEND**
- REMOVE EXISTING ASPHALT REQ'D
 - CLEAR AND GRUB SITE REQ'D
 - REMOVAL ITEMS

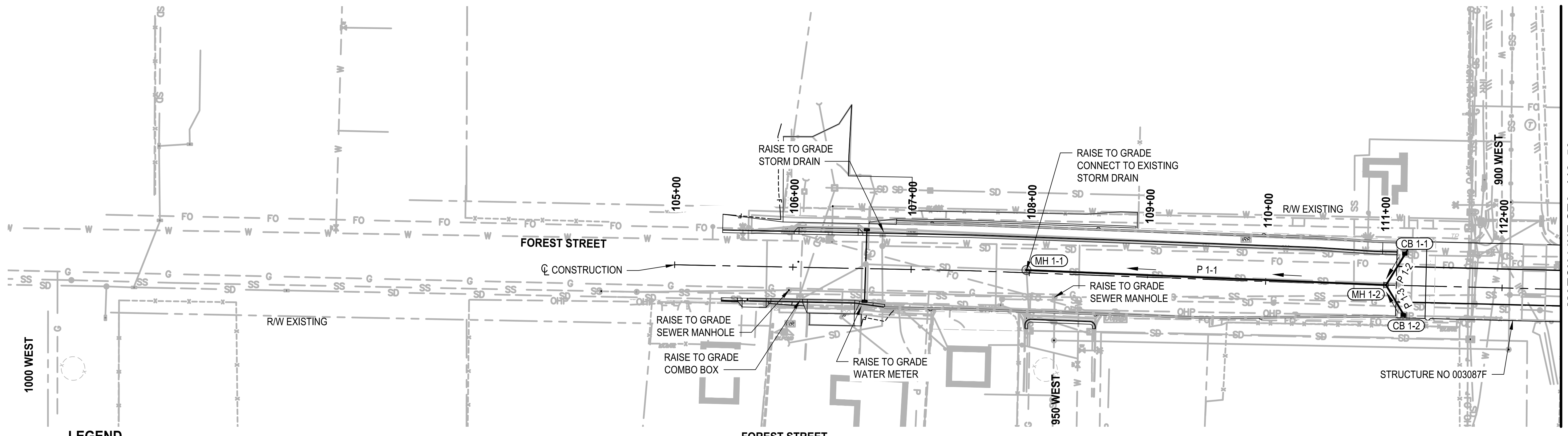
- REMOVE CONCRETE SIDEWALK REQ'D FOREST ST.**
115+45.20 LT 106.21 TO 115+79.09 LT 35.56
- REMOVE CONCRETE DRIVEWAY REQ'D FOREST ST.**
115+61.30 LT 33.24 TO 122+63.17 LT 54.00
- REMOVE CURB AND GUTTER REQ'D FOREST ST.**
120+17.36 RT 70.86 TO 120+45.54 RT 69.70
- RELOCATE MAIL BOX REQ'D FOREST ST.**
119+81.01 RT 62.02 TO 122+63.17 RT 39.51
- REMOVE TREE REQ'D FOREST ST.**
119+87.17 RT 66.36
122+10.39 RT 43.79
122+45.47 RT 44.42
122+82.65 RT 44.32

- REMOVE EXISTING FENCE REQ'D FOREST ST.**
115+45.20 LT 106.21 TO 115+79.09 LT 35.56
- REMOVE CONCRETE SIDEWALK REQ'D FOREST ST.**
115+61.30 LT 33.24 TO 122+63.17 LT 54.00
- REMOVE CONCRETE DRIVEWAY REQ'D FOREST ST.**
115+13.60 LT 33.70 TO 115+61.25 LT 33.57
- REMOVE CURB AND GUTTER REQ'D FOREST ST.**
115+13.60 LT 33.70 TO 115+61.25 LT 33.57
- REMOVE TREE REQ'D FOREST ST.**
115+51.26 LT 90.71 TO 116+07.82 LT 23.69
- REMOVE CURB AND GUTTER REQ'D FOREST ST.**
116+07.82 LT 23.69 TO 116+34.86 LT 23.61 TO 122+63.17 LT 38.94
- CITY POOL ACCESS**
80+37.55 LT 31.63 TO 81+50.00 LT 14.87
- REMOVE TREE REQ'D FOREST ST.**
115+79.35 LT 84.08
115+87.50 LT 52.38
117+28.56 LT 41.63
117+48.19 LT 36.21
117+65.85 LT 23.29
117+69.94 LT 38.57
118+06.27 LT 23.30
118+62.89 LT 36.82
118+87.04 LT 42.65
120+18.17 LT 23.52
120+85.44 LT 30.13
121+34.26 LT 35.46
122+18.06 LT 44.16
122+61.83 LT 43.57

- NOTES:**
- SEE DR SHEETS FOR ALL DRAINAGE REMOVALS AND RELOCATIONS.
 - SEE SS SHEETS FOR ALL SIGNING REMOVALS AND RELOCATIONS.

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| BRIGHAM CITY CONNECTION PROJECT | | | | | | | |
| REMOVAL | | | | | | | |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | | | | | | | |
| DATE: 03/20/2024 JOB No.: 344-8541-002 DESIGNED: BKP DRAWN: BKP CHECKED: AP APPROVED: AP | | | | | | | |
| DRAWING NO. 51 OF 63 RMV-02 | | | | | | | |

LAYOUT: DR-01 PATH: U:\Self\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995secs\CADD\DWG\Civil PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 12:31:11 PM



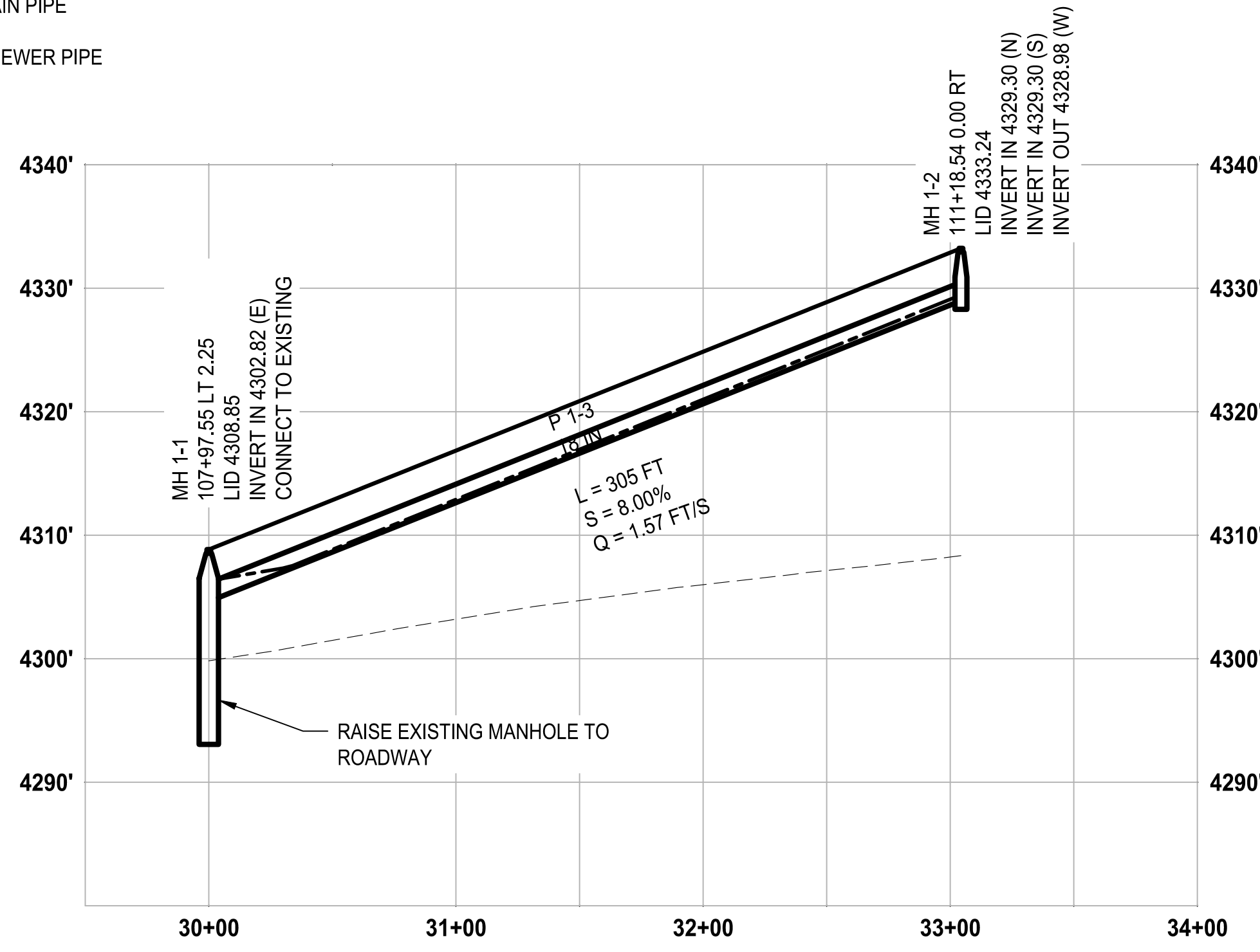
LEGEND

- PROPOSED STORM DRAIN MANHOLE
- PROPOSED STORM DRAIN CATCH BASIN
- PROPOSED STORM DRAIN PIPE
- EXISTING STORM DRAIN MANHOLE
- EXISTING SANITARY SEWER MANHOLE
- EXISTING STORM DRAIN CATCH BASIN
- EXISTING STORM DRAIN PIPE
- EXISTING SANITARY SEWER PIPE

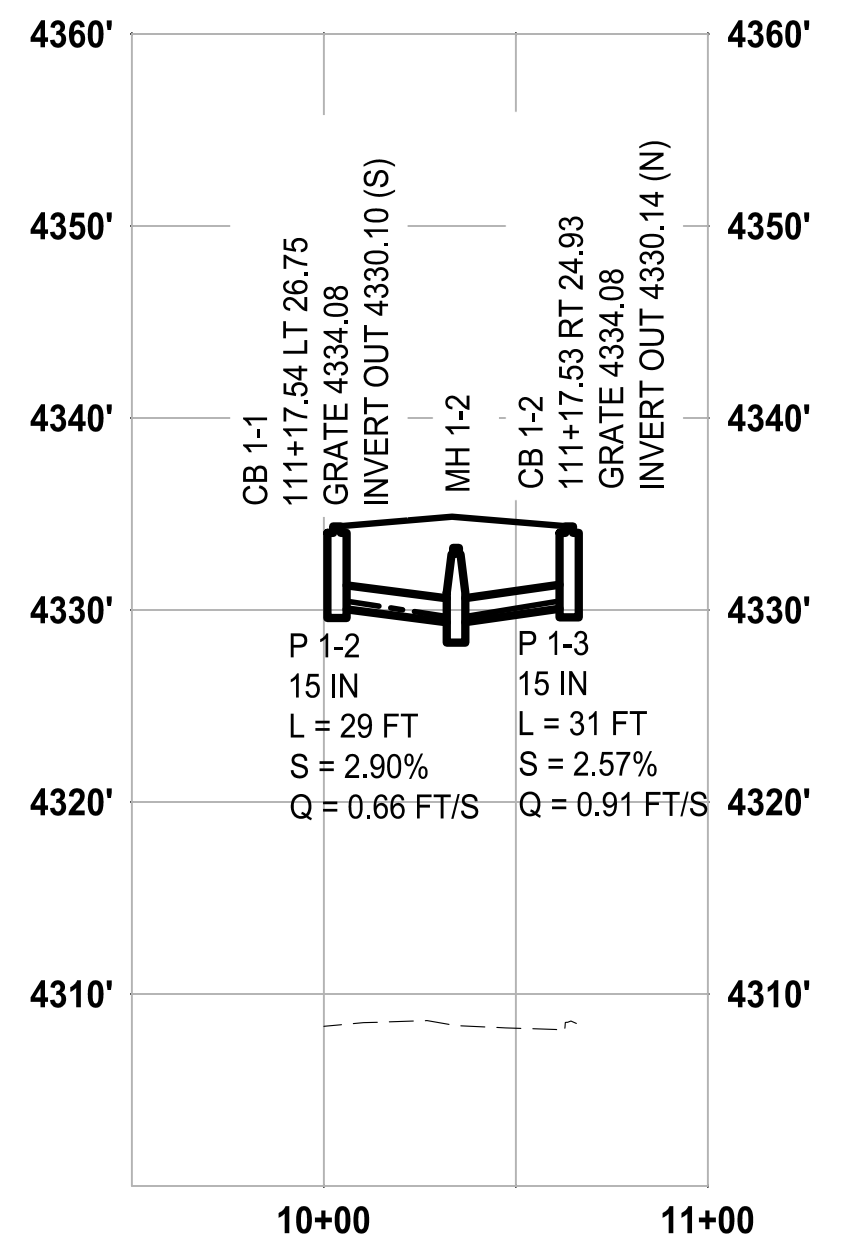
FOREST STREET PLAN

SCALE: 1" = 50'

- RAISE FRAME TO GRADE 360.1**
105+97.05 RT 19.49
106+06.49 RT 28.08
106+61.55 LT 3.73
106+63.18 RT 32.03
107+97.55 LT 2.25
108+22.15 RT 19.76
- CONNECT NEW STORM DRAIN TO EXISTING STRUCTURE**
107+97.55 LT 2.25
- PRECAST MANHOLE 341.2 - A**
30" FRAME AND COVER - 302
111+01.87 RT 0.00
- CLEANOUT BOX 331.1 - 305**
48" GRID GRATE AND FRAME - 310
111+17.54 LT 26.75
111+17.53 RT 24.93
- DRAINAGE PIPE - 15 INCH, REINFORCED CONCRETE, LEAK-RESISTANT REQ'D**
111+01.87 RT 0.00 TO
111+17.54 LT 26.75
111+01.87 RT 0.00 TO
111+17.53 RT 24.93
- DRAINAGE PIPE - 18 INCH, REINFORCED CONCRETE, LEAK-RESISTANT REQ'D**
107+97.55 LT 2.25 TO
111+01.87 RT 0.00
- COVER COLLAR - 362**
105+97.05 RT 19.49
106+06.49 RT 28.08
106+61.55 LT 3.73
106+63.18 RT 32.03
107+97.55 LT 2.25
108+22.15 RT 19.76
111+01.87 RT 0.00
111+17.54 LT 26.75
111+17.53 RT 24.93



MH 1-1 TO MH 1-2



CB 1-1 TO CB 1-2

- LEGEND**
- FINISHED GRADE
 - Existing Grade
 - HGL 25-yr

PIPE PROFILES

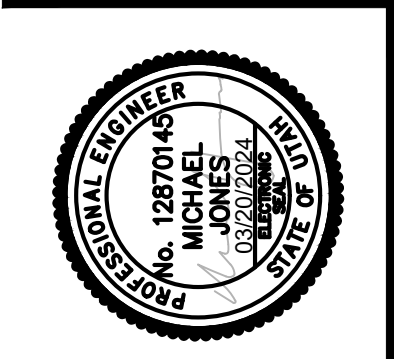
HORIZ: 1" = 50'
VERT: 1" = 10'

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NECESSARY

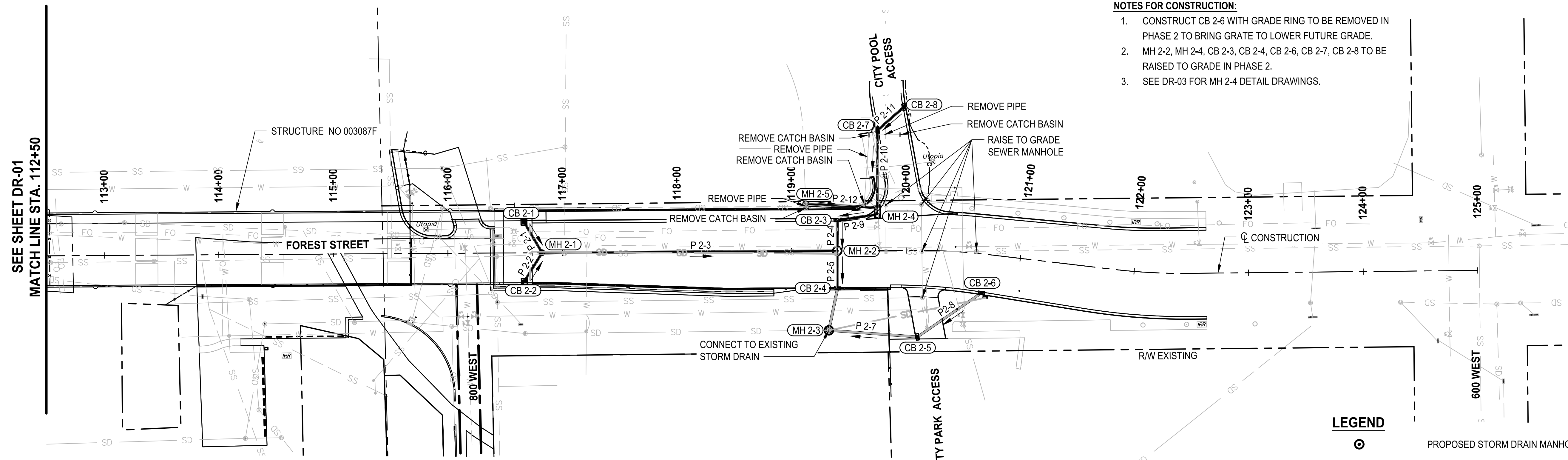
Parametrix

DATE: 03/20/2024 DESIGNED: MCJ CHECKED: JHB
JOB No.: 344-8541-002 DRAWN: MCJ APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DRAINAGE PLAN AND PROFILE



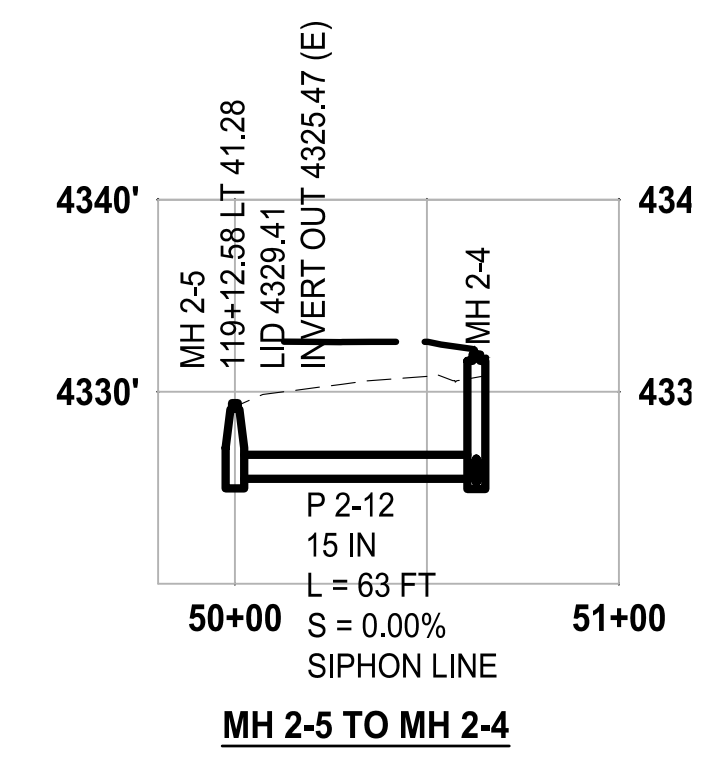
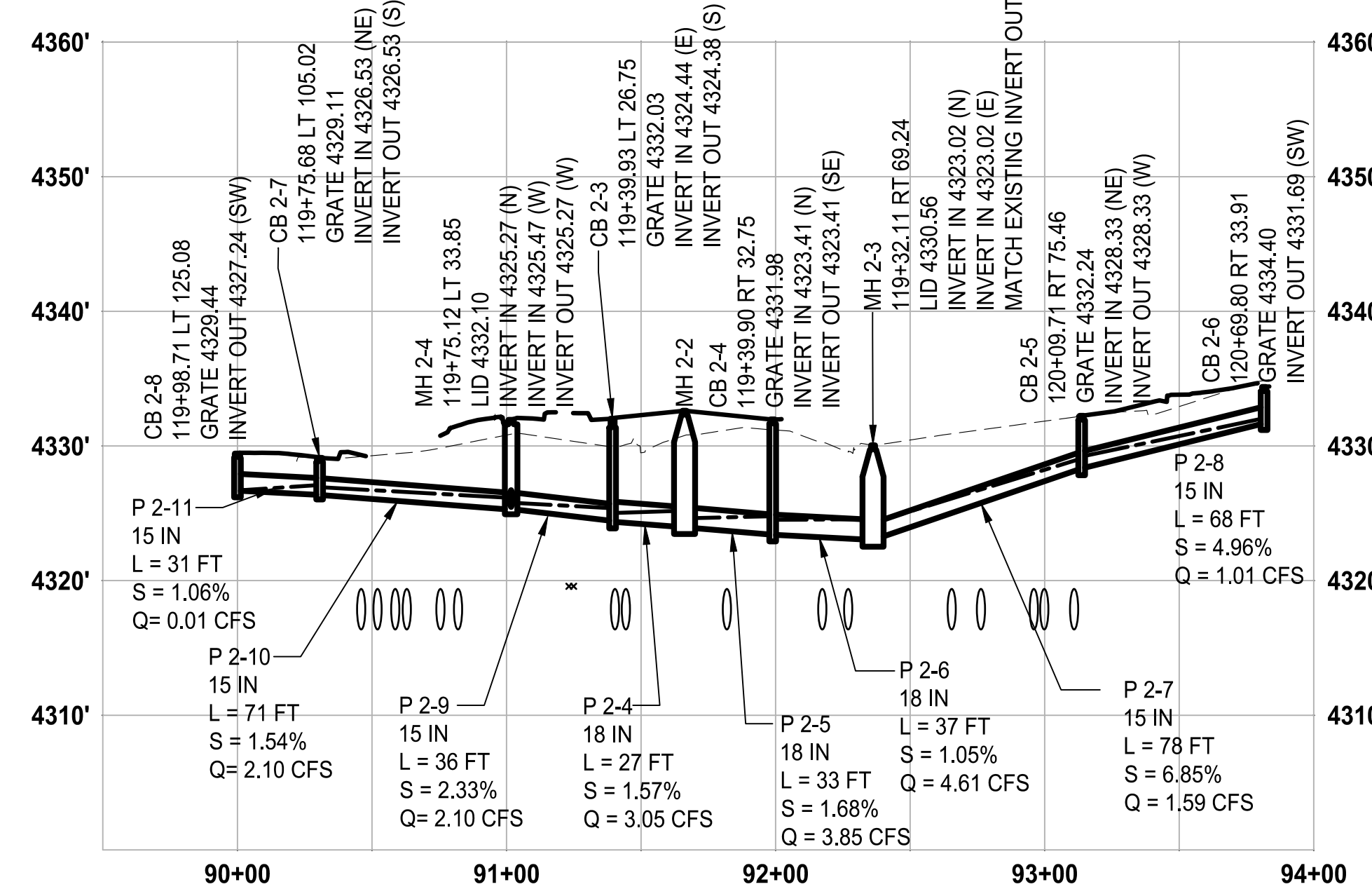
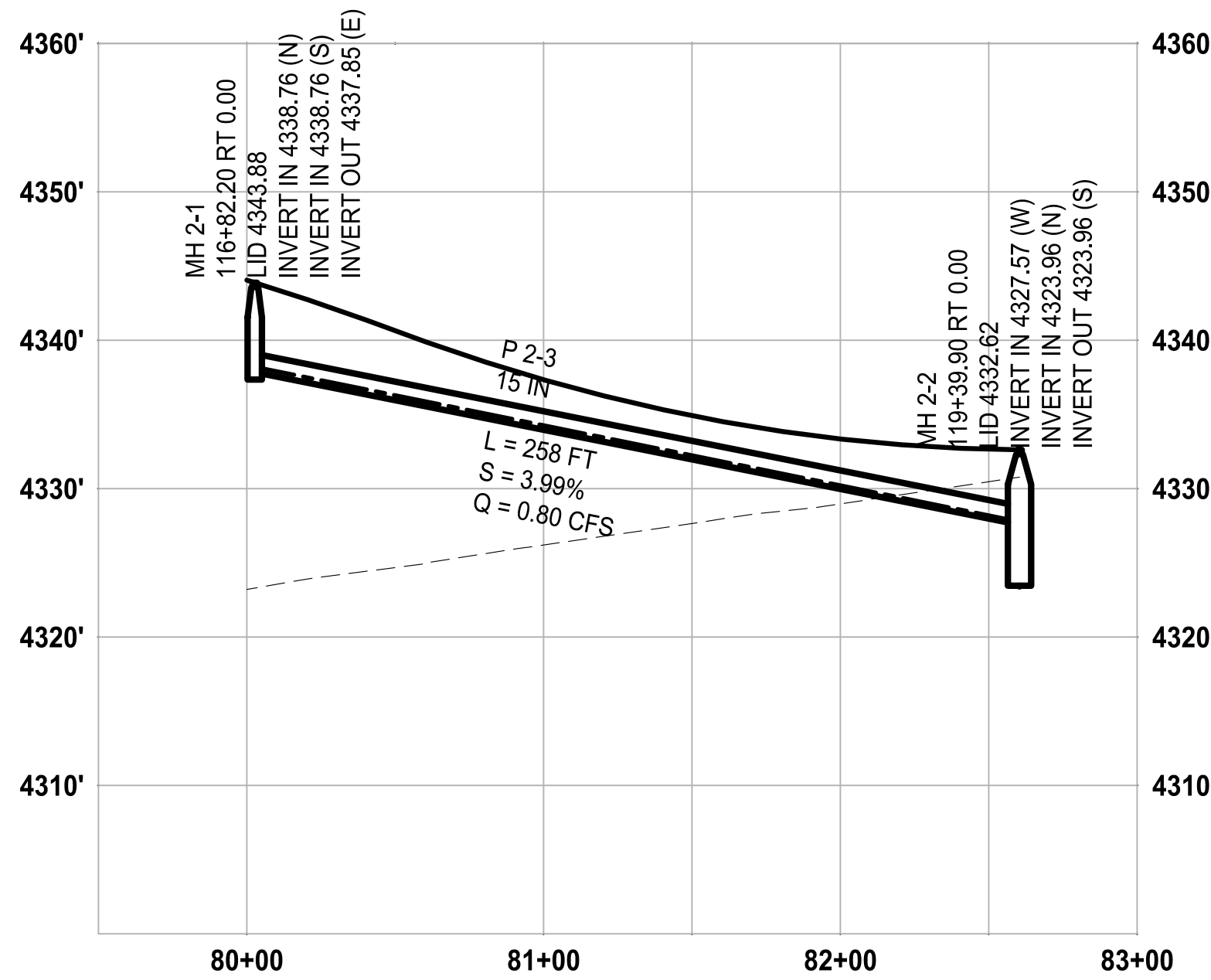
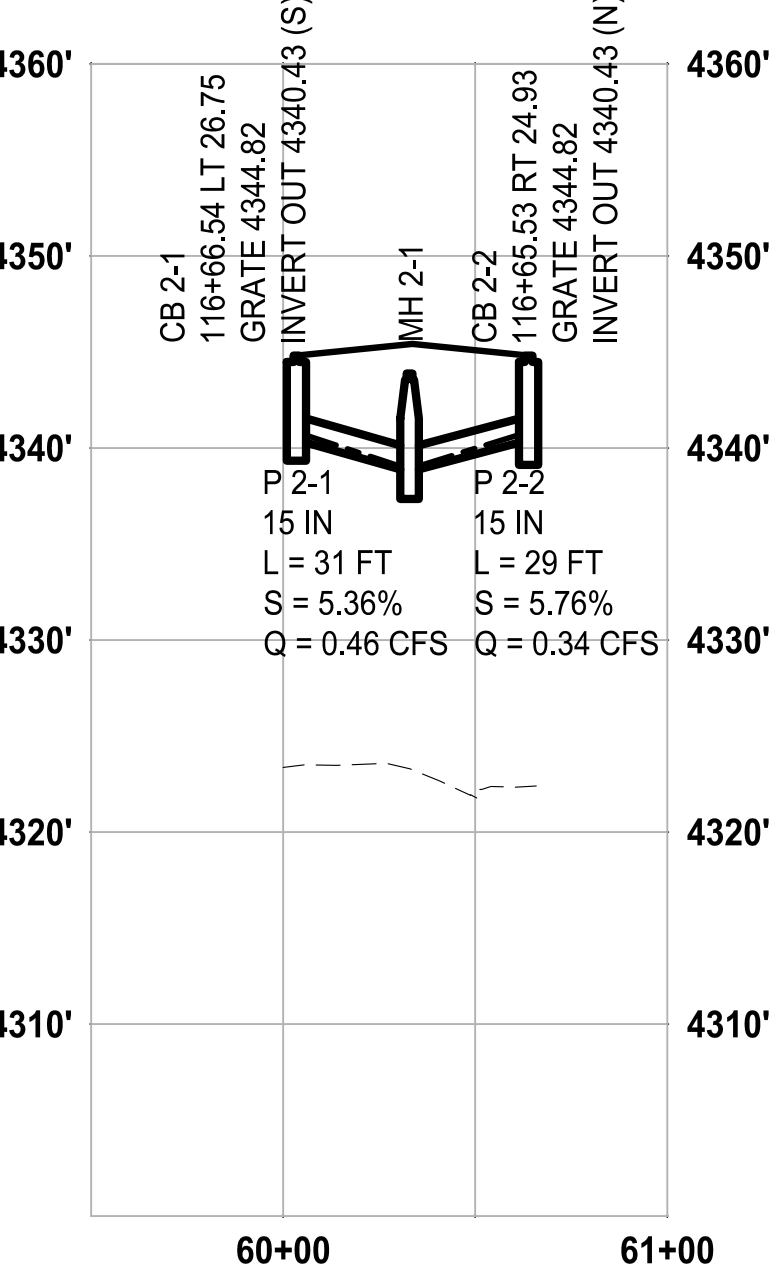
- NOTES FOR CONSTRUCTION:**
- CONSTRUCT CB 2-6 WITH GRADE RING TO BE REMOVED IN PHASE 2 TO BRING GRATE TO LOWER FUTURE GRADE.
 - MH 2-2, MH 2-4, CB 2-3, CB 2-4, CB 2-6, CB 2-7, CB 2-8 TO BE RAISED TO GRADE IN PHASE 2.
 - SEE DR-03 FOR MH 2-4 DETAIL DRAWINGS.

- PRECAST MANHOLE 341.1 - A**
30" FRAME AND COVER - 302
116+82.20 RT 0.00
- PRECAST BOX 332**
44" FRAME AND COVER - 303
119+75.12 LT 33.85
- CLEANOUT BOX 331.1 - 305**
48" GRID GRATE AND FRAME - 310
116+66.54 LT 26.75
116+66.53 RT 24.93
119+76.51 LT 100.33
120+00.67 LT 114.92
120+09.71 RT 75.46
120+69.80 RT 33.91
- DRAINAGE PIPE - 15 INCH, REINFORCED CONCRETE, LEAK-RESISTANT REQD.**
116+66.54 LT 26.75 TO 116+82.20 RT 0.00
116+66.53 RT 24.73 TO 116+82.20 RT 0.00
116+82.20 RT 0.00 TO 119+39.90 RT 0.00
120+00.67 LT 114.92 TO 119+76.51 LT 100.33
119+75.12 LT 33.85 TO 119+12.58 LT 41.28 TO
119+75.19 LT 37.36 TO 119+75.17 LT 31.64 TO
119+39.93 LT 26.75 TO 119+32.11 RT 69.24 TO
120+09.71 RT 75.46 TO 120+69.80 RT 33.91

- RAISE FRAME TO GRADE - 360.1**
119+39.90 RT 0.00
119+39.90 RT 32.75
119+39.93 LT 26.75
119+82.52 LT 39.89
120+13.12 LT 39.96
120+13.76 RT 0.91
120+14.50 RT 41.24
120+61.41 LT 1.72
120+69.80 RT 33.91
- COVER COLLAR - 362**
116+66.54 LT 26.75
116+66.53 RT 24.93
116+82.20 RT 0.00
119+39.90 RT 0.00
119+39.90 RT 32.75
119+39.93 LT 26.75
119+75.12 LT 33.85
119+76.51 LT 100.33
120+00.67 LT 114.92
120+09.71 RT 75.46
120+13.12 LT 39.96
120+13.76 RT 0.91
120+14.50 RT 41.24
120+61.41 LT 1.72
120+69.80 RT 33.91
- REMOVE PIPE**
119+12.58 LT 41.28 TO 119+12.69 LT 37.70
119+12.69 LT 37.70 TO 119+63.66 LT 40.58
119+63.66 LT 40.58 TO 119+68.11 LT 100.89
119+68.11 LT 100.89 TO 119+95.28 LT 101.05
- REMOVE CATCH BASIN**
119+12.69 LT 37.70
119+63.66 LT 40.58
119+68.11 LT 100.89
119+95.28 LT 101.05
- CONNECT NEW STORM DRAIN TO EXISTING STRUCTURE**
119+32.11 RT 69.24 (2 CONNECTIONS)

FOREST STREET PLAN
SCALE: 1" = 50'

- LEGEND**
- PROPOSED STORM DRAIN MANHOLE
 - PROPOSED STORM DRAIN CATCH BASIN
 - PROPOSED STORM DRAIN PIPE
 - EXISTING STORM DRAIN MANHOLE
 - EXISTING SANITARY SEWER MANHOLE
 - EXISTING STORM DRAIN CATCH BASIN
 - EXISTING STORM DRAIN PIPE
 - EXISTING SANITARY SEWER PIPE



- LEGEND**
- FINISHED GRADE
 - - - Existing Grade
 - HGL 25-yr

PIPE PROFILES
HORIZ: 1" = 50'
VERT: 1" = 10'

- UTILITIES LEGEND**
- Ex Fiber Optic
 - Existing Pipe

NOTE:
1. Sizes and depths of existing utilities not provided.

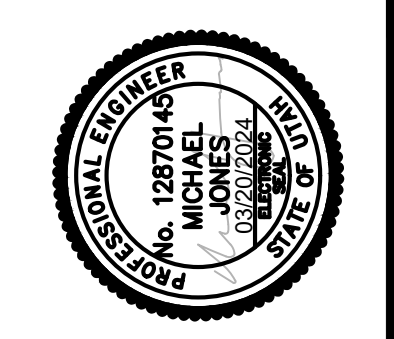
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ONE INCH AT FULL SCALE IF NOT INDICATED OTHERWISE

Parametrix

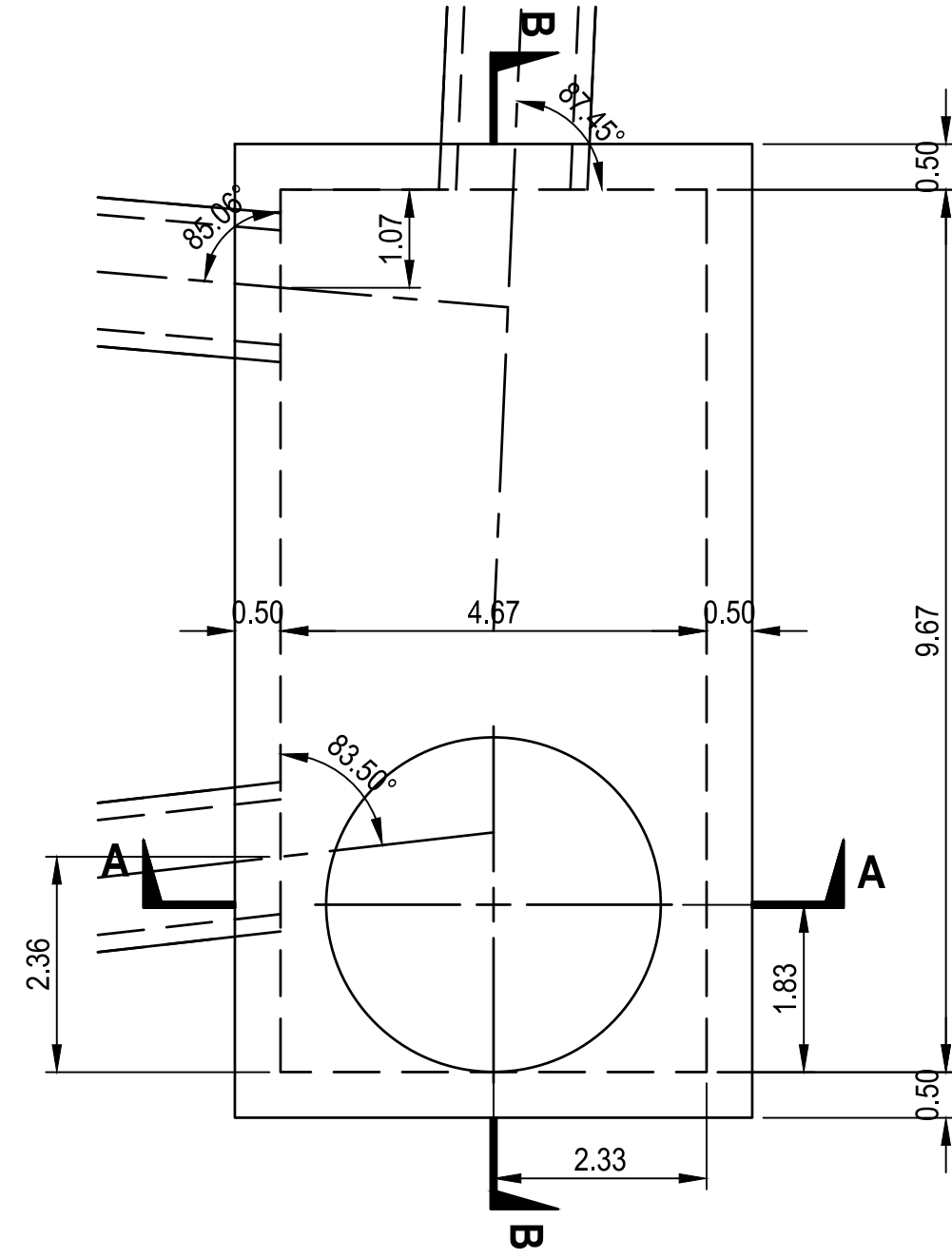
DESIGNED: MCJ
DRAWN: MCJ
CHECKED: JHB
APPROVED: AP

DATE: 03/20/2024
JOB No.: 344-8541-002

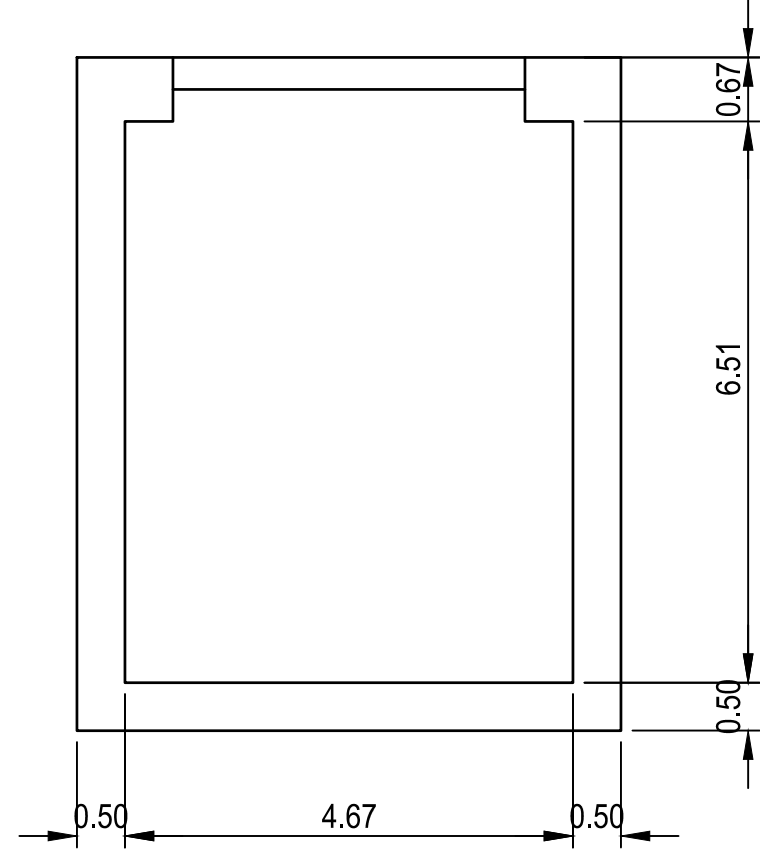


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

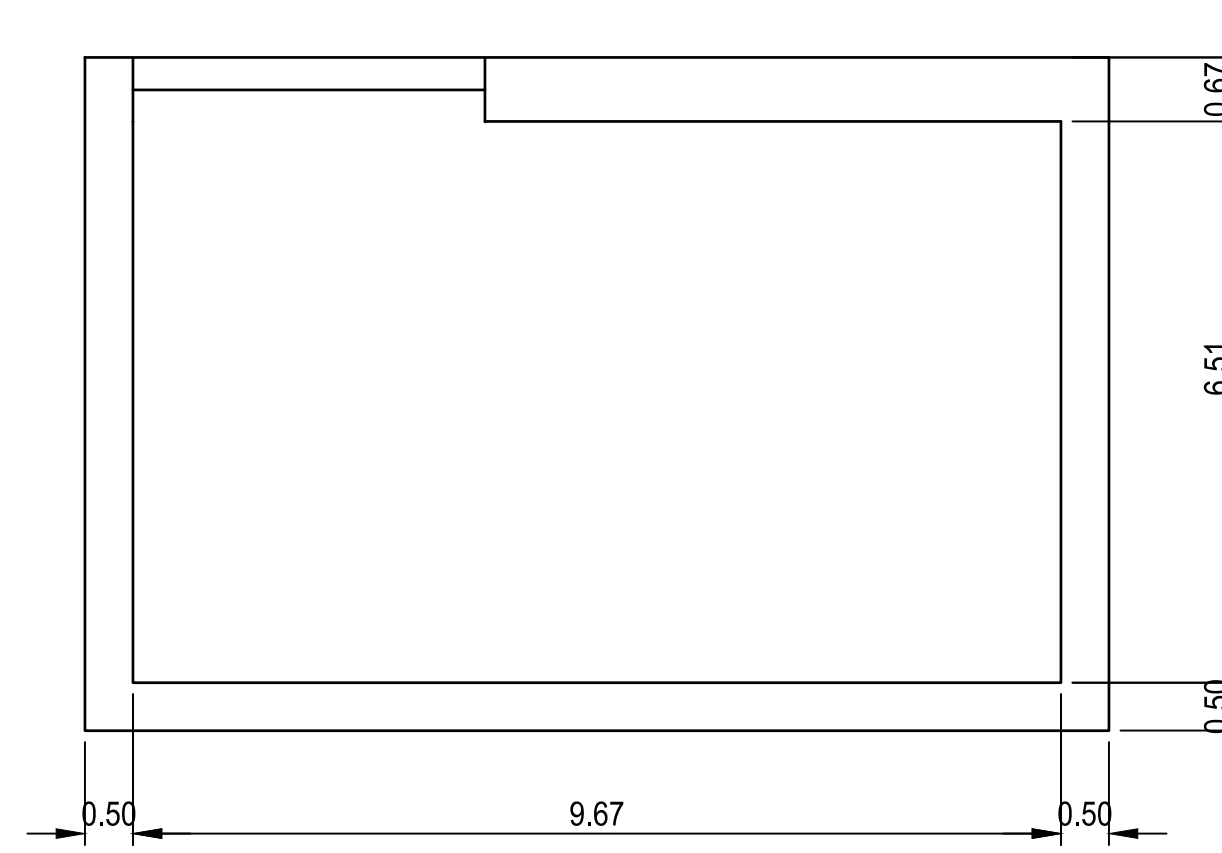
DRAINAGE PLAN AND PROFILE



PLAN



SECTION A-A



SECTION B-B

NOTES

1. USE COATED DEFORMED-CARBON REINFORCING STEEL BARS CONFORMING TO AASHTO M 284 OR M 111 AND M 31 GRADE 60 RESPECTIVELY.
2. FIELD CUT AND BEND REINFORCING STEEL AS NECESSARY TO CLEAR PIPES AND MAINTAIN 2 INCH COVER. REPAIR ANY DAMAGE OR CUTS TO THE COATING ON REINFORCING BARS.
3. USE CLASS AA (AE) CONCRETE.
4. USE TYPE II CEMENT (LOW ALKALI).
5. PROVIDE 2 INCH CONCRETE COVER TO REINFORCING STEEL.
6. PROVIDE 3/4 INCH CHAMFER ON ALL EXPOSED CONCRETE CORNERS.
7. USE APPROVED NON-SHRINK GROUT TO SEAL OPENING AROUND PIP OR USE APPROVED PIPE MANUFACTURER'S PIPE BOOT.
8. SET EDGES OF THE BOX TO MATCH PAVEMENT FINISH GRADE AROUND THE BOX PERIMETER WHEN USING THE BOX AS AN INLET. SET TOP OF SURFACE TO MATCH PAVEMENT CROSS AND LONGITUDINAL SLOPE. RESET ANY BOXES WHERE BOX SURFACE OR GRATE AND FRAME IS NOT FLUSH WITH PAVEMENT. DO NOT EXCEED 1/4 INCH GRATE DEPRESSION.

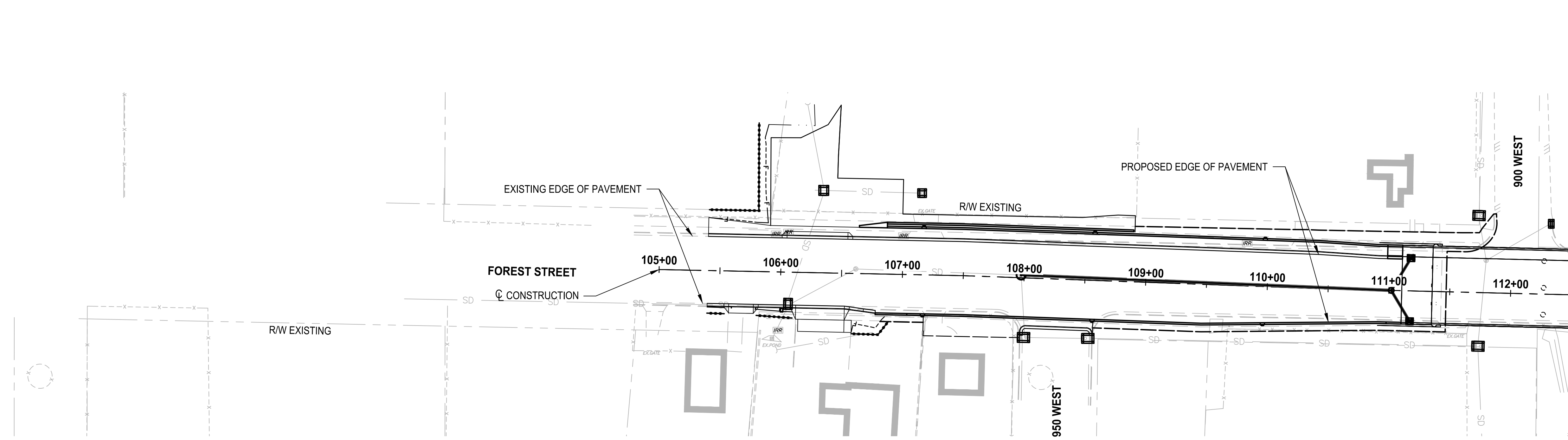
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|--|--|-----------------------------|--|-----------------------------|--|
| REVISIONS | | DATE | | BY | |
| ▲ | | | | | |
| <div style="border: 1px solid black; padding: 5px; display: inline-block;"> ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED ACCORDINGLY </div> | | | | | |
| | | | | | |
| DATE: 03/20/2024 JOB No.: 344-8541-002 | | DESIGNED: MCJ DRAWN: MCJ | | CHECKED: AP APPROVED: AP | |
| | | | | | |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | | | | | |
| DRAINAGE | | | | | |
| DRAWING NO. 54 OF 63 DR-03 | | | | | |

SILT FENCE REQ'D
FOREST ST.
 105+39.93 RT 34.99 TO
 105+54.26 RT 35.19
 105+80.44 RT 36.05 TO
 106+09.99 RT 37.61
 106+58.74 RT 49.52 TO
 106+80.85 RT 49.86 TO
 106+89.10 RT 39.00

DROP-INLET BARRIER -
FIBER ROLL REQ'D
FOREST ST.
 106+06.46 RT 26.06
 108+01.31 RT 48.94
 108+54.14 RT 47.80
 111+7.53 RT 24.92
 111+74.26 RT 44.08

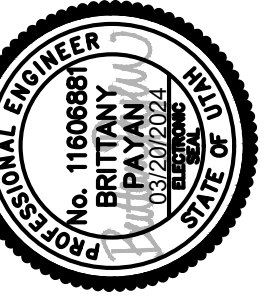
SILT FENCE REQ'D
FOREST ST.
 105+40.33 LT 50.00 TO
 105+80.99 LT 50.00 TO
 105+79.82 LT 122.07

DROP-INLET BARRIER -
FIBER ROLL REQ'D
FOREST ST.
 106+33.97 LT 67.66
 107+14.83 LT 67.10
 111+17.53 LT 26.92
 111+72.58 LT 63.26
 112+32.07 LT 57.87



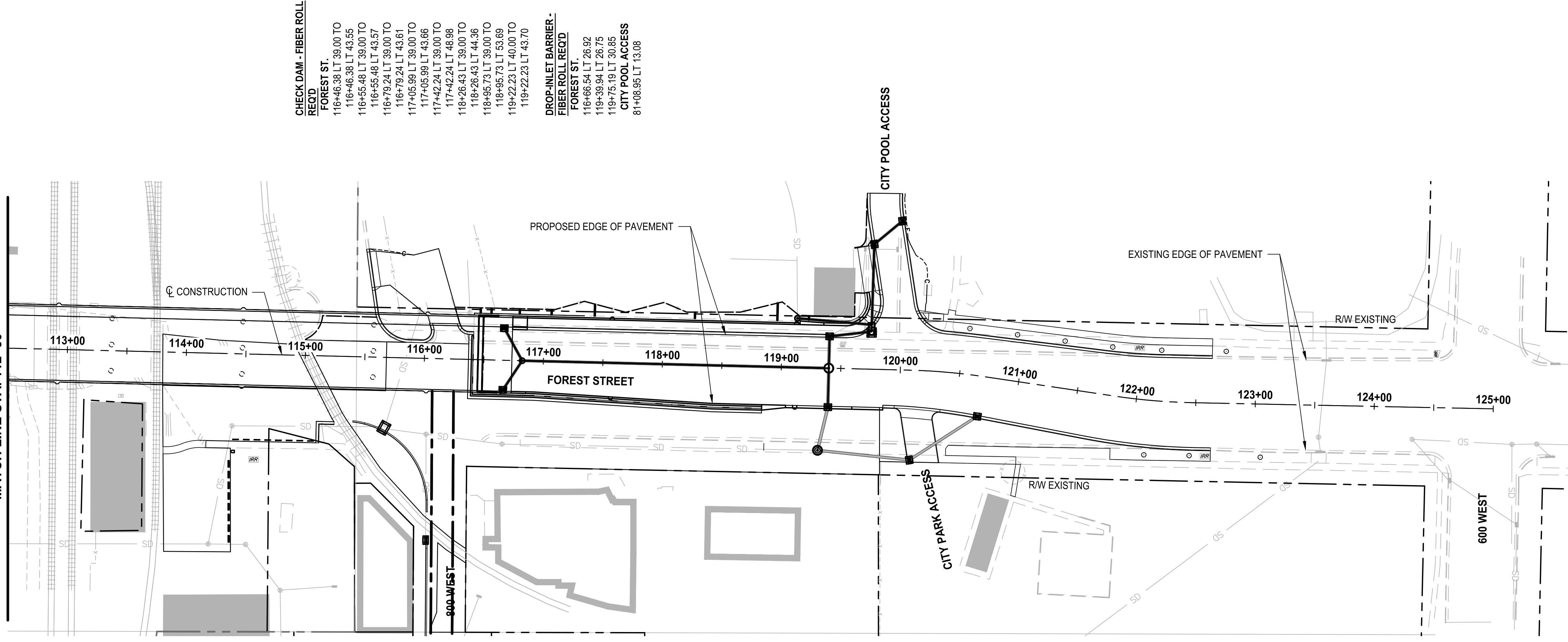
FOREST STREET
EROSION CONTROL
 SCALE: 1" = 50'

MATCH LINE STA. 112+50
 SEE SHEET EC-02



| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
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| | | |

SEE SHEET EC-01
MATCH LINE STA. 112+50



DROP-INLET BARRIER - FIBER ROLL REQD
FOREST ST.
 115+67.38 RT 59.20
 116+04.77 RT 152.64
 116+66.53 RT 24.92
 119+39.90 RT 32.75
 120+09.71 RT 75.46
 120+69.80 RT 33.92
CITY POOL ACCESS
 81+24.71 RT 13.08

CHECK DAM - FIBER ROLL REQD
FOREST ST.
 116+46.38 LT 39.00 TO 116+46.38 LT 43.55
 116+55.48 LT 39.00 TO 116+55.48 LT 43.57
 116+79.24 LT 39.00 TO 116+79.24 LT 43.61
 117+05.99 LT 39.00 TO 117+05.99 LT 43.66
 117+42.24 LT 39.00 TO 117+42.24 LT 48.98
 118+26.43 LT 39.00 TO 118+26.43 LT 44.36
 118+95.73 LT 39.00 TO 118+95.73 LT 53.69
 119+22.23 LT 40.00 TO 119+22.23 LT 43.70
DROP-INLET BARRIER - FIBER ROLL REQD
FOREST ST.
 116+66.54 LT 26.92
 119+39.94 LT 26.75
 119+75.19 LT 30.85
CITY POOL ACCESS
 81+08.95 LT 13.08

**FOREST STREET
EROSION CONTROL**
SCALE: 1" = 50'

PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**



Parametrix

| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | MCJ |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE. IF NOT SHOWN, SCALE ACCORDINGLY.



SIGN CODE LEGEND:
 N - NEW SIGN
 R - RELOCATE SIGN
 X - REMOVE SIGN

XXX
 SIGN CODE
 SIGN NUMBER
 SHEET NUMBER

STRIPING KEY:
 SWL - SOLID WHITE LINE
 SYL - SOLID YELLOW LINE
 DYL - DOUBLE YELLOW LINE
 BWL - BROKEN WHITE LINE
 BYL - BROKEN YELLOW LINE
 DWL - DOTTED WHITE LINE
 LDL - LANE DROP LINE
 S&BYL - SOLID AND BROKEN YELLOW LINE
 SL - STOP LINE
 XLW - CROSS WALK LINE

**REMOVE SIGN REQ'D
 FOREST ST.**
 [1.3-X] 109+77.75 RT 28.33
 [1.5-X] 110+19.08 RT 25.93

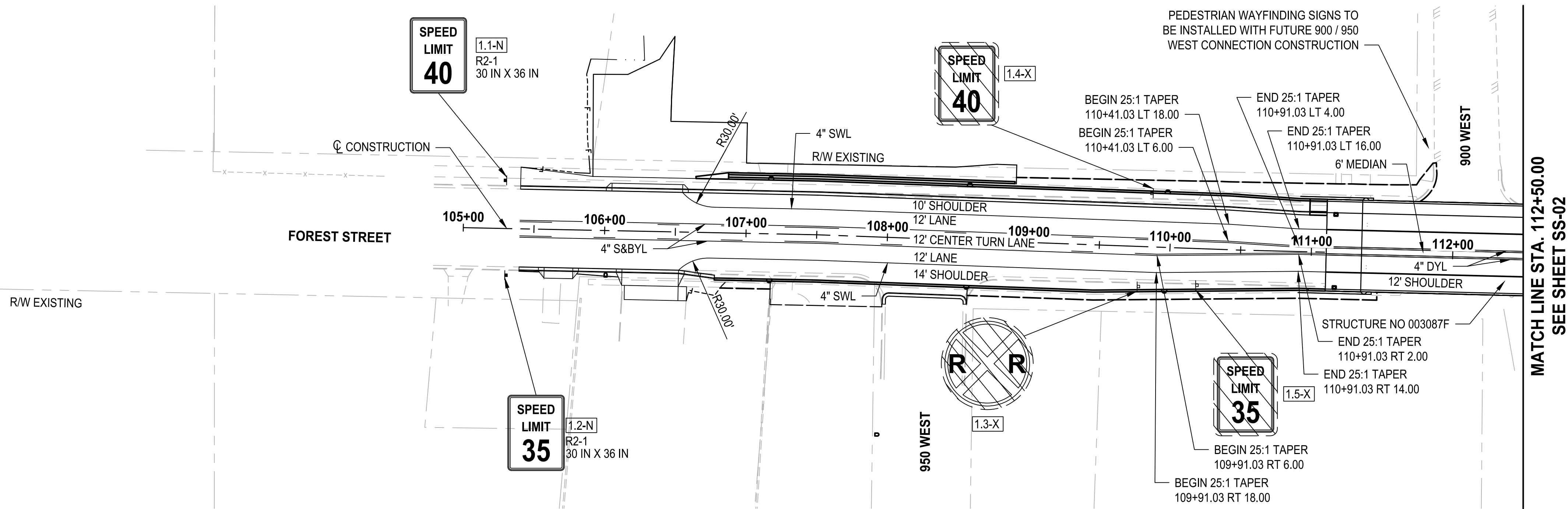
**REGULATORY SIGN, POST,
 AND BASE REQ'D
 FOREST ST.**
 [1.2-N] 106+30.00 RT 33.00

**PAVEMENT MARKING
 PAINT REQ'D
 FOREST ST.**
 (4" SWL)
 106+52.89 RT 27.00 TO
 106+74.32 RT 18.00 TO
 109+91.03 RT 18.00 TO
 110+91.03 RT 14.00 TO
 MATCH LINE RT 14.00
 (4" DYL)
 109+91.03 RT 6.00 TO
 110+91.03 RT 2.00 TO
 MATCH LINE RT 2.00
 (4" S&BYL)
 105+40.00 RT 6.00 TO
 109+91.03 RT 6.00

**REMOVE SIGN REQ'D
 FOREST ST.**
 [1.4-X] 109+86.78 LT 37.84

**REGULATORY SIGN, POST,
 AND BASE REQ'D
 FOREST ST.**
 [1.1-N] 105+30.00 LT 34.00

**PAVEMENT MARKING PAINT
 REQ'D
 FOREST ST.**
 (4" SWL)
 106+54.59 LT 28.00 TO
 106+76.96 LT 18.00 TO
 110+41.03 LT 18.00 TO
 110+91.03 LT 16.00 TO
 MATCH LINE LT 16.00
 (4" DYL)
 109+91.03 LT 6.00 TO
 110+41.03 LT 6.00 TO
 110+91.03 LT 4.00 TO
 MATCH LINE LT 4.00
 (4" S&BYL)
 105+40.00 LT 6.00 TO
 109+91.03 LT 6.00



MATCH LINE STA. 112+50.00
 SEE SHEET SS-02

| | | | |
|--|---|--------------------------------|---------------------------|
| Parametrix | PROJECT NAME BRIGHAM CITY CONNECTION PROJECT | DRAWING NO. 57 OF 63 | SHEET NO. SS-01 |
| CHECKED CCS APPROVED AP | DESIGNED BKP DRAWN BKP | DATE 03/20/2024 | JOB No. 344-8541-002 |
| PROFESSIONAL ENGINEER No. 1160880 BRIGHAM CITY UTAH STATE OF | | | |
| BRIGHAM CITY CONNECTION PROJECT SIGNING AND STRIPING | | | |
| REVISIONS DATE BY | | | |
| ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY | | | |

SIGN CODE LEGEND:
 N - NEW SIGN
 R - RELOCATE SIGN
 X - REMOVE SIGN

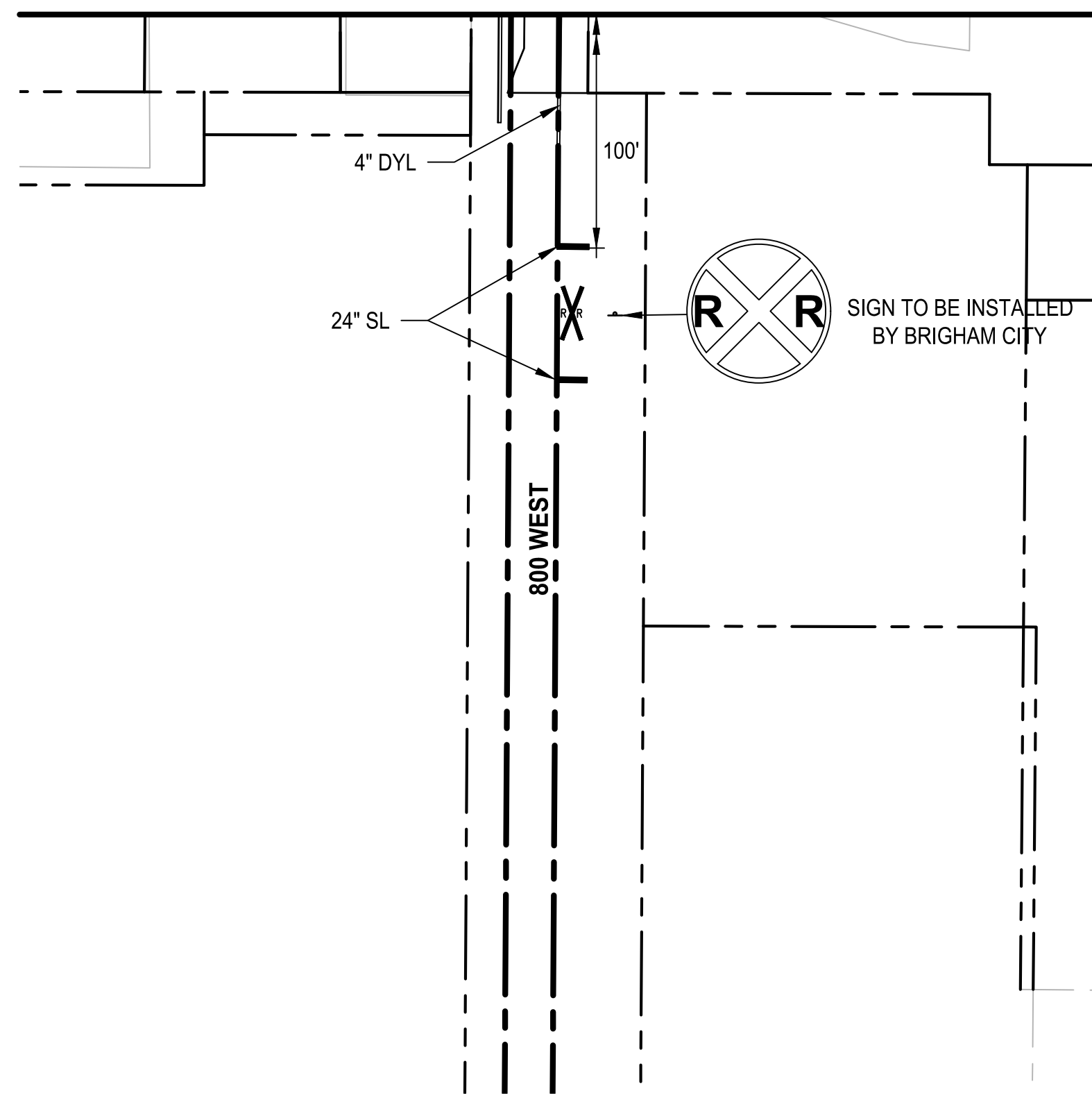
XXX
 SIGN CODE
 SIGN NUMBER
 SHEET NUMBER

STRIPING KEY:
 SWL - SOLID WHITE LINE
 SYL - SOLID YELLOW LINE
 DYL - DOUBLE YELLOW LINE
 BWL - BROKEN WHITE LINE
 BYL - BROKEN YELLOW LINE
 DWL - DOTTED WHITE LINE
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 S&BYL - SOLID AND BROKEN YELLOW LINE
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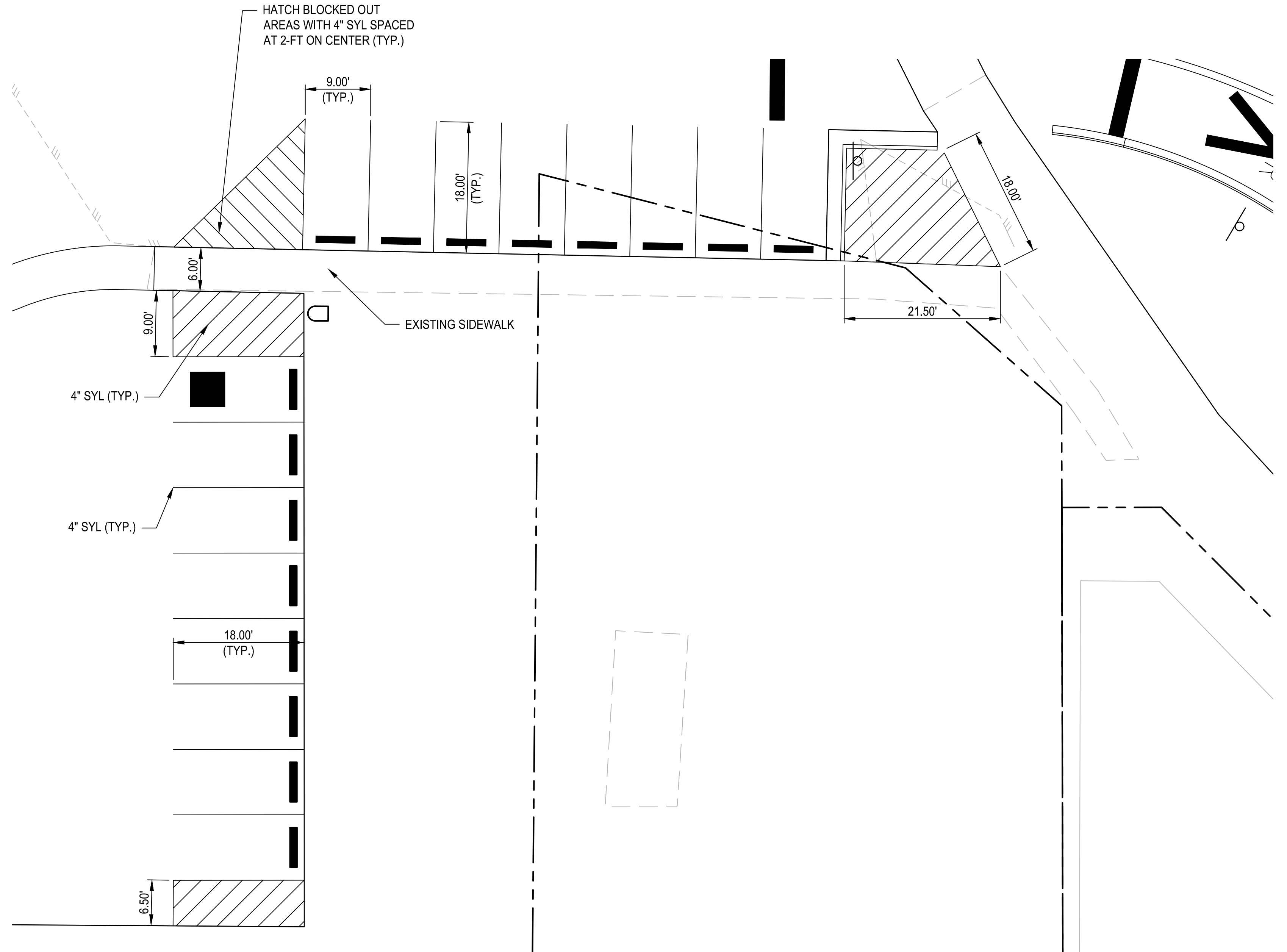
**PAVEMENT MARKING
 PAINT REQ'D**
FOREST ST.
 MATCHLINE RT 200.00 TO
 116+29.46 RT 260.95
 (24" SL, 76 RT, 937.01 TO
 116+29.46 RT 260.95)
 116+30.26 RT 337.01 TO
 116+42.26 RT 338.89

**PAVEMENT SYMBOL PAINT
 REQ'D**
FOREST ST.
 (RR CROSSING)
 116+36.01 RT 311.95

SEE SHEET SS-02
 MATCH LINE OFF. 200-FT



R X R
 SIGN TO BE INSTALLED
 BY BRIGHAM CITY

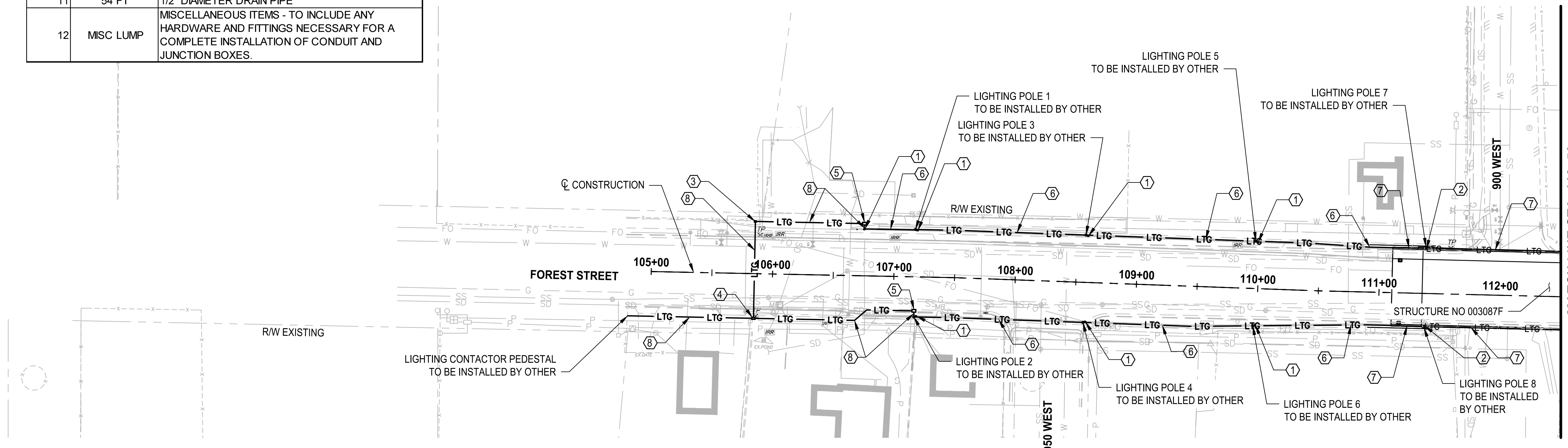


**FOREST STREET
 PARKING LOT DETAIL**
 SCALE 1" = 10'

| | |
|-------------------------------------|---|
| BY | |
| DATE | |
| REVISIONS | |
| ONE INCH AT FULL SCALE IF NECESSARY | |
| Parametrix | |
| DATE | DESIGNED |
| 03/20/2024 | BKP |
| JOB No. | DRAWN |
| 344-8541-002 | BKP |
| CHECKED | APPROVED |
| CCS | AP |
| | |
| PROJECT NAME | BRIGHAM CITY CONNECTION PROJECT |
| SIGNING AND STRIPING | |
| DRAWING NO. | 59 OF 63 |
| SS-03 | |

LAYOUT: LT-01
 PATH: u:\Sot\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995secs\CADD\DWG\Civil
 PLOTTED BY: Olivestio DATE: Friday, March 22, 2024 12:34:34 PM

| MATERIALS TO BE FURNISHED AND INSTALLED BY CONTRACTOR | | |
|---|-----------|---|
| ITEM | QTY. | DESCRIPTION |
| 1 | 12 EACH | LIGHTING JUNCTION BOX BUILT IN BARRIER |
| 2 | 8 EACH | LIGHTING JUNCTION BOX BUILT IN PARAPET |
| 3 | 11 EACH | LIGHTING JUNCTION BOX TYPE I - WITH "LIGHTING" LOGO IMPRINTED ON LID |
| 4 | 2 EACH | LIGHTING JUNCTION BOX TYPE II - WITH "LIGHTING" LOGO IMPRINTED ON LID |
| 5 | 3 EACH | LIGHTING JUNCTION BOX TYPE III - WITH DIAMOND PLATE STEEL TRAFFIC RATED LID AND "LIGHTING" LOGO IMPRINTED ON LID |
| 6 | 2913 FT | 1 1/2" PVC CONDUIT CONSTRUCTED IN BARRIER |
| 7 | 1457 FT | 2" PVC CONDUIT CONSTRUCTED IN BARRIER |
| 8 | 2475 FT | 1 1/2" PVC CONDUIT CONSTRUCTED IN PARAPET |
| 9 | 1238 FT | 2" PVC CONDUIT CONSTRUCTED IN PARAPET |
| 10 | 2698 FT | 3" SCHEDULE 40 GRAY PVC CONDUIT |
| 11 | 54 FT | 1/2" DIAMETER DRAIN PIPE |
| 12 | MISC LUMP | MISCELLANEOUS ITEMS - TO INCLUDE ANY HARDWARE AND FITTINGS NECESSARY FOR A COMPLETE INSTALLATION OF CONDUIT AND JUNCTION BOXES. |



FOREST STREET LIGHTING PLAN
 SCALE: 1" = 50'

- LEGEND**
- JUNCTION BOX
 - LIGHTING POLE
 - LTG — LIGHTING CONDUIT
 - ⓧ PROJECT NOTE NUMBER

- NOTES:**
- ① LIGHTING JUNCTION BOX BUILT IN BARRIER (SEE LT-03 FOR LIGHTING DETAILS)
 - ② LIGHTING JUNCTION BOX BUILT IN PARAPET (SEE STRUCTURE DETAILS)
 - ③ LIGHTING JUNCTION BOX TYPE I (SEE APWA STANDARD PLAN 731)
 - ④ LIGHTING JUNCTION BOX TYPE II (SEE APWA STANDARD PLAN 731)
 - ⑤ LIGHTING JUNCTION BOX TYPE III (SEE APWA STANDARD PLAN 731)
 - ⑥ CONSTRUCTED IN BARRIER (SEE LT-03 FOR LIGHTING DETAILS)
 2 - 1 1/2" PVC CONDUITS AND 1 - 2" PVC CONDUITS
 - ⑦ CONSTRUCTED IN PARAPET (SEE STRUCTURE DETAILS)
 2 - 1 1/2" PVC CONDUITS AND 1 - 2" PVC CONDUITS
 - ⑧ 2 - 3" SCHEDULE 40 GRAY PVC CONDUIT
1. ILLUMINATION FIXTURES, WIRING, AND OTHER APPURTENANCES RELATED TO LIGHTING SERVICE AND CONNECTION TO BE INSTALLED BY BRIGHAM CITY SUBSEQUENT TO CONDUIT AND JUNCTION BOX INSTALLATION.
 2. THE CONDUIT LAYOUT SHOWN IS SCHEMATIC. CONTRACTOR SHALL COORDINATE LAYOUT WITH BRIGHAM CITY PUBLIC POWER DIRECTOR PRIOR TO INSTALLATION.
 3. REFER TO APWA STANDARD PLANS AND SPECIFICATIONS FOR CONSTRUCTION DETAILS AND INSTALLATION REQUIREMENTS.
 4. LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE. FIELD VERIFY THE EXACT UTILITY LOCATIONS PRIOR TO CONSTRUCTION AND BRING ANY CONFLICTS TO THE ATTENTION OF THE ENGINEER.
 5. CONTRACTOR SHALL COORDINATE WITH FIBER OPTIC UTILITY OWNER FOR CONFLICTS WITH FIBER OPTIC LINE.

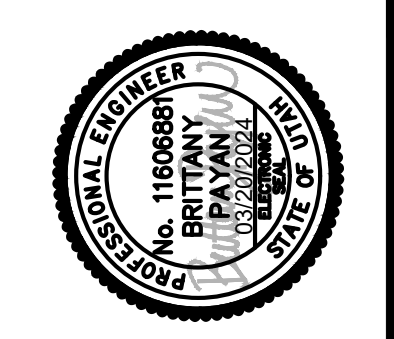
MATCH LINE STA. 112+50
 SEE SHEET LT-02

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT SHOWN OTHERWISE

Parametrix

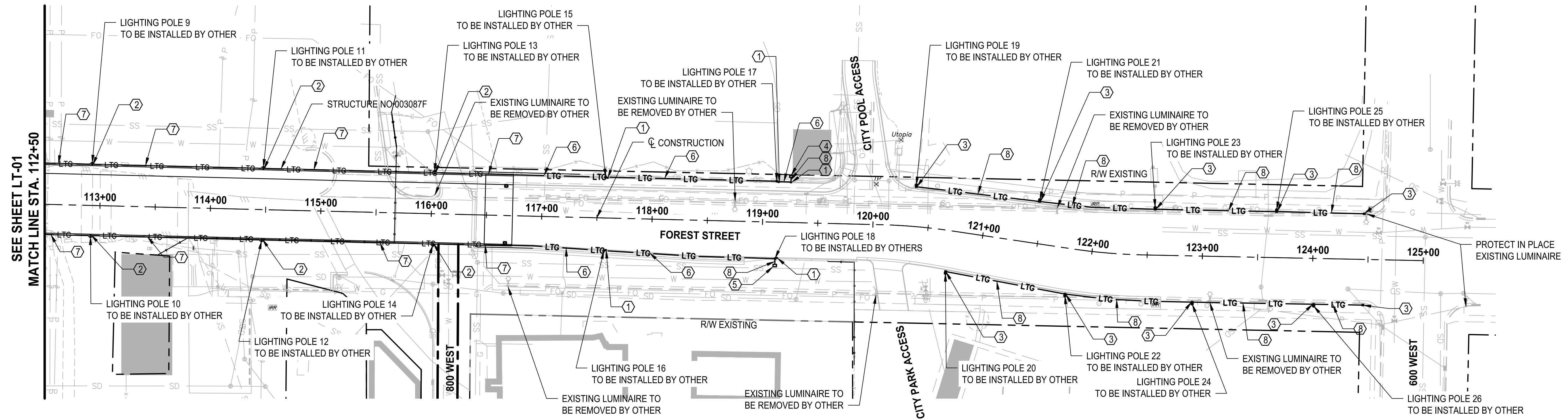
DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: BKP
 DRAWN: BKP
 CHECKED: CCS
 APPROVED: AP



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

LIGHTING

LAYOUT: LT-02 PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St. Final Design\995Secs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:34:54 PM



**FOREST STREET
LIGHTING PLAN**
SCALE: 1" = 50'

- LEGEND**
- JUNCTION BOX
 - LIGHTING POLE
 - LTG — LIGHTING CONDUIT
 - ⓧ PROJECT NOTE NUMBER

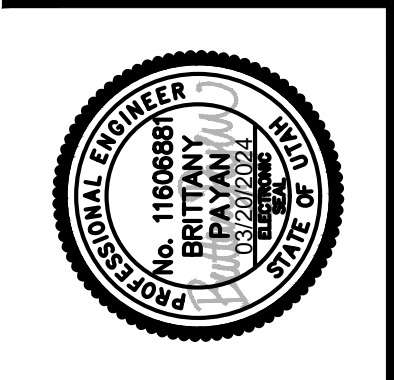
- NOTES:**
- ① LIGHTING JUNCTION BOX BUILT IN BARRIER (SEE LT-03 FOR LIGHTING DETAILS)
 - ② LIGHTING JUNCTION BOX BUILT IN PARAPET (SEE STRUCTURE DETAILS)
 - ③ LIGHTING JUNCTION BOX TYPE I (SEE APWA STANDARD PLAN 731)
 - ④ LIGHTING JUNCTION BOX TYPE II (SEE APWA STANDARD PLAN 731)
 - ⑤ LIGHTING JUNCTION BOX TYPE III (SEE APWA STANDARD PLAN 731)
 - ⑥ CONSTRUCTED IN BARRIER (SEE LT-03 FOR LIGHTING DETAILS)
 - ⑦ CONSTRUCTED IN PARAPET (SEE STRUCTURE DETAILS)
 - ⑧ 2 - 3" SCHEDULE 40 GRAY PVC CONDUIT
1. ILLUMINATION FIXTURES, WIRING, AND OTHER APPURTENANCES RELATED TO LIGHTING SERVICE AND CONNECTION TO BE INSTALLED BY BRIGHAM CITY SUBSEQUENT TO CONDUIT AND JUNCTION BOX INSTALLATION.
 2. THE CONDUIT LAYOUT SHOWN IS SCHEMATIC. CONTRACTOR SHALL COORDINATE LAYOUT WITH BRIGHAM CITY PUBLIC POWER DIRECTOR PRIOR TO INSTALLATION.
 3. REFER TO APWA STANDARD PLANS AND SPECIFICATIONS FOR CONSTRUCTION DETAILS AND INSTALLATION REQUIREMENTS.
 4. LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE. FIELD VERIFY THE EXACT UTILITY LOCATIONS PRIOR TO CONSTRUCTION AND BRING ANY CONFLICTS TO THE ATTENTION OF THE ENGINEER.
 5. CONTRACTOR SHALL COORDINATE WITH FIBER OPTIC UTILITY OWNER FOR CONFLICTS WITH FIBER OPTIC LINE.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY

Parametrix

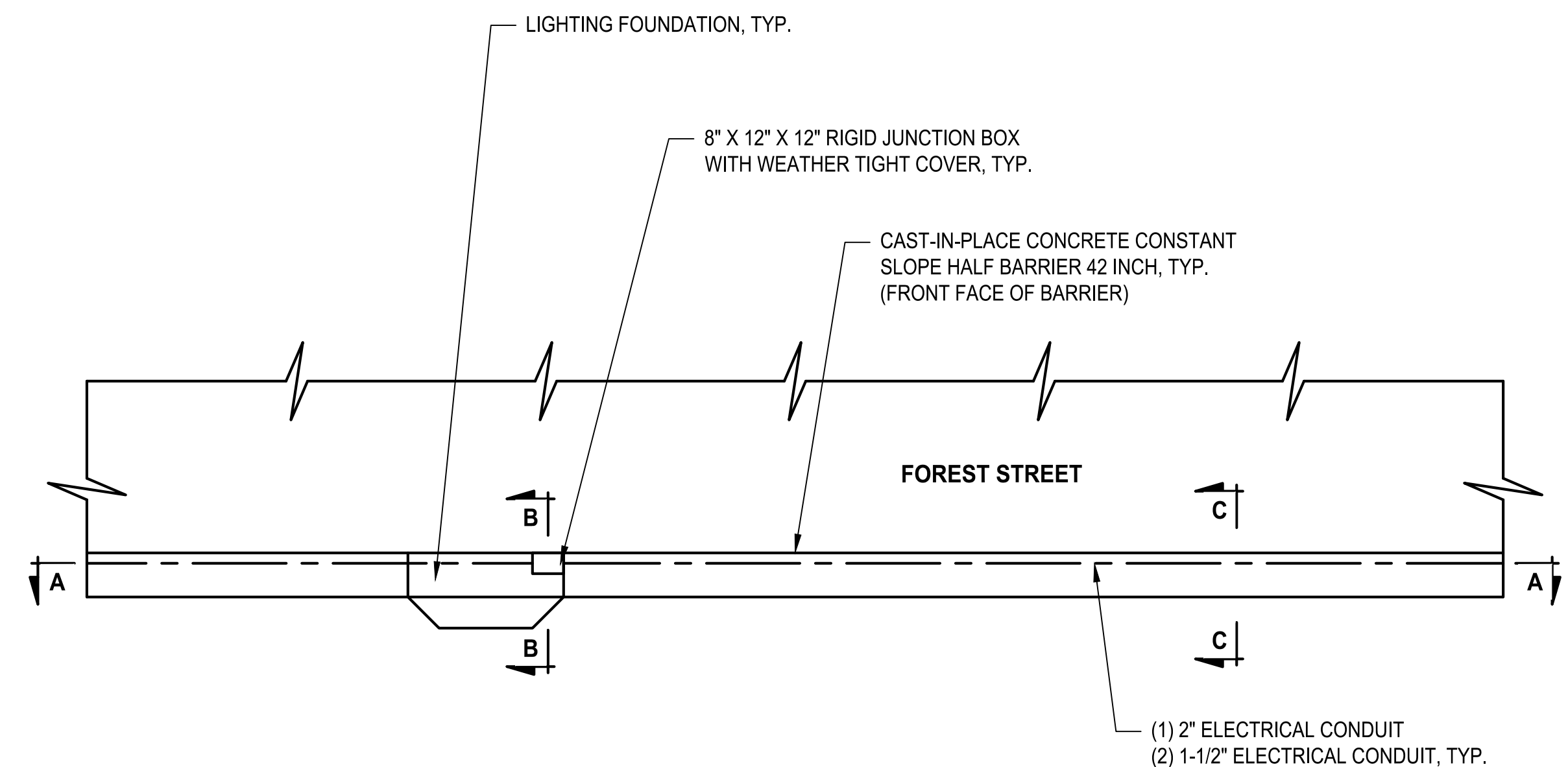
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|-----------------------|---------------|--------------|
| DATE: 03/20/2024 | DESIGNED: BKP | CHECKED: CCS |
| JOB No.: 344-8541-002 | DRAWN: BKP | APPROVED: AP |



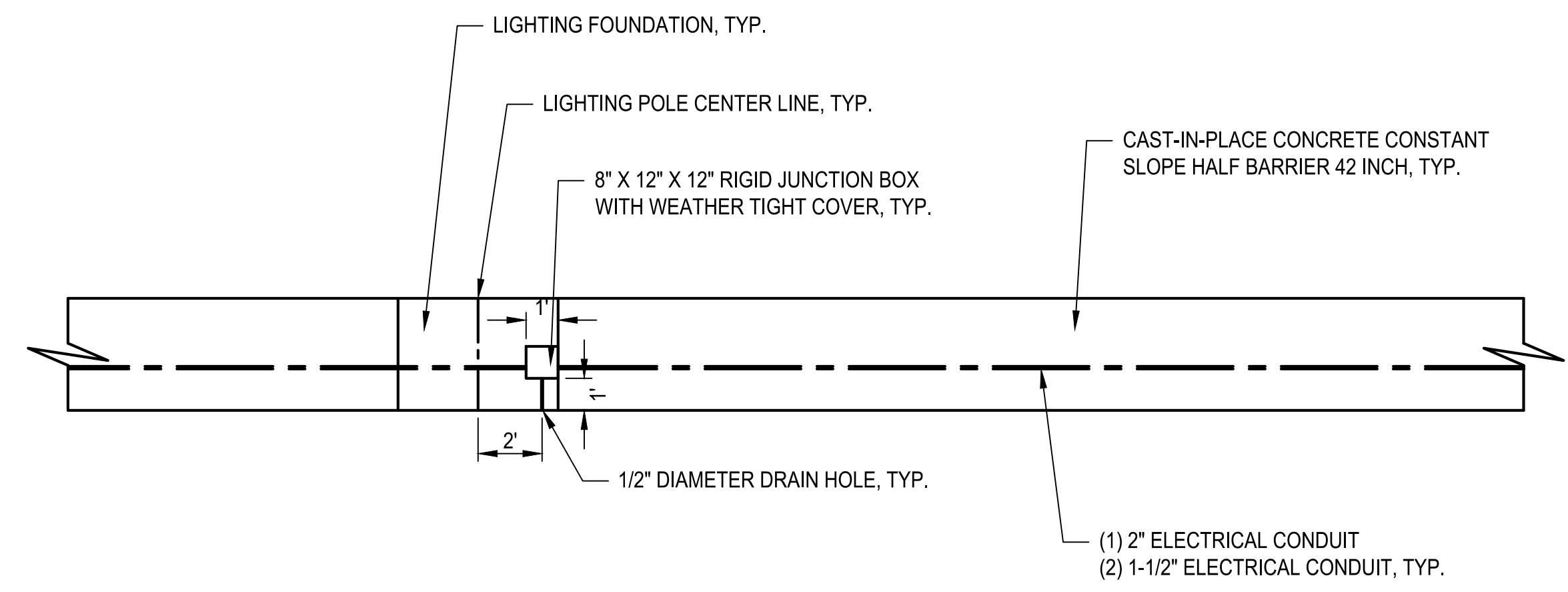
**BRIGHAM CITY
CONNECTION PROJECT**

LIGHTING

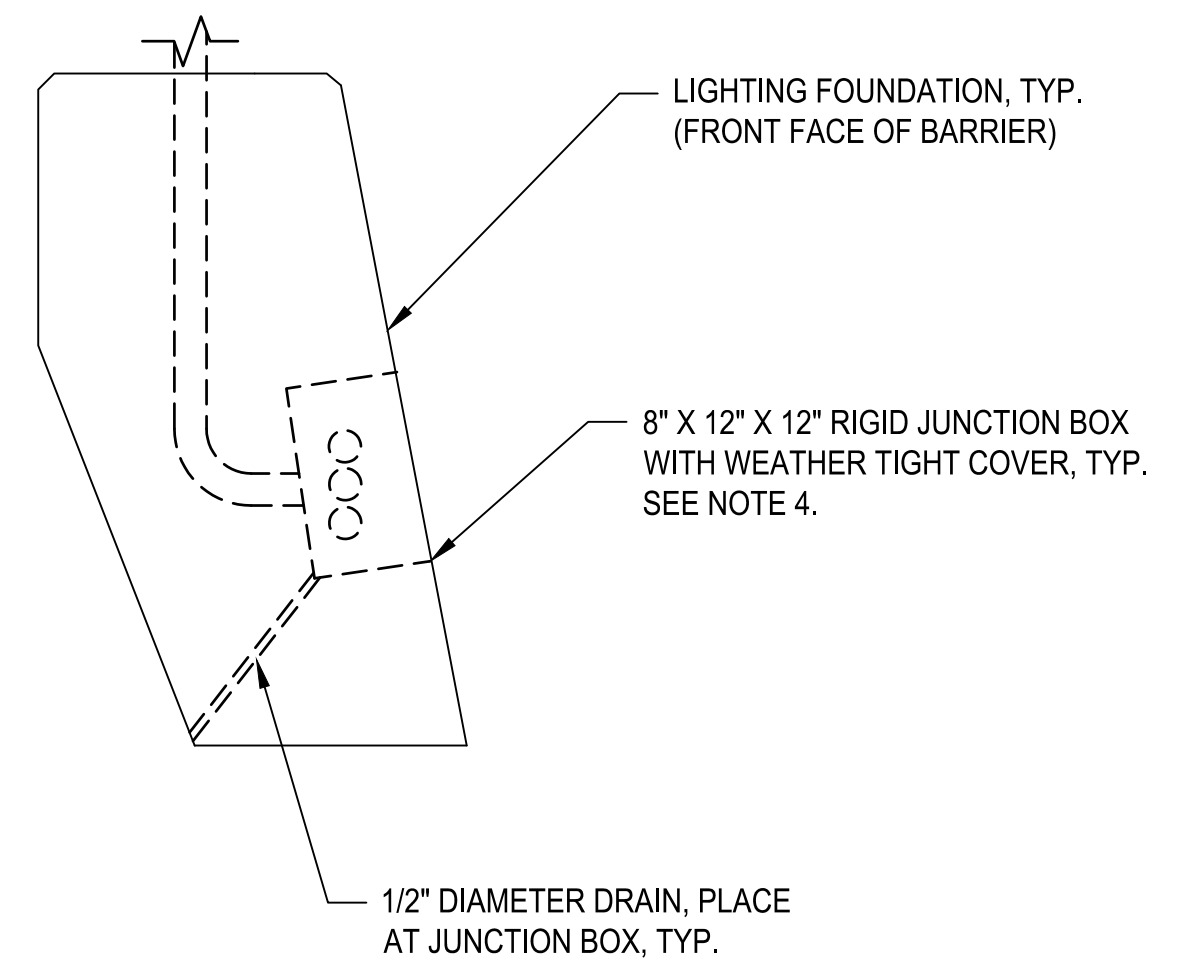
LAYOUT: LT-03 PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St. Final Design\995secs\CADD\DWG\Civil PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 12:35:09 PM



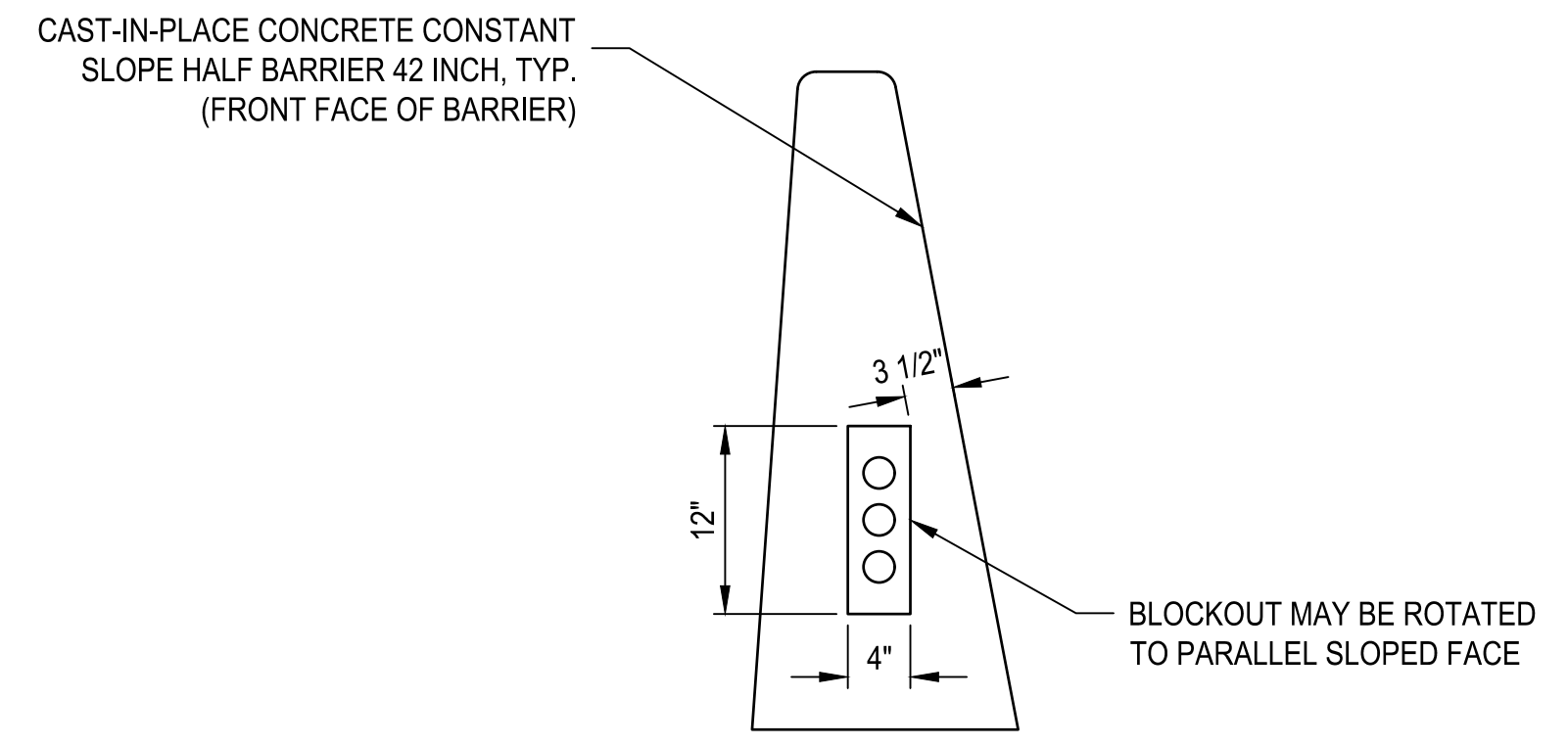
TYPICAL PLAN VIEW



SECTION A-A



SECTION B-B



SECTION C-C

**FOREST STREET
LIGHTING DETAILS**
NOT TO SCALE

NOTES:

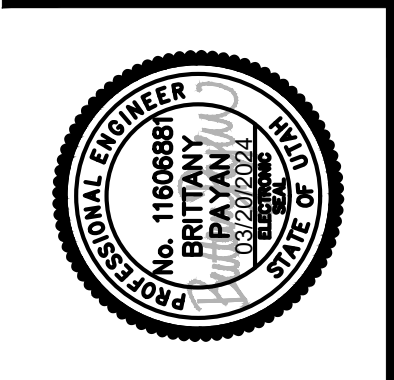
1. LOCK ELECTRICAL CONDUIT AT JUNCTION BOXES WITH DOUBLE LOCK NUTS.
2. PROVIDE ALL WORK CONFORMING WITH THE LATEST EDITIONS OF THE NATIONAL ELECTRICAL CODE AND UNDERWRITERS LABORATORIES INC. STANDARDS WHERE APPLICABLE.
3. CUT OR BEND REBAR IN OUTSIDE FACE OF BARRIER TO ALLOW INSTALLATION OF JUNCTION BOX ON VERTICAL FACE OF BARRIER.
4. SEE UDOT SPECIFICATION 16526 FOR ALL BUILT IN RIGID JUNCTION BOXES.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NECESSARY

Parametrix

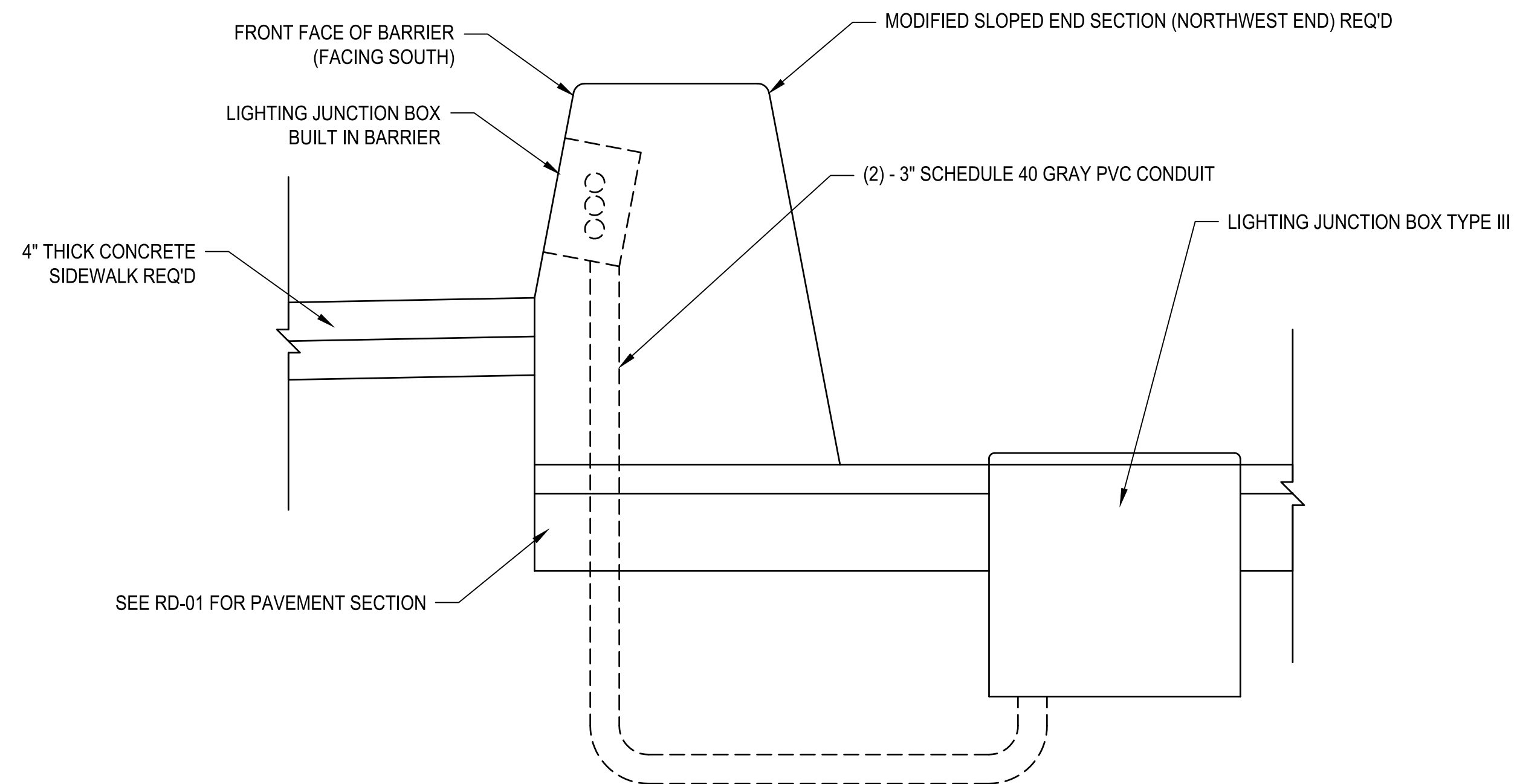
| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | CCS |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



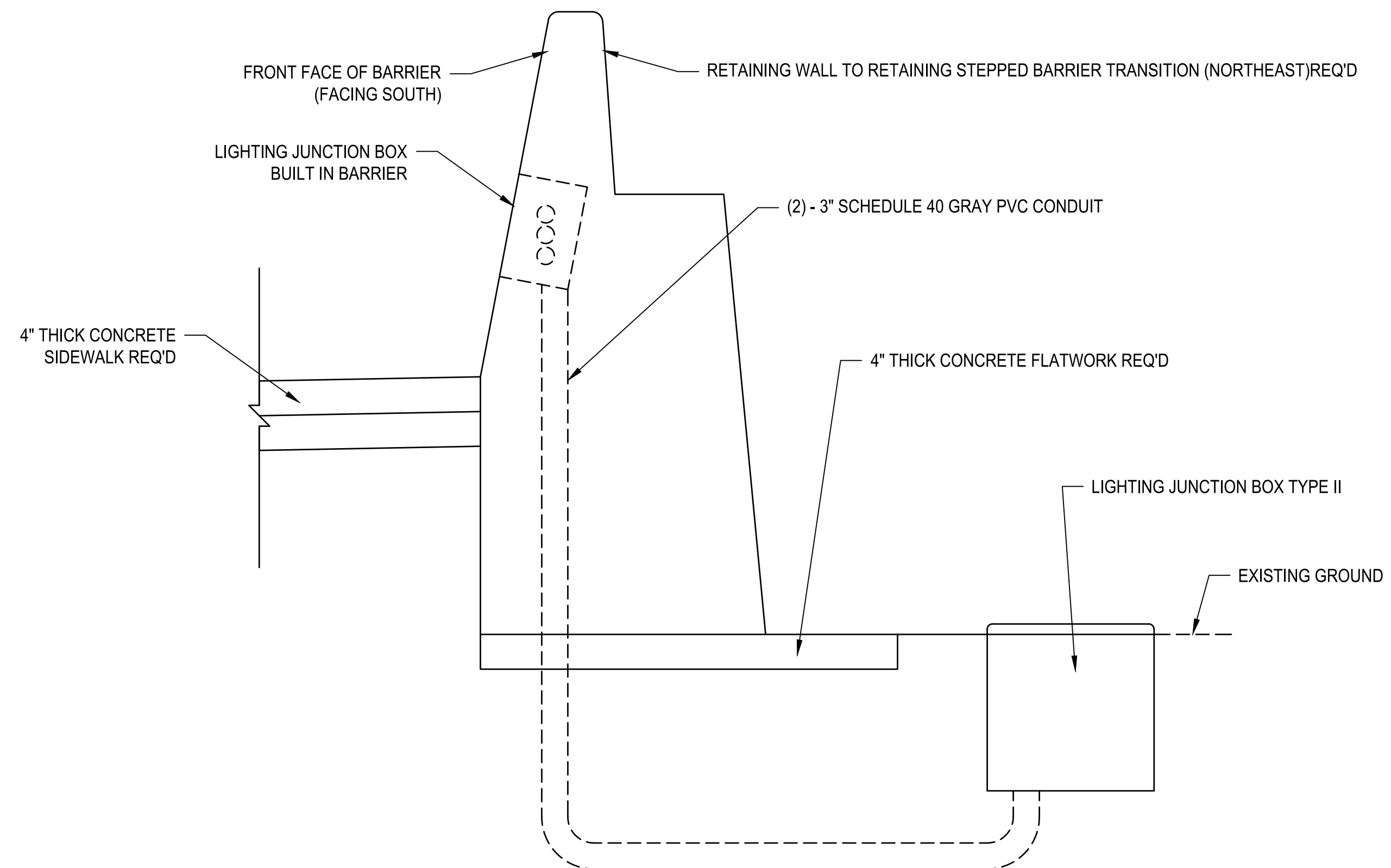
PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

LIGHTING

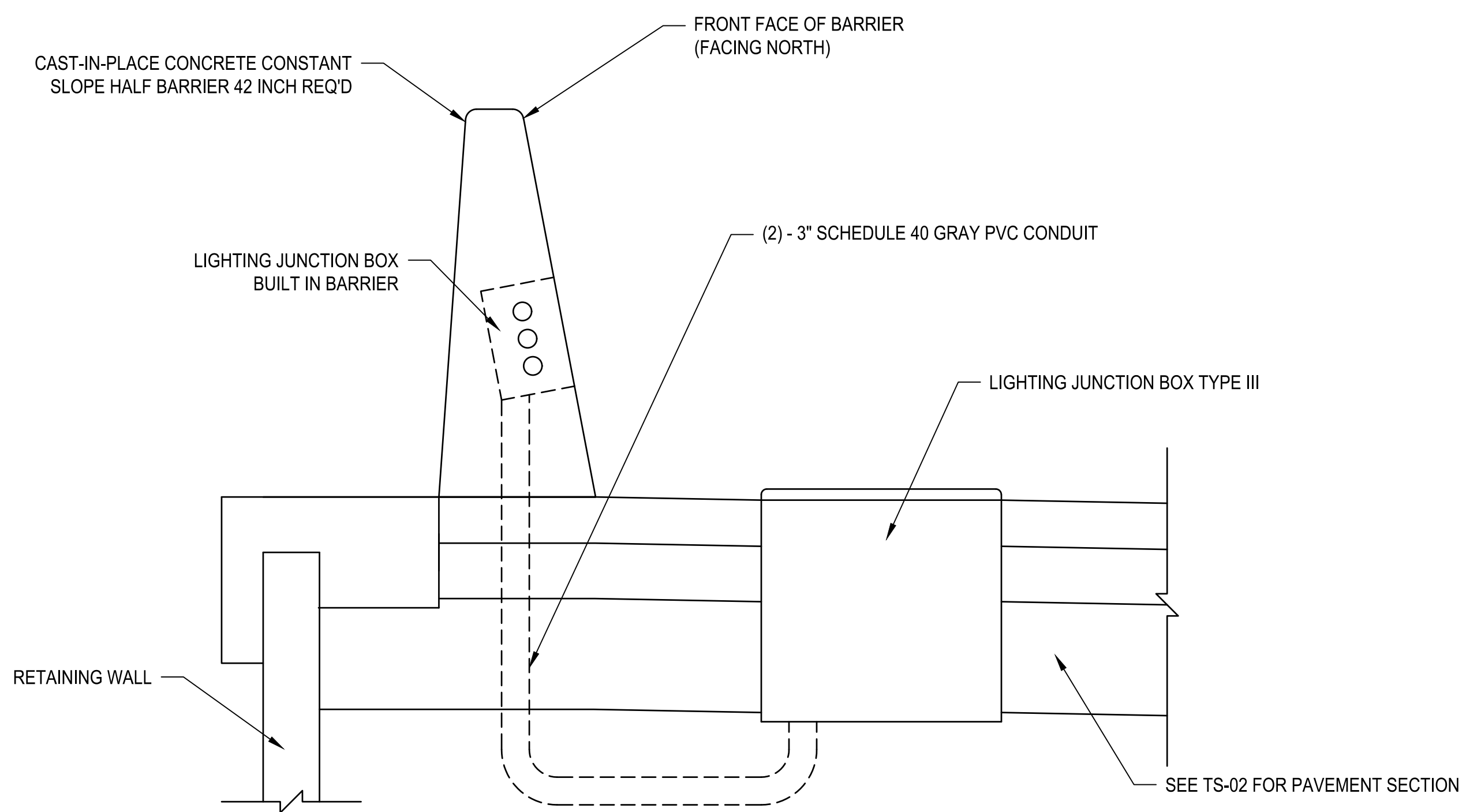
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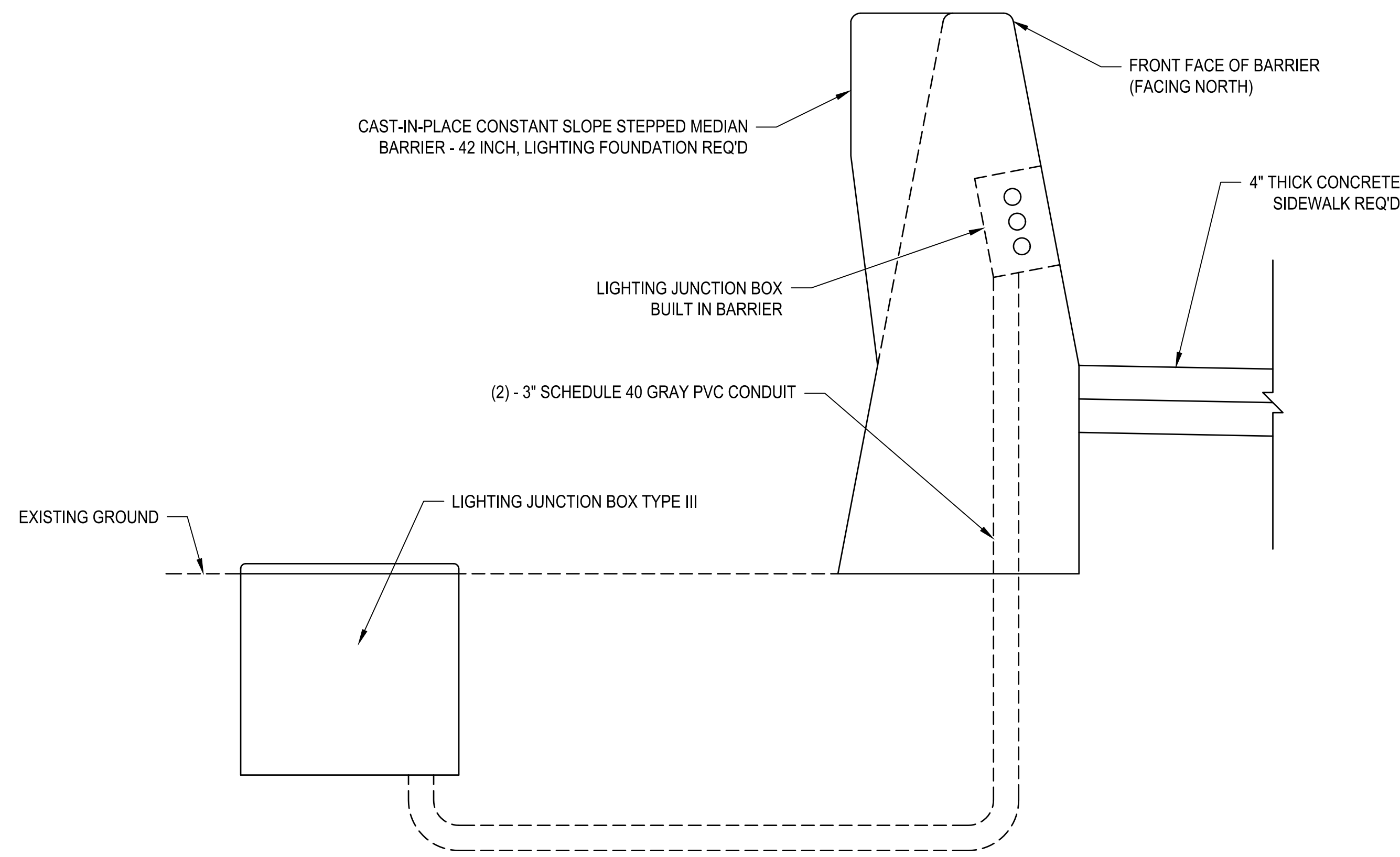
NORTHWEST CORNER BARRIER CONDUIT TERMINAL



NORTHEAST CORNER BARRIER CONDUIT TERMINAL



SOUTHWEST CORNER BARRIER CONDUIT TERMINAL



SOUTHEAST CORNER BARRIER CONDUIT TERMINAL

**FOREST STREET
 LIGHTING DETAILS**
 NOT TO SCALE

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | BKP | CCS |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | BKP | AP |



PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

LIGHTING

LAYOUT: S01
 PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995secs\CADD\DWG\Structure
 PLOTTED BY: OliveSlo DATE: Friday, March 22, 2024 9:37:12 AM

GENERAL NOTES

- USE COATED DEFORMED CARBON STEEL BARS CONFORMING TO AASHTO M111 OR ASTM A775, AND AASHTO M31 GRADE 60, UNLESS SHOWN OTHERWISE.
- USE COATED DEFORMED CARBON STEEL BARS CONFORMING TO AASHTO M111 OR ASTM A775, AND ASTM A706 GRADE 60 IN CONCRETE COLUMNS.
- USE UNCOATED DEFORMED LOW CARBON CHROMIUM STEEL BARS CONFORMING TO ASTM A1035-CM AND ASTM A1035-CS/AASHTO M334 IN CONCRETE DECK AND PARAPETS.
- USE STRUCTURAL STEEL CONFORMING TO ASTM A36, UNLESS SHOWN OTHERWISE.
- CHAMFER EXPOSED CONCRETE CORNERS 3/4 INCH UNLESS SHOWN OTHERWISE.
- PROVIDE 2 INCH MINIMUM CONCRETE COVER TO REINFORCING STEEL UNLESS SHOWN OTHERWISE.
- VERIFY UTILITY LOCATIONS BEFORE CONSTRUCTION. PROTECT EXISTING UTILITIES IN PLACE UNLESS SHOWN OTHERWISE.
- COAT OR GALVANIZE MISCELLANEOUS STEEL PLACED IN STRUCTURAL CONCRETE, UNLESS SHOWN OTHERWISE.
- DO NOT SCALE DRAWINGS. HORIZONTAL DIMENSIONS ARE PLAN. VERTICAL DIMENSIONS ARE PLUMB.

DESIGN DATA:

HL-93 LOADING IN ACCORDANCE WITH AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION 2020 AND THE UDOT STRUCTURES DESIGN AND DETAILING MANUAL, 2022. SEISMIC DESIGN IN ACCORDANCE WITH AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN, 2ND EDITION WITH 2011 AND 2014 INTERIM REVISIONS. LOAD RATING IN ACCORDANCE WITH THE MANUAL FOR BRIDGE EVALUATION, SECOND EDITION, WITH 2013 INTERIMS AND THE UDOT BRIDGE MANAGEMENT MANUAL, 2022.

STRUCTURAL CONCRETE

| | | | | |
|--------------------------------------|---------|--------------------------|---------------------------|---------------------|
| ALL LOCATIONS UNLESS SHOWN OTHERWISE | 145 PCF | f _c = 4.0 KSI | n = 8 | CLASS AA(AE) |
| PRESTRESSED CONCRETE | 150 PCF | f _c = 9.5 KSI | f _{ci} = 6.0 KSI | n = 5 CLASS AAA(AE) |
| PILES | 145 PCF | f _c = 4.0 KSI | n = 8 | CLASS AA (AE) |
| PARTIAL-DEPTH PRECAST PANEL | 145 PCF | f _c = 6.0 KSI | f _{ci} = 4.5 KSI | n = 6 CLASS AAA(AE) |

STRUCTURAL CONCRETE - FIBER

| | | | | |
|---------------------------------|---------|--------------------------|-------|---------------|
| DECK, APPROACH SLAB, DIAPHRAGMS | 145 PCF | f _c = 4.0 KSI | n = 7 | CLASS AA(LSF) |
|---------------------------------|---------|--------------------------|-------|---------------|

CONCRETE WEIGHT FOR LOADS 150 PCF UNLESS SHOWN OTHERWISE

REINFORCING STEEL

| | |
|--------------------------------------|--------------------------|
| ALL LOCATIONS UNLESS SHOWN OTHERWISE | f _y = 60 KSI |
| DECK | f _y = 100 KSI |

| | |
|---------------------|---|
| PRESTRESSED STRAND: | 0.375" DIA GRADE 270 LOW RELAXATION STRAND (PANELS) |
| | 0.600" DIA GRADE 270 LOW RELAXATION STRAND (GIRDER) |

| | |
|------------------------------|--|
| SACRIFICIAL WEARING SURFACE: | 1/2" CONCRETE |
| FUTURE WEARING SURFACE: | 40 PSF |
| DESIGN SPEED: | 35 MPH FOREST STREET |
| SEISMIC: | 7% PROBABILITY OF EXCEEDANCE IN 75 YR DESIGN EVENT |
| | PGA = 0.372g, S _s = 0.846g, S ₁ = 0.239g |
| | As = 0.420g, SDs = 0.983g, SD1 = 0.460g |
| | SITE CLASS D, SDC D |
| | BRIDGE CLASSIFICATION - NORMAL |
| TRAFFIC DATA: | 2050 ADT = 9,419 2021 ADT = 6,200 |
| PARAPET TEST LEVEL: | TL-3 |

| ITEM | QUANTITIES | | |
|--|------------|-------|----------|
| | EST QTY | UNIT | AS CONST |
| GRANULAR BACKFILL BORROW (PLAN QUANTITY) | 2,311 | CU YD | |
| TEMPORARY RETAINING WALL | 1 | LUMP | |
| PILE DRIVING EQUIPMENT | 1 | LUMP | |
| DRIVEN PILE, 16 INCH | 18,558 | FT | |
| CHAIN LINK FENCE ON STRUCTURE | 1,126 | FT | |
| REINFORCING STEEL - UNCOATED CM (PLAN QUANTITY) | 70,660 | LB | |
| REINFORCING STEEL - UN COATED CS (PLAN QUANTITY) | 182,404 | LB | |
| REINFORCING STEEL - COATED (PLAN QUANTITY) | 411,548 | LB | |
| STRUCTURAL CONCRETE (EST QTY (1,575 CY)) | 1 | LUMP | |
| STRUCTURAL CONCRETE - LOW SHRINKAGE FIBER (EST QTY (1,373 CY)) | 1 | LUMP | |
| PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL | 20,178 | SQ FT | |
| THIN BONDED POLYMER OVERLAY, TYPE I | 30,402 | SQ FT | |
| CONCRETE COATING PARAPET | 1,126 | FT | |
| STRUCTURAL STEEL | 1,872 | LB | |
| PRESTRESSED CONCRETE MEMBER, 109 FT 0 INCH TYPE UBT58 | 21 | EACH | |
| PRESTRESSED CONCRETE MEMBER, 90 FT 4 INCH TYPE UBT58 | 14 | EACH | |
| COMPRESSION SEAL JOINT (TYPE A) | 130 | FT | |
| CONCRETE COATING | 23,438 | SQ FT | |
| ELECTRICAL WORK BRIDGES | 1 | LUMP | |

INDEX TO STRUCTURE DRAWINGS

| SHT NO. | DWG NO. | SHEET TITLE |
|---------|---------|--|
| 01 | S01 | STRUCTURE GENERAL NOTES AND INDEX |
| 02 | S02 | SITUATION AND LAYOUT 1 OF 2 |
| 03 | S03 | SITUATION AND LAYOUT 2 OF 2 |
| 04 | S04 | STRUCTURE UTILITIES |
| 05 | S05 | STRUCTURE CONSTRUCTION PHASING |
| 06 | S06 | SOIL DATA SHEET 1 OF 12 |
| 07 | S07 | SOIL DATA SHEET 2 OF 12 |
| 08 | S08 | SOIL DATA SHEET 3 OF 12 |
| 09 | S09 | SOIL DATA SHEET 4 OF 12 |
| 10 | S10 | SOIL DATA SHEET 5 OF 12 |
| 11 | S11 | SOIL DATA SHEET 6 OF 12 |
| 12 | S12 | SOIL DATA SHEET 7 OF 12 |
| 13 | S13 | SOIL DATA SHEET 8 OF 12 |
| 14 | S14 | SOIL DATA SHEET 9 OF 12 |
| 15 | S15 | SOIL DATA SHEET 10 OF 12 |
| 16 | S16 | SOIL DATA SHEET 11 OF 12 |
| 17 | S17 | SOIL DATA SHEET 12 OF 12 |
| 18 | S18 | RAILROAD NOTES AND CLEARANCE |
| 19 | S19 | FOUNDATION PLAN |
| 20 | S20 | PILE DETAILS 1 OF 2 |
| 21 | S21 | PILE DETAILS 2 OF 2 - 16 INCH FIXED |
| 22 | S22 | ABUTMENT #1 PLAN AND ELEVATION |
| 23 | S23 | ABUTMENT #6 PLAN AND ELEVATION |
| 24 | S24 | ABUTMENT DETAILS |
| 25 | S25 | WINGWALL DETAILS |
| 26 | S26 | BENT PLAN AND ELEVATION |
| 27 | S27 | BENT FOOTING DETAILS |
| 28 | S28 | BENT CAP DETAILS |
| 29 | S29 | COLUMN DETAILS |
| 30 | S30 | FRAMING PLAN |
| 31 | S31 | UBT58 GIRDER 90 FOOT SPAN |
| 32 | S32 | UBT58 GIRDER 110 FOOT SPAN |
| 33 | S33 | ELASTOMERIC BEARING PAD DETAILS |
| 34 | S34 | INTERMEDIATE DIAPHRAGM DETAILS FOR PRESTRESSED GIRDERS |
| 35 | S35 | PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL 1 OF 2 |
| 36 | S36 | PARTIAL-DEPTH PRECAST CONCRETE DECK PANE 2 OF 2 |
| 37 | S37 | DECK PLAN 1 OF 2 |
| 38 | S38 | DECK PLAN 2 OF 2 |
| 39 | S39 | DECK SECTIONS |
| 40 | S40 | DECK AND ABUTMENT DIAPHRAGM DETAILS |
| 41 | S41 | BENT DIAPHRAGM DETAILS 1 OF 2 |
| 42 | S42 | BENT DIAPHRAGM DETAILS 2 OF 2 |
| 43 | S43 | SCREED ELEVATIONS 1 OF 3 |
| 44 | S44 | SCREED ELEVATIONS 2 OF 3 |
| 45 | S45 | SCREED ELEVATIONS 3 OF 3 |
| 46 | S46 | APPROACH SLAB PLAN 1 OF 2 |
| 47 | S47 | APPROACH SLAB PLAN 2 OF 2 |
| 48 | S48 | APPROACH SLAB DETAILS |
| 49 | S49 | COMPRESSION JOINT DETAILS |
| 50 | S50 | APPROACH SLAB DRAIN DETAILS 2X2 GRATE |
| 51 | S51 | 42-INCH SINGLE SLOPE PEDESTRIAN PARAPET W/ SIDEWALK |
| 52 | S52 | 42-INCH SINGLE SLOPE PEDESTRIAN PARAPET |
| 53 | S53 | LIGHTING LOCATION PLAN AND DETAILS |
| 54 | S54 | CHAIN LINK FENCE ON STRUCTURE |
| 55 | S55 | ELECTRICAL AND STRUCTURE NUMBER DETAILS |
| 56 | S56 | GRAFFITI COVER |
| 57 | S57 | REINFORCING SCHEDULE 1 OF 3 |
| 58 | S58 | REINFORCING SCHEDULE 2 OF 3 |
| 59 | S59 | REINFORCING SCHEDULE 3 OF 3 |

INDEX TO MSE WALL DRAWINGS

| SHT NO. | DWG NO. | SHEET TITLE |
|---------|---------|--|
| 01 | W01 | MSE WALLS LOCATION PLAN AND GENERAL NOTES |
| 02 | W02 | WEST MSE WALL SITUATION AND LAYOUT 1 OF 5 |
| 03 | W03 | WEST MSE WALL SITUATION AND LAYOUT 2 OF 5 |
| 04 | W04 | WEST MSE WALL SITUATION AND LAYOUT 3 OF 5 |
| 05 | W05 | WEST MSE WALL SITUATION AND LAYOUT 4 OF 5 |
| 06 | W06 | WEST MSE WALL SITUATION AND LAYOUT 5 OF 5 |
| 07 | W07 | EAST MSE WALL SITUATION AND LAYOUT 1 OF 3 |
| 08 | W08 | EAST MSE WALL SITUATION AND LAYOUT 2 OF 3 |
| 09 | W09 | EAST MSE WALL SITUATION AND LAYOUT 3 OF 3 |
| 10 | W10 | SINGLE STAGE MSE WALL COPING REQUIREMENTS 1 OF 2 |
| 11 | W11 | SINGLE STAGE MSE WALL COPING REQUIREMENTS 2 OF 2 |

| | | |
|-----------|-------|-----|
| REVISIONS | DATE | BY |
| △ | 03/24 | AUB |
| △ | | |
| △ | | |

REVISOR QUANTITIES

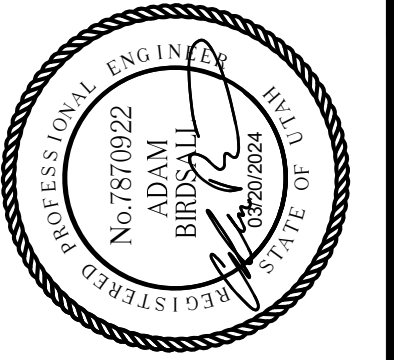
REVISED CONCRETE QUANTITIES

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB

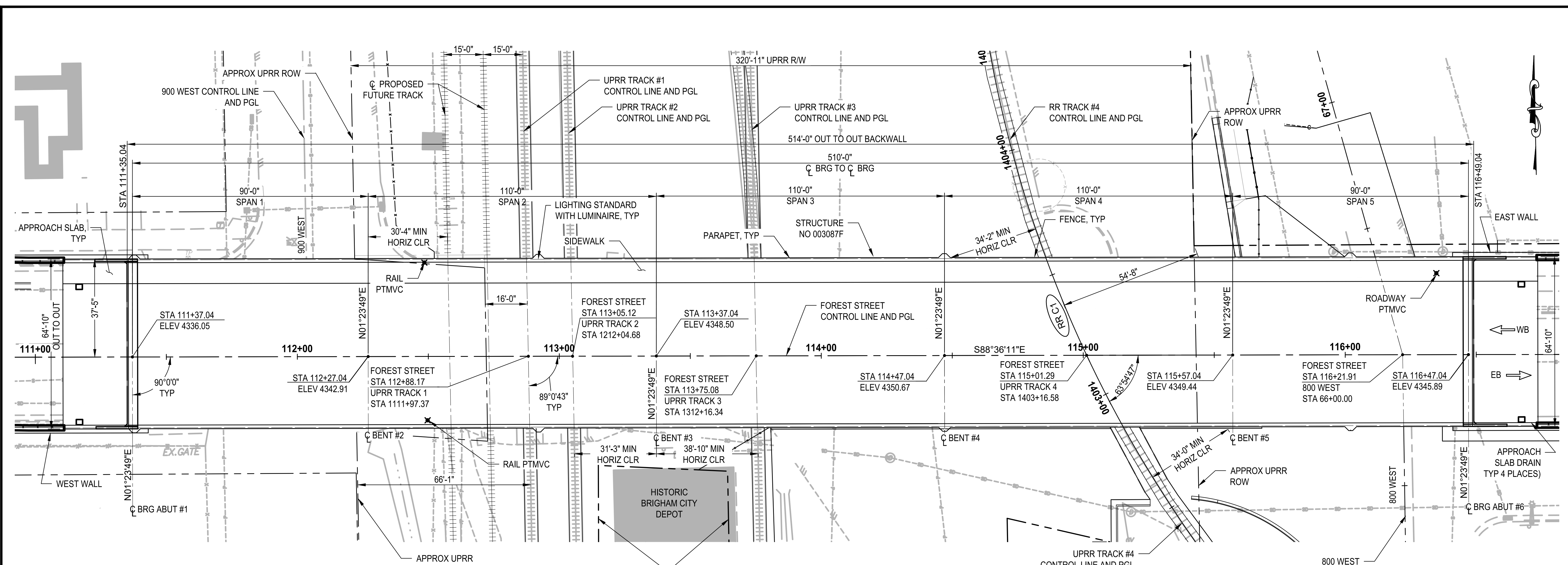
DATE: 03/20/2024
 JOB No.: 344-8541-002



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

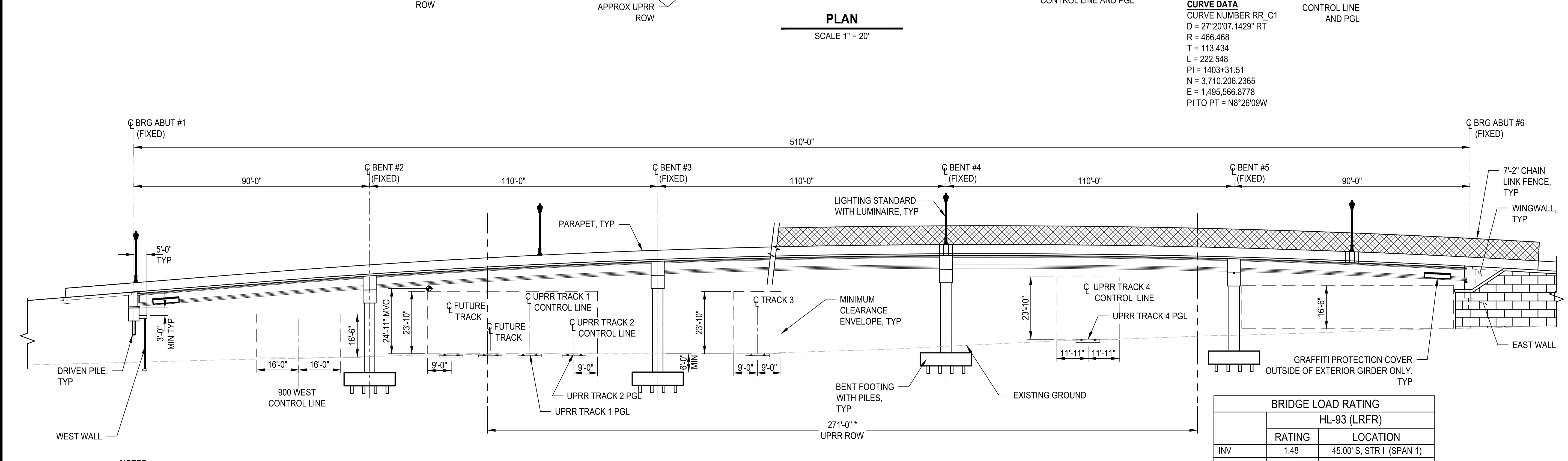
STRUCTURE GENERAL NOTES AND INDEX

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PLAN
SCALE 1" = 20'

CURVE DATA
 CURVE NUMBER RR_C1
 D = 27°20'07.1429" RT
 R = 466.468
 T = 113.434
 L = 222.548
 PI = 1403+31.51
 N = 3,710,206.2365
 E = 1,495,566.8778
 PI TO PT = N8°26'09W



ELEVATION
SCALE 1" = 20'

- NOTES**
1. UTILITIES NOT SHOWN FOR CLARITY. SEE "UTILITY PLAN" FOR MORE INFORMATION

| BRIDGE LOAD RATING | | |
|--------------------|--------|--------------------------|
| HL-93 (LRFR) | | |
| | RATING | LOCATION |
| INV | 1.48 | 45.00' S, STR 1 (SPAN 1) |
| OPER | 1.87 | 45.00' S, STR 1 (SPAN 1) |

F DENOTES RATING CONTROLLED BY FLEXURE
 S DENOTES RATING CONTROLLED BY SHEAR
 Mr AT 45.00' = 8,289.97 K-FT
 Vr AT 45.00' = 459.20 K

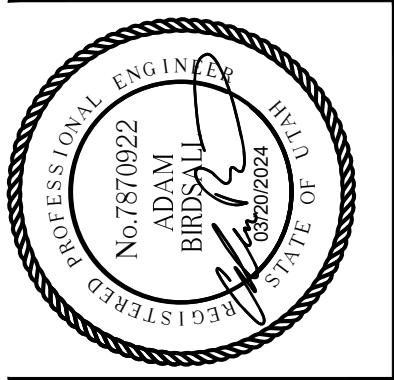
| | |
|-----------|--|
| BY | |
| DATE | |
| REVISIONS | |

ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: SLO
 DRAWN BY: AJB
 CHECKED BY: MJC
 APPROVED BY: AJB

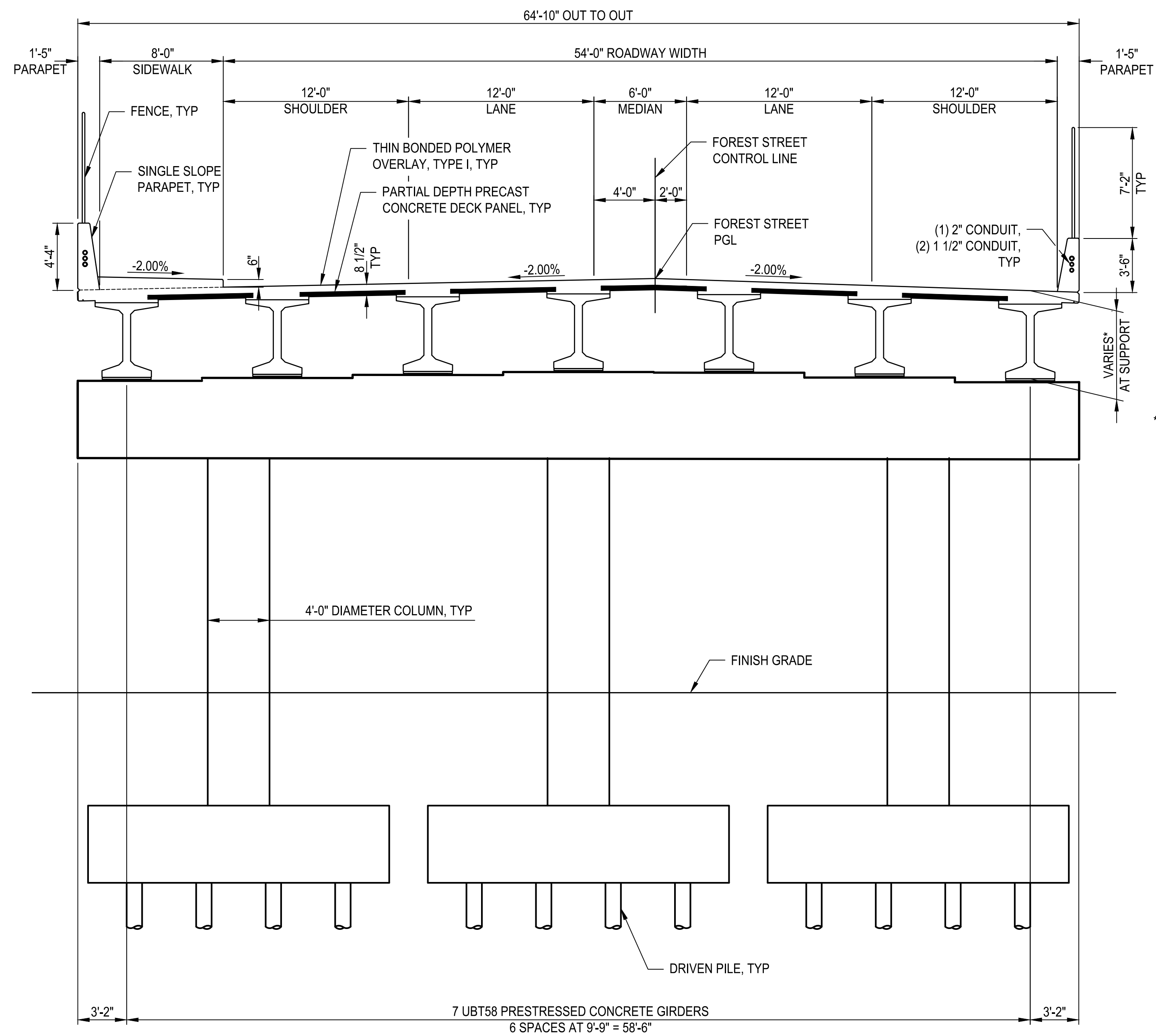
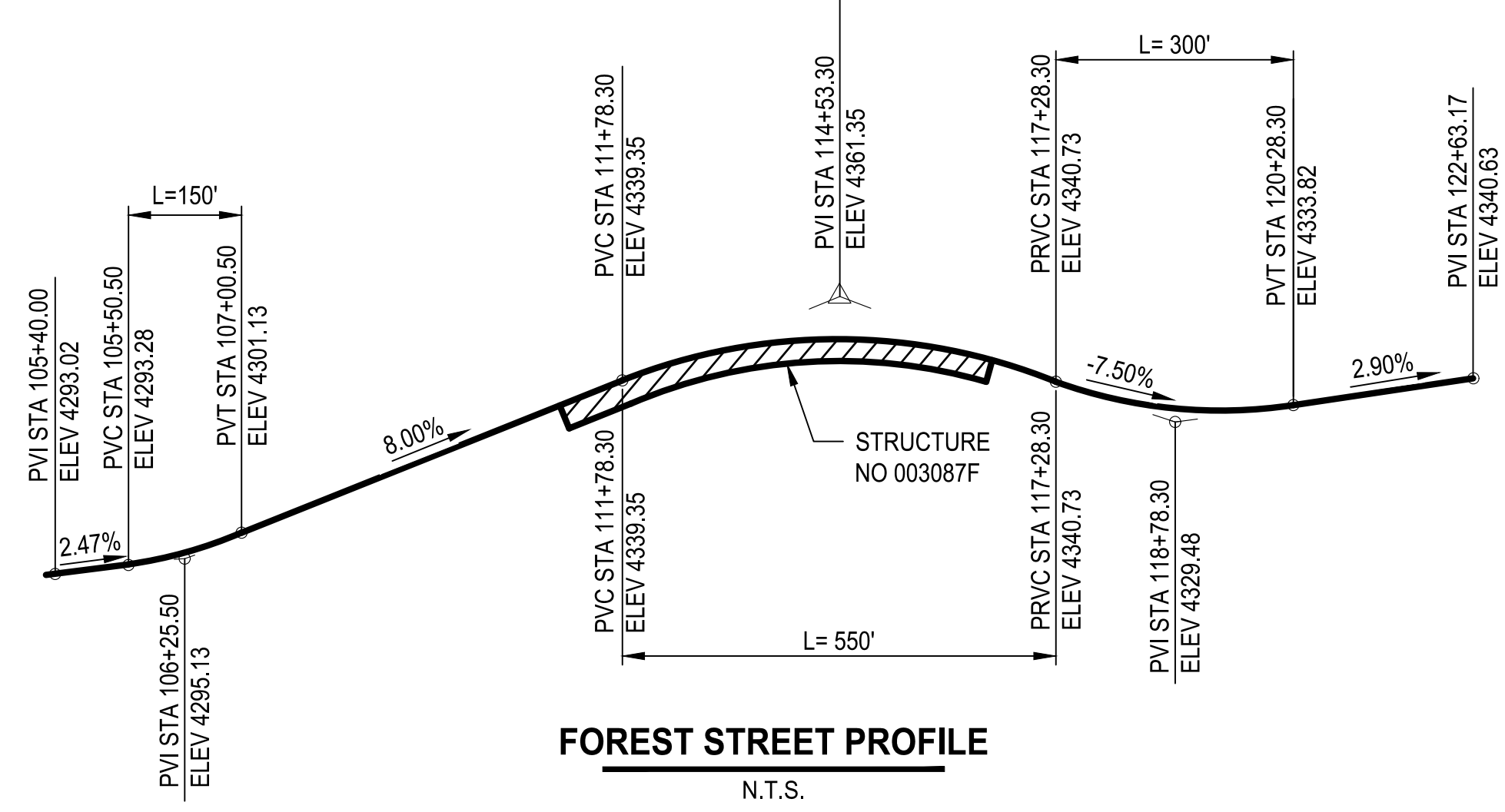
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 JOB No.: 344-8541-002



BRIGHAM CITY CONNECTION PROJECT

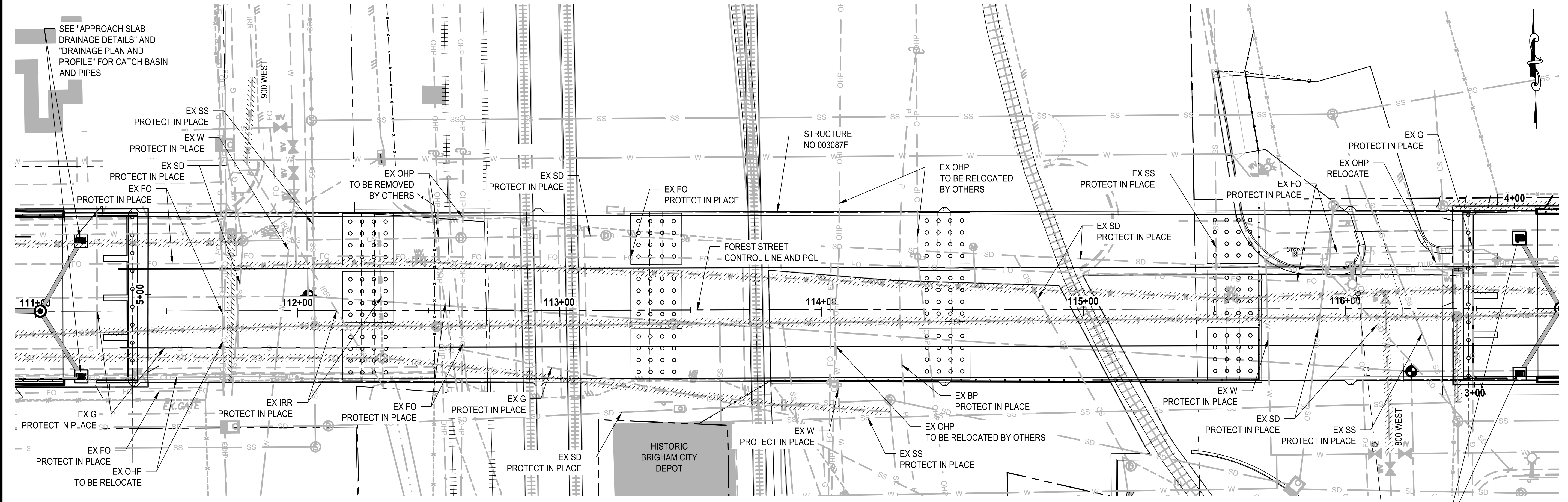
SITUATION AND LAYOUT 1 OF 2

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| | |
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| DATE | BY |
| | |
| REVISIONS | |
| | |
| ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY | |
| Parametrix | |
| DATE 03/20/2024 | DESIGNED TWP |
| JOB No. 344-8541-002 | DRAWN SLO |
| CHECKED NICC | APPROVED AUB |
| PROJECT NAME BRIGHAM CITY CONNECTION PROJECT | |
| SITUATION AND LAYOUT 2 OF 2 | |
| DRAWING NO. 3 OF 59 | |
| S03 | |

LAYOUT: ST PATH: U:\Sst\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99\Specs\CADD\DWG\Structure
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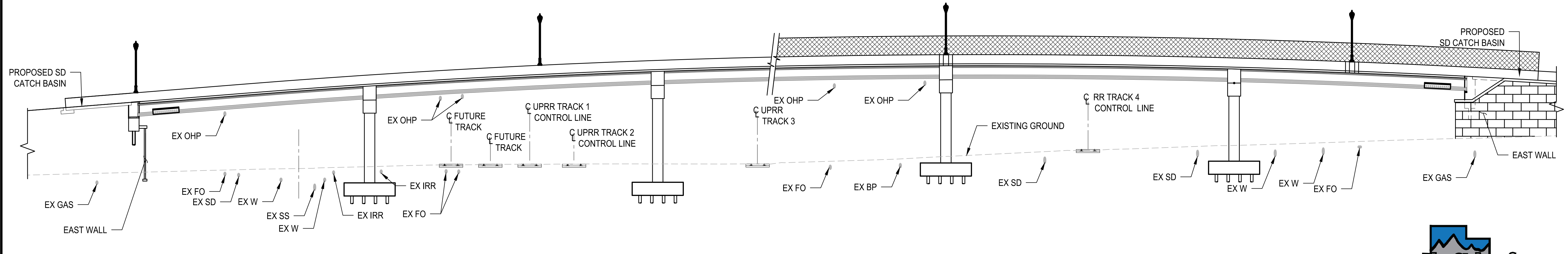


LEGEND
 // // // // // ABANDONED UTILITY LINE

PLAN
 SCALE 1" = 20'

- NOTES**
1. FIELD VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION. CALL "BLUE STAKES OF UTAH" AT 811 AT LEAST TWO (2) BUSINESS DAYS PRIOR TO CONSTRUCTION.
 2. NOTIFY ALL UTILITIES LOCATED IN THE VICINITY OF THE PROJECT AND ANTICIPATED CONSTRUCTION TIMES.
 3. LOCATION OF EXISTING UNDERGROUND UTILITIES HAS BEEN DETERMINED BY INFORMATION PROVIDED BY OTHERS, AND SHALL BE CONSIDERED APPROXIMATE ONLY. CONTRACTOR SHALL DETERMINE EXACT LOCATION AND SIZE OF ALL EXISTING UTILITY LINES PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ALL DAMAGES RESULTING FROM FAILURE TO LOCATE UTILITIES.

SEE "APPROACH SLAB DRAINAGE DETAILS" AND "DRAINAGE PLAN AND PROFILE" FOR CATCH BASIN AND PIPES



NOTE
 EXISTING UTILITIES TO BE PROTECTED IN PLACE UNLESS OTHERWISE NOTED.

* MEASURED ALONG FOREST STREET CONTROL LINE
ELEVATION
 SCALE 1" = 20'

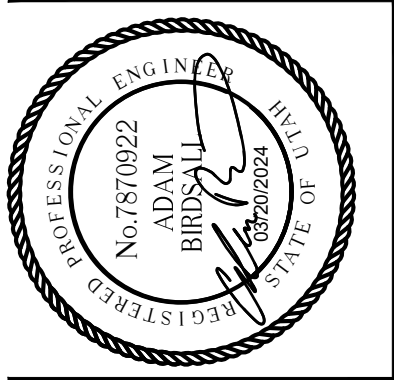
| | |
|-----------|--|
| BY | |
| DATE | |
| REVISIONS | |

ONE INCH AT FULL SCALE IF NOT SCALE ACCORDINGLY

Parametrix

DESIGNED BY: SLO
 DRAWN BY: SLO
 CHECKED BY: AUB
 APPROVED BY: AUB

DATE: 03/20/2024
 JOB No.: 344-8541-002

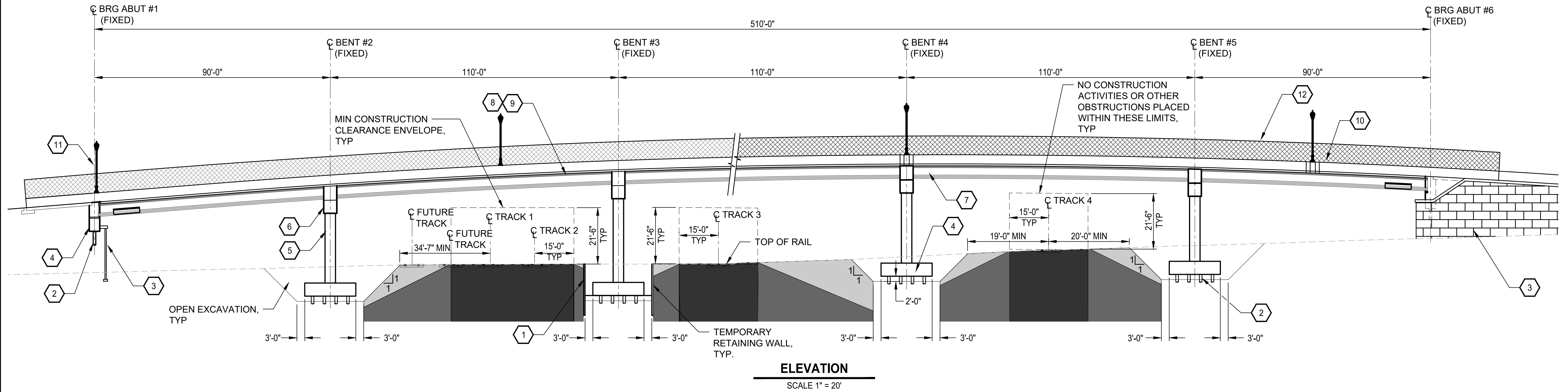


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

STRUCTURE UTILITIES



LAYOUT: S-03_CP
 PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St. Final Design\995svcs\CADD\DWG\Structure
 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:38:59 AM



STRUCTURE CONSTRUCTION PHASING

- 1 INSTALL TEMPORARY RETAINING WALLS AND EXCAVATE FOOTINGS
- 2 INSTALL DRIVEN PILES AT ABUTMENTS AND BENTS
- 3 CONSTRUCT MSE WALLS
- 4 FORM AND POUR BENT AND ABUTMENT PILE CAPS
- 5 FORM AND POUR COLUMNS
- 6 FORM AND POUR BENT CAPS
- 7 SET CONCRETE GIRDERS
- 8 PLACE PARTIAL-DEPTH PRECAST DECK PANELS
- 9 POUR DECK AND DIAPHRAGMS
- 10 FORM AND POUR PARAPET
- 11 INSTALL LAMP POSTS
- 12 CONSTRUCT FENCE

LEGEND

- NO EXCAVATION ALLOWED
- ZONE A SHORING
- ZONE B SHORING

NOTES

1. FOREST STREET SHALL BE CLOSED DURING THE FULL DURATION OF CONSTRUCTION OF THE BRIDGE AND MSE WALLS.
2. ALL TEMPORARY RETAINING WALLS SHALL BE CONSIDERED ZONE A SHORING.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

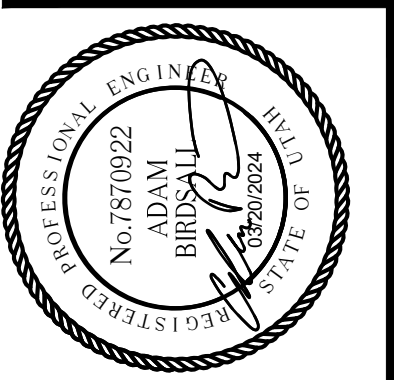
ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: TWP
 DRAWN: SLO

CHECKED: NGB
 APPROVED: AJB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

STRUCTURE CONSTRUCTION PHASING

GENERAL NOTES
DESCRIPTION OF SYMBOLS AND ABBREVIATIONS
Brigham City Connection Project ■ Brigham City, UT
Terracon Project No. 61215166



| SAMPLING | WATER LEVEL | FIELD TESTS |
|--|--|---|
| Modified Dames & Moore Ring Sampler Standard Penetration Test | Water Initially Encountered Water Level After a Specified Period of Time Water Level After a Specified Period of Time Cave In Encountered | N Standard Penetration Test Resistance (Blows/Ft.) (HP) Hand Penetrometer (T) Torvane (DCP) Dynamic Cone Penetrometer UC Unconfined Compressive Strength (PID) Photo-Ionization Detector (OVA) Organic Vapor Analyzer |

Water levels indicated on the soil boring logs are the levels measured in the borehole at the times indicated. Groundwater level variations will occur over time. In low permeability soils, accurate determination of groundwater levels is not possible with short term water level observations.

DESCRIPTIVE SOIL CLASSIFICATION

Soil classification as noted on the soil boring logs is based Unified Soil Classification System. Where sufficient laboratory data exist to classify the soils consistent with ASTM D2487 "Classification of Soils for Engineering Purposes" this procedure is used. ASTM D2488 "Description and Identification of Soils (Visual-Manual Procedure)" is also used to classify the soils, particularly where insufficient laboratory data exist to classify the soils in accordance with ASTM D2487. In addition to USCS classification, coarse grained soils are classified on the basis of their in-place relative density, and fine-grained soils are classified on the basis of their consistency. See "Strength Terms" table below for details. The ASTM standards noted above are for reference to methodology in general. In some cases, variations to methods are applied as a result of local practice or professional judgment.

LOCATION AND ELEVATION NOTES

Exploration point locations as shown on the Exploration Plan and as noted on the soil boring logs in the form of Latitude and Longitude are approximate. See **Exploration and Testing Procedures** in the report for the methods used to locate the exploration points for this project. Surface elevation data annotated with +/- indicates that no actual topographical survey was conducted to confirm the surface elevation. Instead, the surface elevation was approximately determined from topographic maps of the area.

STRENGTH TERMS

| RELATIVE DENSITY OF COARSE-GRAINED SOILS (More than 50% retained on No. 200 sieve.) Density determined by Standard Penetration Resistance | | CONSISTENCY OF FINE-GRAINED SOILS (50% or more passing the No. 200 sieve.) Consistency determined by laboratory shear strength testing, field visual-manual procedures or standard penetration resistance | | |
|---|---|---|---|---|
| Descriptive Term (Density) | Standard Penetration or N-Value Blows/Ft. | Descriptive Term (Consistency) | Unconfined Compressive Strength Qu, (tsf) | Standard Penetration or N-Value Blows/Ft. |
| Very Loose | 0 - 3 | Very Soft | less than 0.25 | 0 - 1 |
| Loose | 4 - 9 | Soft | 0.25 to 0.50 | 2 - 4 |
| Medium Dense | 10 - 29 | Medium Stiff | 0.50 to 1.00 | 4 - 8 |
| Dense | 30 - 50 | Stiff | 1.00 to 2.00 | 8 - 15 |
| Very Dense | > 50 | Very Stiff | 2.00 to 4.00 | 15 - 30 |
| | | Hard | > 4.00 | > 30 |

RELEVANCE OF SOIL BORING LOG

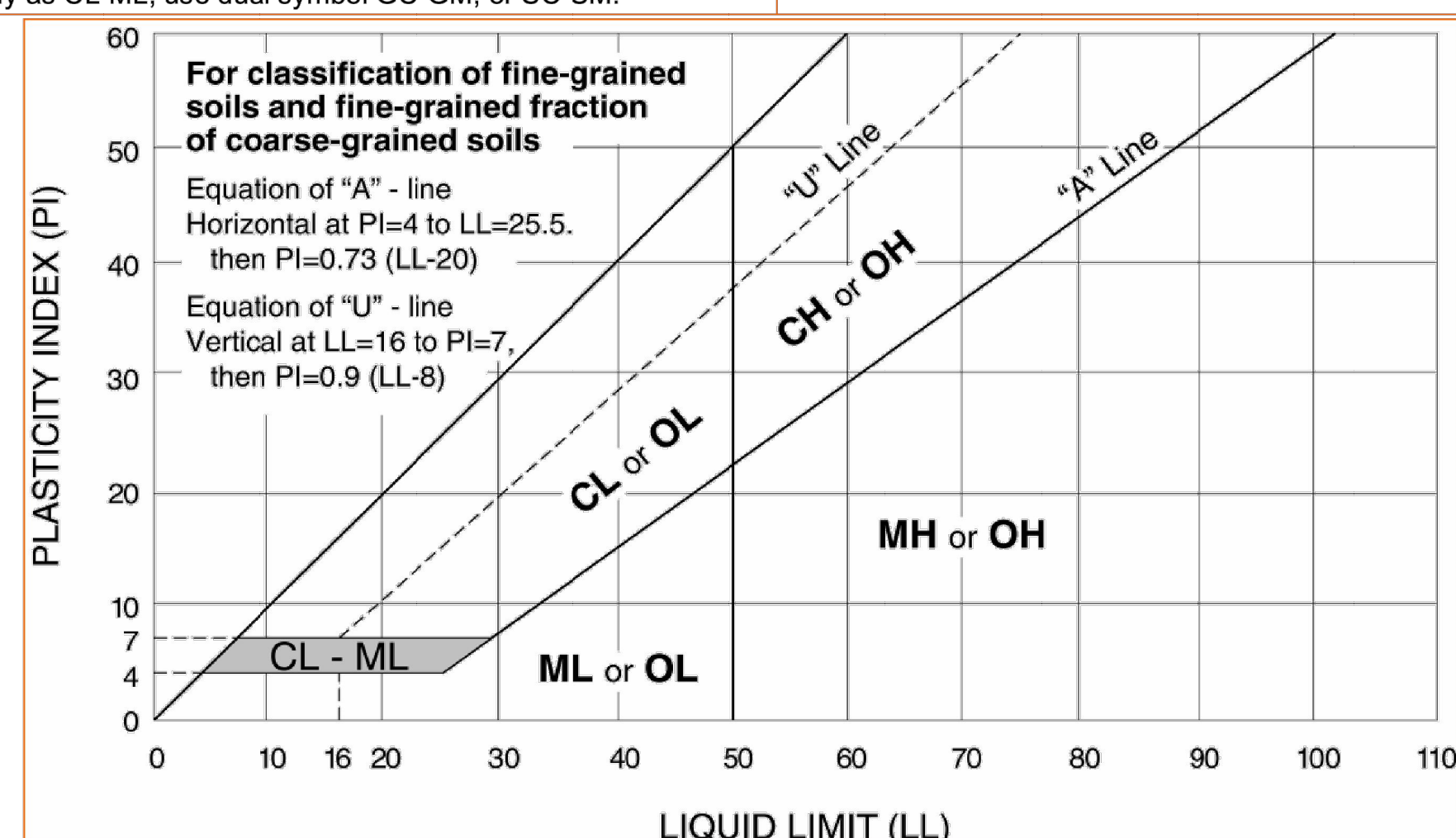
The soil boring logs contained within this document are intended for application to the project as described in this document. Use of these soil boring logs for any other purpose may not be appropriate.

UNIFIED SOIL CLASSIFICATION SYSTEM



| Criteria for Assigning Group Symbols and Group Names Using Laboratory Tests ^A | | | | Soil Classification | |
|--|---|--|---|------------------------------------|------------------------------------|
| | | | | Group Symbol | Group Name ^B |
| Coarse-Grained Soils: More than 50% retained on No. 200 sieve | Gravels: More than 50% of coarse fraction retained on No. 4 sieve | Clean Gravels: Less than 5% fines ^C | Cu ≥ 4 and 1 ≤ Cc ≤ 3 ^E | GW | Well-graded gravel ^F |
| | | | Cu < 4 and/or [Cc < 1 or Cc > 3.0] ^E | GP | Poorly graded gravel ^F |
| | | Gravels with Fines: More than 12% fines ^C | Fines classify as ML or MH | GM | Silty gravel ^{F, G, H} |
| | Sands: 50% or more of coarse fraction passes No. 4 sieve | Clean Sands: Less than 5% fines ^D | Cu ≥ 6 and 1 ≤ Cc ≤ 3 ^E | SW | Well-graded sand ^I |
| | | | Cu < 6 and/or [Cc < 1 or Cc > 3.0] ^E | SP | Poorly graded sand ^I |
| | | Sands with Fines: More than 12% fines ^D | Fines classify as ML or MH | SM | Silty sand ^{G, H, I} |
| Fine-Grained Soils: 50% or more passes the No. 200 sieve | Silts and Clays: Liquid limit less than 50 | Inorganic: | PI > 7 and plots on or above "A" line | CL | Lean clay ^{K, L, M} |
| | | | PI < 4 or plots below "A" line ^J | ML | Silt ^{K, L, M} |
| | | Organic: | Liquid limit - oven dried < 0.75 | OL | Organic clay ^{K, L, M, N} |
| | | | Liquid limit - not dried < 0.75 | OH | Organic silt ^{K, L, M, O} |
| | Silts and Clays: Liquid limit 50 or more | Inorganic: | PI plots on or above "A" line | CH | Fat clay ^{K, L, M} |
| | | | PI plots below "A" line | MH | Elastic Silt ^{K, L, M} |
| | Organic: | Liquid limit - oven dried < 0.75 | OH | Organic clay ^{K, L, M, P} | |
| | | Liquid limit - not dried < 0.75 | OH | Organic silt ^{K, L, M, Q} | |
| Highly organic soils: | Primarily organic matter, dark in color, and organic odor | | | PT | Peat |

- ^A Based on the material passing the 3-inch (75-mm) sieve.
- ^B If field sample contained cobbles or boulders, or both, add "with cobbles or boulders, or both" to group name.
- ^C Gravels with 5 to 12% fines require dual symbols: GW-GM well-graded gravel with silt, GW-GC well-graded gravel with clay, GP-GM poorly graded gravel with silt, GP-GC poorly graded gravel with clay.
- ^D Sands with 5 to 12% fines require dual symbols: SW-SM well-graded sand with silt, SW-SC well-graded sand with clay, SP-SM poorly graded sand with silt, SP-SC poorly graded sand with clay.
- ^E $Cu = D_{60}/D_{10}$ $Cc = \frac{(D_{30})^2}{D_{10} \times D_{60}}$
- ^F If soil contains ≥ 15% sand, add "with sand" to group name.
- ^G If fines classify as CL-ML, use dual symbol GC-GM, or SC-SM.
- ^H If fines are organic, add "with organic fines" to group name.
- ^I If soil contains ≥ 15% gravel, add "with gravel" to group name.
- ^J If Atterberg limits plot in shaded area, soil is a CL-ML, silty clay.
- ^K If soil contains 15 to 29% plus No. 200, add "with sand" or "with gravel," whichever is predominant.
- ^L If soil contains ≥ 30% plus No. 200 predominantly sand, add "sandy" to group name.
- ^M If soil contains ≥ 30% plus No. 200, predominantly gravel, add "gravelly" to group name.
- ^N PI ≥ 4 and plots on or above "A" line.
- ^O PI < 4 or plots below "A" line.
- ^P PI plots on or above "A" line.
- ^Q PI plots below "A" line.



REVISIONS

| NO. | DATE | BY |
|-----|------|----|
| | | |

ONE INCH AT FULL SCALE, IF NOT OTHERWISE INDICATED

Parametrix

DESIGNED: KLB
DRAWN: SLO
CHECKED: RLC
APPROVED: AUB

DATE: 03/20/2024
JOB No.: 344-8541-002

REGISTERED PROFESSIONAL ENGINEER
No. 31318
STATE OF UTAH
3/8/2024

PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT

SOIL DATA SHEET 1 OF 12

DRAWING NO. 6 OF 59
S06

LAYOUT: geo
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 PLOTTED BY: Olivisio DATE: Friday, March 22, 2024 10:08:55 AM

EXPLORATION PLAN

Brigham City Connection Project — Forest Street Overpass ■ Brigham City, Utah
 April 5, 2023 ■ Terracon Project No. 61215166



DIAGRAM IS FOR GENERAL LOCATION ONLY, AND IS NOT INTENDED FOR CONSTRUCTION PURPOSES

MAP PROVIDED BY MICROSOFT BING MAPS

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
| | | |

ONE INCH AT FULL SCALE, IF NOT OTHERWISE NOTED ACCORDINGLY

| | | | |
|-------------------|----------------------|----------|----------|
| Parametrix | DATE | DESIGNED | CHECKED |
| | 03/20/2024 | KJUB | RLC |
| | JOB No. 344-8541-002 | DRAWN | APPROVED |
| | | SLO | AJB |



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SOIL DATA SHEET 2 OF 12

DRAWING NO.
 7 OF 59
S07

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| BORING LOG NO. B-S-1 | | | | | | | | | | Page 1 of 3 | | |
|--|--------------------------|---|----------------|--------------------|--|------------|-------------------|-----------------------|----------|---------------|--|--|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (In.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | LL-PL-PI | PERCENT FINES | | |
| 0.6 | | ASPHALT | | | | | | | | | | |
| 5.0 | | POORLY GRADED SAND WITH SILT AND GRAVEL (SP-SM), tan, medium dense | 10 | 9-5-3-3 N=8 | 14 | A-4 (0) | 2.0 | | 17-14-3 | 41 | | |
| 7.0 | | SILTY SAND WITH GRAVEL (SM), light tan, loose to dense, with oxidation stains | 8 | 2-10-19 N=29 | 46 | | | | | | | |
| 18.0 | | SILTY SAND WITH GRAVEL (SM), light tan, loose to dense, with oxidation stains | 18 | 19-17-16-36 | 31 | | | | | | | |
| 12.0 | | | 12 | 6-7-7 N=14 | 18 | | | | | | | |
| 15.0 | ▽ | | 14 | 3-4-2 N=6 | 6 | A-4 (0) | 19.5 | | NP | 43 | | |
| 23.5 | | POORLY GRADED SAND WITH SILT AND GRAVEL (SP-SM), tan to brown, medium dense | 16 | 7-9-6 N=15 | 20 | | | | | | | |
| 25.0 | | | 16 | 3-4-15 N=19 | 21 | | 14.8 | | | 12 | | |
| 28.0 | | SILTY SAND WITH GRAVEL (SM), tan, medium dense, with oxidation stains | | 7-12-13 N=25 | 28 | | | | | | | |
| 31.0 | | SILTY GRAVEL WITH SAND (GM), tan, medium dense | 8 | 3-7-13-15 N=20 | 20 | | | | | | | |
| 40.0 | | | 8 | 6-12-10-11 N=22 | 20 | | 4.2 | | | 18 | | |
| 45.0 | | | 4 | 2-4-9-16 N=13 | 11 | | | | | | | |

Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency)

| | | |
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| Advancement Method: Hollow Stem Auger(3.25" ID) | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | Notes: 20' - Analytical |
| Abandonment Method: Boring backfilled with Auger Cuttings and/or Bentonite Surface Capped with asphalt patch | See Supporting Information for explanation of symbols and abbreviations. | |
| WATER LEVEL OBSERVATIONS ▽ 15.5' While drilling | | Boring Started: 05-12-2022 Drill Rig: Geoprobe Project No.: 61215166 |
| | | Boring Completed: 05-12-2022 Driller: Terracon |

| BORING LOG NO. B-S-1 | | | | | | | | | | Page 2 of 3 | | |
|--|--------------------------|--|----------------|--------------------|--|--------------|-------------------|-----------------------|----------|---------------|--|--|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (In.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | LL-PL-PI | PERCENT FINES | | |
| 48.0 | | LEAN CLAY (CL), trace silt, dark gray to black, stiff, with organics | 18 | 2-4-6-6 N=10 | 6 | A-4 (10) | 24.7 | | 30-20-10 | 99 | | |
| 53.0 | | SILTY GRAVEL WITH SAND (GM), dark gray to brown, loose, rough drilling | 2 | 2-4-6 N=10 | 8 | | | | | | | |
| 57.5 | | LEAN CLAY (CL), trace gravel, dark gray to black, medium stiff, with organics, with oxidation stains | 15 | 0-1-4 N=5 | 4 | | | | | | | |
| 65.0 | | with silt lenses | 20 | 0-2-3 N=5 | 3 | A-6 (11) | 25.0 | | 31-18-13 | 90 | | |
| 70.0 | | | 12 | PP = 1.5 | | | | | | | | |
| 75.0 | | | 6 | | | | | | | | | |
| 77.5 | | GRAVELLY LEAN CLAY WITH SAND (CL), dark gray to black, with organics | 7 | | | | | | | | | |
| 83.0 | | SANDY SILT (ML), trace gravel, dark gray to black, very stiff, with organics | 15 | 9-11-15 N=26 | 17 | | | | | | | |
| 87.0 | | SILTY SAND (SM), dark gray, dense, with oxidation stains | 17 | 10-15-22 N=37 | 18 | A-2-4 (0) | 21.3 | | NP | 30 | | |
| 91.0 | | | | | | | | | | | | |

Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency)

| | | |
|--|--|--|
| Advancement Method: Hollow Stem Auger(3.25" ID) | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | Notes: 48.5' - Analytical 68.5' - UU: 5.8 ksf |
| Abandonment Method: Boring backfilled with Auger Cuttings and/or Bentonite Surface Capped with asphalt patch | See Supporting Information for explanation of symbols and abbreviations. | |
| WATER LEVEL OBSERVATIONS ▽ 15.5' While drilling | | Boring Started: 05-12-2022 Drill Rig: Geoprobe Project No.: 61215166 |
| | | Boring Completed: 05-12-2022 Driller: Terracon |

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| BRIGHAM CITY CONNECTION PROJECT | SOIL DATA SHEET 3 OF 12 |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | DRAWING NO. 8 OF 59 S08 |

LAYOUT: geo
 PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995svcs\CADD\DWG\Structure PLOTTED BY: OliveSia DATE: Friday, March 22, 2024 10:09:24 AM

| BORING LOG NO. B-S-1 | | | | | | | | | | Page 3 of 3 | | |
|---|--|------------|--|-------------|--|--------------------|--------|--------|-------------------|-----------------------|------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N1)60 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES |
| | See Exploration Plan | | | | | | | | | | LL-PL-PI | |
| | Latitude: 41.5106° Longitude: -112.0301° | | | | | | | | | | | |
| | Surface Elev.: 4309 (Ft.) | | | | | | | | | | | |
| | DEPTH ELEVATION (FL) | | | | | | | | | | | |
| 97.5 | POORLY GRADED SAND WITH SILT (SP-SM), dark gray, very dense (continued) | 4211.5 | | X | 18 | 22-23-28 N=51 | 28 | | | | | |
| 102.0 | SANDY SILT (ML), dark gray, very stiff, with organics | 4207 | | X | 16 | 10-10-12 N=22 | 11 | | | | | |
| 108.0 | POORLY GRADED SAND WITH SILT AND GRAVEL (SP-SM), dark gray, dense, with organics | 4201 | | X | 18 | 15-24-12 N=36 | 18 | | | | | |
| 110.0 | SILT WITH SAND (ML), dark gray, hard, with organics | 4199 | | X | 15 | 7-23-26 N=49 | 26 | | | | | |
| Boring Terminated at 110 Feet | | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency) | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | | Notes: | | | | | |
| Abandonment Method: Boring backfilled with Auger Cuttings and/or Bentonite Surface Capped with asphalt patch | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | |
| WATER LEVEL OBSERVATIONS | | | | | | | | | | | | |
| 15.5' While drilling | | | | | | | | | | | | |
| | | | Boring Started: 05-12-2022 | | Boring Completed: 05-12-2022 | | | | | | | |
| | | | Drill Rig: Geoprobe | | Driller: Terracon | | | | | | | |
| | | | Project No.: 61215166 | | | | | | | | | |

| BORING LOG NO. B-S-2 | | | | | | | | | | Page 1 of 3 | | |
|---|--|------------|--|-------------|--|--------------------|--|--------------|-------------------|-----------------------|------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N1)60 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES |
| | See Exploration Plan | | | | | | | | | | LL-PL-PI | |
| | Latitude: 41.5105° Longitude: -112.0285° | | | | | | | | | | | |
| | Surface Elev.: 4320 (Ft.) | | | | | | | | | | | |
| | DEPTH ELEVATION (FL) | | | | | | | | | | | |
| 0.4 | ASPHALT, approximately 4.5" | 4319.6 | | | | | | | | | | |
| 5 | SILTY SAND (SM), gray brown, loose to medium dense, oxidation stains | | | X | 8 | 2-4-2 N=6 | 11 | | | | | |
| 5 | | | | X | 5 | 1-1-3 N=4 | 6 | | 7.4 | | | |
| 10 | | | | X | 16 | 3-7-9 | 18 | A-2-4 (0) | 4.3 | 86 | NP | 18 |
| 10 | | | | X | 15 | 7-10-11 | 14 | | | | | |
| 15 | | | | X | 11 | 5-7-7 N=14 | 16 | | 23.1 | | | |
| 20 | | | V | X | 10 | 6-7-6 N=13 | 13 | A-2-4 (0) | | | NP | 32 |
| 25 | | | | X | 9 | 9-10-9 N=19 | 23 | A-2-4 (0) | 20.5 | | NP | 22 |
| 30 | | | | X | 7 | 7-9-11 N=20 | 22 | | 17.9 | | | |
| 35 | SILTY SAND WITH GRAVEL (SM), brownish tan, medium dense, oxidation stains | 4285 | | X | 8 | 6-11-10 N=21 | 22 | A-4 (0) | 23.6 | | NP | 43 |
| 40 | POORLY GRADED GRAVEL WITH SILT AND SAND (GP-GM), brown to tan mottled gray, dense, clay lenses, oxidation stains | 4280 | | X | 10 | 2-14-13 N=27 | 25 | A-1-b (0) | 15.1 | | NP | 22 |
| 45 | | | | X | 3 | 6-18-20-31 N=38 | 32 | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | | Notes: 2.5' - Analytical 7.5' - Direct Shear | | | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | |
| WATER LEVEL OBSERVATIONS | | | | | | | | | | | | |
| 21' While drilling | | | | | | | | | | | | |
| | | | Boring Started: 01-19-2022 | | Boring Completed: 01-19-2022 | | | | | | | |
| | | | Drill Rig: Geoprobe | | Driller: Terracon | | | | | | | |
| | | | Project No.: 61215166 | | | | | | | | | |

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| BRIGHAM CITY CONNECTION PROJECT | SOIL DATA SHEET 4 OF 12 |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | DRAWING NO. 9 OF 59 S09 |

LAYOUT: geo PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995socs\CADD\DWG\Structure PLOTTED BY: OliveSia DATE: Friday, March 22, 2024 10:09:34 AM

| BORING LOG NO. B-S-2 | | | | | | | | | | Page 2 of 3 | | |
|--|--|------------|--------------------------|-------------|--|--|----------|--------|---|-----------------------|------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES |
| | See Exploration Plan | | | | | | | | | | LL-PL-PI | |
| | Latitude: 41.5105° Longitude: -112.0285° | | | | | | | | | | | |
| | Surface Elev.: 4320 (Ft.) | | | | | | | | | | | |
| | DEPTH ELEVATION (FL) | | | | | | | | | | | |
| 52.0 | POORLY GRADED GRAVEL WITH SILT AND SAND (GP-GM), brown to tan mottled gray, dense, clay lenses, oxidation stains (continued) | 4268 | | 10 | 8-14-19 N=33 | 27 | | | | | | |
| 55.0 | SILTY SAND (SM), dark gray, medium dense | 4265 | | 12 | 8-8-12 N=20 | 16 | A-4 (0) | 20.5 | | | NP | 36 |
| 55.0 | LEAN CLAY (CL), dark gray to black, soft to very stiff, silt lenses, organic odor | | | 18 | 2-6-8 N=14 | 10 | | 23.4 | | | | |
| 65.0 | | | | 14 | PP=3.0 ksf | | A-4 (10) | 21.0 | 101 | 33-23-10 | | 97 |
| 70.0 | | | | 20 | PP=6.0 ksf | | A-4 (10) | 39.0 | 83 | 33-23-10 | | 97 |
| 75.0 | | | | 18 | 0-2-4 N=6 | 4 | | 34.5 | | | | |
| 80.0 | | | | 21 | PP=2.0 ksf | | A-6 (12) | 29.7 | 96 | 34-20-14 | | 87 |
| 85.0 | | | | 18 | 2-3-5 N=8 | 5 | | 28.7 | | | | |
| 90.0 | | | | 18 | 0-0-4 N=4 | 3 | | 37.8 | | | | |
| 91.0 | | 4229 | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: 63.5' - Consolidation 68.5' - UU: 3.2 ksf, Analytical 78.5' - Consolidation | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | |
| WATER LEVEL OBSERVATIONS 21' While drilling | | | | | | Boring Started: 01-19-2022 Drill Rig: Geoprobe Project No.: 61215166 | | | Boring Completed: 01-19-2022 Driller: Terracon | | | |
| Terracon | | | | | | | | | | | | |
| 6949 S High Tech Dr Ste 100 Midvale, UT | | | | | | | | | | | | |

| BORING LOG NO. B-S-2 | | | | | | | | | | Page 3 of 3 | | |
|--|--|------------|--------------------------|-------------|--|--|----------|--------|---|-----------------------|------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES |
| | See Exploration Plan | | | | | | | | | | LL-PL-PI | |
| | Latitude: 41.5105° Longitude: -112.0285° | | | | | | | | | | | |
| | Surface Elev.: 4320 (Ft.) | | | | | | | | | | | |
| | DEPTH ELEVATION (FL) | | | | | | | | | | | |
| 97.0 | SILTY CLAYEY SAND (SC-SM), dark brown, medium dense (continued) | 4223 | | 16 | 10-8-7 N=15 | 8 | A-4 (10) | 20.6 | | | 20-16-4 | 43 |
| 102.5 | POORLY GRADED SAND WITH SILT (SP-SM), trace clay, dark gray, dense, organic odor | 4217.5 | | 14 | 6-17-27 N=44 | 22 | | | | | | |
| 105.0 | SANDY SILT (ML), dark gray to black, stiff to very stiff, organic odor | | | 18 | 7-10-10 N=20 | 10 | A-4 (10) | 27.2 | | | NP | 70 |
| 110.0 | | | | 18 | 4-5-8 N=13 | 7 | | 23.8 | | | | |
| 115.0 | | 4205 | | 16 | 10-15-21 N=36 | 16 | | | | | | |
| Boring Terminated at 115 Feet | | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: 108' - Analytical | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | |
| WATER LEVEL OBSERVATIONS 21' While drilling | | | | | | Boring Started: 01-19-2022 Drill Rig: Geoprobe Project No.: 61215166 | | | Boring Completed: 01-19-2022 Driller: Terracon | | | |
| Terracon | | | | | | | | | | | | |
| 6949 S High Tech Dr Ste 100 Midvale, UT | | | | | | | | | | | | |

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| BRIGHAM CITY CONNECTION PROJECT | SOIL DATA SHEET 5 OF 12 |
| | |
| PROJECT NAME BRIGHAM CITY CONNECTION PROJECT | PROJECT NO. 61215166 |
| DATE 03/20/2024 | DRAWN SLO |
| DESIGNED KUB | APPROVED AUB |
| CHECKED RLC | DATE 3/8/2024 |
| JOB No. 344-8541-002 | SCALE AS SHOWN |
| REVISIONS 1 | BY DATE |
| DRAWING NO. 10 OF 59 | |
| S10 | |

PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995socs\CADD\DWG\Structure PLOTTED BY: OliveSia DATE: Friday, March 22, 2024 10:09:44 AM LAYOUT: geo

| BORING LOG NO. B-S-3 | | | | | | | | | | | | Page 1 of 3 | |
|--|---|-----------------|--------------------------|-------------|--------------------|--|--------------|--------|-------------------|-----------------------|------------------|---------------|--|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES | |
| | See Exploration Plan Latitude: 41.5107° Longitude: -112.0294° Surface Elev.: 4312 (Ft.) | ELEVATION (Ft.) | | | | | | | | | LL-PL-PI | | |
| | | 4305 | | 6 | 4-5-4 N=9 | 16 | A-1-b (0) | 8.5 | | | NP | 21 | |
| | | 4305 | | 4 | 1-2-5 N=7 | 11 | | | | | | | |
| | | 4302.5 | | 11 | 2-3-3 N=6 | 9 | A-4 (0) | 20.3 | | | NP | 47 | |
| | | 4302.5 | | 0 | 4-5-6 | 8 | | | | | | | |
| | | 4295.5 | | 12 | 1-2-3 N=5 | 7 | A-4 (0) | 26.7 | | | NP | 74 | |
| | | 4295.5 | | 12 | 3-6-6 N=12 | 16 | A-2-4 (0) | 21.5 | | | NP | 27 | |
| | | 4285 | | 9 | 9-9-11 N=20 | 24 | | | | | | | |
| | | 4281 | | 6 | 8-11-11 N=22 | 25 | | | | | | | |
| | | 4281 | | 8 | 7-14-12 N=26 | 26 | A-1-a (0) | 10.2 | | | NP | 5 | |
| | | 4269 | | 5 | 3-6-9-10 N=15 | 14 | | 13.6 | | | | | |
| | | 4269 | | 10 | 4-12-13-14 N=25 | 22 | A-4 (0) | 24.6 | | | NP | 60 | |

Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength

| | | |
|--|--|--------|
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 16.5' Mud Rotary(2.5" Bit) 16.5' to 100' | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | Notes: |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | See Supporting Information for explanation of symbols and abbreviations. | |

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|---------------------------------|--|----------------------------|------------------------------|
| WATER LEVEL OBSERVATIONS | | Boring Started: 01-18-2022 | Boring Completed: 01-18-2022 |
| 8' While drilling | | Drill Rig: Geoprobe | Driller: Terracon |
| | 6949 S High Tech Dr Ste 100 Midvale, UT | Project No.: 61215166 | |

| BORING LOG NO. B-S-3 | | | | | | | | | | | | Page 2 of 3 | |
|--|---|-----------------|--------------------------|-------------|-----------------|--|--------|--------------|-------------------|-----------------------|------------------|---------------|--|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS | PERCENT FINES | |
| | See Exploration Plan Latitude: 41.5107° Longitude: -112.0294° Surface Elev.: 4312 (Ft.) | ELEVATION (Ft.) | | | | | | | | | LL-PL-PI | | |
| | | 4264.5 | | 2 | 7-5-4 N=9 | 8 | | | | | | | |
| | | 4259 | | 14 | 1-1-1 N=2 | 2 | | 39.2 | | | | | |
| | | 4251.5 | | 20 | PP=5.0 ksf | | | A-4 (11) | 36.7 | 85 | 34-24-10 | 100 | |
| | | 4251.5 | | 18 | 0-0-3 N=3 | 2 | | A-6 (13) | 33.1 | | 33-19-14 | 95 | |
| | | 4223.5 | | 10 | 1-2-3 N=5 | 4 | | | | | | | |
| | | 4223.5 | | 18 | 1-1-4 N=5 | 4 | | | | | | | |
| | | 4221 | | 18 | 0-0-3 N=3 | 2 | | A-6 (19) | 36.9 | | 39-21-18 | 97 | |
| | | 4221 | | 18 | 0-7-5 N=12 | 8 | | | | | | | |
| | | 4221 | | 11 | 10-9-12 N=21 | 12 | | A-2-4 (0) | 27.7 | | NP | 35 | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |

Stratification lines are approximate. In-situ, the transition may be gradual. Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength

| | | |
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| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 16.5' Mud Rotary(2.5" Bit) 16.5' to 100' | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | Notes: 53.5' - Analytical 58.5' - Consolidation, UU: 2.3 ksf |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | See Supporting Information for explanation of symbols and abbreviations. | |

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|---------------------------------|--|----------------------------|------------------------------|
| WATER LEVEL OBSERVATIONS | | Boring Started: 01-18-2022 | Boring Completed: 01-18-2022 |
| 8' While drilling | | Drill Rig: Geoprobe | Driller: Terracon |
| | 6949 S High Tech Dr Ste 100 Midvale, UT | Project No.: 61215166 | |

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| BY | |
| DATE | |
| REVISIONS | |
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| | |
| BRIGHAM CITY CONNECTION PROJECT | |
| SOIL DATA SHEET 6 OF 12 | |
| DRAWING NO. | 11 OF 59 |
| S11 | |

LAYOUT: geo PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995svcs\CADD\DWG\Structure PLOTTED BY: OliveSio DATE: Friday, March 22, 2024 10:09:54 AM

| BORING LOG NO. B-S-3 | | | | | | | | | | | | Page 3 of 3 | |
|--|--|--|--|--------------------------|-------------|--|--------------------|------------|------------------------------|-------------------|-----------------------|---|---------------|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT.) | | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)60 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES |
| | See Exploration Plan | Latitude: 41.5107° Longitude: -112.0294° | DEPTH | | | | | | | | | | |
| | SANDY SILT (ML), dark gray, very stiff (continued) | | 95 | 4212 | 14 | 1-6-11 N=17 | 9 | | | 25.9 | | | |
| | Boring Terminated at 100 Feet | | 100 | 4212 | 16 | 4-5-17 N=22 | 11 | A-4 (0) | | | 25.4 | | NP |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 16.5' Mud Rotary(2.5" Bit) 16.5' to 100' | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: | | | | | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | | |
| WATER LEVEL OBSERVATIONS 8' While drilling | | | | | | Boring Started: 01-18-2022 | | | Boring Completed: 01-18-2022 | | | | |
| | | | | | | Drill Rig: Geoprobe | | | Driller: Terracon | | | | |
| | | | | | | Project No.: 61215166 | | | | | | | |

| BORING LOG NO. B-S-4 | | | | | | | | | | | | Page 1 of 3 | | |
|--|--|--|--|--------------------------|-------------|--|--------------------|--------------|------------------------------|-------------------|-----------------------|---|---------------|-----------------|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT.) | | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)60 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES | |
| | See Exploration Plan | Latitude: 41.5105° Longitude: -112.0282° | DEPTH | | | | | | | | | | | ELEVATION (FL.) |
| | ASPHALT, approximately 5" | | 0.4 | 4321.6 | | | | | | | | | | |
| | POORLY GRADED GRAVEL WITH SILT AND SAND (GP-GM), dark brown, dense | | 4.5 | 4317.5 | 12 | 7-13-22 N=35 | 62 | A-1-a (0) | | | 7.0 | | NP | 9 |
| | SILTY SAND (SM), tan, medium dense, oxidation stains | | | | 10 | 4-6-8 N=14 | 22 | | | | 15.2 | | | |
| | | | | | 13 | 3-6-7 N=13 | 18 | | | | | | | |
| | | | | | 16 | 6-9-11 | 10 | A-4 (0) | | | 11.2 | | NP | 50 |
| | | | | | 10 | 7-9-10 N=19 | 27 | | | | 14.5 | | | |
| | | | | | 9 | 6-6-7 N=13 | 17 | A-2-4 (0) | | | 21.5 | | NP | 28 |
| | | | | | 10 | 5-8-8 N=16 | 19 | A-2-4 (0) | | | 13.1 | | NP | 21 |
| | | | | | 8 | 3-7-10 N=17 | 19 | | | | | | | |
| | | | | | 11 | 6-11-14 N=25 | 26 | A-4 (0) | | | 17.6 | | NP | 36 |
| | SANDY SILT WITH GRAVEL (ML), tan, very stiff, oxidation stains | | 37.5 | 4284.5 | 8 | 8-12-17 N=29 | 27 | | | | | | | |
| | POORLY GRADED GRAVEL WITH SAND (GP), tan, medium dense, oxidation stains | | 42.5 | 4279.5 | 12 | 6-10-14-14 N=24 | 20 | A-1-a (0) | | | 7.7 | | NP | 3 |
| | | 45.5 | 4276.5 | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: | | | | | | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | See Supporting Information for explanation of symbols and abbreviations. | | | 10' - Direct Shear 15' - Analytical | | | | | | | | |
| WATER LEVEL OBSERVATIONS 21' While drilling | | | | | | Boring Started: 01-20-2022 | | | Boring Completed: 01-20-2022 | | | | | |
| | | | | | | Drill Rig: Geoprobe | | | Driller: Terracon | | | | | |
| | | | | | | Project No.: 61215166 | | | | | | | | |

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| BRIGHAM CITY CONNECTION PROJECT | SOIL DATA SHEET 7 OF 12 |
| PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | DRAWING NO. 12 OF 59 |
| DATE: 03/20/2024 JOB No.: 344-8541-002 | S12 |

PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St. Final Design\995socs\CADD\DWG\Structure PLOTTED BY: OliveSlo DATE: Friday, March 22, 2024 10:10:04 AM LAYOUT: geo

| BORING LOG NO. B-S-4 | | | | | | | | | | | | Page 2 of 3 | |
|--|--|----------------|--|-------------|--------------------|--|--------------|--------|---|-----------------------|------------------------------|---|--|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT.) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES | |
| | See Exploration Plan Latitude: 41.5105° Longitude: -112.0282° Surface Elev.: 4322 (Ft.) ELEVATION (FT.) | | | | | | | | | | | | |
| | WELL GRADED GRAVEL WITH SAND (GW) , tan, medium dense (continued) - heaving sands and gravel | 50.5 4271.5 | | 4 | 2-4-4-2 N=8 | 6 | A-1-a (0) | 18.2 | | | NP | 4 | |
| | SILTY SAND WITH GRAVEL (SM) , gray, dense | 55.0 4265 | | 9 | 9-15-16-18 N=31 | 25 | A-2-4 (0) | 16.7 | | | NP | 18 | |
| | LEAN CLAY WITH SAND (CL) , with silt, gray, very stiff | 61.0 4261 | | 10 | 10-8-11 N=19 | 14 | | 21.7 | | | | | |
| | SILTY CLAY (CL-ML) , dark gray, soft to stiff | 65.0 4257 | | 5 | 3-4-5 N=9 | 7 | | 32.3 | | | | | |
| | LEAN CLAY (CL) , dark gray, soft to very stiff, oxidation stains | 70.0 4252 | | 24 | 0-1-2 N=3 | 2 | A-4 (5) | 34.3 | | | 28-22-6 | 97 | |
| | | 75.0 | | 23 | PP=5.0 ksf | | A-4 (8) | 27.2 | 98 | | 28-19-9 | 95 | |
| | | 80.0 | | 24 | 2-6-6 N=12 | 8 | | 29.5 | | | | | |
| | | 85.0 | | 24 | 0-0-5 N=5 | 3 | | 34.5 | | | | | |
| | | 90.0 | | 24 | 0-0-0 N=0 | 3 | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: 73.5' - Consolidation, UU: 3.8 ksf 83.5' - Analytical | | | | | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | | |
| WATER LEVEL OBSERVATIONS 21' While drilling | | | | | | Boring Started: 01-20-2022 Drill Rig: Geoprobe Project No.: 61215166 | | | Boring Completed: 01-20-2022 Driller: Terracon | | | | |

| BORING LOG NO. B-S-4 | | | | | | | | | | | | Page 3 of 3 | |
|--|--|---------------|--|-------------|----------------|---|-------------|--------|---|-----------------------|------------------------------|---|--|
| PROJECT: Brigham City Connection Project | | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT.) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES | |
| | See Exploration Plan Latitude: 41.5105° Longitude: -112.0282° Surface Elev.: 4322 (Ft.) ELEVATION (FT.) | | | | | | | | | | | | |
| | LEAN CLAY (CL) , dark gray, soft to very stiff, oxidation stains (continued) | 95.0 4225 | | 24 | 0-2-5 N=7 | 4 | A-6 (19) | 32.7 | | | 40-21-19 | 95 | |
| | LEAN CLAY WITH SAND (CL) , dark gray to black, stiff, silt lenses | 101.0 4221 | | 15 | 15-8-4 N=12 | 6 | A-4 (7) | 33.0 | | | 30-20-10 | 82 | |
| | SILT WITH SAND (ML) , dark gray to black, soft to medium stiff, silt lenses | 105.0 4217 | | 24 | 0-1-1 N=2 | 1 | A-4 (6) | 37.3 | | | 33-24-9 | 74 | |
| | | 110.0 | | 24 | 0-0-7 N=7 | 3 | | | | | | | |
| | SILTY CLAYEY SAND (SC-SM) , dark gray to black, hard | 112.0 4210 | | | | | | | | | | | |
| | Boring Terminated at 115 Feet | 115.0 4207 | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | Hammer Type: Automatic (96.6% Efficiency) PP = Pocket Pen Undrained Shear Strength | |
| Advancement Method: Hollow Stem Auger(3.25" ID) 0' to 11.5' Mud Rotary(2.5" Bit) 11.5' to 115' | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | | | | | |
| Abandonment Method: Boring backfilled with bentonite grout upon completion Pavement secured with Utilibond | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | | |
| WATER LEVEL OBSERVATIONS 21' While drilling | | | | | | Boring Started: 01-20-2022 Drill Rig: Geoprobe Project No.: 61215166 | | | Boring Completed: 01-20-2022 Driller: Terracon | | | | |

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| REVISIONS | BY | DATE |
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ONE INCH AT FULL SCALE, IF NECESSARY, ACCORDINGLY

Parametrix

REGISTERED PROFESSIONAL ENGINEER
No. 31318
STATE OF UTAH
3/8/2024

PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SOIL DATA SHEET 8 OF 12

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| DRAWING NO. | 13 OF 59 |
| S13 | |

PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995Secs\CADD\DWG\Structure PLOTTED BY: OliveSia DATE: Friday, March 22, 2024 10:10:24 AM LAYOUT: geo

| BORING LOG NO. B-W-2 | | | | | | | | | | Page 2 of 2 | | |
|--|--|------------|--|-------------|--|------------------------------|--------|--------|-------------------|-----------------------|------------------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES |
| | See Exploration Plan Latitude: 41.5106° Longitude: -112.0306° Surface Elev.: 4304 (Ft.) | | | | | | | | | | | |
| | | 47.5 | | | 12 | N=17 | | | | | | |
| | SILTY CLAY (CL-ML), trace sand, dark gray to black, very stiff, with organic odor, with pinholes (continued) | | | | 14 | 1-3-5 N=8 | | | | | | |
| | LEAN CLAY (CL), with silt lenses, dark gray to black, medium stiff, with organics | | | | | | | | | | | |
| | | 53.5 | | | 24 | 0-0-0 N=0 | | | | | | |
| | LEAN CLAY (CL), dark gray to black, very soft | | | | | | | | | | | |
| | | 56.5 | | | | | | | | | | |
| | POORLY GRADED SAND (SP), black, medium dense, with organics, with oxidation stains | | | | 12 | 9-12-13 N=25 | | | | | | |
| | Boring Terminated at 60 Feet | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | |
| Hammer Type: Automatic (96.6% Efficiency) | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: | | | | | | |
| Abandonment Method: Boring backfilled with Auger Cuttings and/or Bentonite Surface Capped with asphalt patch | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | |
| WATER LEVEL OBSERVATIONS | | | Boring Started: 01-25-2022 | | | Boring Completed: 01-25-2022 | | | | | | |
| 15' While drilling | | | Drill Rig: Geoprobe | | | Driller: Terracon | | | | | | |
| | | | Project No.: 61215166 | | | | | | | | | |

| BORING LOG NO. B-W-3 | | | | | | | | | | Page 1 of 1 | | |
|--|---|------------|--|-------------|--|------------------------------|--------|--------------|-------------------|-----------------------|------------------------------|---------------|
| PROJECT: Brigham City Connection Project | | | | | CLIENT: Parametrix Inc Salt Lake City, UT | | | | | | | |
| SITE: 800 West Forest Street Brigham City, UT | | | | | | | | | | | | |
| GRAPHIC LOG | LOCATION | DEPTH (FT) | WATER LEVEL OBSERVATIONS | SAMPLE TYPE | RECOVERY (in.) | FIELD TEST RESULTS | (N)160 | AASHTO | WATER CONTENT (%) | DRY UNIT WEIGHT (pcf) | ATTERBERG LIMITS LL-PL-PI | PERCENT FINES |
| | See Exploration Plan Latitude: 41.5106° Longitude: -112.0280° Surface Elev.: 4324 (Ft.) | | | | | | | | | | | |
| | | 0.5 | | | | | | | | | | |
| | ASPHALT, approximately 6" | | | | | | | | | | | |
| | POORLY GRADED SAND WITH SILT AND GRAVEL (SP-SM), tan, medium dense | | | | 5 | 1-5-6 N=11 | 27 | A-1-b (0) | 2.9 | | NP | 9 |
| | | 4.0 | | | 9 | 3-5-7 N=12 | 30 | | | | | |
| | SILTY SAND (SM), tan, medium dense, oxidation stains | | | | 12 | 3-6-6 N=12 | 27 | A-4 (0) | 8.9 | | NP | 44 |
| | | | | | 13 | 4-5-6 N=11 | 21 | | 11.5 | | | |
| | | | | | 15 | 5-7-7 N=14 | 24 | | 10.4 | | | 39 |
| | | | | | 14 | 5-6-6 N=12 | 19 | A-2-4 (0) | 18.0 | | NP | 29 |
| | | | | | 8 | 3-5-9 N=14 | 20 | A-2-4 (0) | 14.2 | | NP | 18 |
| | | | | | 10 | 6-5-6 N=11 | 15 | A-2-4 (0) | 14.9 | | NP | 28 |
| | | 33.5 | | | | | | | | | | |
| | WELL GRADED SAND WITH SILT AND GRAVEL (SW-SM), tan, medium dense, oxidation stains | | | | 12 | 4-6-18 N=24 | 31 | | | | | |
| | | 38.5 | | | | | | | | | | |
| | WELL GRADED GRAVEL WITH SAND (GW), trace silt, tan, dense, oxidation stains | | | | 5 | 11-17-16 N=33 | 40 | | | | | |
| | Boring Terminated at 41.5 Feet | | | | | | | | | | | |
| Stratification lines are approximate. In-situ, the transition may be gradual. | | | | | | | | | | | | |
| Hammer Type: Automatic (96.6% Efficiency) | | | | | | | | | | | | |
| Advancement Method: Hollow Stem Auger(3.25" ID) | | | See Exploration and Testing Procedures for a description of field and laboratory procedures used and additional data (if any). | | | Notes: | | | | | | |
| Abandonment Method: Boring backfilled with Auger Cuttings and/or Bentonite Surface Capped with asphalt patch | | | See Supporting Information for explanation of symbols and abbreviations. | | | | | | | | | |
| WATER LEVEL OBSERVATIONS | | | Boring Started: 01-21-2022 | | | Boring Completed: 01-21-2022 | | | | | | |
| 21' While drilling | | | Drill Rig: Geoprobe | | | Driller: Terracon | | | | | | |
| | | | Project No.: 61215166 | | | | | | | | | |

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| REVISIONS | BY | DATE |
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ONE INCH AT FULL SCALE. IF NECESSARY, SCALE ACCORDINGLY.

Parametrix

DATE: 03/20/2024
JOB No.: 344-8541-002
DESIGNED: KJB
DRAWN: SLO
CHECKED: RLC
APPROVED: AUB

REGISTERED PROFESSIONAL ENGINEER
No. 31318
CIVIL
STATE OF UTAH
3/8/2024

PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT

SOIL DATA SHEET 10 OF 12

DRAWING NO. 15 OF 59

S15

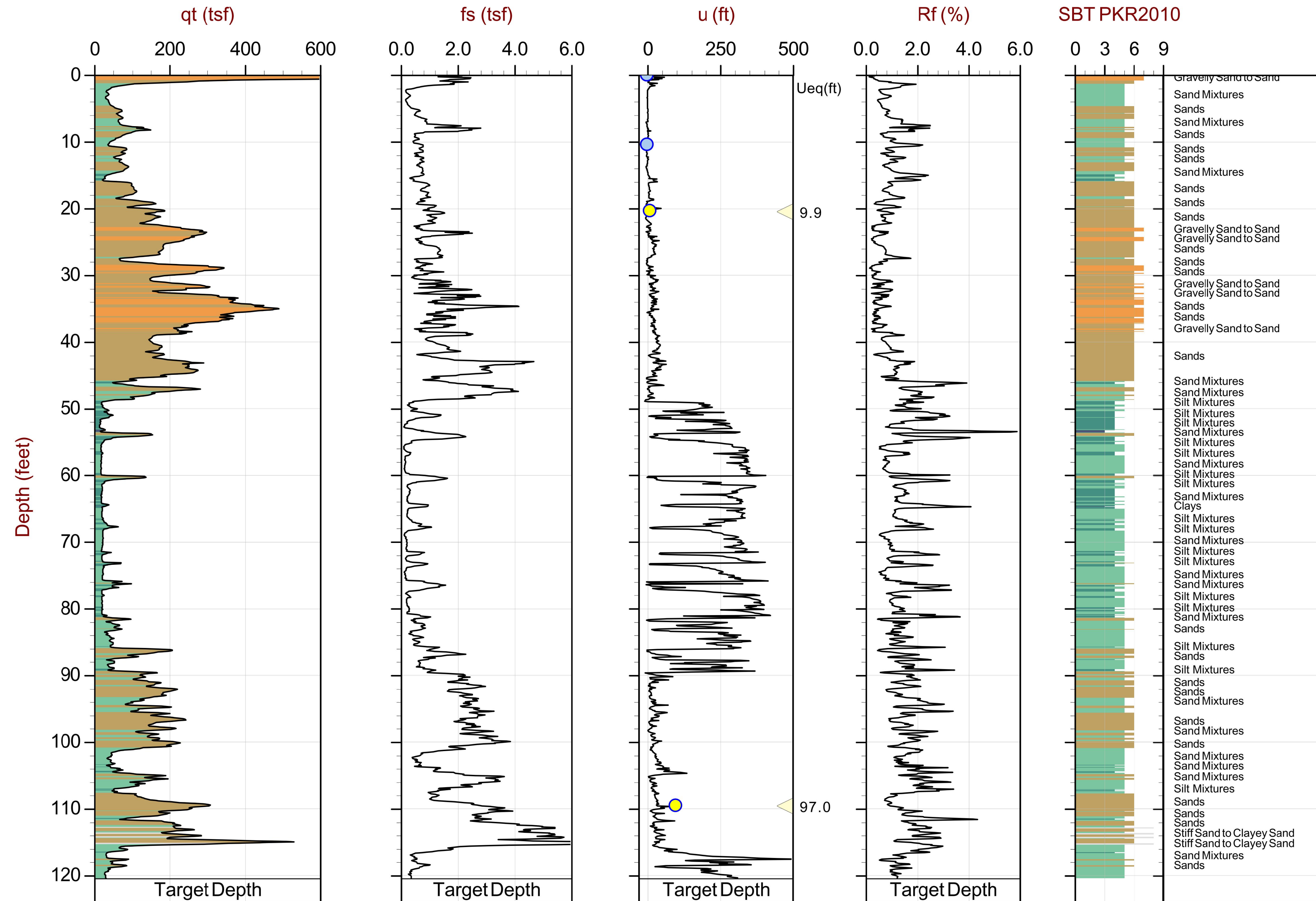
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Terracon

Job No: 22-52-23573
 Date: 2022-01-20 11:30
 Site: Forest St., Brigham City

Sounding: CPT-01
 Cone: 825:T1500F15U35



Max Depth: 36.725 m / 120.49 ft File: 22-52-23573_CP01.COR SBT: Robertson, 2010 (CPT '10)
 Depth Inc: 0.025 m / 0.082 ft Coords: Lat: 41.510735 Long: -112.029774
 Avg Int: Every Point Sheet No: 1 of 1

● Equilibrium Pore Pressure (Ueq)
 ● Assumed Ueq
 ◀ Dissipation, Ueq achieved
 ◀ Dissipation, Ueq not achieved
 — Hydrostatic Line

The reported coordinates were acquired from consumer grade GPS equipment and are only approximate locations. The coordinates should not be used for design purposes.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

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|-------------------|--|---------------|---------------|
| Parametrix | DATE: 03/20/2024 | DESIGNED: KJB | CHECKED: RLC |
| | JOB No.: 344-8541-002 | DRAWN: SLO | APPROVED: AUB |
| | REGISTERED PROFESSIONAL ENGINEER No. 31319 STATE OF UTAH 3/8/2024 | | |
| | PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT | | |

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| SOIL DATA SHEET 11 OF 12 |
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| DRAWING NO. 16 OF 59 |
| S16 |

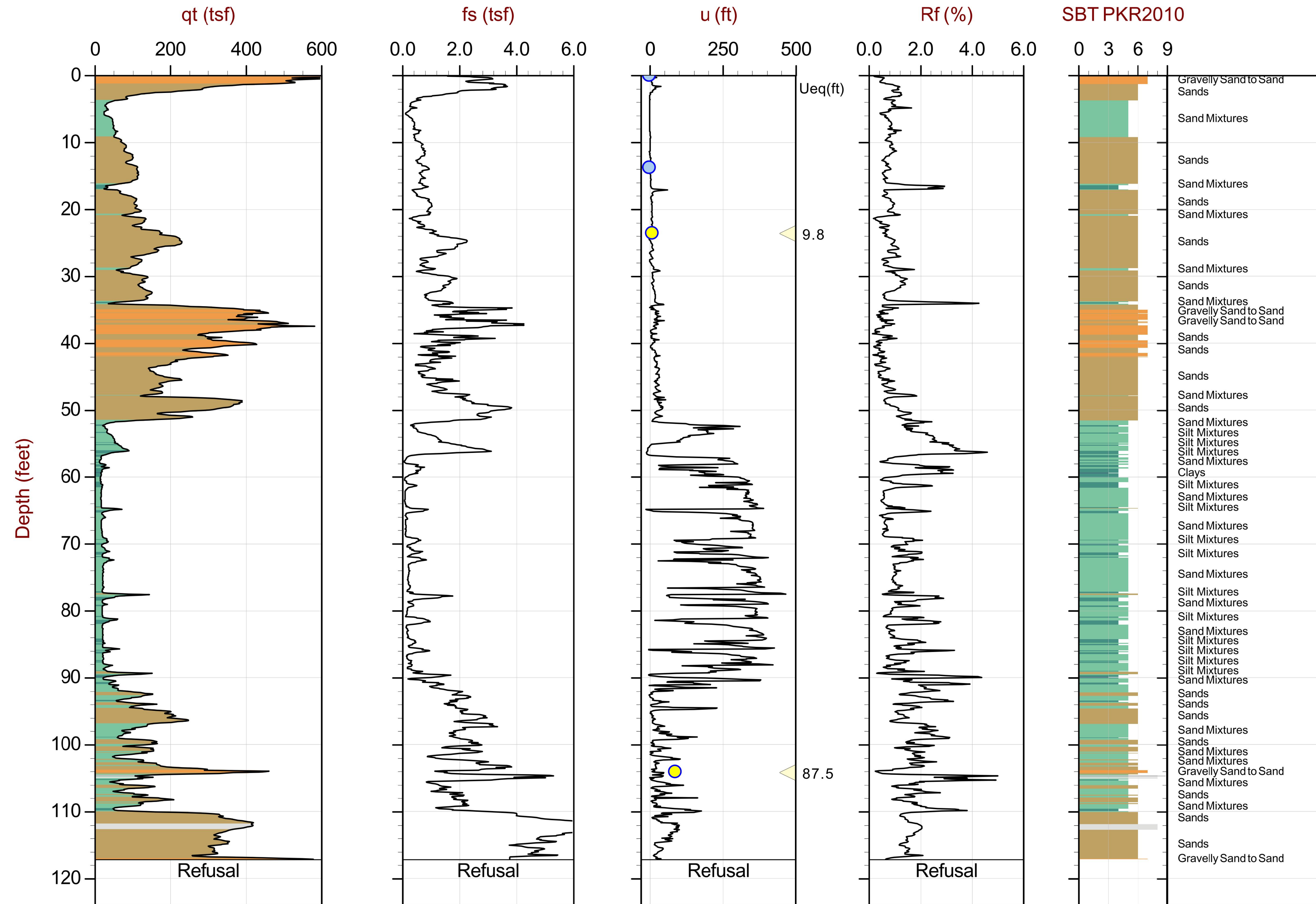
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Terracon

Job No: 22-52-23573
 Date: 2022-01-20 09:12
 Site: Forest St., Brigham City

Sounding: CPT-02
 Cone: 825:T1500F15U35



Max Depth: 35.725 m / 117.21 ft File: 22-52-23573_SP02.COR SBT: Robertson, 2010 (CPT '10)
 Depth Inc: 0.025 m / 0.082 ft Coords: Lat: 41.510603 Long: -112.029011
 Avg Int: Every Point Sheet No: 1 of 1

● Equilibrium Pore Pressure (Ueq)
 ● Assumed Ueq
 ◀ Dissipation, Ueq achieved
 ◀ Dissipation, Ueq not achieved
 — Hydrostatic Line

The reported coordinates were acquired from consumer grade GPS equipment and are only approximate locations. The coordinates should not be used for design purposes.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

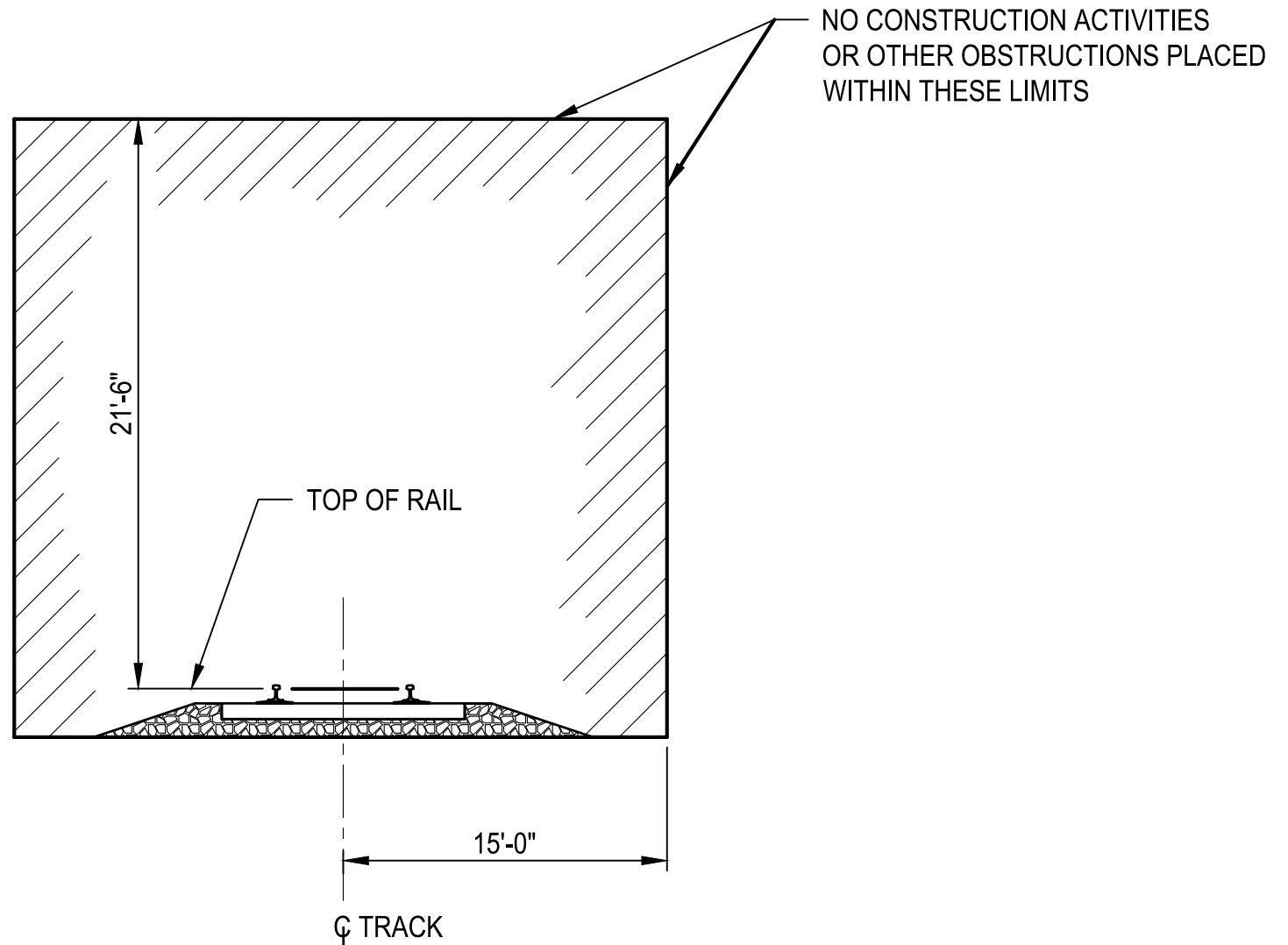
| | | |
|-----------------------|---------------|---------------|
| DATE: 03/20/2024 | DESIGNED: KUB | CHECKED: RLC |
| JOB No.: 344-8541-002 | DRAWN: SLO | APPROVED: AUB |



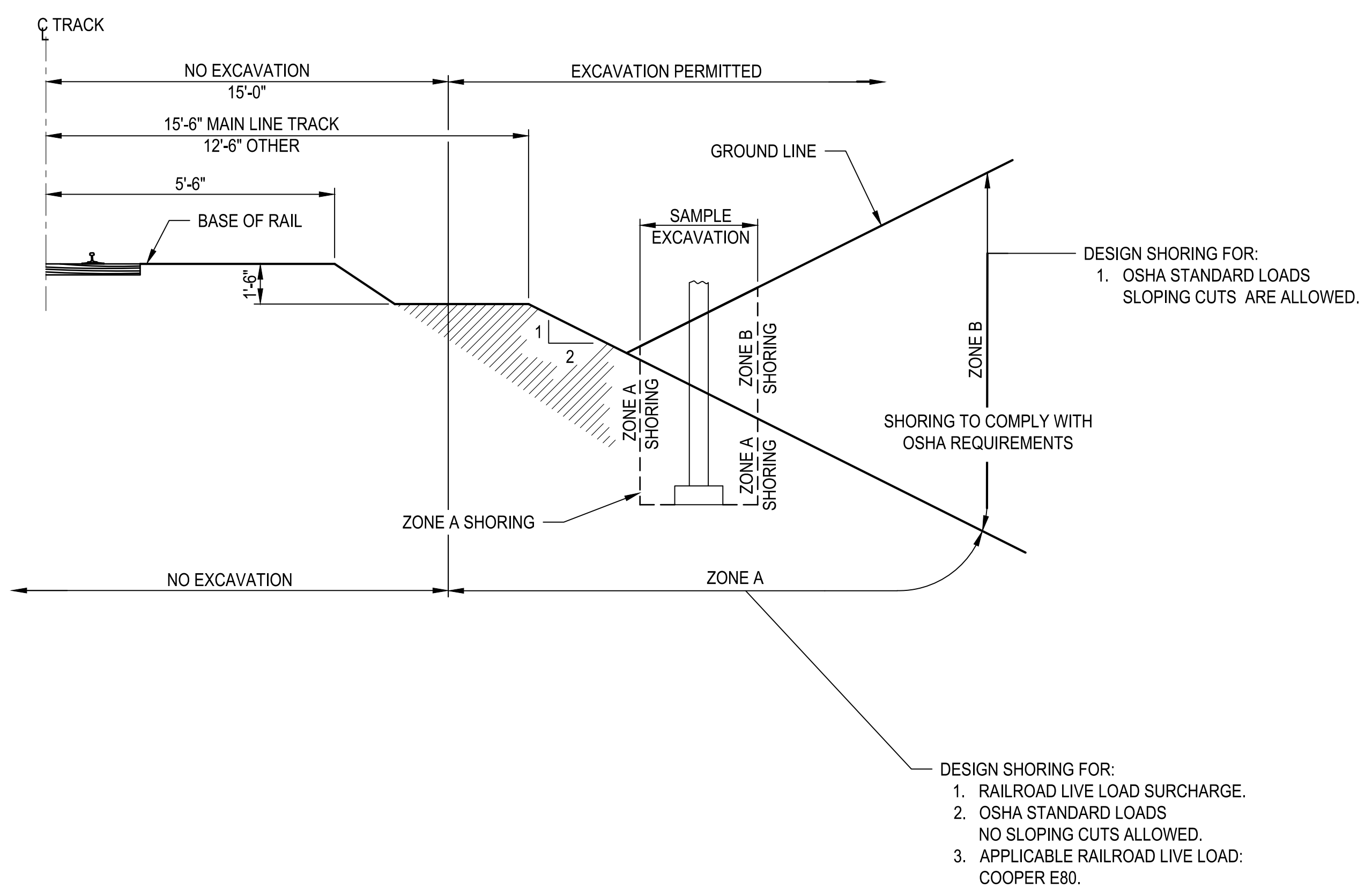
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SOIL DATA SHEET 12 OF 12

LAYOUT: RR PATH: U:\SQA\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Svcs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:41:18 AM



MINIMUM CONSTRUCTION CLEARANCE ENVELOPE
NORMAL TO RAILROAD



TRACK PROTECTION SHORING REQUIREMENTS

SHORING NOTES

1. ALL DIMENSIONS ARE MEASURED PERPENDICULAR TO CL TRACK.
2. PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR TO SUBMIT FOR APPROVAL BY THE RAILROAD DETAILED PLANS INDICATING THE NATURE AND EXTENT OF THE TRACK PROTECTION SHORING PROPOSED. THE CONTRACTOR TO INSTALL THE TEMPORARY SHORING SYSTEM PER THE APPROVED PLANS. DESIGN OF THE TEMPORARY SHORING SYSTEM TO COMPLY WITH UPRR GUIDELINES FOR TEMPORARY SHORING.
3. FOR EXCAVATIONS WHICH ENCOACH INTO ZONE A OR B, SHORING PLANS MUST BE ACCOMPANIED BY DESIGN CALCULATIONS, PLANS AND CALCULATIONS MUST BE SIGNED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF UTAH.

RAILROAD GENERAL NOTES

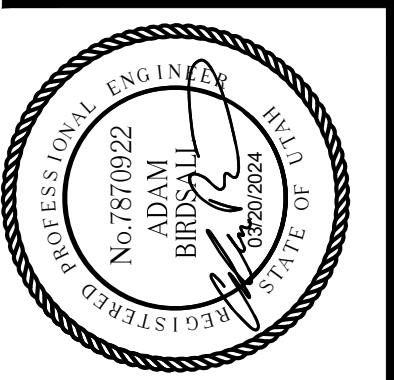
1. RAILROAD COMPANY REVIEW AND APPROVAL OF SHORING, ERECTION, DEMOLITION, AND FALSEWORK IS REQUIRED.
2. DO NOT INCREASE THE QUANTITY AND CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S DITCHES AND DRAINAGE STRUCTURES.
3. VERIFY THE ELEVATION OF THE EXISTING TOP OF RAIL PROFILE BEFORE BEGINNING CONSTRUCTION. NOTIFY THE RAILROAD COMPANY AND THE ENGINEER OF DISCREPANCIES BEFORE BEGINNING CONSTRUCTION.
4. SUBMIT A METHOD OF EROSION AND SEDIMENT CONTROL AND RECEIVE APPROVAL FROM THE RAILROAD COMPANY BEFORE BEGINNING GRADING WHICH IMPACTS THE RAILROAD PROPERTY.
5. COMPLY WITH THE RAILROAD COMPANY'S DEMOLITION REQUIREMENTS FOR DEMOLITIONS WITHIN THE RAILROAD COMPANY'S RIGHT-OF-WAY AND/OR DEMOLITION THAT MAY IMPACT THE RAILROAD COMPANY'S TRACKS OR OPERATIONS.
6. DESIGN ERECTION PROCEDURES OVER THE RAILROAD COMPANY'S RIGHT-OF-WAY TO CAUSE NO INTERRUPTION TO THE RAILROAD COMPANY'S OPERATION, ENABLING THE TRACK(S) TO REMAIN OPEN TO TRAFFIC PER THE RAILROAD COMPANY'S REQUIREMENTS.
7. NO WORK MAY BE PERFORMED WITHIN 50 FEET OF THE TRACK CENTERLINE WHEN A TRAIN PASSES THE SITE. ALL PERSONNEL MUST CLEAR THE AREA WITHIN 25 FEET OF THE TRACK AND ALL EQUIPMENT MUST BE SECURED WHEN A TRAIN PASSES THE SITE.

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ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

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| 03/20/2024 | TWP | NCC |
| JOB No. 344-8541-002 | DRAWN | APPROVED |
| | SLO | AJB |

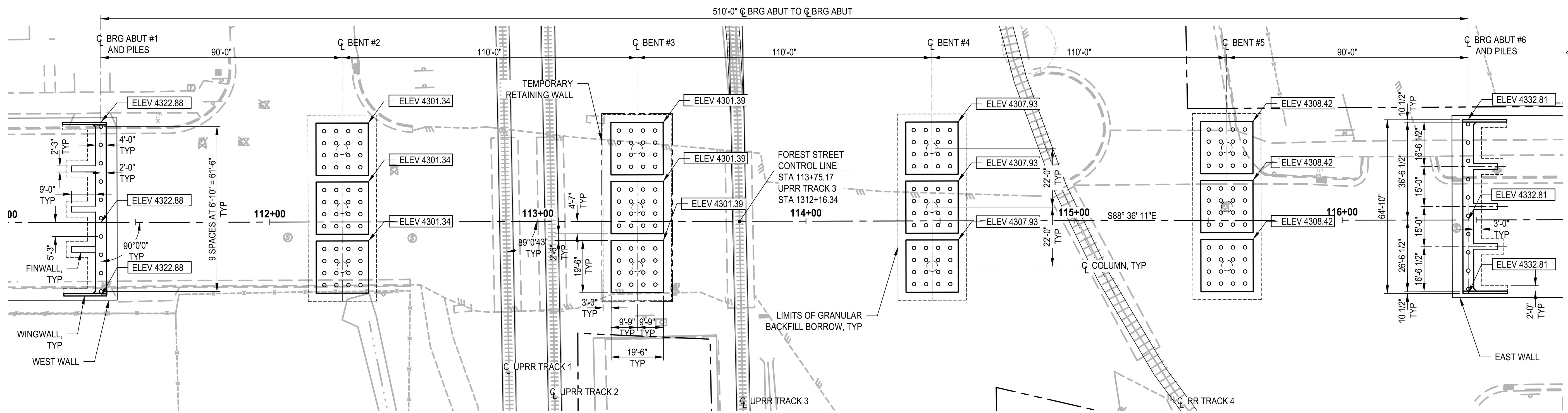


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

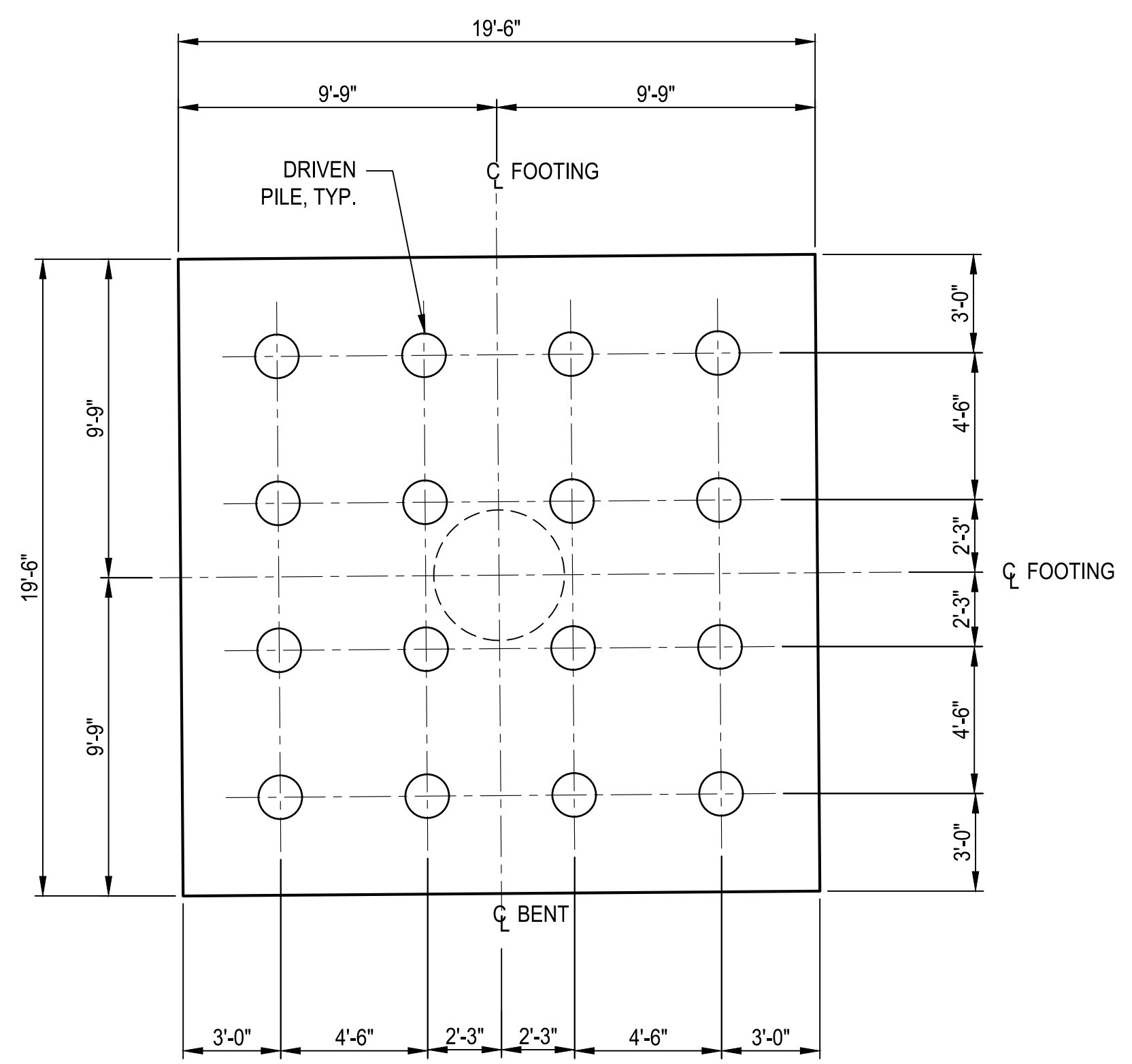
RAILROAD NOTES AND CLEARANCE

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18 OF 59
S18

PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Structure
 PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:41:46 AM
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FOUNDATION PLAN



TYPICAL BENT FOOTING AND PILE LAYOUT

NOTES

- ELEVATIONS ENCLOSED IN RECTANGLES INDICATE BOTTOM OF FOOTING ELEVATIONS.
- SEE SOIL DATA SHEETS FOR SOIL EXPLORATION INFORMATION.
- SEE "PILE DETAILS 2 OF 2 - 16 INCH FIXED" FOR SIZE, DETAILS AND THE PILE EXTENSION INTO THE FOUNDATION. MAINTAIN A MINIMUM EDGE DISTANCE OF 6" AND A MINIMUM PILE SPACING OF 3'-6".
- PERFORM AT LEAST ONE PDA TEST AT EACH FOUNDATION SUPPORT.
- GRANULAR BACKFILL BORROW EXTENDS FROM THE BOTTOM OF THE ABUTMENT AND WINGWALL TO THE APPROACH SLAB.
- BOTTOM OF FOUNDATIONS ARE LEVEL.
- OVER EXCAVATE 2'-0" BELOW BOTTOM OF BENT FOOTING ELEVATIONS AND REPLACE WITH GRANULAR BACKFILL BORROW TO THE TOP OF THE BENT FOOTING.
- ALL DIMENSIONS SHOWN ON ABUTMENTS ARE TYPICAL FOR ALL ABUTMENTS.
- SEE "UTILITY PLAN" FOR UTILITY INFORMATION.

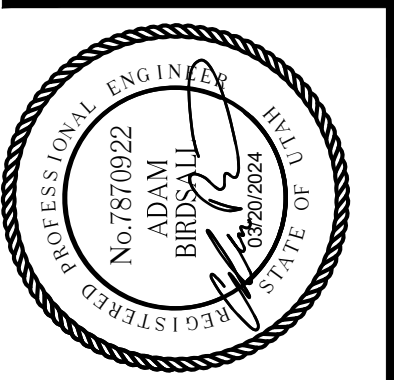
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|--------------------------|-------------|---------|-------|
| GRANULAR BACKFILL BORROW | ABUTMENT #1 | 98 | CU YD |
| GRANULAR BACKFILL BORROW | BENT #2 | 526 | CU YD |
| GRANULAR BACKFILL BORROW | BENT #3 | 526 | CU YD |
| GRANULAR BACKFILL BORROW | BENT #4 | 526 | CU YD |
| GRANULAR BACKFILL BORROW | BENT #5 | 526 | CU YD |
| GRANULAR BACKFILL BORROW | ABUTMENT #6 | 98 | CU YD |

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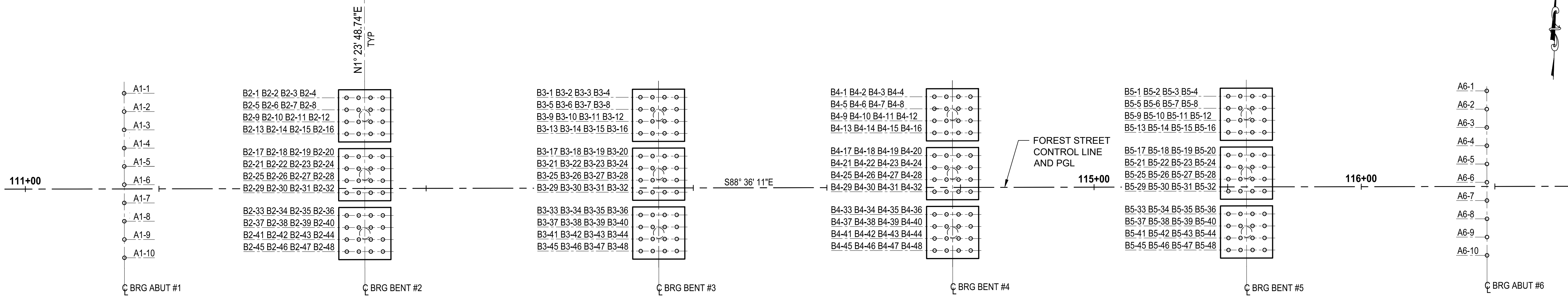
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 CHECKED: EA
 APPROVED: AUB



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FOUNDATION PLAN

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PILING PLAN

| | |
|-------|--|
| A1-1 | |
| A1-2 | |
| A1-3 | |
| A1-4 | |
| A1-5 | |
| A1-6 | |
| A1-7 | |
| A1-8 | |
| A1-9 | |
| A1-10 | |

| | | | |
|-------|-------|-------|-------|
| B2-1 | B2-13 | B2-25 | B2-37 |
| B2-2 | B2-14 | B2-26 | B2-38 |
| B2-3 | B2-15 | B2-27 | B2-39 |
| B2-4 | B2-16 | B2-28 | B2-40 |
| B2-5 | B2-17 | B2-29 | B2-41 |
| B2-6 | B2-18 | B2-30 | B2-42 |
| B2-7 | B2-19 | B2-31 | B2-43 |
| B2-8 | B2-20 | B2-32 | B2-44 |
| B2-9 | B2-21 | B2-33 | B2-45 |
| B2-10 | B2-22 | B2-34 | B2-46 |
| B2-11 | B2-23 | B2-35 | B2-47 |
| B2-12 | B2-24 | B2-36 | B2-48 |

| | | | |
|-------|-------|-------|-------|
| B4-1 | B4-13 | B4-25 | B4-37 |
| B4-2 | B4-14 | B4-26 | B4-38 |
| B4-3 | B4-15 | B4-27 | B4-39 |
| B4-4 | B4-16 | B4-28 | B4-40 |
| B4-5 | B4-17 | B4-29 | B4-41 |
| B4-6 | B4-18 | B4-30 | B4-42 |
| B4-7 | B4-19 | B4-31 | B4-43 |
| B4-8 | B4-20 | B4-32 | B4-44 |
| B4-9 | B4-21 | B4-33 | B4-45 |
| B4-10 | B4-22 | B4-34 | B4-46 |
| B4-11 | B4-23 | B4-35 | B4-47 |
| B4-12 | B4-24 | B4-36 | B4-48 |

| | | | |
|-------|-------|-------|-------|
| B3-1 | B3-13 | B3-25 | B3-37 |
| B3-2 | B3-14 | B3-26 | B3-38 |
| B3-3 | B3-15 | B3-27 | B3-39 |
| B3-4 | B3-16 | B3-28 | B3-40 |
| B3-5 | B3-17 | B3-29 | B3-41 |
| B3-6 | B3-18 | B3-30 | B3-42 |
| B3-7 | B3-19 | B3-31 | B3-43 |
| B3-8 | B3-20 | B3-32 | B3-44 |
| B3-9 | B3-21 | B3-33 | B3-45 |
| B3-10 | B3-22 | B3-34 | B3-46 |
| B3-11 | B3-23 | B3-35 | B3-47 |
| B3-12 | B3-24 | B3-36 | B3-48 |

| | | | |
|-------|-------|-------|-------|
| B5-1 | B5-13 | B5-25 | B5-37 |
| B5-2 | B5-14 | B5-26 | B5-38 |
| B5-3 | B5-15 | B5-27 | B5-39 |
| B5-4 | B5-16 | B5-28 | B5-40 |
| B5-5 | B5-17 | B5-29 | B5-41 |
| B5-6 | B5-18 | B5-30 | B5-42 |
| B5-7 | B5-19 | B5-31 | B5-43 |
| B5-8 | B5-20 | B5-32 | B5-44 |
| B5-9 | B5-21 | B5-33 | B5-45 |
| B5-10 | B5-22 | B5-34 | B5-46 |
| B5-11 | B5-23 | B5-35 | B5-47 |
| B5-12 | B5-24 | B5-36 | B5-48 |

| | |
|-------|--|
| A6-1 | |
| A6-2 | |
| A6-3 | |
| A6-4 | |
| A6-5 | |
| A6-6 | |
| A6-7 | |
| A6-8 | |
| A6-9 | |
| A6-10 | |

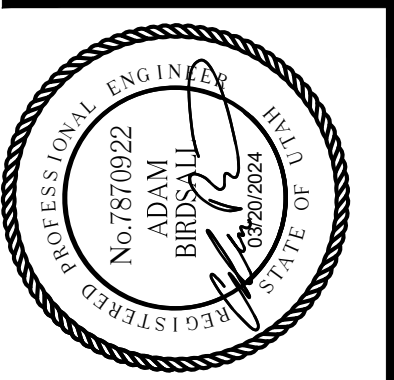
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JOB No.: 344-8541-002

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DRAWN: SLO

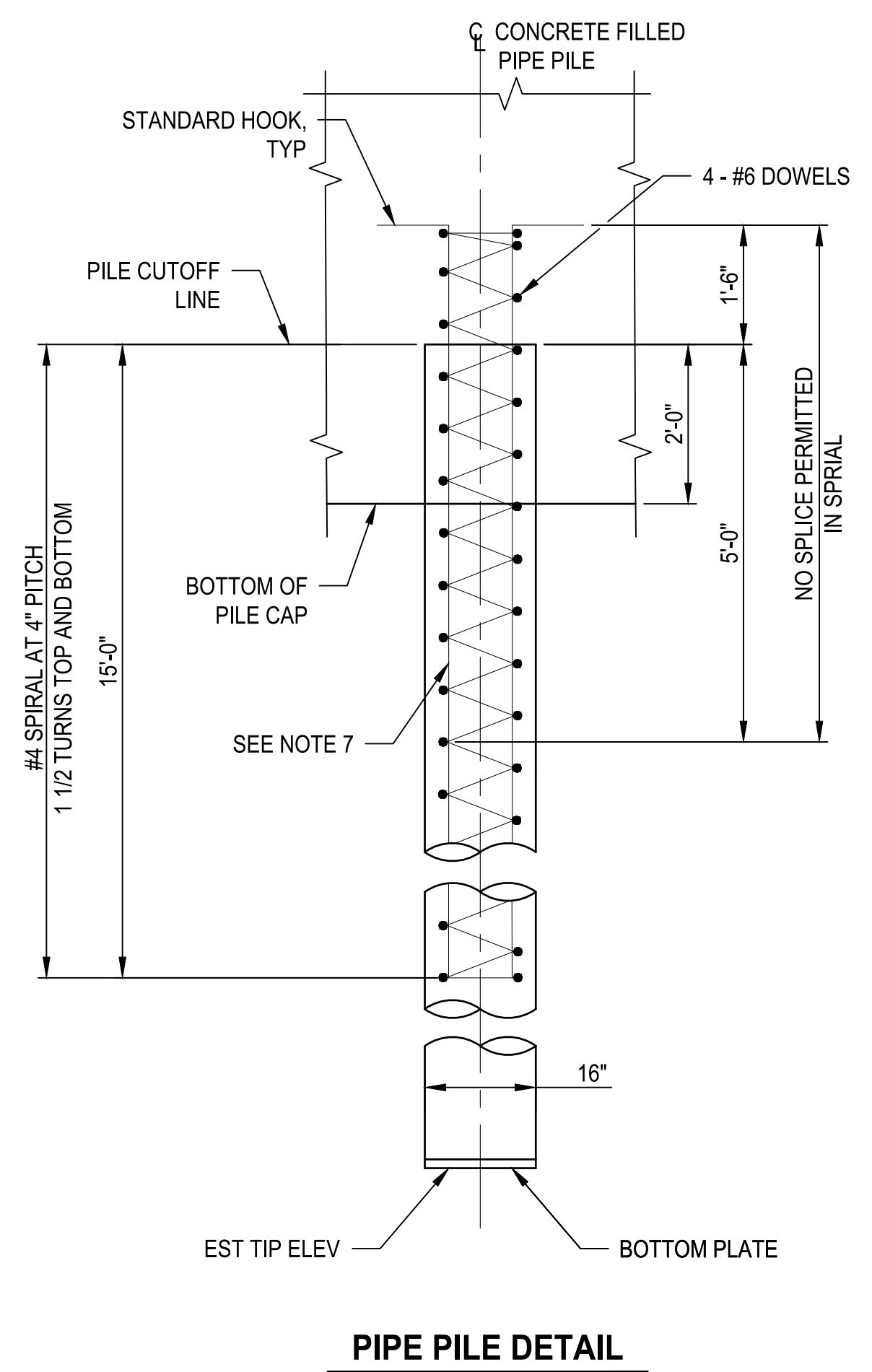
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APPROVED: AUB



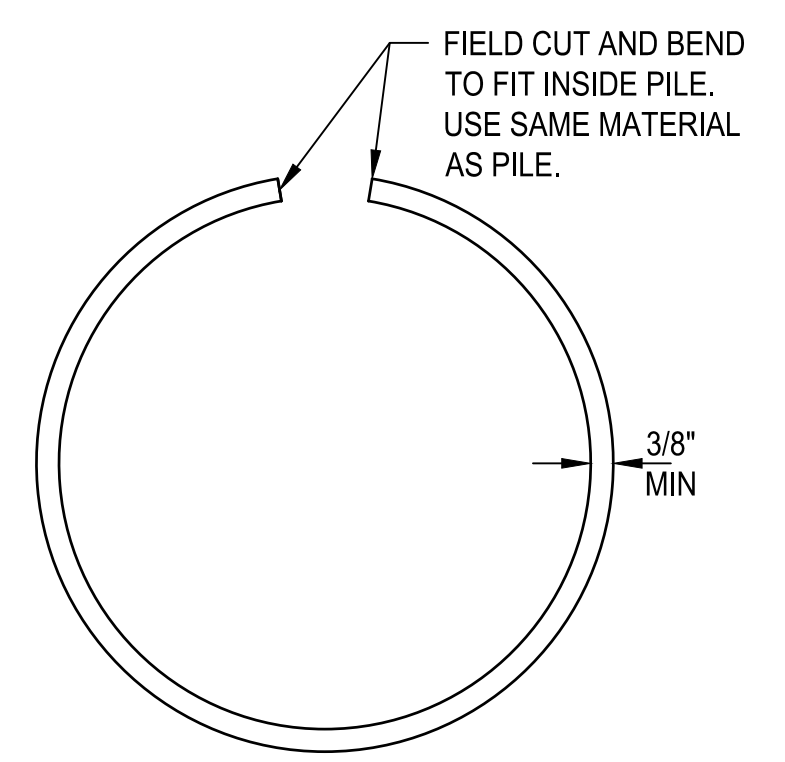
PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

**PILE DETAILS
1 OF 2**

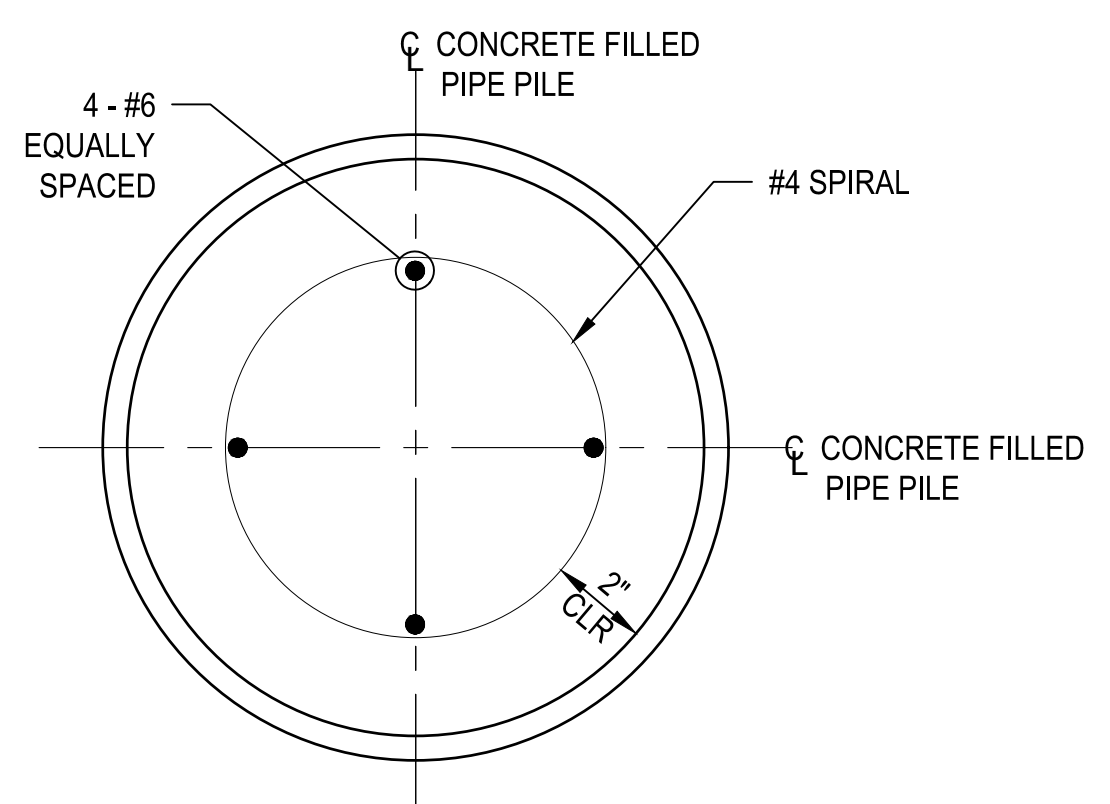
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 PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:42:09 AM



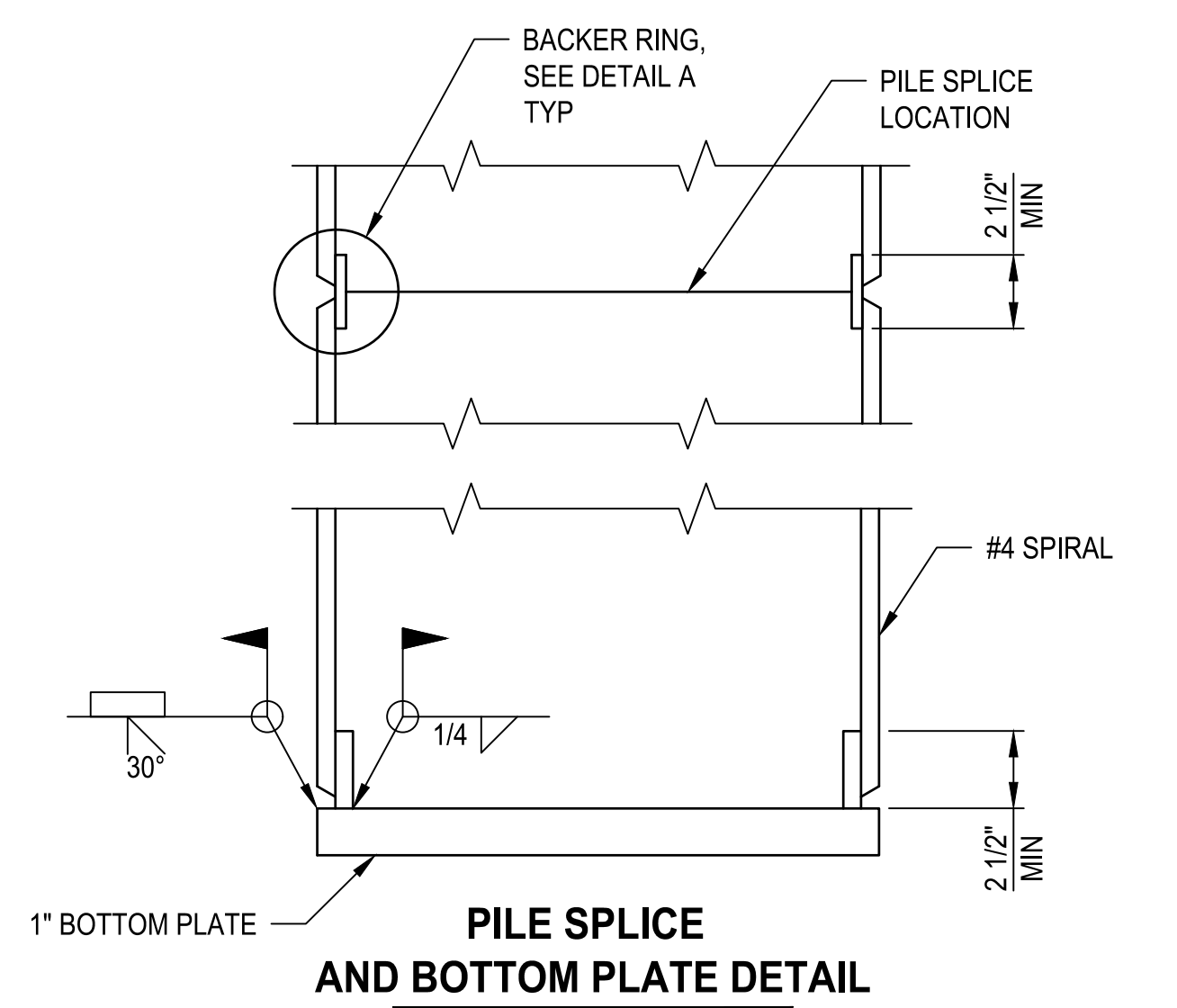
PIPE PILE DETAIL



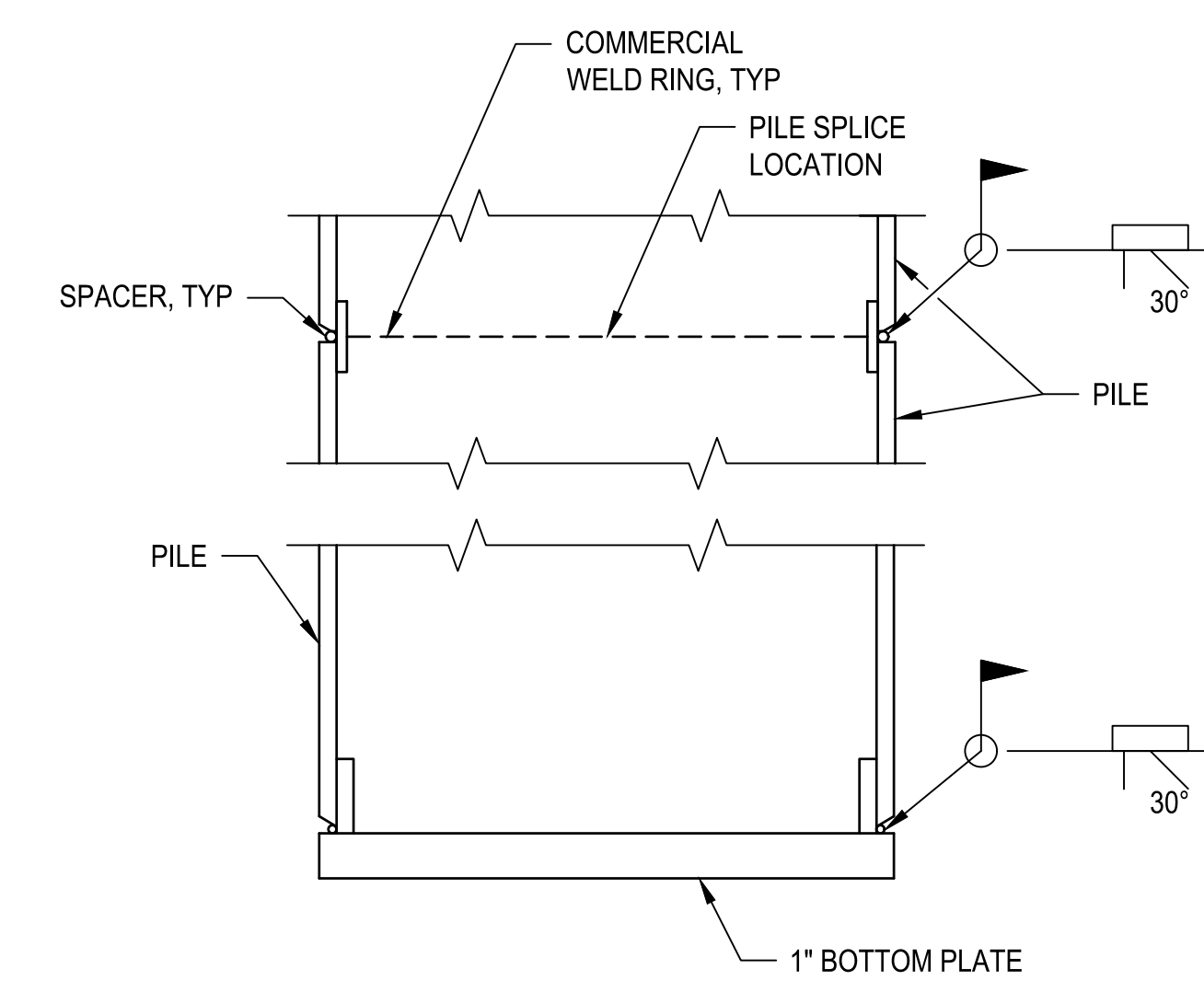
BACKER RING DETAIL



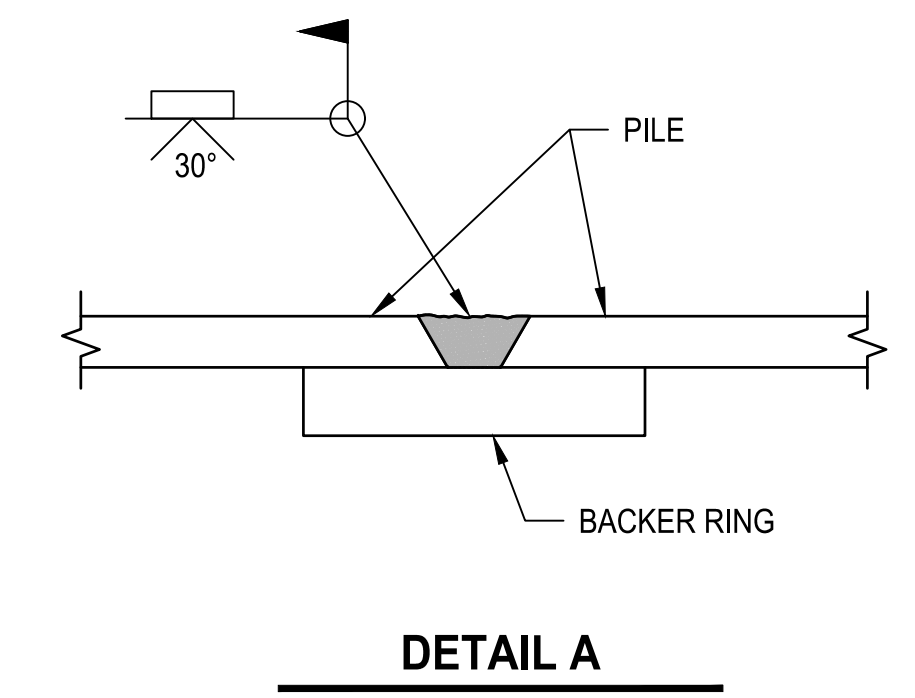
SECTION A-A



PILE SPLICE AND BOTTOM PLATE DETAIL



ALTERNATE PILE SPLICE AND BOTTOM PLATE DETAIL



DETAIL A

| PHYSICAL PILE DATA | | | | | | | |
|--------------------|------------|-----------------|------------------------|--------------|--------------|------------|-----------------|
| LOCATION | PILE SIZE | NUMBER OF PILES | MIN DRIVING RESISTANCE | EST TIP ELEV | MIN TIP ELEV | SCOUR ELEV | EST PILE LENGTH |
| | | | KIPS | FEET | FEET | FEET | FEET |
| ABUTMENT #1 | 16" X 1/2" | 10 | 740.0 | 4219.0 | 4222.0 | N/A | 106 |
| BENT #2 | 16" X 1/2" | 16 | 580.0 | 4222.0 | 4225.0 | N/A | 82 |
| BENT #3 | 16" X 1/2" | 16 | 580.0 | 4222.0 | 4225.0 | N/A | 82 |
| BENT #4 | 16" X 1/2" | 16 | 580.0 | 4222.0 | 4225.0 | N/A | 88 |
| BENT #5 | 16" X 1/2" | 16 | 580.0 | 4222.0 | 4225.0 | N/A | 89 |
| ABUTMENT #6 | 16" X 1/2" | 10 | 740.0 | 4222.0 | 4225.0 | N/A | 113 |

| PILE LOADS | | | | | | | | | | | | |
|-------------|----------------|----------------|----------------|----------------|----------------|--------------------|---------------------|---------------------|-----------------------|----------------|---------------------|---------------------|
| LOCATION | DC γ = 1.00 | DW γ = 1.00 | EH γ = 1.00 | DD γ = 1.00 | LL γ = 1.00 | UPLIFT γ = 1.00 | EQ HORZ γ = 1.00 | EQ VERT γ = 1.00 | EQ UPLIFT γ = 1.00 | BR γ = 1.00 | TU LONG γ = 1.00 | TU VERT γ = 1.00 |
| | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS |
| ABUTMENT #1 | 135.0 | 11.4 | 0.0 | 460.0 | 18.2 | N/A | 111.7 | 215.6 | N/A | 3.6 | N/A | N/A |
| BENT #2 | 71.9 | 5.7 | 0.0 | 340.0 | 18.7 | 88.9 | 41.3 | 231.8 | 36.1 | 0.8 | 11.3 | 88.2 |
| BENT #3 | 71.9 | 5.7 | 0.0 | 340.0 | 18.7 | 88.9 | 41.3 | 231.8 | 36.1 | 0.8 | 11.3 | 88.2 |
| BENT #4 | 71.9 | 5.7 | 0.0 | 340.0 | 18.7 | 88.9 | 41.3 | 231.8 | 36.1 | 0.8 | 11.3 | 88.2 |
| BENT #5 | 71.9 | 5.7 | 0.0 | 340.0 | 18.7 | 88.9 | 41.3 | 231.8 | 36.1 | 0.8 | 11.3 | 88.2 |
| ABUTMENT #6 | 135.0 | 11.4 | 0.0 | 370.0 | 18.2 | N/A | 111.7 | 215.6 | N/A | 3.6 | N/A | N/A |

| PILE RESISTANCE | | | | | | | | | | |
|-----------------|------------------|----------------------------|----------------------------------|--------------------------------|-----------------------------|-------------------------------------|------------------------------------|---------------------------------|---|-------------------------------------|
| LOCATION | Q _{ULT} | Q _R φ = 0.65 | Q _{LATERAL} φ = 1.00 | Q _{SCOUR} φ = 1.00 | Q _{EQ} φ = 1.00 | Q _{EQ LATERAL} φ = 1.00 | Q _{EQ UPLIFT} φ = 0.80 | Q _{UPLIFT} φ = 0.50 | Q _{DL} γ _{DD} = 1.00 | Q _{SERVICE} ** φ = 1.00 |
| | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS | KIPS |
| ABUTMENT #1 | 740.0 | 481.0 | 96.0 | N/A | 345.0 | 115.0 | 276.0 | 145.0 | 945.0 | 259.0 |
| BENT #2 | 580.0 | 377.0 | 78.0 | N/A | 415.0 | 98.0 | 332.0 | 115.0 | 1,113.0 | 242.0 |
| BENT #3 | 580.0 | 377.0 | 78.0 | N/A | 415.0 | 98.0 | 332.0 | 115.0 | 1,113.0 | 242.0 |
| BENT #4 | 580.0 | 377.0 | 78.0 | N/A | 415.0 | 98.0 | 332.0 | 115.0 | 1,113.0 | 242.0 |
| BENT #5 | 580.0 | 377.0 | 78.0 | N/A | 415.0 | 98.0 | 332.0 | 115.0 | 1,113.0 | 242.0 |
| ABUTMENT #6 | 620.0 | 403.0 | 97.0 | N/A | 450.0 | 118.0 | 360.0 | 125.0 | 1,071.0 | 259.0 |

* LATERAL DISPLACEMENT = 1.00 INCHES
 ** SETTLEMENT < 1.00 INCHES

NOTES

- FILL PILE WITH CLASS AA(AE) CONCRETE, f_c = 4000 PSI.
- HOLD REINFORCING STEEL IN POSITION DURING PLACEMENT OF CONCRETE IN PILE.
- USE PILES CONFORMING TO ASTM A 252 GRADE 3, f_y = 45 KSI.
- MAXIMUM ALLOWABLE DRIVING STRESS = 0.9 fy.
- USE UNCOATED DEFORMED CARBON STEEL BARS CONFORMING TO ASTM A 706 GRADE 60 OR AASHTO M 31 GRADE 60, MEETING THE DUCTILITY REQUIREMENTS FOR ASTM A 706 FOR VERTICAL BARS IN PILES. USE UNCOATED DEFORMED CARBON STEEL BARS CONFORMING TO AASHTO M 31 GRADE 60 FOR SPIRAL.
- SEE "SOIL DATA SHEET 1 OF 12" TO "SOIL DATA SHEET 12 OF 12" FOR ADDITIONAL DATA.
- NO SPLICES PERMITTED IN VERTICAL REINFORCING STEEL.
- REINFORCING STEEL IS NOT INCLUDED IN REINFORCING SCHEDULE.
- USE A PILE HAMMER WITH A MINIMUM RATED HAMMER ENERGY OF 72 KIP-FT.
- PROVIDE COMMERCIAL WELD RING WITH MATERIAL PROPERTIES MEETING AASHTO/AWS D1.5 BRIDGE WELDING CODE REQUIREMENTS.

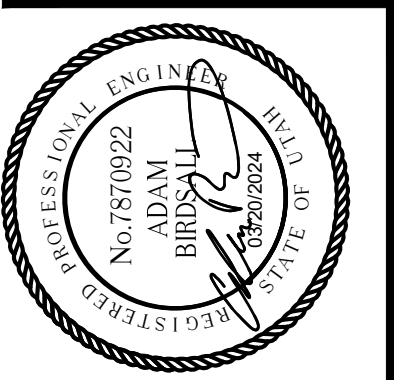
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|---------------------|-------------|---------|------|
| DRIVEN PILE 16 INCH | ABUTMENT #1 | 1,060 | FT |
| DRIVEN PILE 16 INCH | BENT #2 | 3,936 | FT |
| DRIVEN PILE 16 INCH | BENT #3 | 3,936 | FT |
| DRIVEN PILE 16 INCH | BENT #4 | 4,224 | FT |
| DRIVEN PILE 16 INCH | BENT #5 | 4,272 | FT |
| DRIVEN PILE 16 INCH | ABUTMENT #6 | 1,130 | FT |

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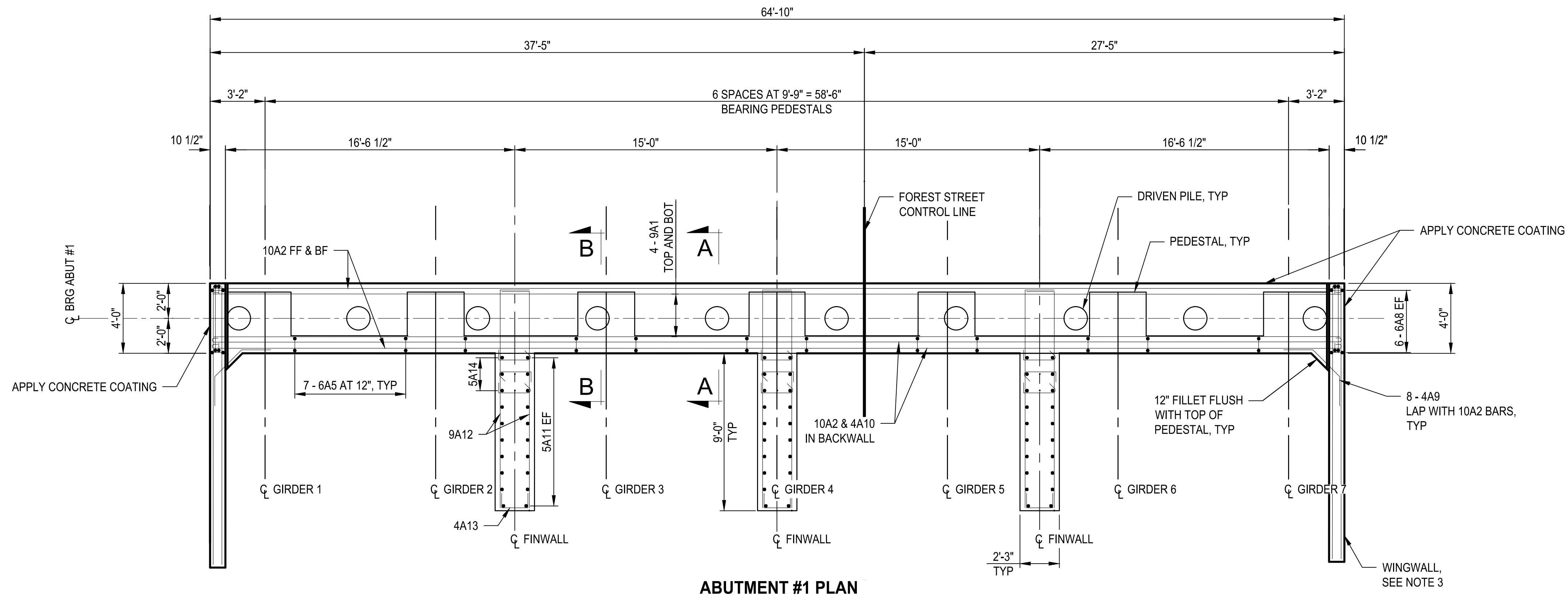
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

**PILE DETAILS
 2 OF 2 -
 16 INCH FIXED**

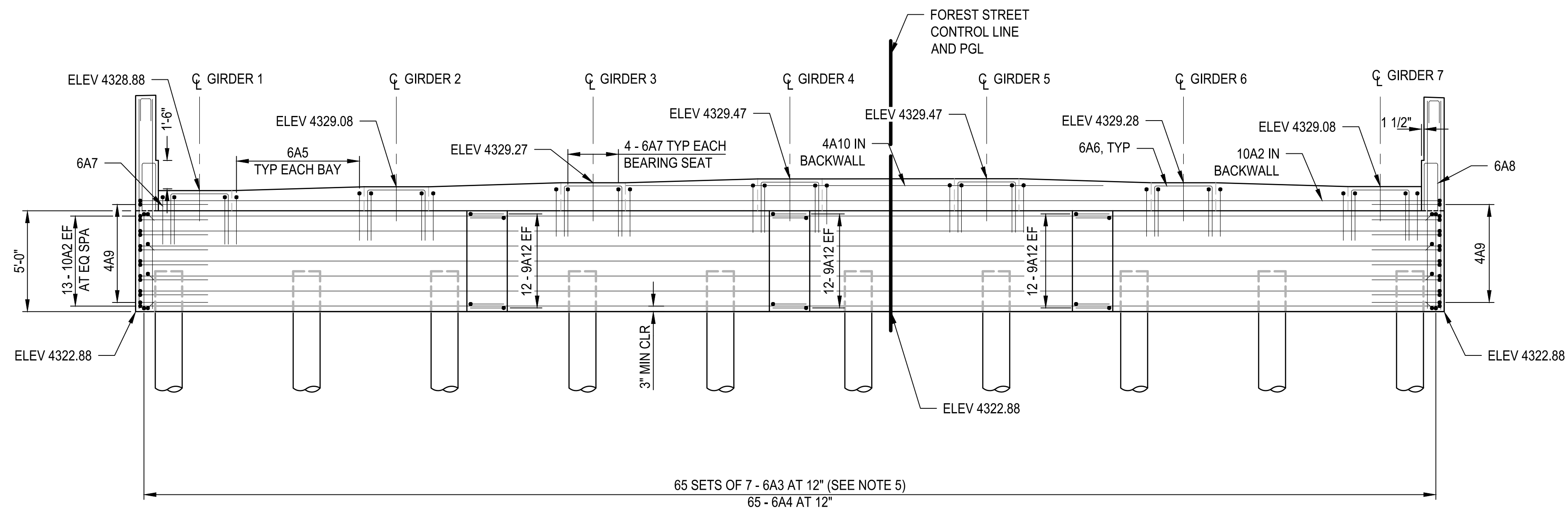
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ABUTMENT #1 PLAN



ABUTMENT #1 ELEVATION
LOOKING AHEAD ON STATION

NOTES

1. SEE "ABUTMENT DETAILS" FOR SECTION A-A AND SECTION B-B.
2. SEE "FOUNDATION PLAN" FOR PILE LAYOUT.
3. SEE "WINGWALL DETAILS" FOR WINGWALL REINFORCEMENT.
4. FINISH BEARING SEAT HIGH AND RUB OR GRIND LEVEL TO ELEVATION SHOWN ± 1/8 INCH. NO GROUTING PERMITTED.
5. ADJUST STIRRUP SPACING TO AVOID PILES AND PILE REINFORCING.
6. STAGGER SPLICES AT 12 FT 6 INCH MINIMUM. SPLICE NO MORE THAN 1/2 OF BARS AT ONE LOCATION.

| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|-------------|---------|-------|
| STRUCTURAL CONCRETE | ABUTMENT #1 | 66 | CU YD |
| CONCRETE COATING | ABUTMENT #1 | 365 | SQ FT |

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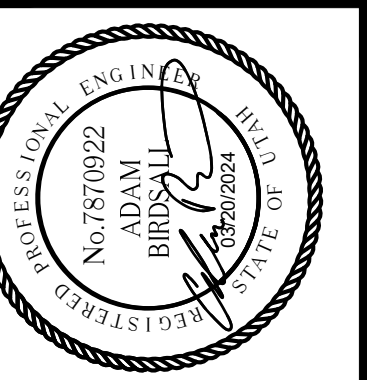
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DRAWN: SLO

CHECKED: EA
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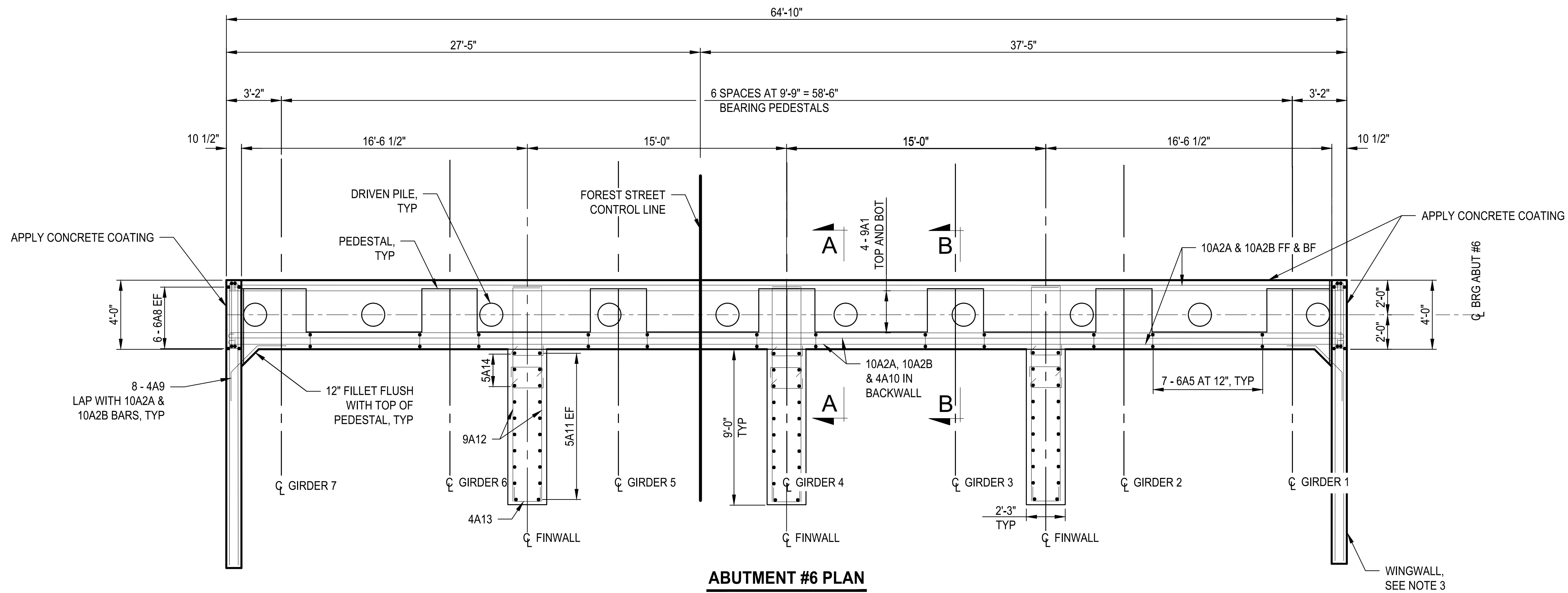
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

ABUTMENT #1 PLAN AND ELEVATION

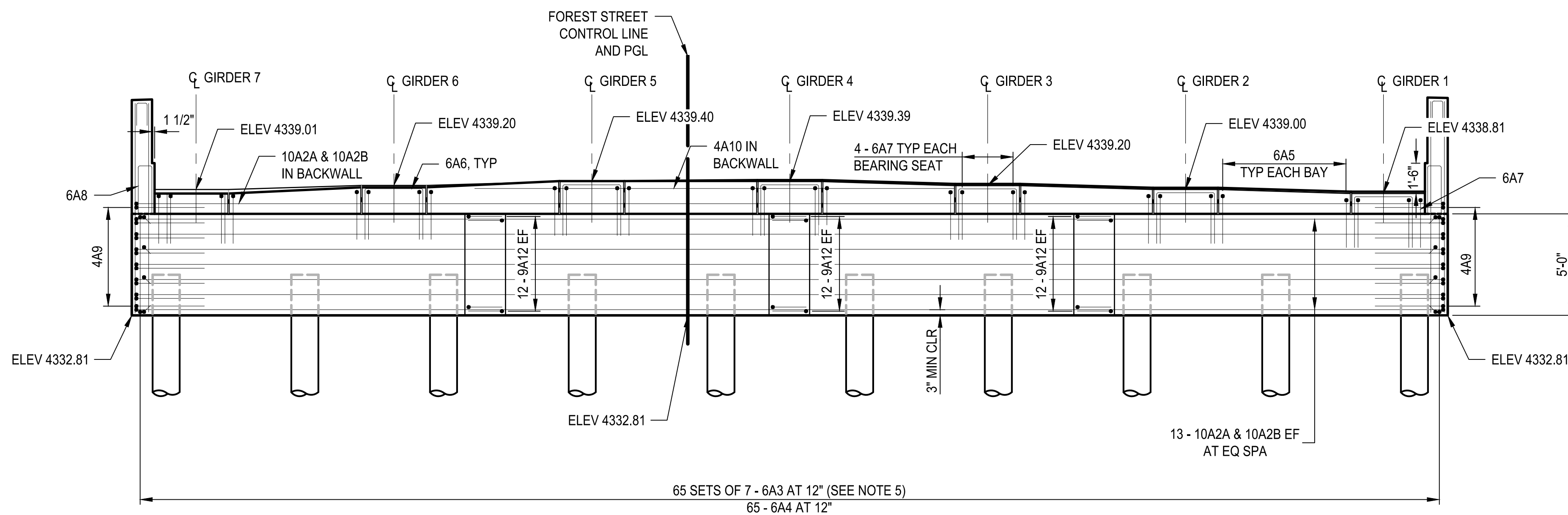
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ABUTMENT #6 PLAN



**ABUTMENT #6 ELEVATION
LOOKING BACK ON STATION**

NOTES

1. SEE "ABUTMENT DETAILS" FOR SECTION A-A AND SECTION B-B.
2. SEE "FOUNDATION PLAN" FOR PILE LAYOUT.
3. SEE "WINGWALL DETAILS" FOR WINGWALL REINFORCEMENT.
4. FINISH BEARING SEAT HIGH AND RUB OR GRIND LEVEL TO ELEVATION SHOWN ± 1/8 INCH. NO GROUTING PERMITTED.
5. ADJUST STIRRUP SPACING TO AVOID PILES AND PILE REINFORCING.
6. STAGGER SPLICES AT 12 FT 6 INCH MINIMUM. SPLICE NO MORE THAN 1/2 OF BARS AT ONE LOCATION.

| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|-------------|---------|-------|
| STRUCTURAL CONCRETE | ABUTMENT #6 | 66 | CU YD |
| CONCRETE COATING | ABUTMENT #6 | 365 | SQ FT |

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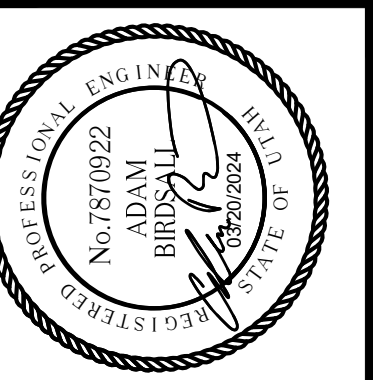
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 APPROVED: AUB

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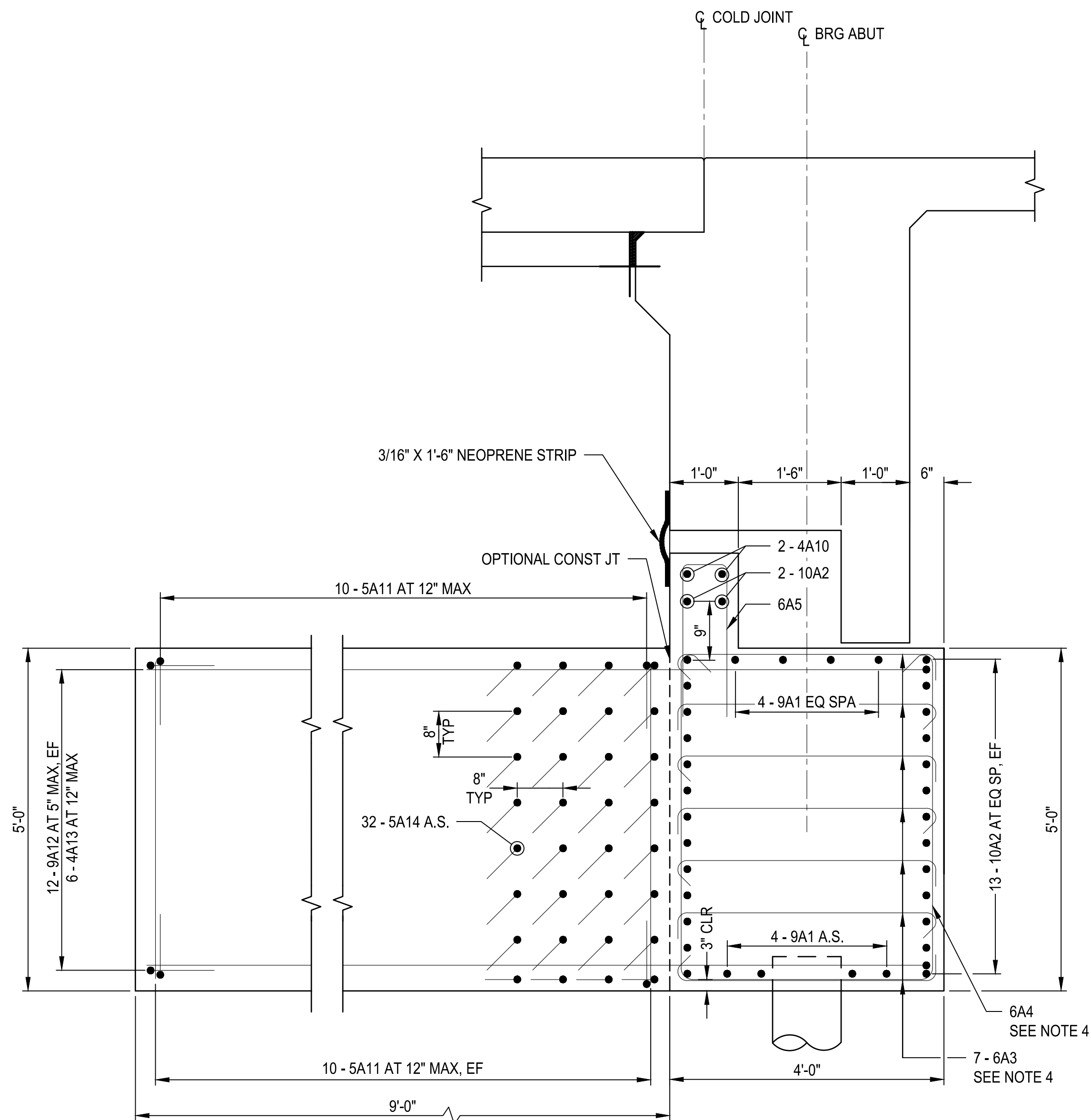
PROJECT NAME
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**ABUTMENT #6
 PLAN
 AND
 ELEVATION**

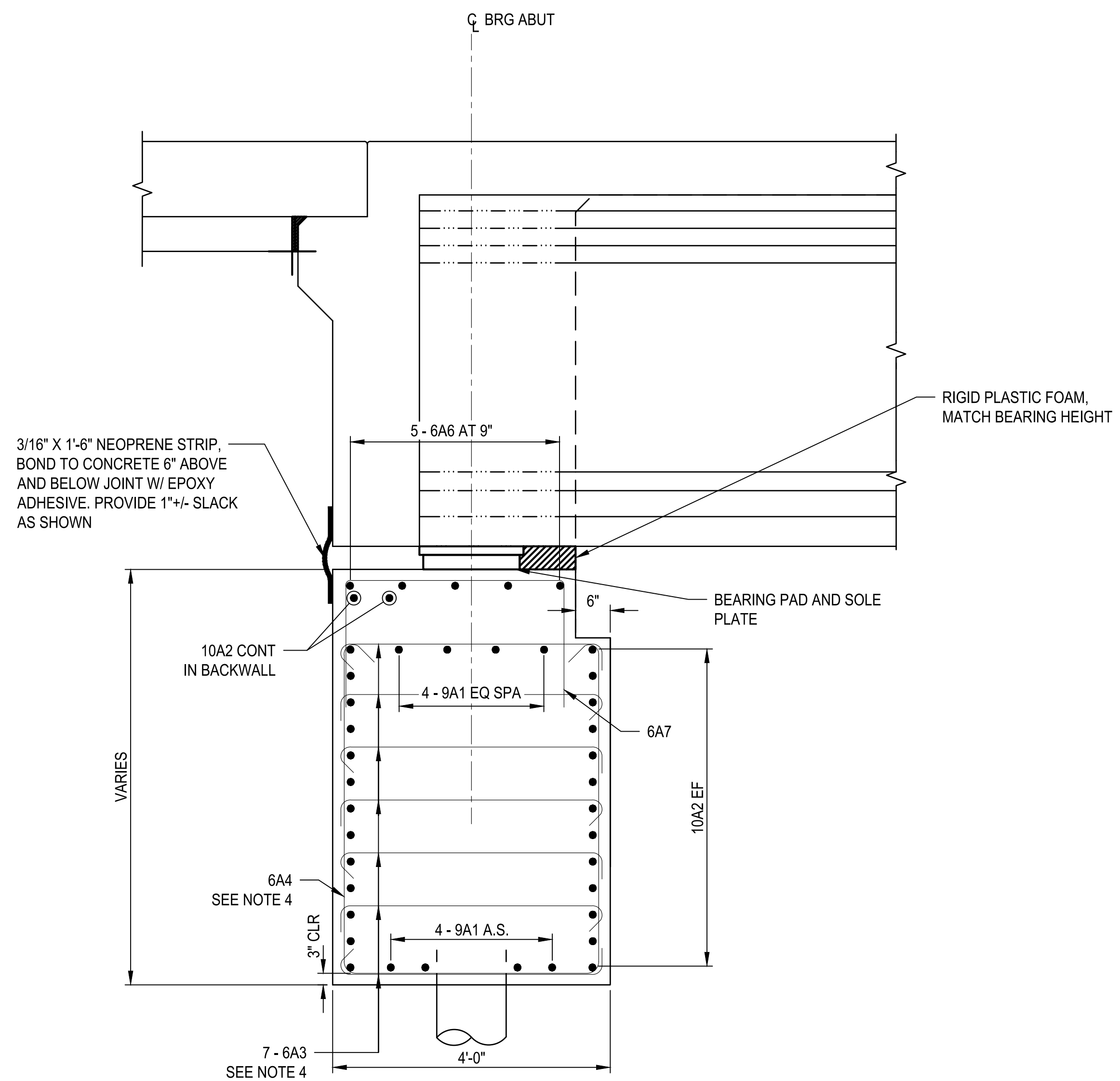
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S23

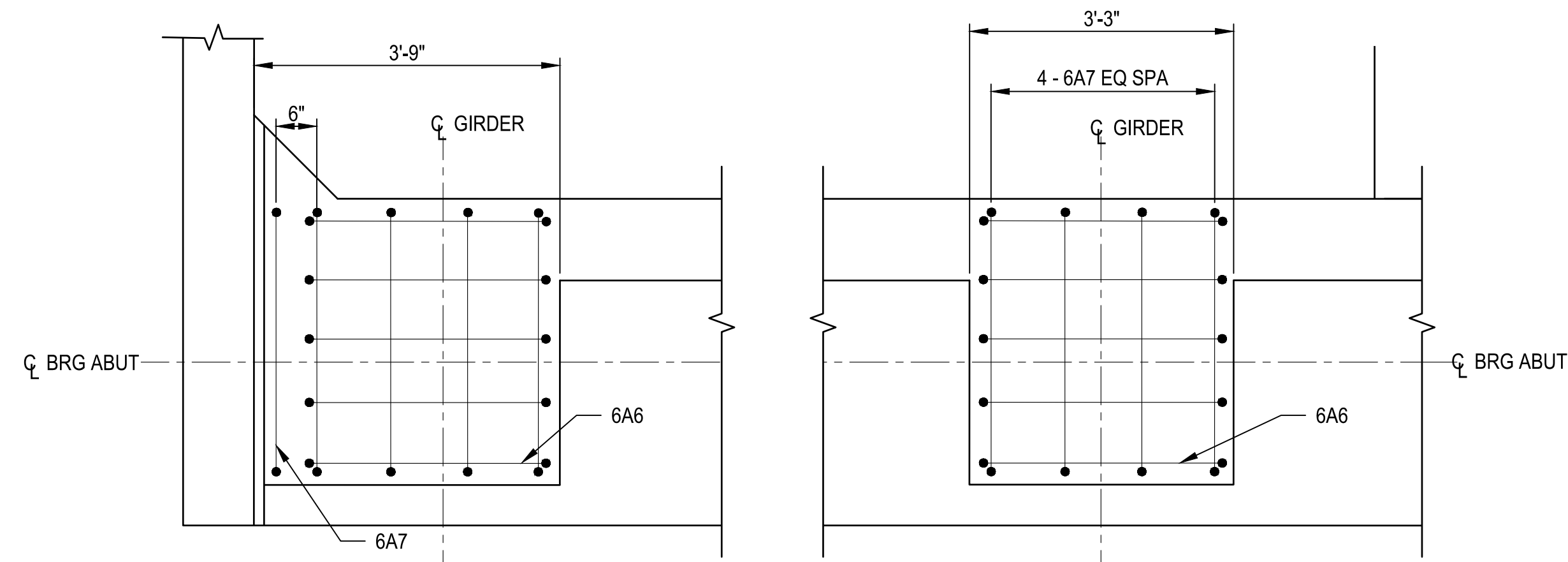
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SECTION A-A



SECTION B-B



PLAN - BEARING PEDESTAL

NOTES

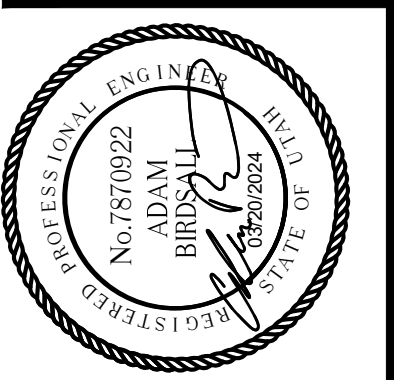
1. SEE "ABUTMENT PLAN AND ELEVATION" FOR LOCATION OF FINWALL.
2. SEE "ABUTMENT PLAN AND ELEVATION" AND "DIAPHRAGM ELEVATIONS" FOR REINFORCING DETAILS.
3. SEE "ABUTMENT PLAN AND ELEVATION" FOR LOCATION OF SECTION A-A AND SECTION B-B.
4. ADJUST STIRRUP SPACING TO AVOID PILES AND PILE REINFORCING.

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 CHECKED: EA
 APPROVED: AUB



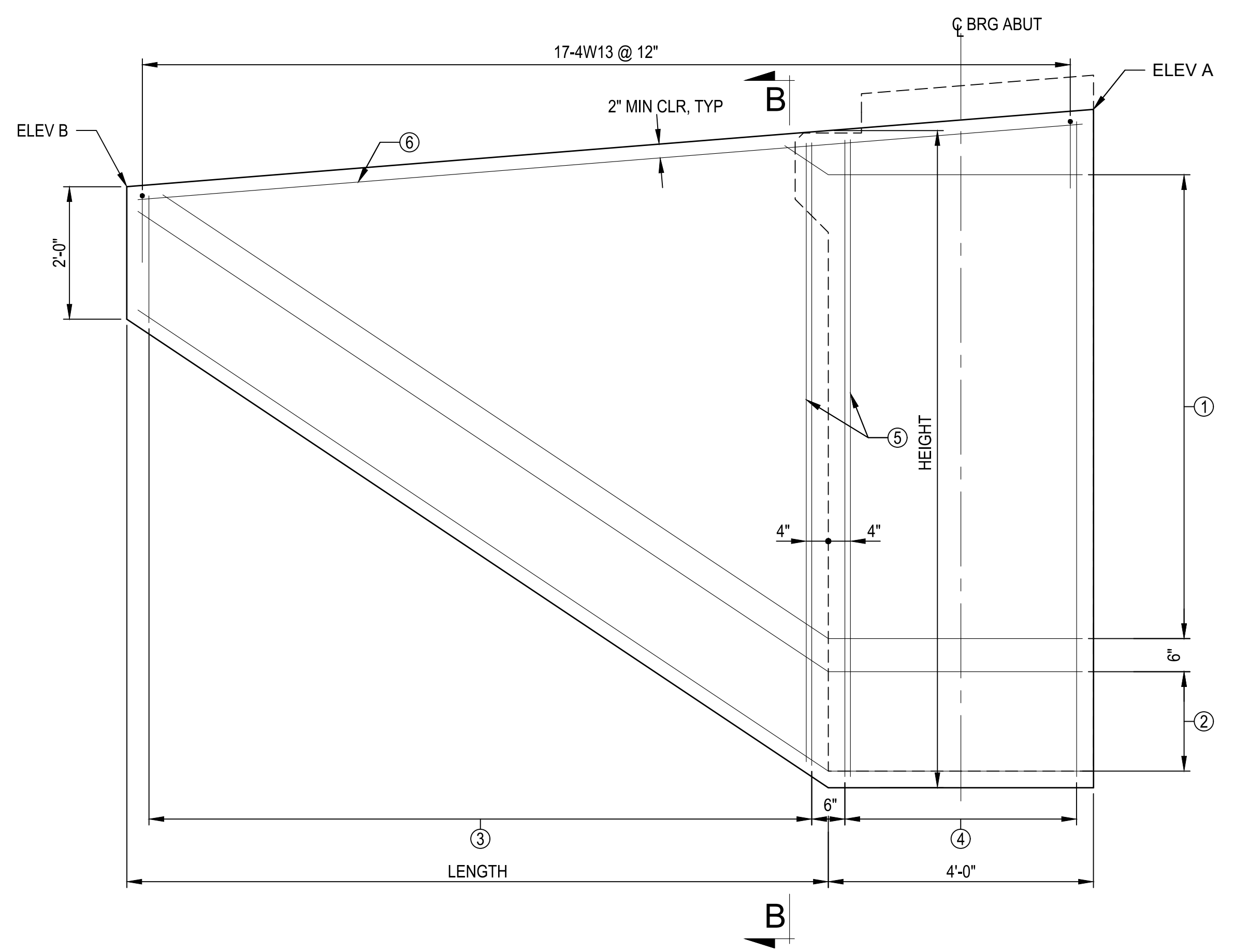
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

ABUTMENT DETAILS

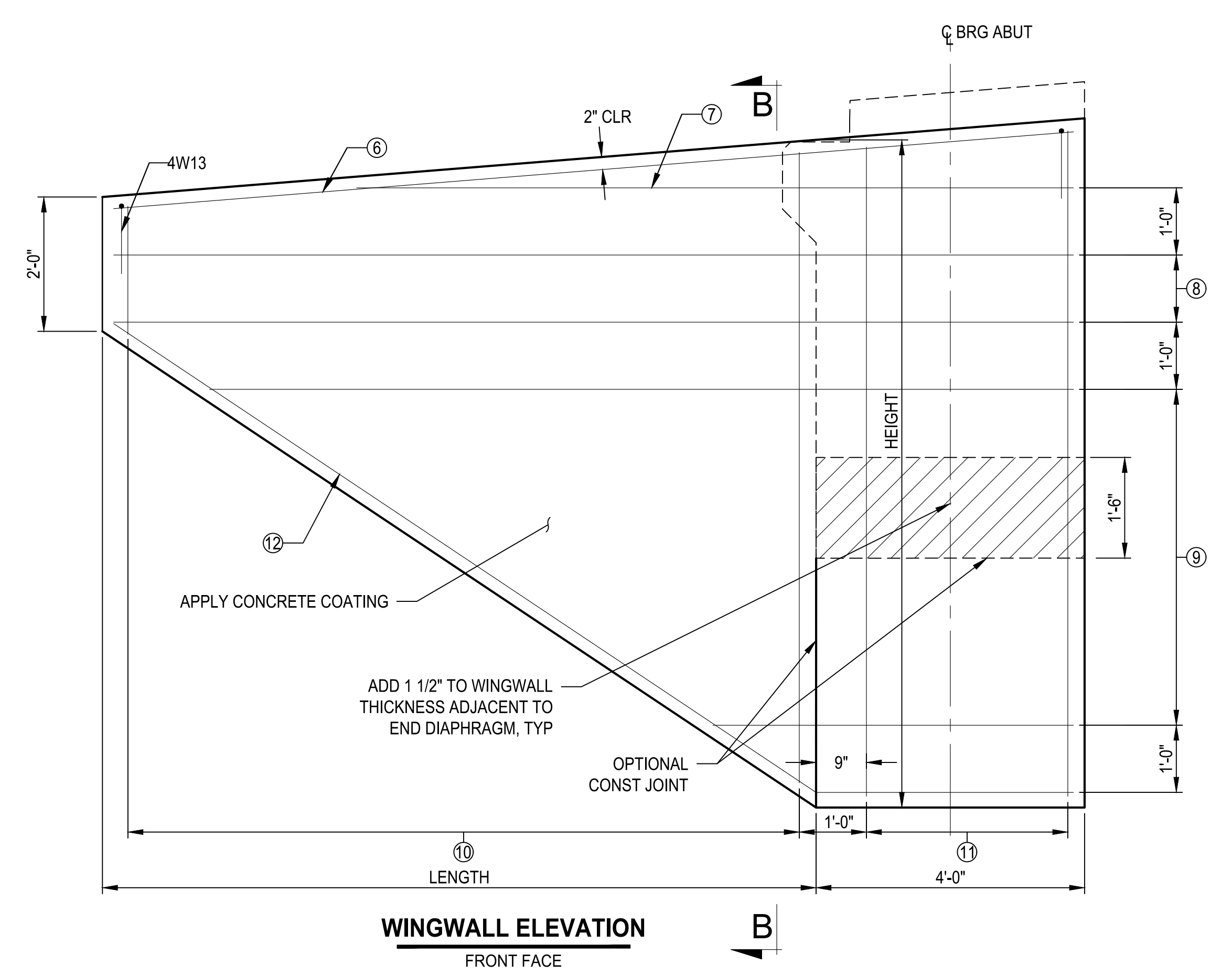
DRAWING NO.
 24 OF 59

S24

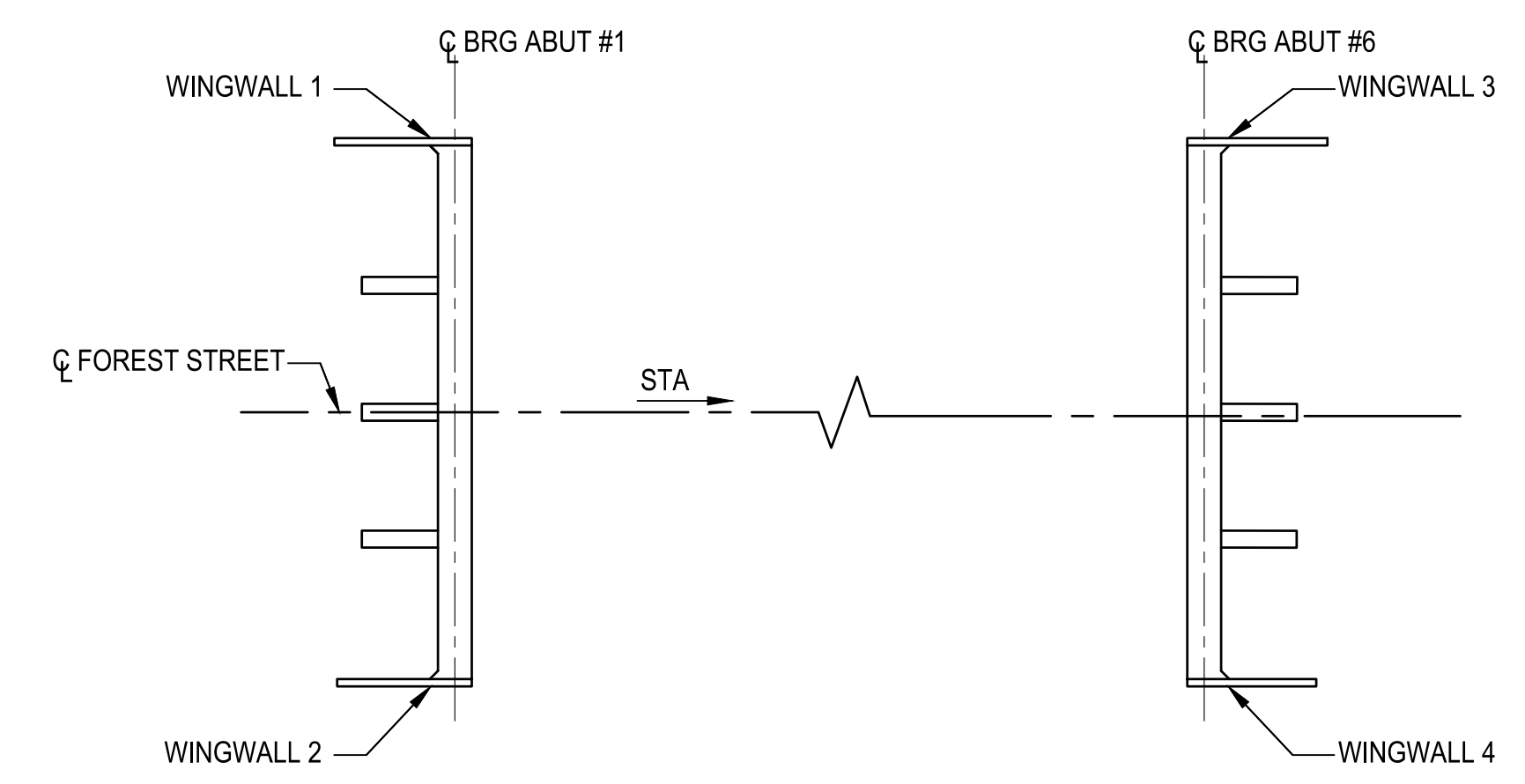
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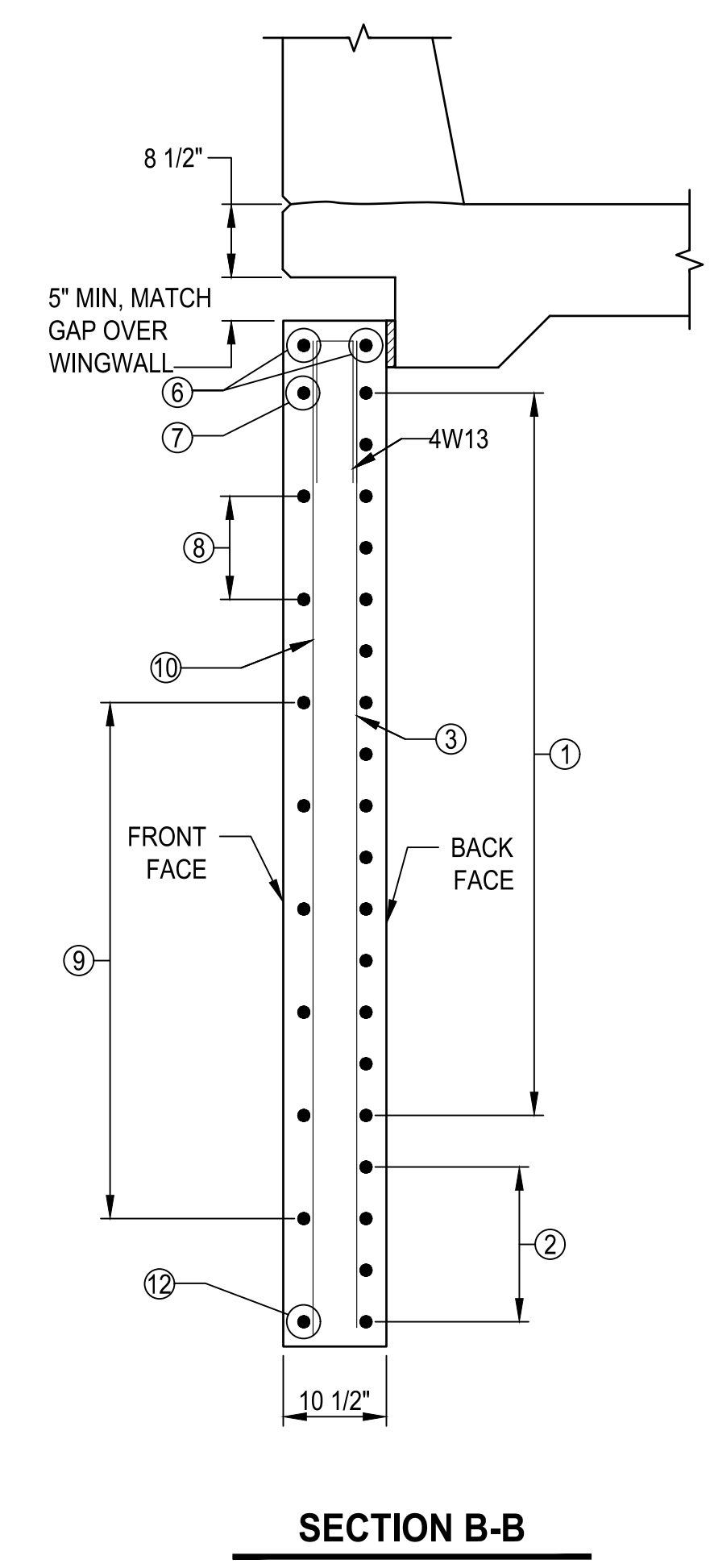
WINGWALL ELEVATION
BACK FACE



WINGWALL ELEVATION
FRONT FACE



WINGWALL KEY PLAN



SECTION B-B

| | ABUTMENT 1 | | ABUTMENT 6 | |
|--------|----------------|-----------------|-----------------|-----------------|
| | WINGWALL 1 | WINGWALL 2 | WINGWALL 3 | WINGWALL 4 |
| LENGTH | 12'-3" | 12'-6" | 12'-7" | 12'-10" |
| HEIGHT | 11'-2" | 11'-4" | 11'-1" | 11'-4" |
| ELEV A | 4334.34' | 4334.54' | 4344.12' | 4344.32' |
| ELEV B | 4333.04' | 4333.22' | 4343.23' | 4343.41' |
| ① | 18-7W1 @ 6" BF | 18-7W14 @ 6" BF | 18-7W26 @ 6" BF | 18-7W38 @ 6" BF |
| ② | 4-7W2 @ 6" BF | 4-7W15 @ 6" BF | 4-7W27 @ 6" BF | 4-7W39 @ 6" BF |
| ③ | 24-6W3 @ 6" BF | 25-6W16 @ 6" BF | 25-6W28 @ 6" BF | 25-6W40 @ 6" BF |
| ④ | 8-6W4 @ 6" BF | 8-6W17 @ 6" BF | 8-6W29 @ 6" BF | 8-6W41 @ 6" BF |
| ⑤ | 2-7W5 BF | 2-7W18 BF | 2-7W30 BF | 2-7W42 BF |
| ⑥ | 2-7W6 BF & FF | 2-7W19 BF & FF | 2-7W31 BF & FF | 2-7W43 BF & FF |
| ⑦ | 1-4W7 FF | 1-4W20 FF | 1-4W32 FF | NOT USED |
| ⑧ | 2-4W8 @ 12" FF | 1-4W21 @ 12" FF | 1-4W33 @ 12" FF | 2-4W44 @ 12" FF |
| ⑨ | 7-4W9 @ 12" | 9-4W22 @ 12" | 8-4W34 @ 12" | 9-4W45 @ 12" |
| ⑩ | 12-4W10 @ 12" | 13-4W23 @ 12" | 13-4W35 @ 12" | 13-4W46 @ 12" |
| ⑪ | 4-4W11 @ 12" | 4-4W24 @ 12" | 4-4W36 @ 12" | 4-4W47 @ 12" |
| ⑫ | 1-4W12 FF | 1-4W25 FF | 1-4W37 FF | 1-4W48 FF |

| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|------------|---------|-------|
| STRUCTURAL CONCRETE | WINGWALL 1 | 2.60 | CU YD |
| STRUCTURAL CONCRETE | WINGWALL 2 | 2.70 | CU YD |
| STRUCTURAL CONCRETE | WINGWALL 3 | 2.67 | CU YD |
| STRUCTURAL CONCRETE | WINGWALL 4 | 2.77 | CU YD |
| CONCRETE COATING | WINGWALLS | 331.41 | SQ FT |

NOTES

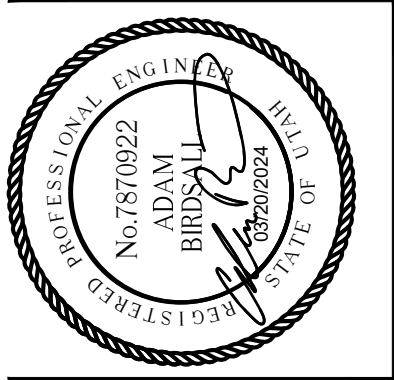
1. WINGWALL QUANTITIES INCLUDE LIMITS OF CONCRETE BEHIND ABUTMENT AND THE CHEEKWALL ABOVE THE BEARING SEAT.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH
 AT FULL
 SCALE IF
 NOT SCALE
 ACCORDINGLY

Parametrix

PROJECT NO: 344-8541-002
 DATE: 03/20/2024
 DESIGNED: SLO
 DRAWN: SLO
 CHECKED: AJB
 APPROVED: AJB



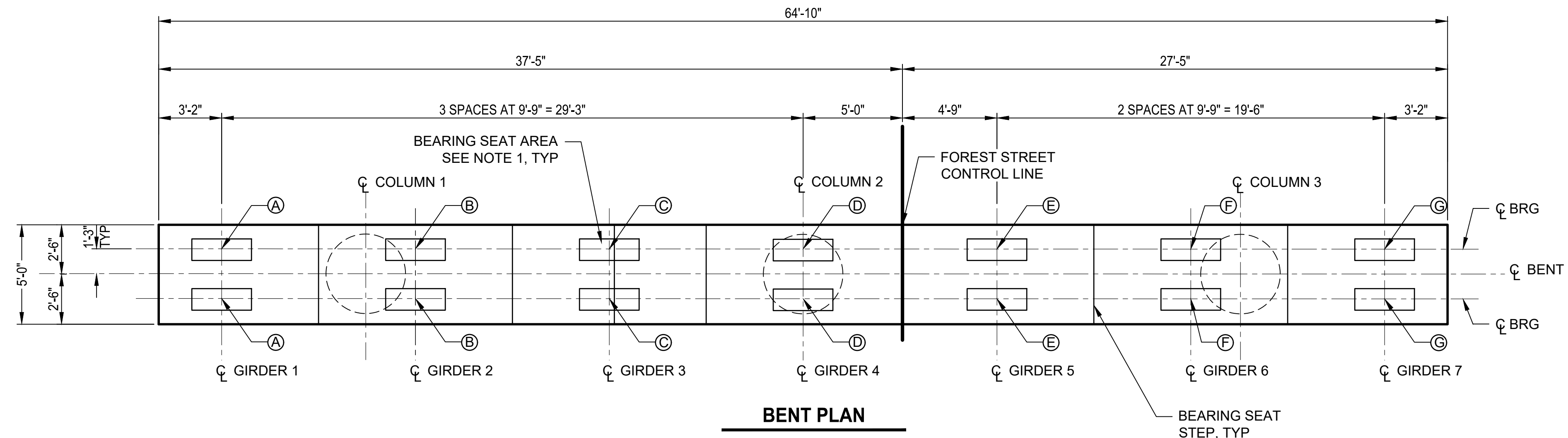
PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

**WINGWALL
 DETAILS**

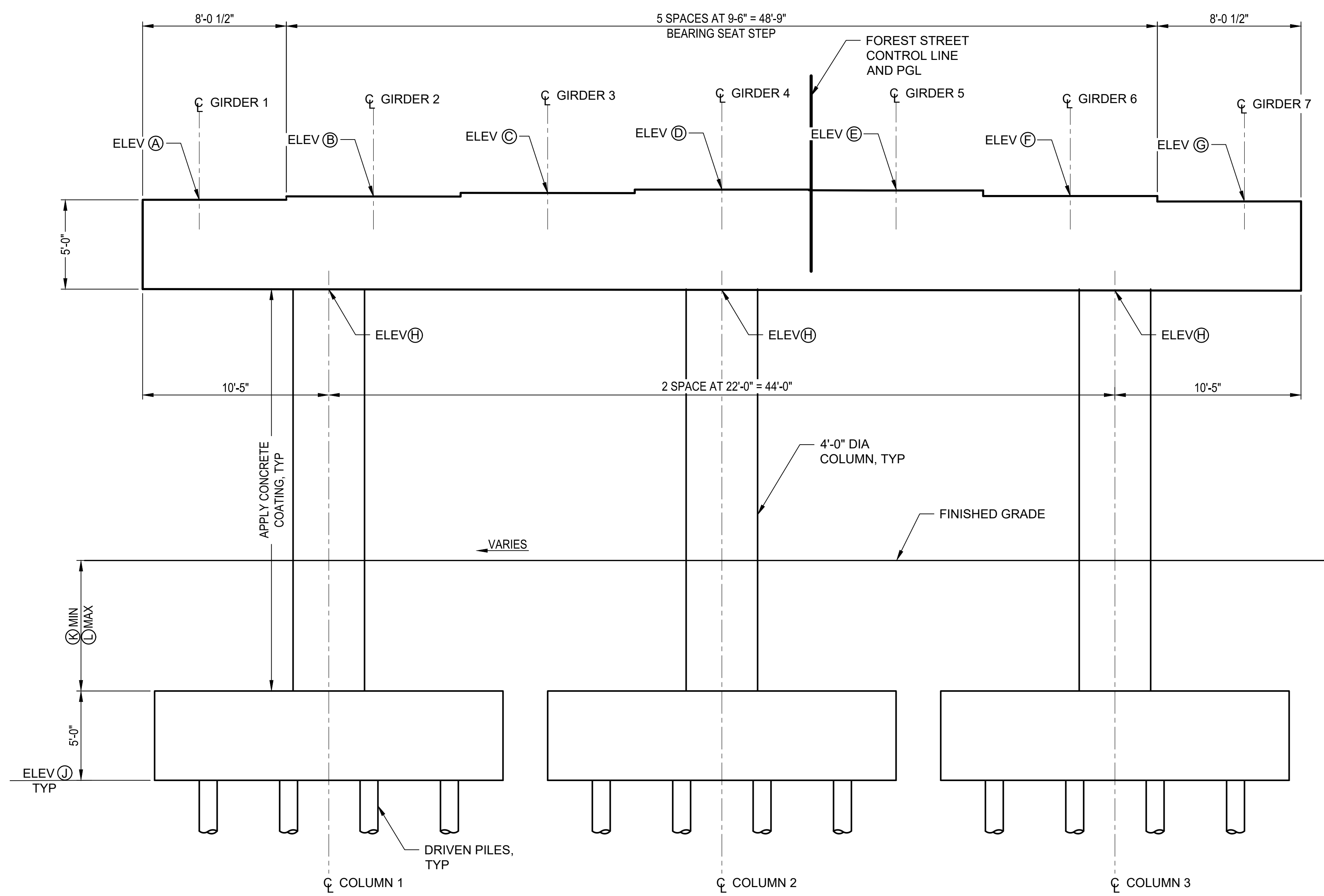
DRAWING NO.
 25 OF 59

S25

PATH: U:\Sola\Projects\Clients\8541-8541-002 Forest St Final Design\995svcs\CADD\DWG\Structure PLOTTED BY: OllweSta DATE: Friday, March 22, 2024 9:44:05 AM
 LAYOUT: BENT



BENT PLAN



BENT ELEVATION

LOOKING AHEAD ON STATION

| GIRDER ELEVATIONS (FT) | | | | | | | | |
|------------------------|------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|
| BENT | SPAN | GIRDER 1 (A) | GIRDER 2 (B) | GIRDER 3 (C) | GIRDER 4 (D) | GIRDER 5 (E) | GIRDER 6 (F) | GIRDER 7 (G) |
| #2 | 1 | 4336.04 | 4336.24 | 4336.43 | 4336.63 | 4336.63 | 4336.44 | 4336.24 |
| | 2 | 4341.72 | 4341.92 | 4342.12 | 4342.31 | 4342.32 | 4342.12 | 4341.92 |
| #3 | 3 | 4341.84 | 4342.03 | 4342.23 | 4342.42 | 4342.43 | 4342.23 | 4342.04 |
| | 4 | 4343.99 | 4344.19 | 4344.38 | 4344.58 | 4344.58 | 4344.39 | 4344.19 |
| #4 | 4 | 4344.00 | 4344.19 | 4344.39 | 4344.58 | 4344.59 | 4344.39 | 4344.20 |
| | 5 | 4342.77 | 4342.97 | 4343.16 | 4343.36 | 4343.36 | 4343.17 | 4342.97 |
| #5 | 5 | 4342.71 | 4342.91 | 4343.10 | 4343.30 | 4343.30 | 4343.11 | 4342.91 |

| BENT CAP ELEVATIONS (FT) | |
|--------------------------|--------------------|
| BENT | BOTTOM ELEV (H) |
| #2 | 4331.04 |
| #3 | 4336.72 |
| #4 | 4338.99 |
| #5 | 4337.71 |

| FOOTING DEPTHS (FT) | | | |
|---------------------|--------------------|------------------|------------------|
| BENT | BOTTOM ELEV (J) | MIN DEPTH (K) | MAX DEPTH (L) |
| #2 | 4301.34 | 4.91 | 5.30 |
| #3 | 4301.39 | 6.08 | 6.10 |
| #4 | 4307.93 | 2 | 2.75 |
| #5 | 4308.42 | 6 | 6.322 |

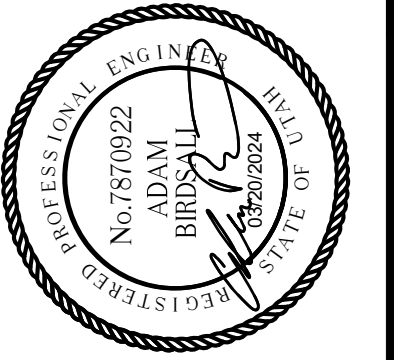
| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|----------|---------|-------|
| STRUCTURAL CONCRETE | FOOTINGS | 845 | CU YD |
| STRUCTURAL CONCRETE | COLUMNS | 148 | CU YD |
| STRUCTURAL CONCRETE | BENT CAP | 256 | CU YD |
| CONCRETE COATING | COLUMNS | 3219 | SQ FT |

- NOTES**
1. FINISH BEARING SEAT HIGH AND RUB OR GRIND LEVEL TO ELEVATION SHOWN ± 1/8 INCH. NO GROUTING PERMITTED.
 2. ROUGHEN TOP OF BENT CAP TO 1/4 INCH AMPLITUDE.

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| BY | |
| DATE | |
| REVISIONS | |
| ONE INCH AT FULL SCALE IF NOT ACCORDINGLY | |

Parametrix

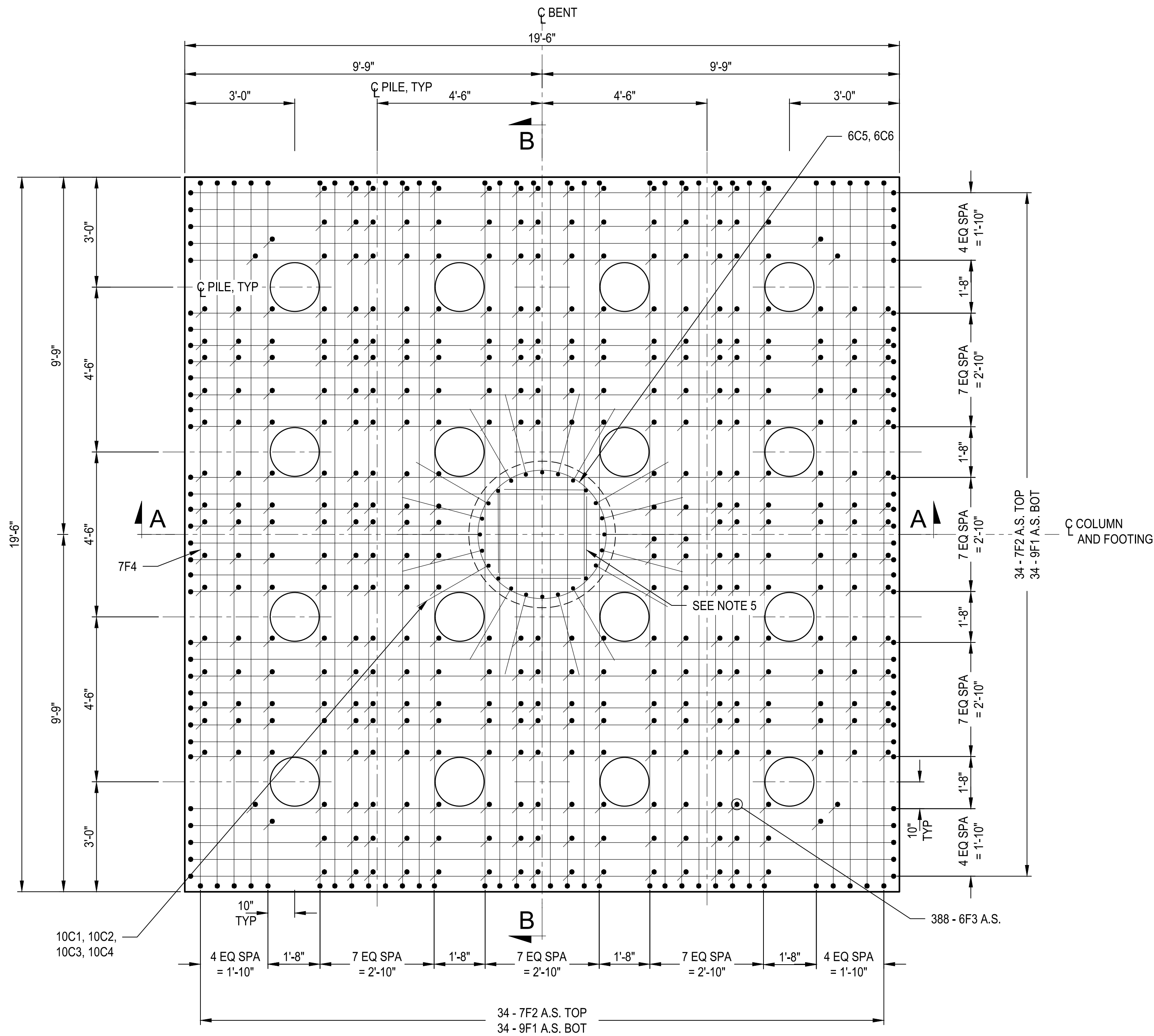
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| DATE | DESIGNED | CHECKED |
| 03/20/2024 | TWP | EA |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | SLO | AJB |



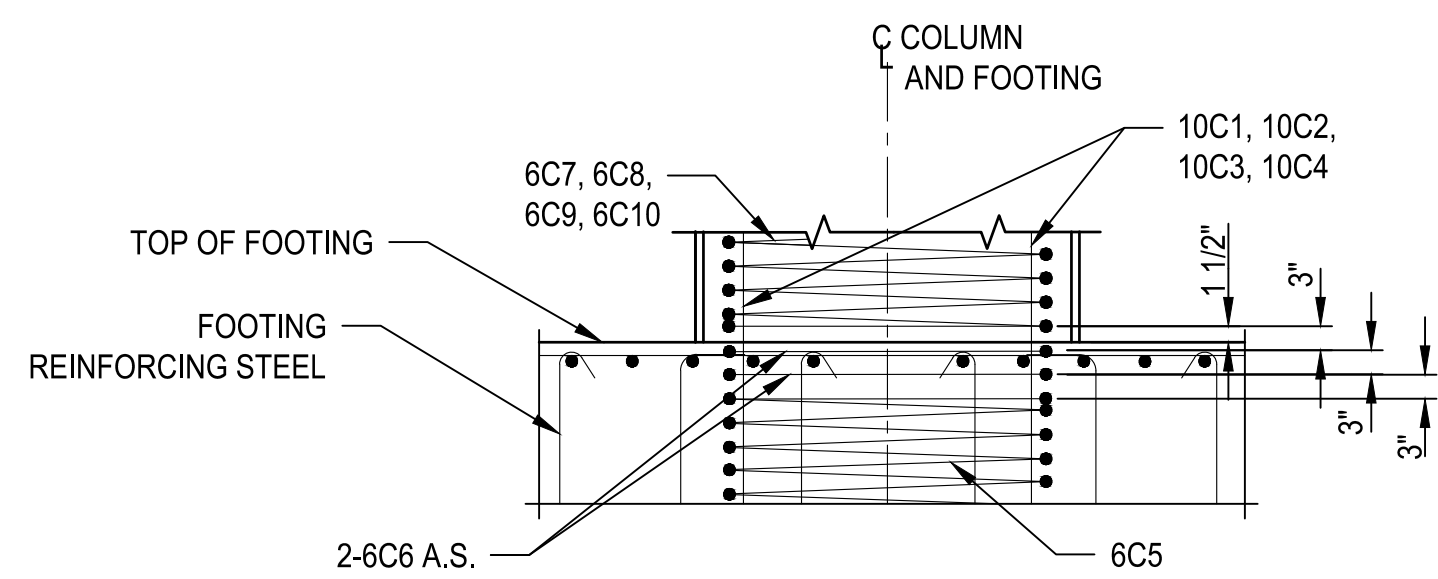
PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

**BENT PLAN
 AND
 ELEVATION**

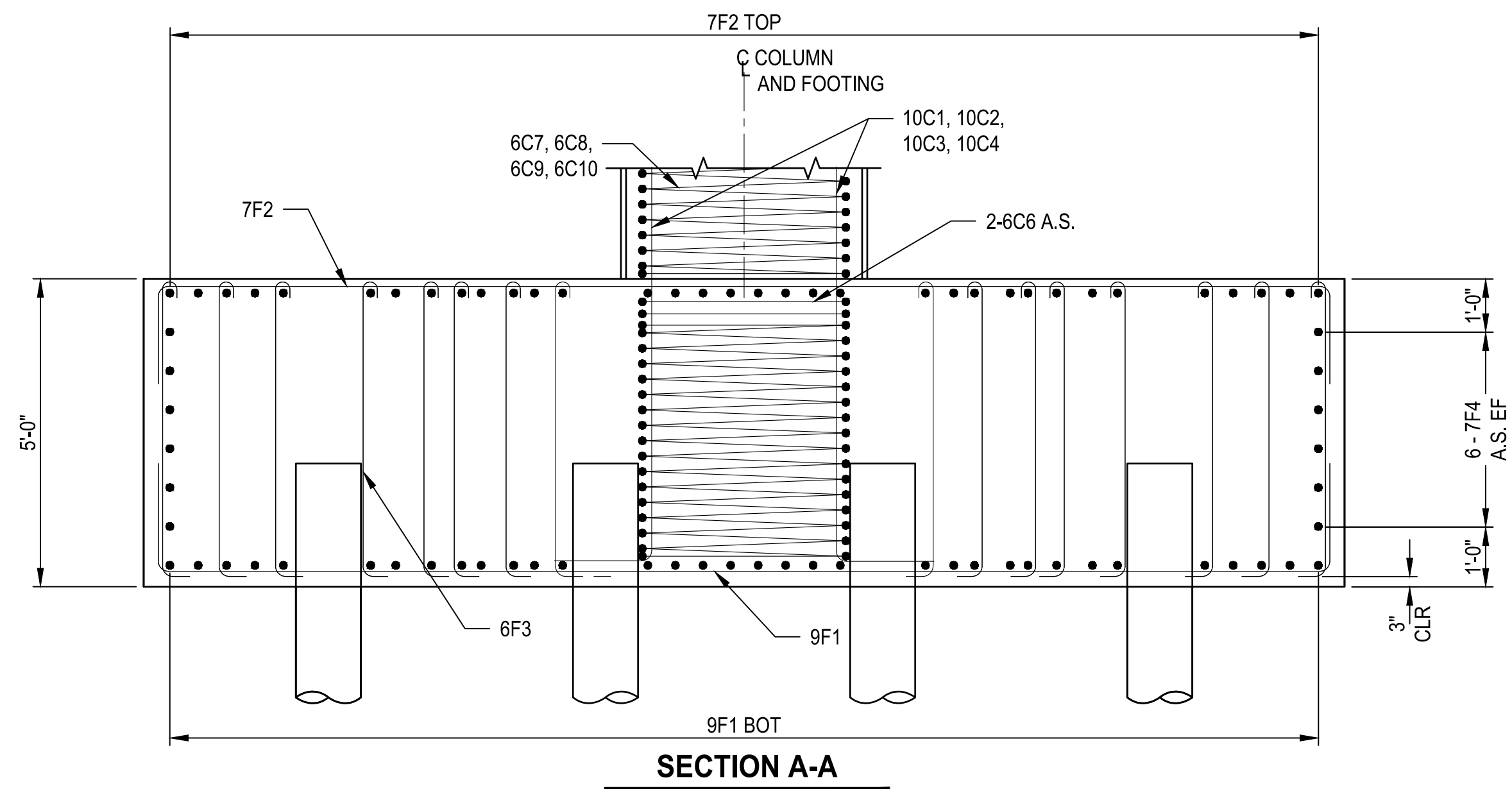
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TYPICAL FOOTING AT COLUMN



SPIRAL SPLICE DETAIL



SECTION A-A

NOTES

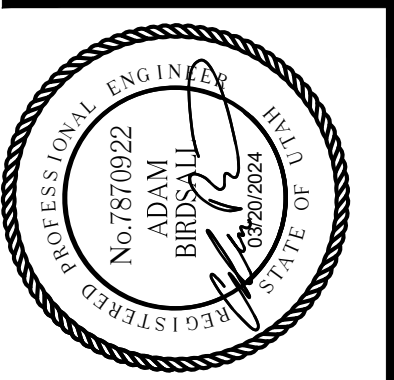
- FOR PILE REINFORCEMENT DETAILS, SEE "DRIVEN PILE DETAILS".
- FOR PILE PLACEMENT, SEE "FOUNDATION PLAN".
- FOR COLUMN REINFORCING DETAILS, SEE "COLUMN DETAILS".
- FOOTING REINFORCEMENT MAY BE ADJUSTED SLIGHTLY, UP TO 3", TO AVOID CONFLICT WITH COLUMN REINFORCEMENT.
- ADJUST COLUMN REINFORCEMENT TO AVOID PILES.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: EA
 APPROVED: AUB



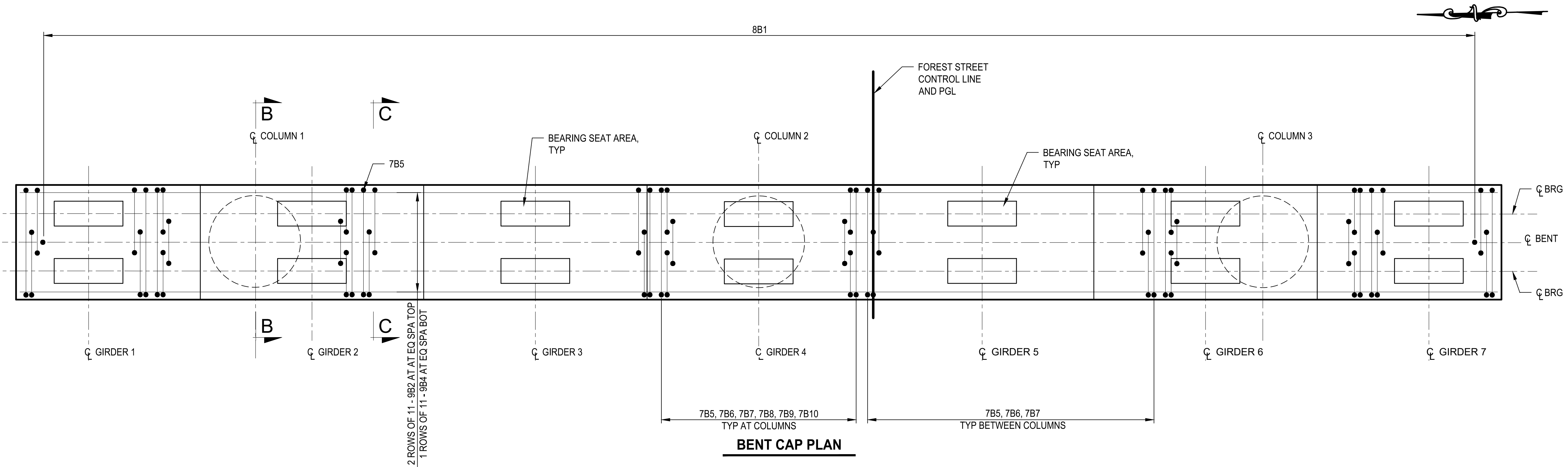
PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT

BENT FOOTING DETAILS

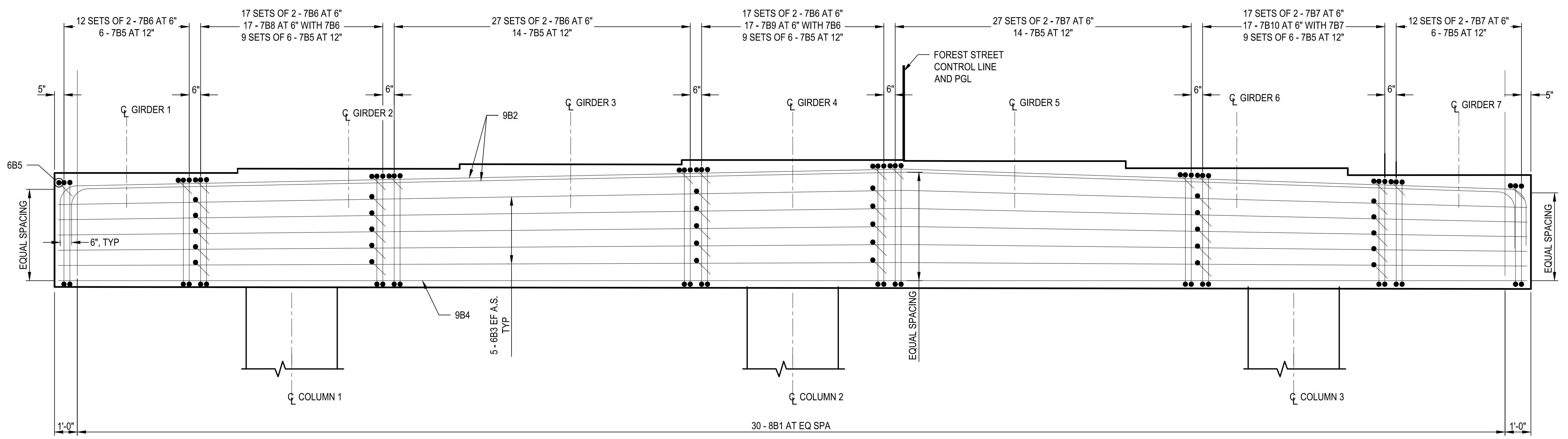
DRAWING NO. 27 OF 59

S27

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BENT CAP PLAN



BENT CAP ELEVATION

COLUMN REINFORCING STEEL NOT SHOWN FOR CLARITY

NOTES

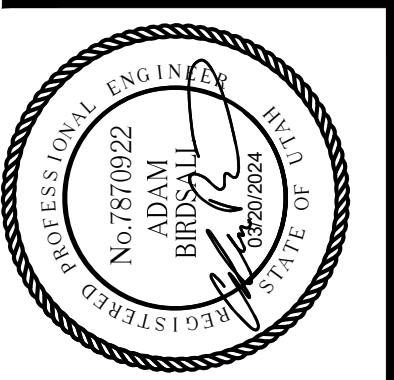
- B6 AND B7 REINFORCING ARE SETS OF 2 (4 LEGS).
- LAP SPLICES ARE NOT PERMITTED IN B1 BARS. STAGGER SPLICES 12 FT 6 INCH MINIMUM. SPLICE NO MORE THAN 1/2 OF TOP AND BOTTOM BARS AT ONE LOCATION.
- SEE "COLUMN DETAILS" FOR SECTIONS B-B AND C-C.
- SEE "BENT PLAN AND ELEVATION" FOR DIMENSIONS AND ELEVATIONS.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
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 DRAWN: SLO
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 APPROVED: AJB



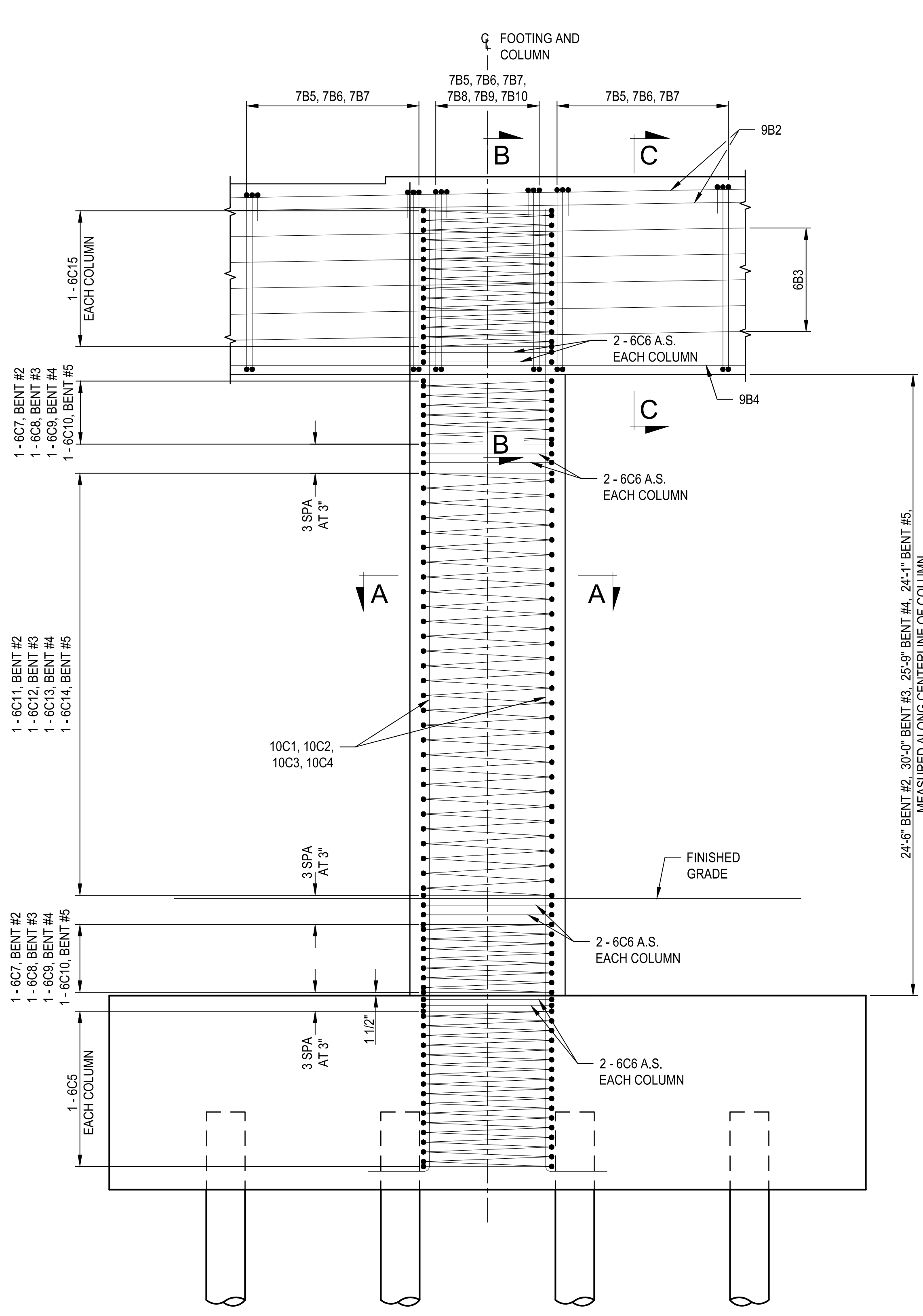
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

BENT CAP DETAILS

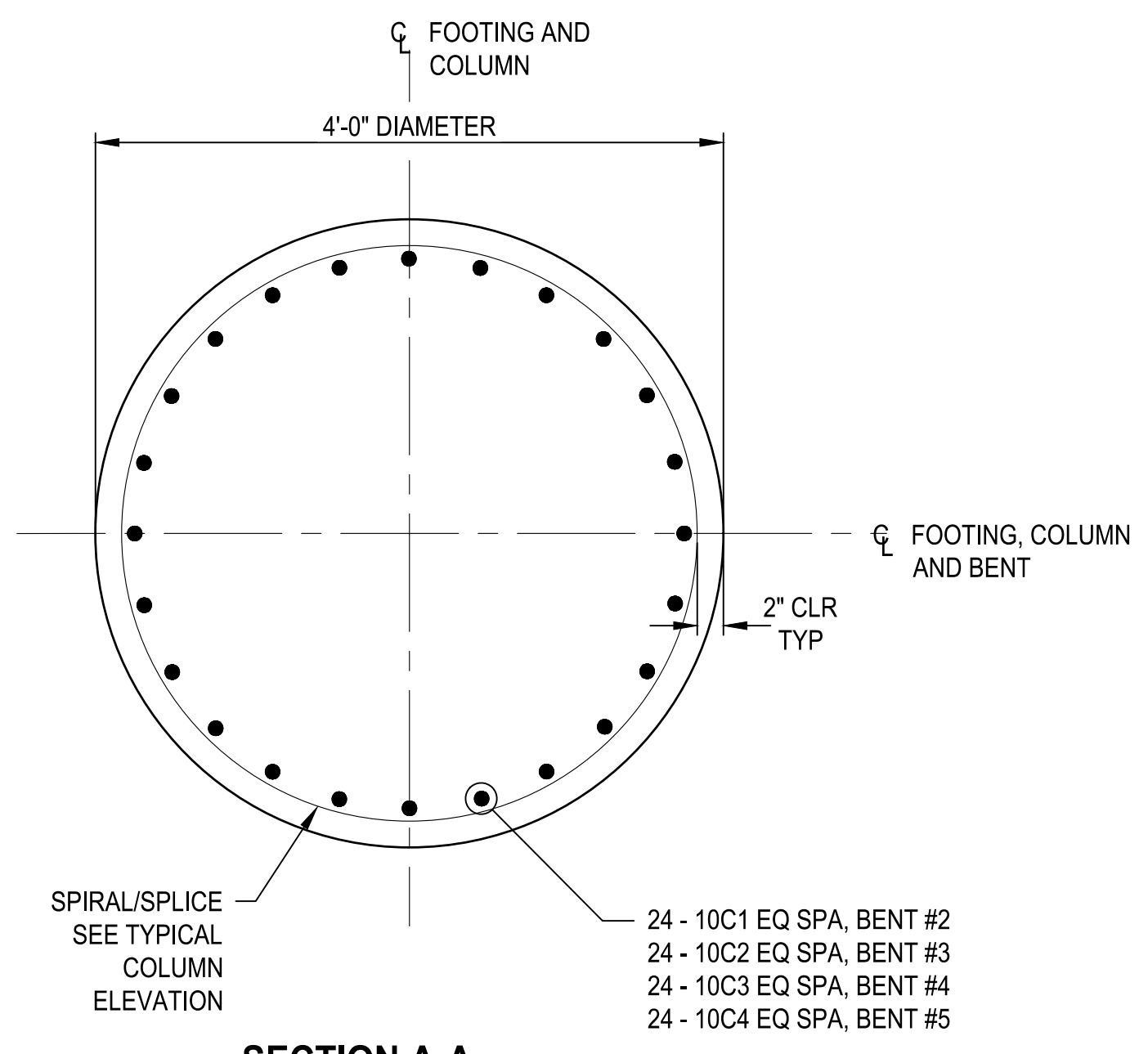
DRAWING NO.
 28 OF 59

S28

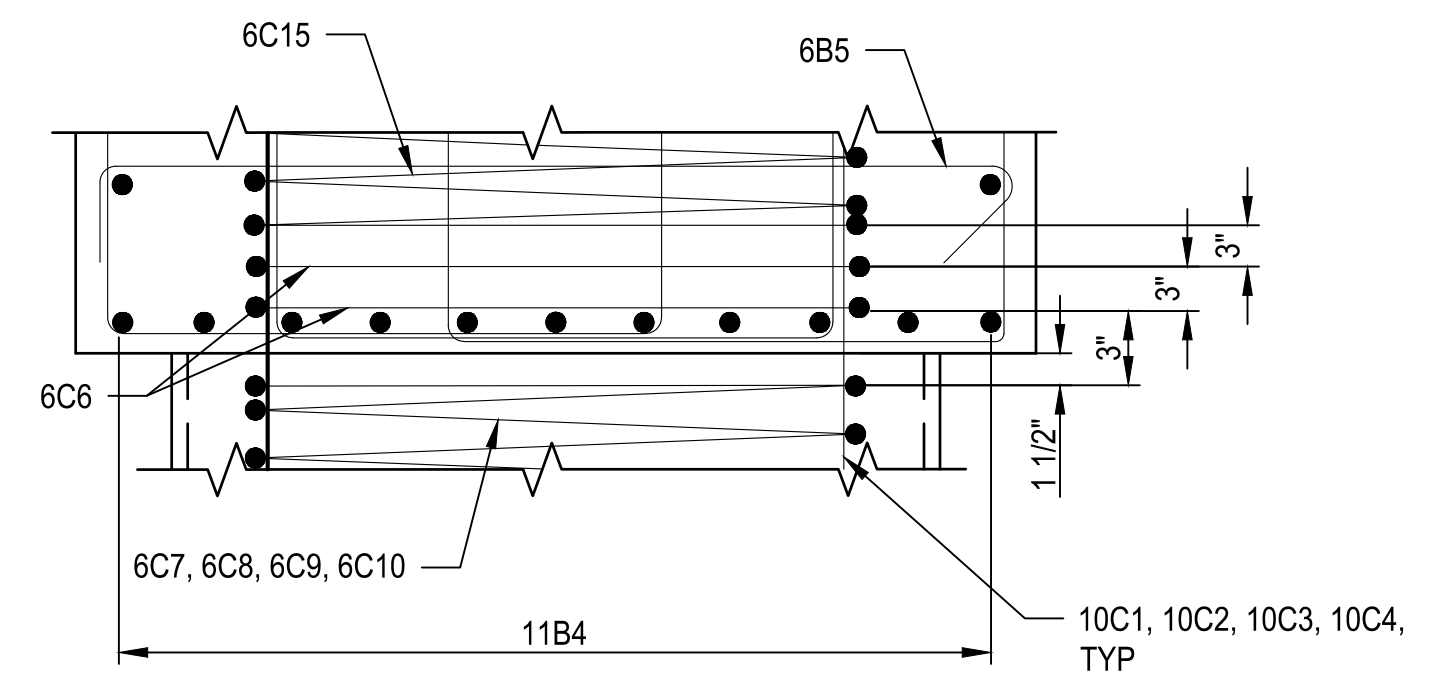
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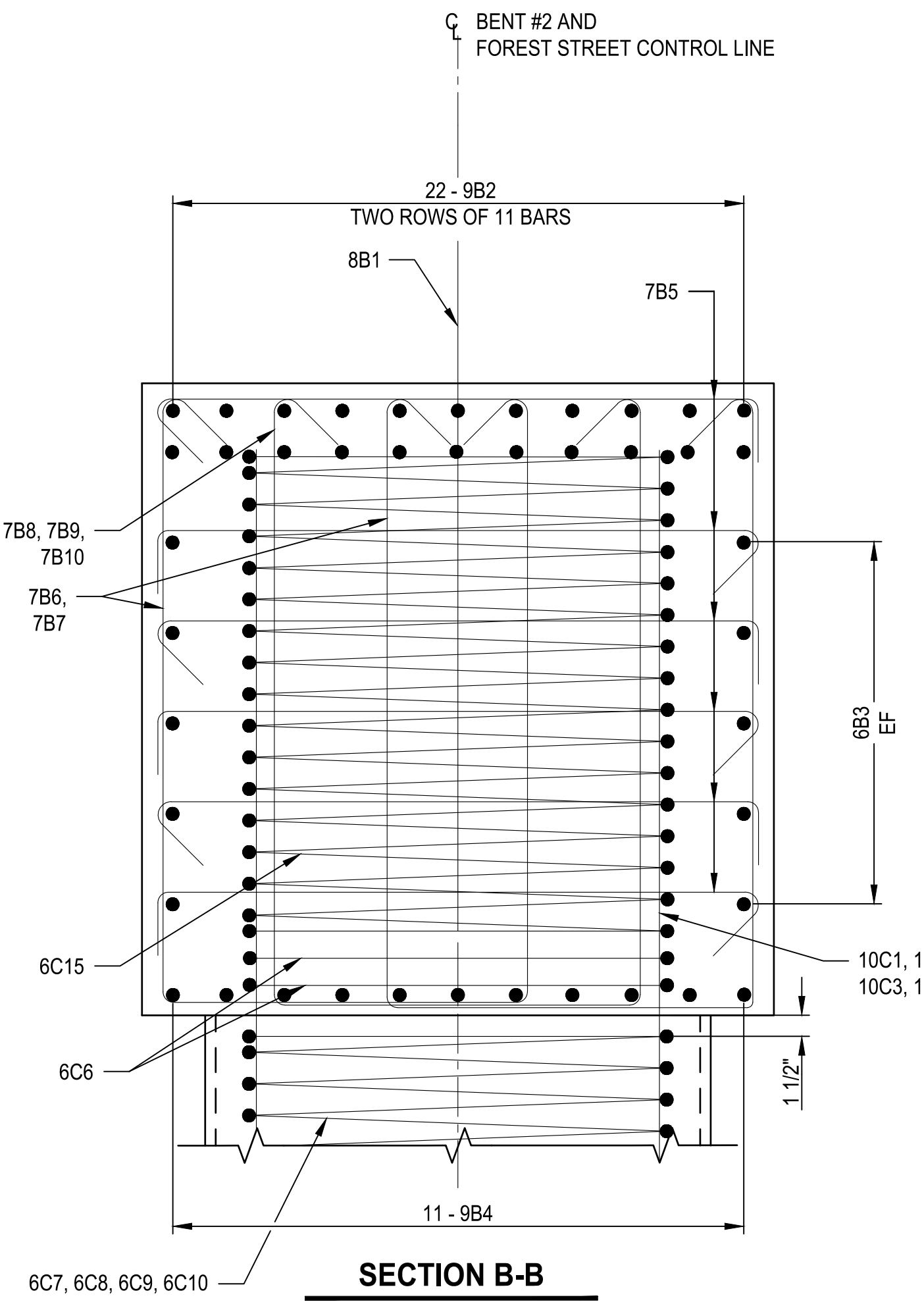
TYPICAL COLUMN ELEVATION
FOOTING REINFORCING NOT SHOWN



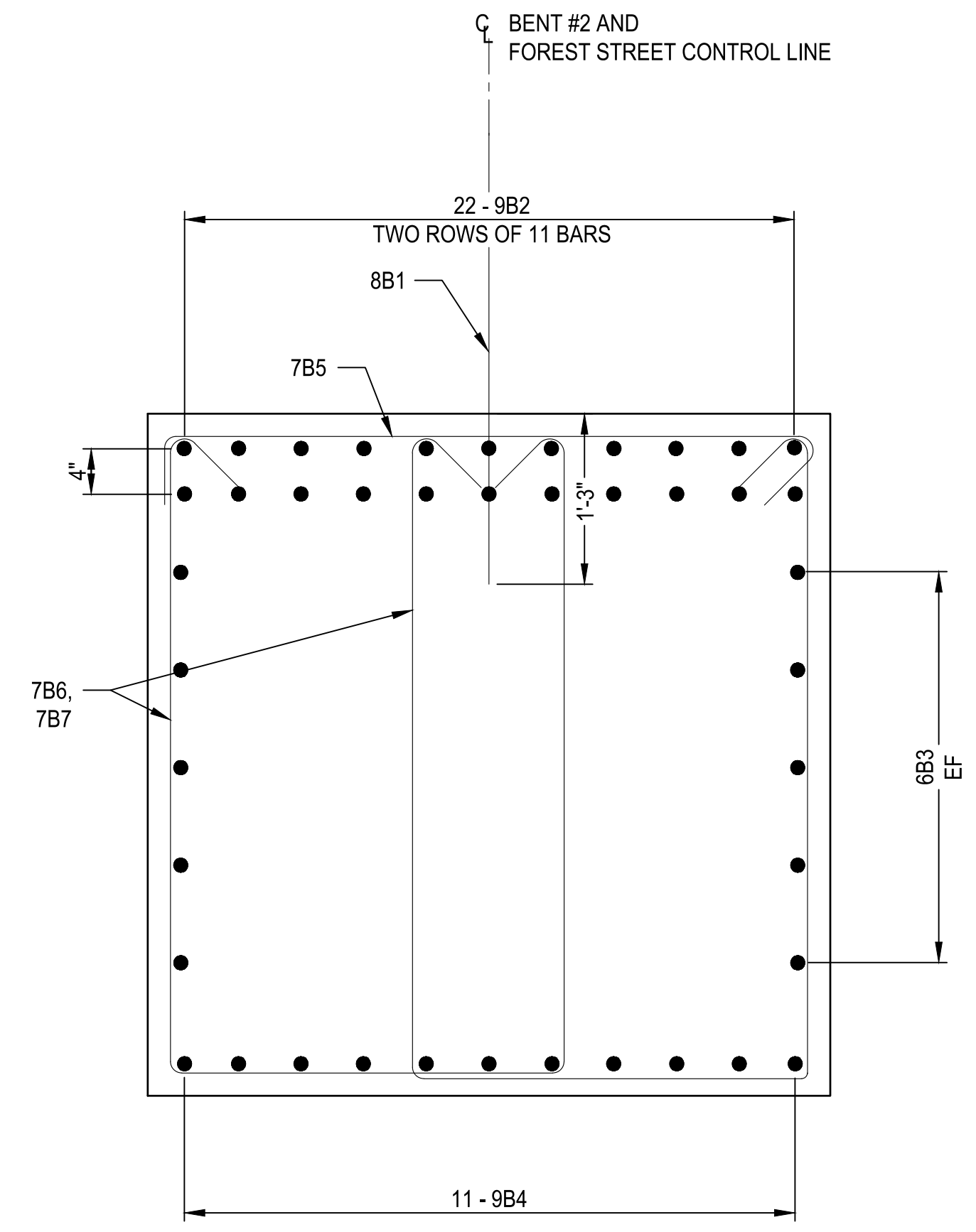
SECTION A-A



SPIRAL SPLICE DETAIL



SECTION B-B



SECTION C-C

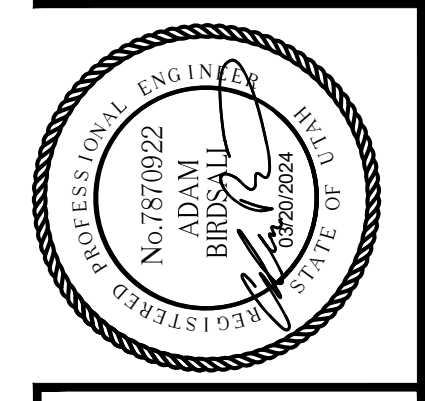
NOTES

- 1. SPLICES ARE NOT PERMITTED IN MAIN COLUMN REINFORCING BARS (C1-C4).

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ONE INCH AT FULL SCALE, IF NOT INDICATED OTHERWISE, ACCORDINGLY.

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| DATE | DESIGNED | CHECKED |
| 03/20/2024 | TWP | EA |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | SLO | AUB |



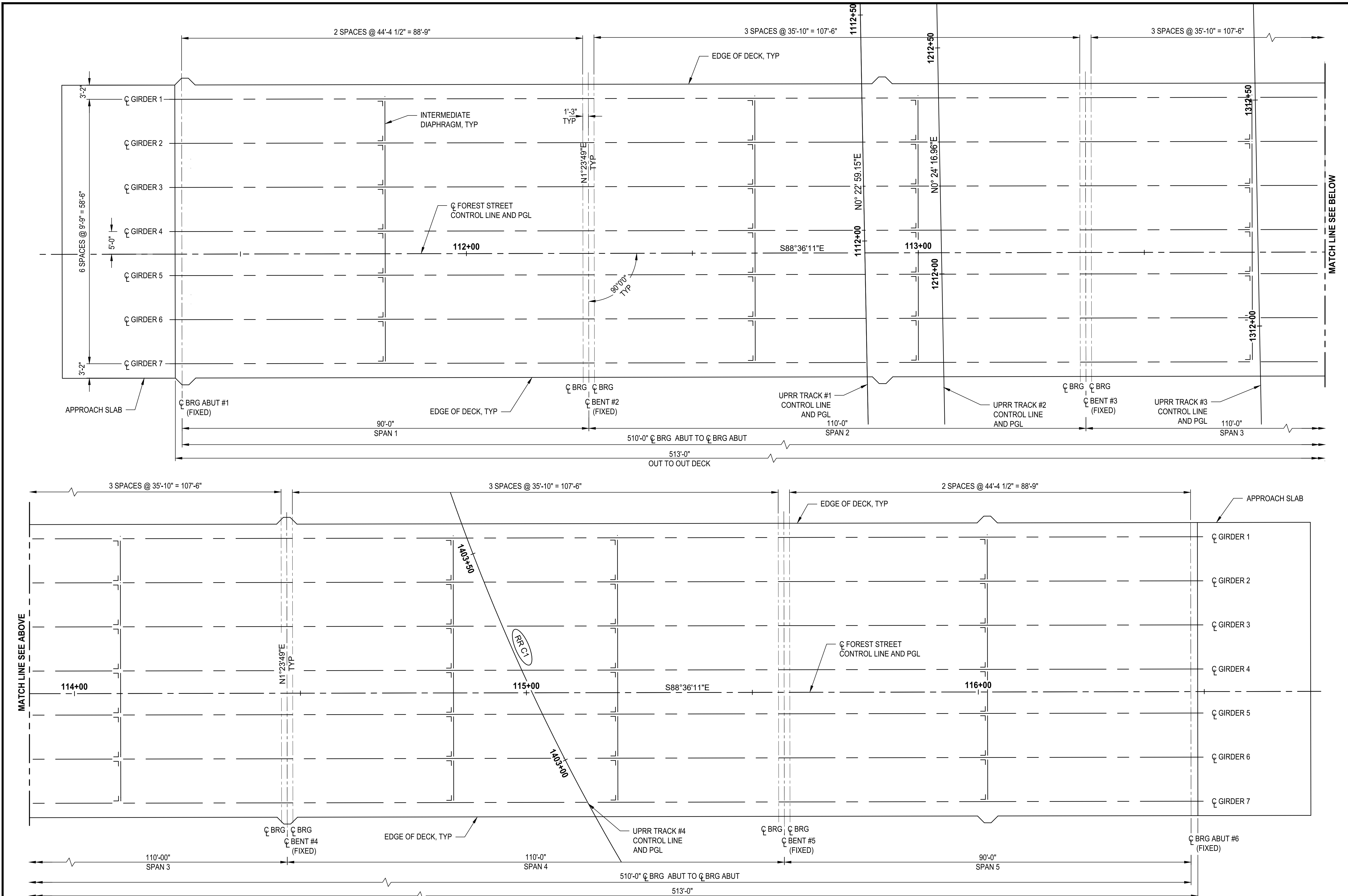
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

COLUMN DETAILS

DRAWING NO.
29 OF 59

S29

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FRAMING PLAN

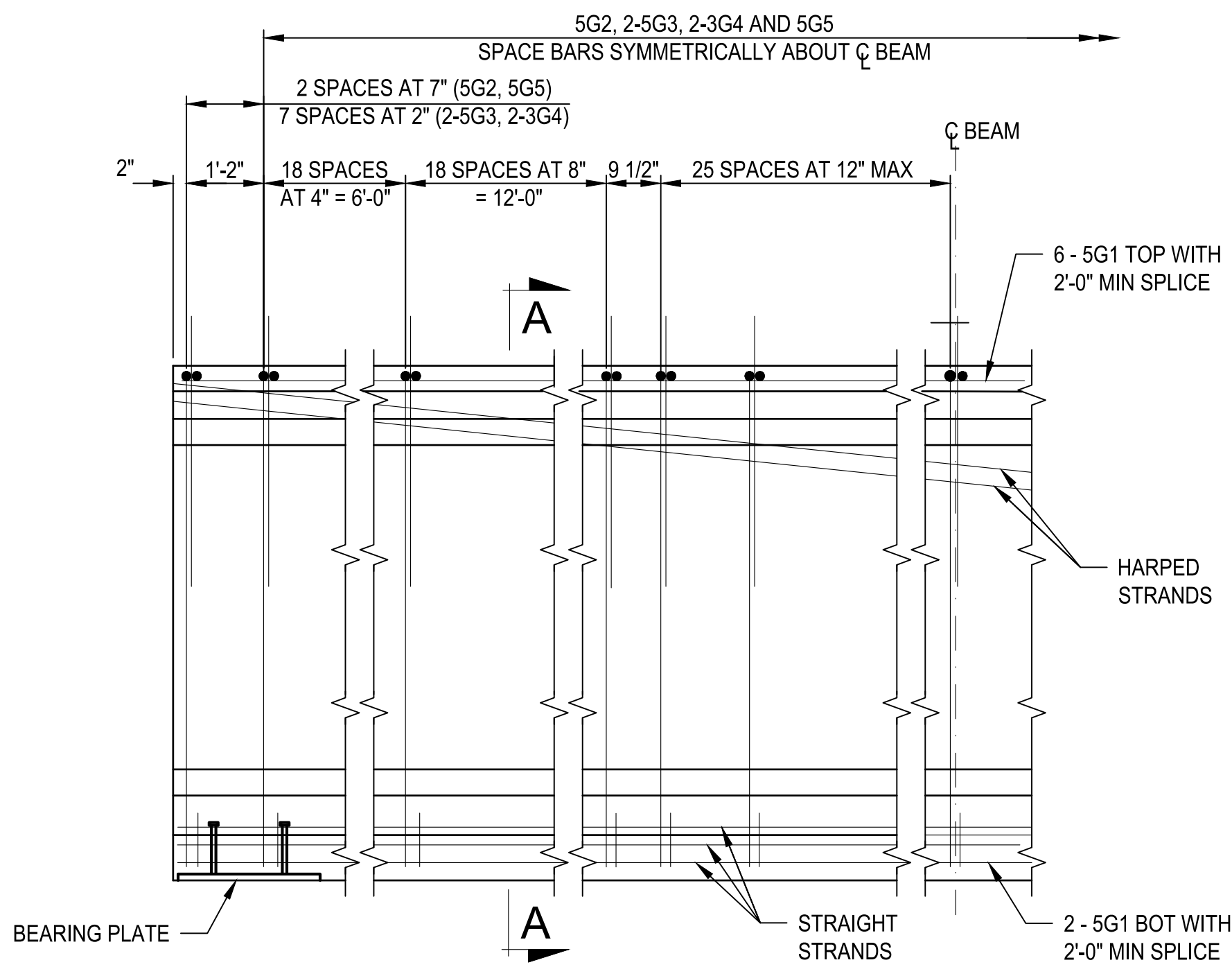
NOTES

- DIMENSIONS ARE MEASURED ALONG G GIRDER AND ARE TO G CONNECTION PLATE AT G GIRDER.
- SEE "SITUATION AND LAYOUT 1 OF 2" FOR HORIZONTAL CURVE DATA.

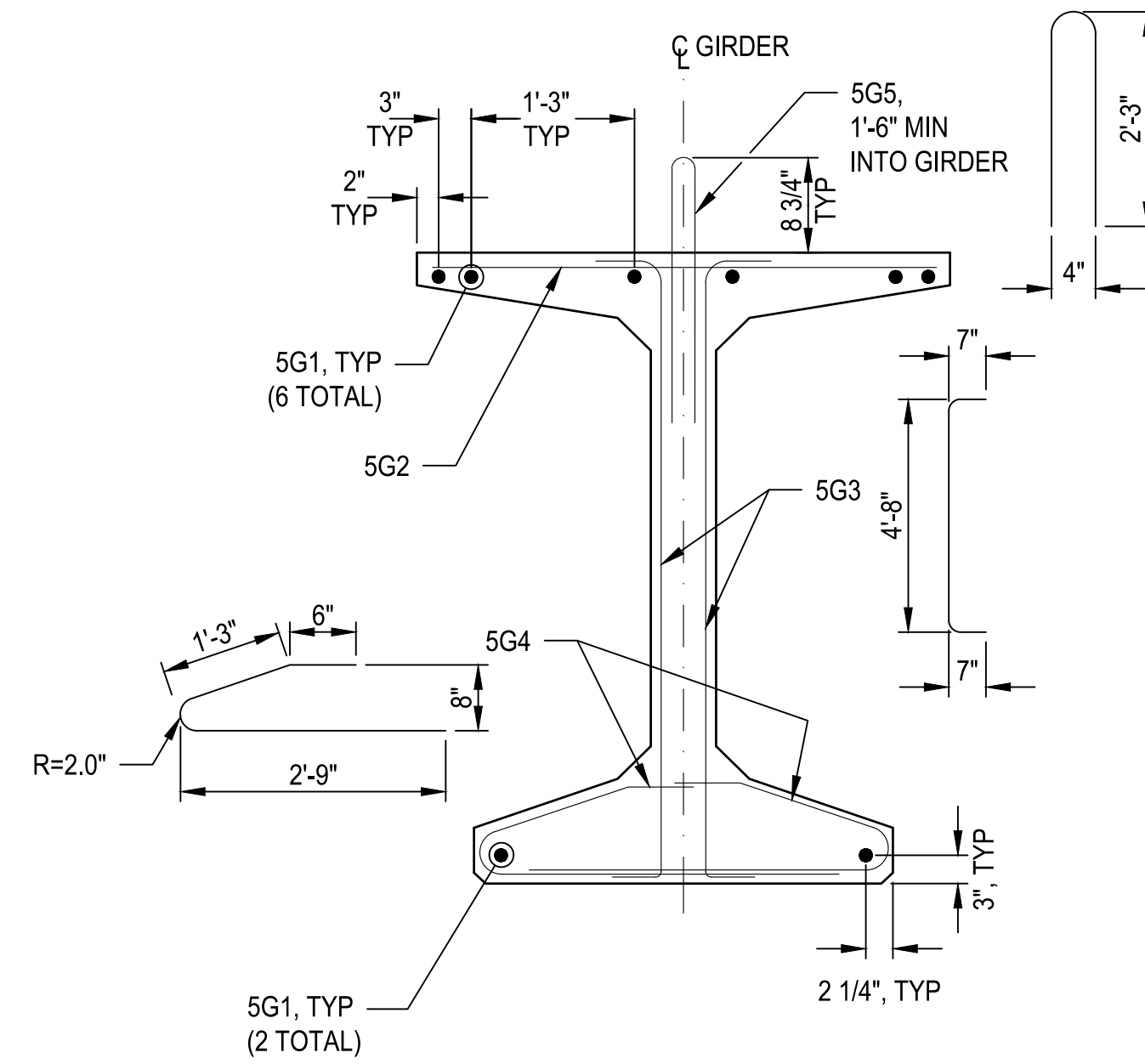
RAILROAD MILEPOST: 21.141 RAILROAD SUBDIVISION: OGDEN SUB CROSSING LOCATION: BRIGHAM CITY, BOX ELDER COUNTY, UTAH LAT/LONG: 41.51061168/-112.02954543

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| BY | |
| DATE | |
| REVISIONS | |
| <p>ONE INCH AT FULL SCALE, IF NECESSARY ACCORDINGLY</p> | |
| | |
| DATE | CHECKED |
| 03/20/2024 | NCC |
| JOB No. | APPROVED |
| 344-8541-002 | AJB |
| DESIGNED | |
| TWP | |
| DRAWN | |
| SLO | |
| | |
| <p>BRIGHAM CITY CONNECTION PROJECT</p> | |
| <p>FRAMING PLAN</p> | |
| DRAWING NO. | |
| 30 OF 59 | |
| <p>S30</p> | |

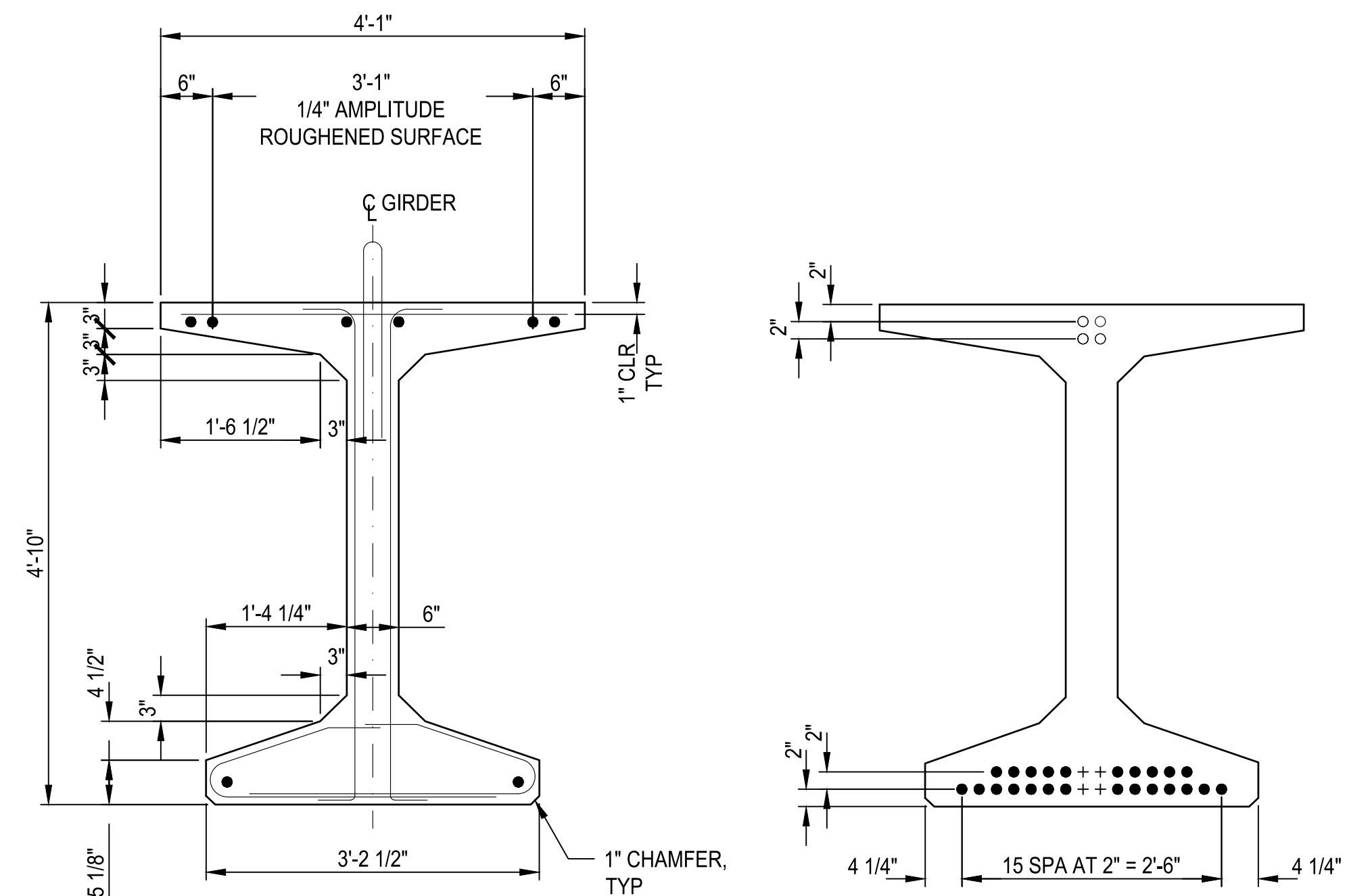
LAYOUT: GIRDER DET
 PATH: U:\Sort\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995Secs\CADD\DWG\Structure
 PLOTTED BY: Ohwsta DATE: Friday, March 22, 2024 9:48:02 AM



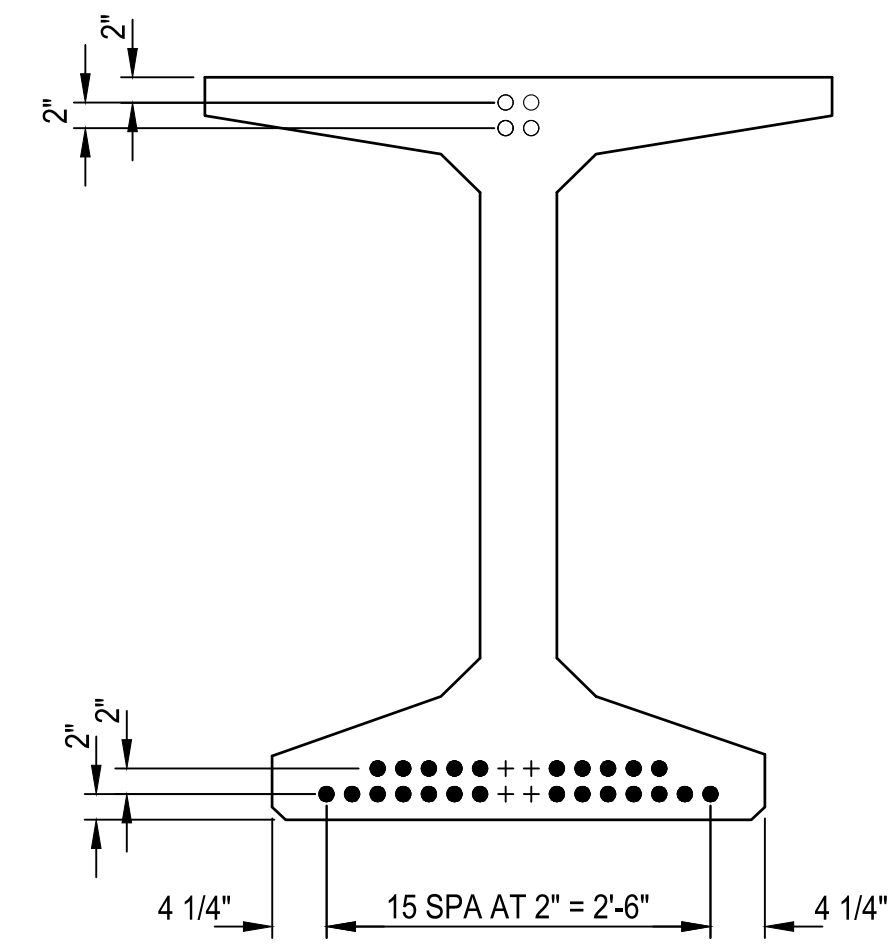
TYPICAL END ELEVATION



SECTION A-A

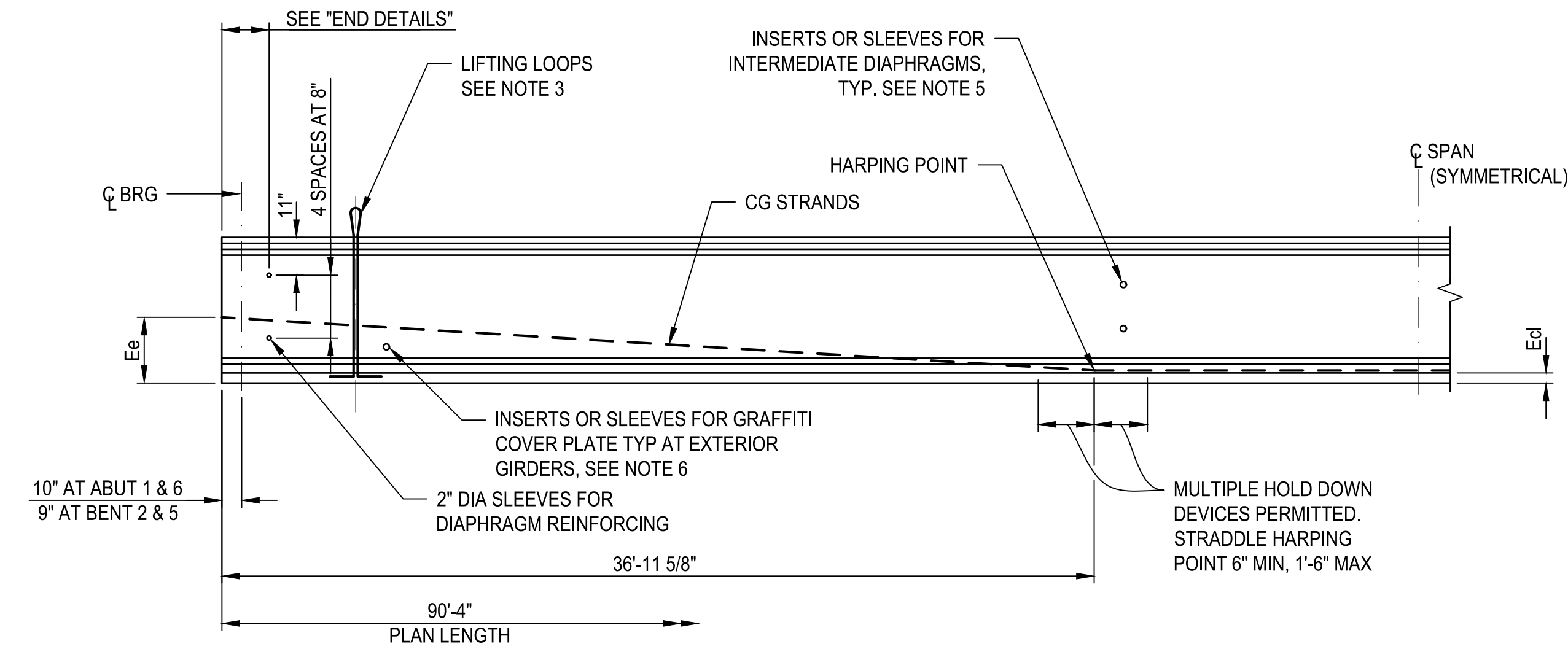


TYPICAL SECTION

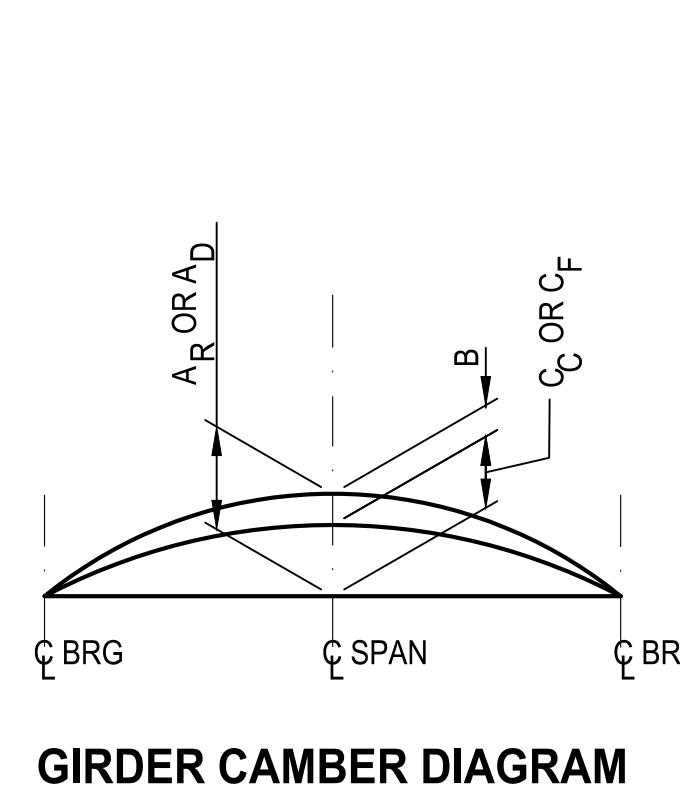


STRAND PATTERN

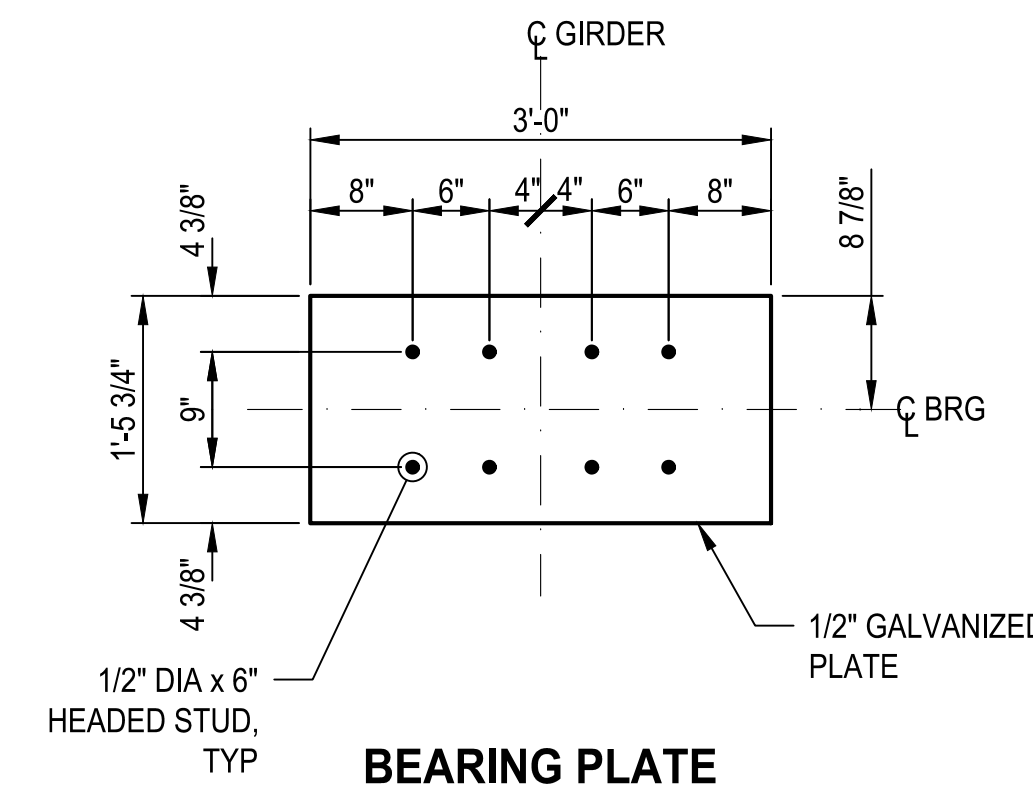
- DENOTES STRAIGHT STRANDS
- DENOTES HARPED STRANDS AT END
- + DENOTES HARPED STRANDS AT MIDSPAN



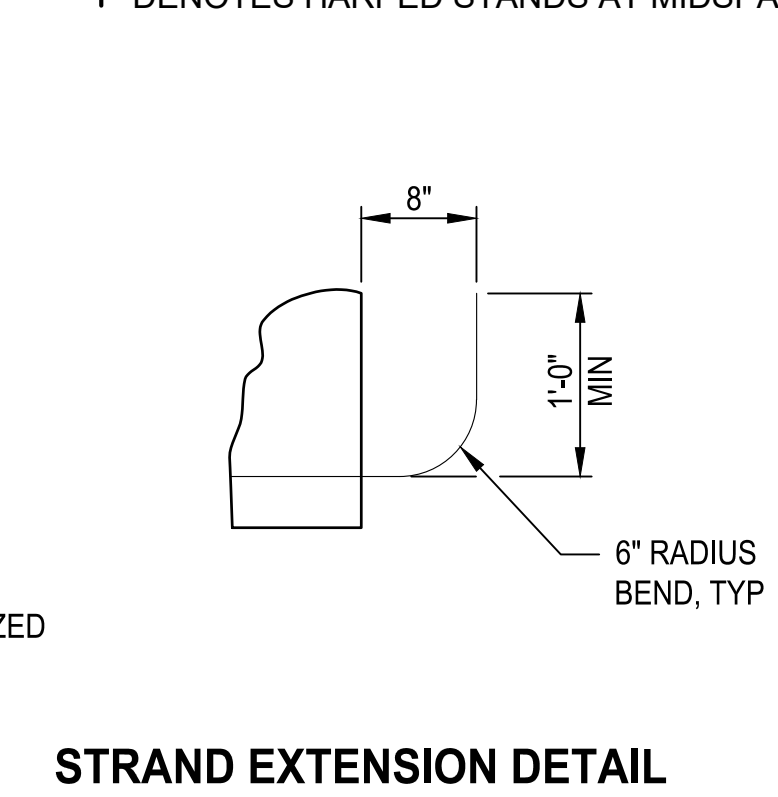
GIRDER ELEVATION



GIRDER CAMBER DIAGRAM



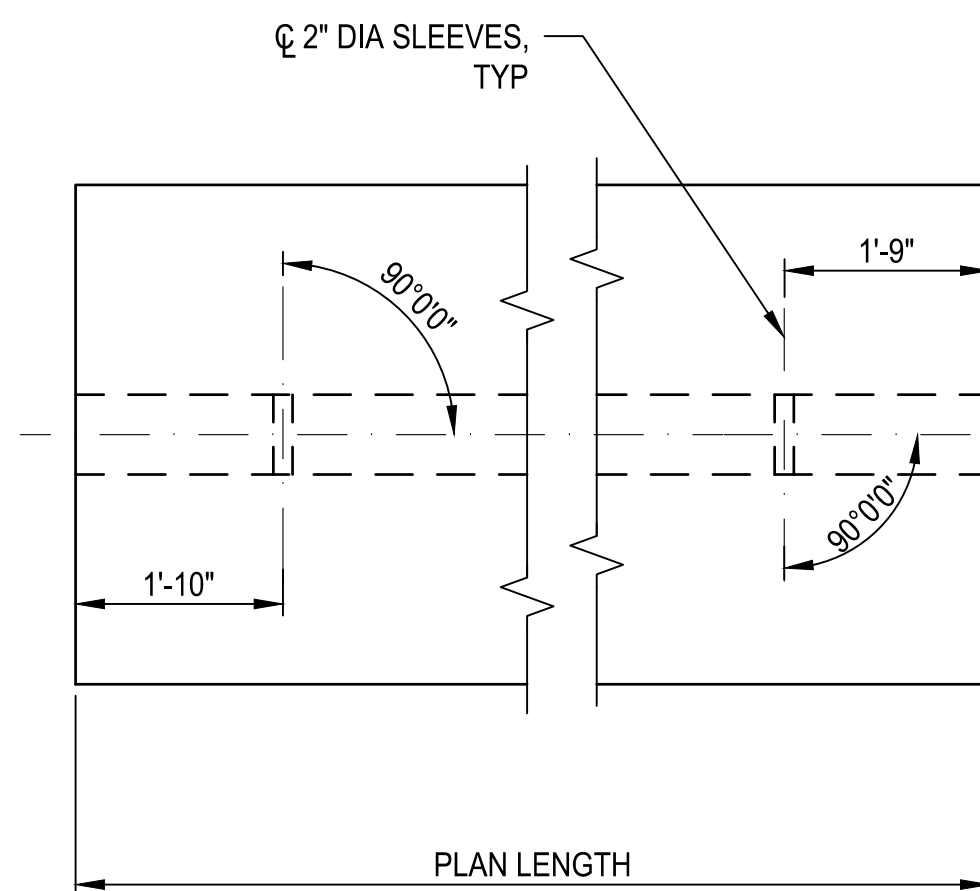
BEARING PLATE



STRAND EXTENSION DETAIL

NOTES

1. INCREASE PLAN LENGTH AS NECESSARY TO COMPENSATE FOR SHORTENING DUE TO PRESTRESS AND SHRINKAGE.
2. USE 0.6 INCH DIAMETER LOW RELAXATION STRANDS (AASHTO M 203 GRADE 270).
3. DESIGN AND INSTALL LIFTING EMBEDMENTS IN ACCORDANCE WITH THE SPECIFICATIONS.
4. BRACE GIRDERS TO PREVENT TIPPING AND TO CONTROL LATERAL BENDING DURING SHIPPING. BRACE GIRDERS Laterally TO PREVENT TIPPING UNTIL THE DIAPHRAGMS ARE INSTALLED.
5. SEE "FRAMING PLAN" FOR INTERMEDIATE DIAPHRAGM LOCATIONS AND SEE "INTERMEDIATE DIAPHRAGM DETAILS FOR PRESTRESSED GIRDERS" FOR CONNECTION REQUIREMENTS.
6. USE INSERTS OR SLEEVES FOR GRAFFITI COVER PLATE. ADJUST HOLES TO AVOID CONFLICT WITH GIRDER STRANDS. SEE "GRAFFITI COVER AND ELASTOMERIC PAD - INTEGRAL UBT GIRDER" FOR LOCATION OF INSERTS OR SLEEVES.
7. NOTIFY ENGINEER IF ACTUAL CAMBER VARIES FROM THEORETICAL CAMBER BY MORE THAN 1 1/8 INCH.
8. EXTEND 8 STRANDS AS SHOWN FOR END OF GIRDER AT BENT ONLY. CUT ALL OTHER STRANDS FLUSH. OFFSET EXTENDED STRANDS FROM SPAN TO SPAN. FABRICATOR TO SELECT AND SHOW EXTENDED STRANDS IN SHOP DRAWINGS.
9. USE STRUCTURAL STEEL CONFORMING TO AASHTO M 270 GRADE 36 FOR BEARING PLATE. GALVANIZE ACCORDING TO AASHTO M 111 AFTER FABRICATION.



END DETAILS

CLIP TOP FLANGE ONLY

| SPAN | GIRDER | GIRDER TYPE | THETA 1 | THETA 2 | PLAN LENGTH | f'ci | f'c | HARPED | | STRAIGHT | | PRESTRESSED FORCE AFTER ALL LOSSES | LOCATION OF CG STRANDS | | CAMBER | | | | | |
|------|--------|-------------|---------|---------|-------------|------|-----|-------------------|---------------|-------------------|---------------|------------------------------------|------------------------|------|----------------|----------------|-----|----------------|----------------|------|
| | | | | | | | | NUMBER OF STRANDS | JACKING FORCE | NUMBER OF STRANDS | JACKING FORCE | | Ee | Ecl | A _R | A _D | B | C _C | C _F | |
| | | | | | | | | | | | | | | | | | | | | KIPS |
| 1 | 1 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,031.8 | 10.29 | 2.86 | 1.3 | 2.4 | 0.7 | 1.6 | 2.1 | |
| 1 | 2 TO 6 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,039.9 | 10.29 | 2.86 | 1.3 | 2.4 | 0.8 | 1.5 | 2.0 | |
| 1 | 7 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,033.7 | 10.29 | 2.86 | 1.3 | 2.4 | 0.8 | 1.6 | 2.1 | |
| 5 | 1 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,010.4 | 10.29 | 2.86 | 1.3 | 2.4 | 0.8 | 1.5 | 2.0 | |
| 5 | 2 TO 6 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,021.0 | 10.29 | 2.86 | 1.3 | 2.4 | 0.9 | 1.4 | 1.9 | |
| 5 | 7 | UBT58 | N/A | N/A | 90'-4" | 6.0 | 9.5 | 4 | 175.8 | 24 | 1054.6 | 1,013.4 | 10.29 | 2.86 | 1.3 | 2.4 | 0.8 | 1.5 | 2.0 | |

CAMBER NOTES:
 A_R = GIRDER CAMBER AT RELEASE
 A_D = GIRDER CAMBER JUST BEFORE DECK PLACEMENT (USE 40 DAYS)
 B = DEFLECTION DUE TO ALL DEAD LOADS APPLIED TO THE GIRDER (NO FUTURE WEARING SURFACE)
 C_C = GIRDER CAMBER AT CONSTRUCTION AFTER ALL DEAD LOADS APPLIED (NO FUTURE WEARING SURFACE)
 C_F = GIRDER CAMBER AT 20 YEARS (INCLUDES FUTURE WEARING SURFACE)

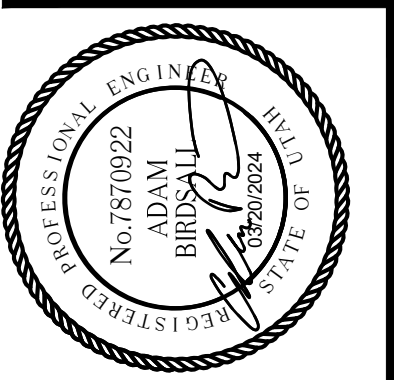
CG STRAND NOTES: Ee = CG OF ALL STRANDS AT END OF GIRDER
 Ecl = CG OF ALL STRANDS AT MIDSPAN OF GIRDER

| REVISIONS | DATE | BY |
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| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

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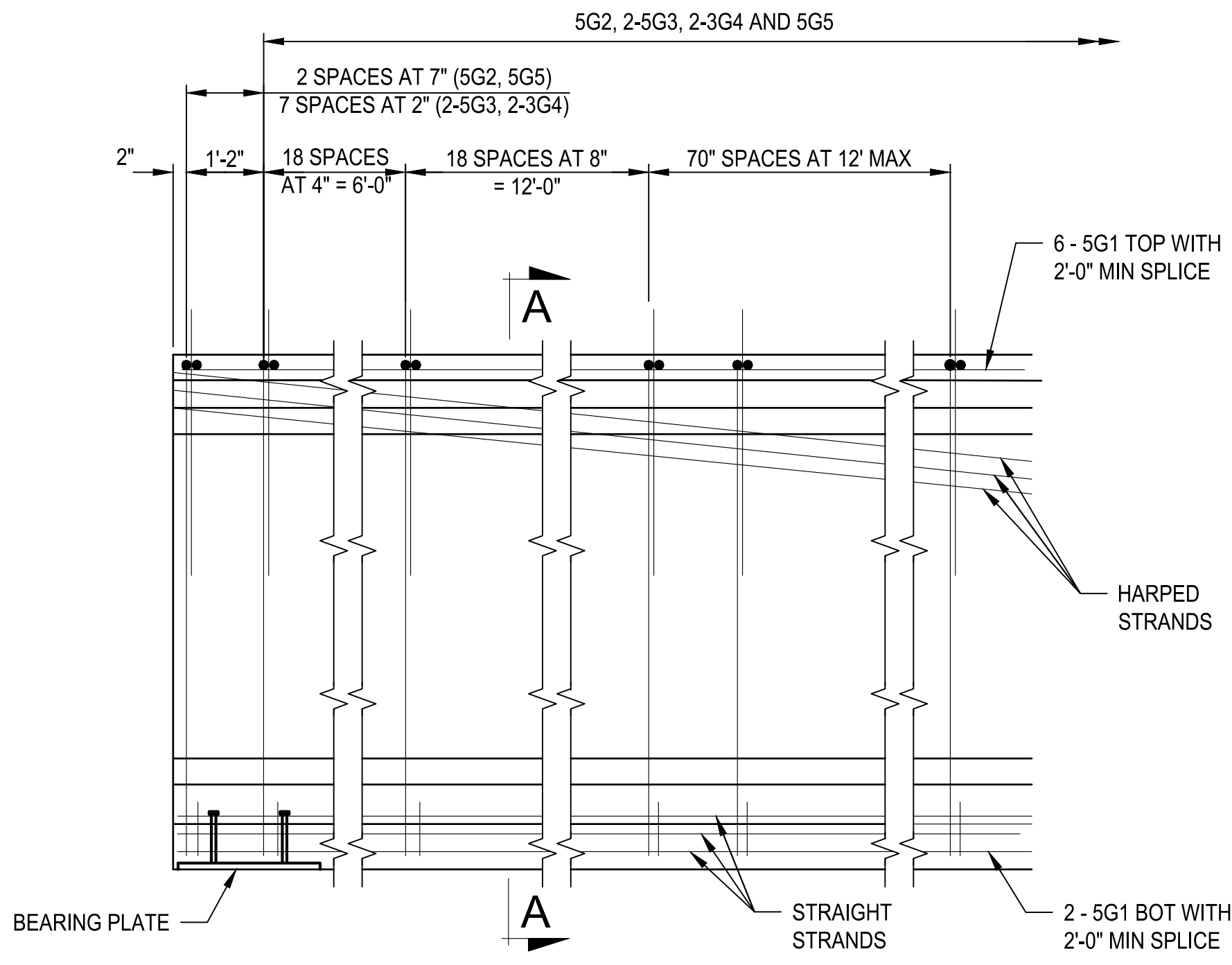
DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



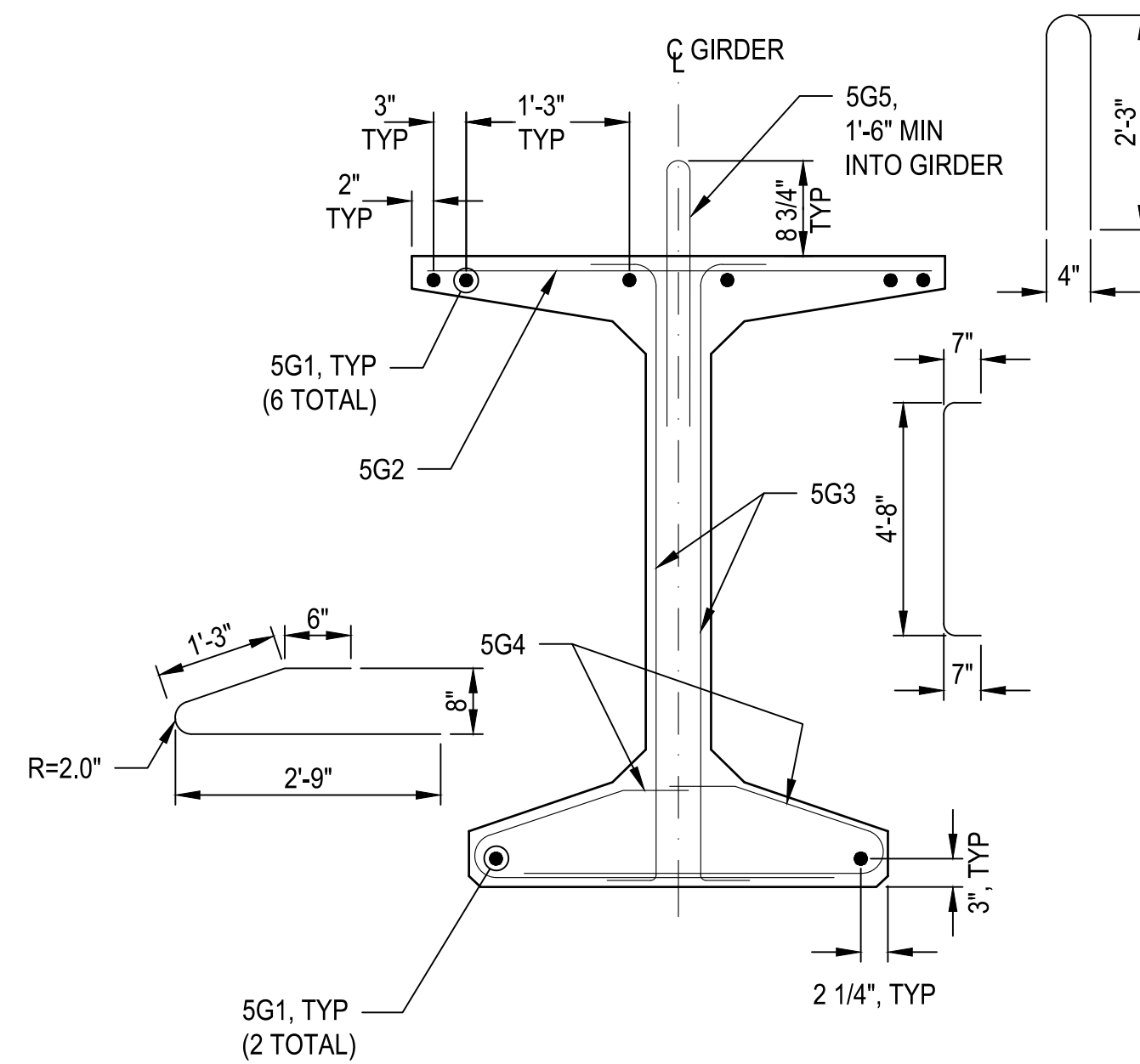
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

UBT58 GIRDER 90 FOOT SPAN

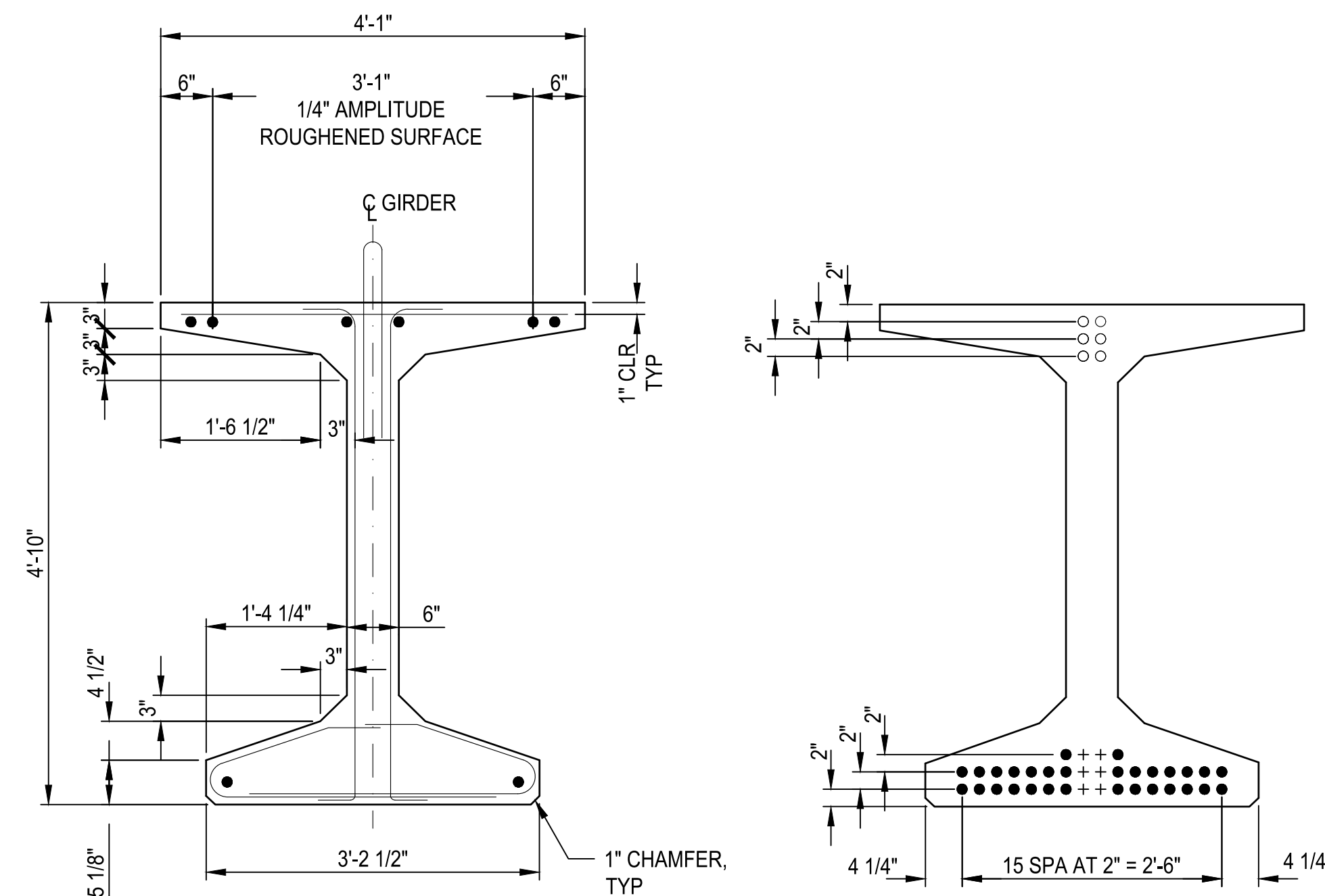
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TYPICAL END ELEVATION

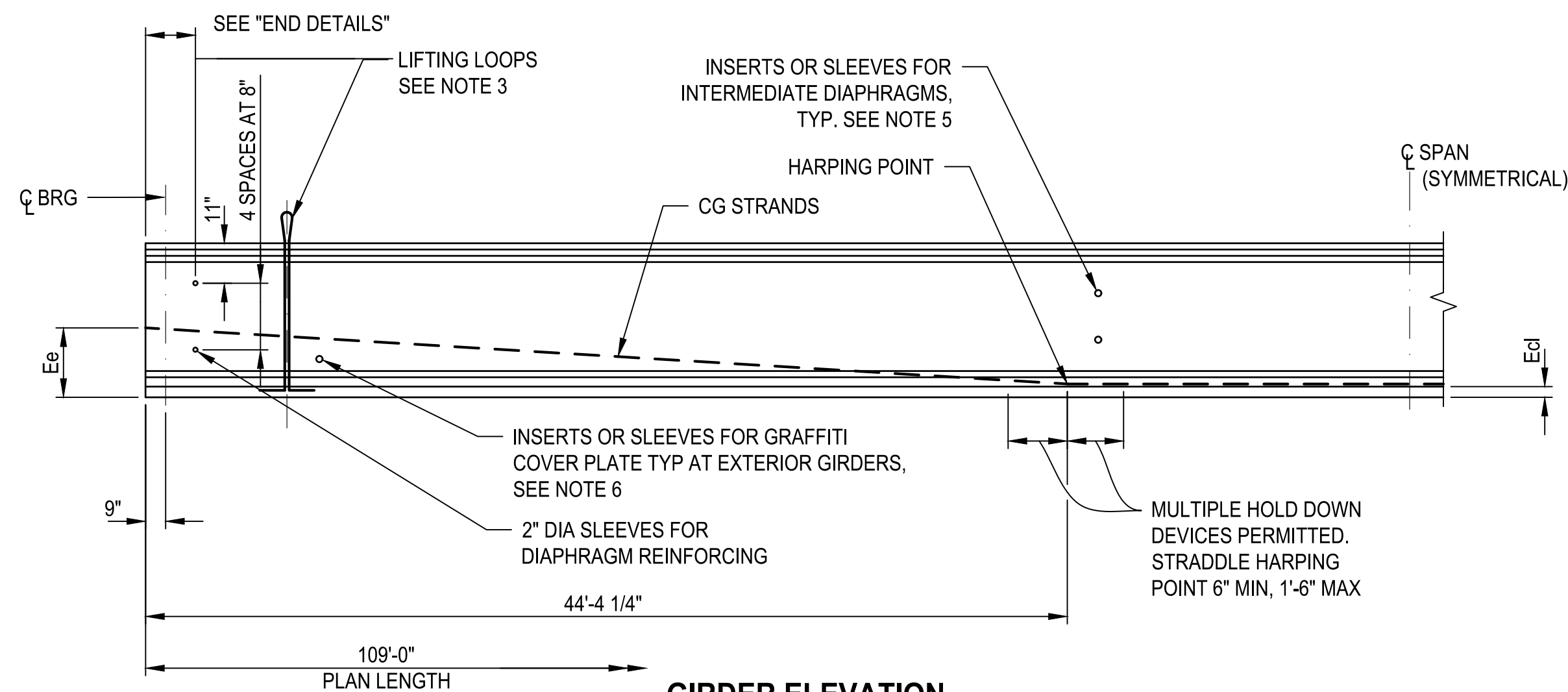


SECTION A-A

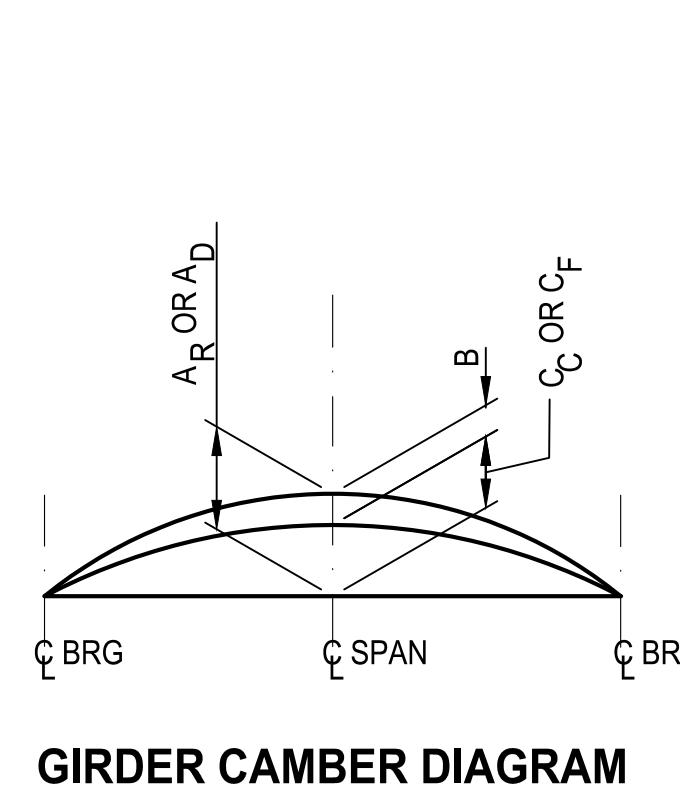


TYPICAL SECTION

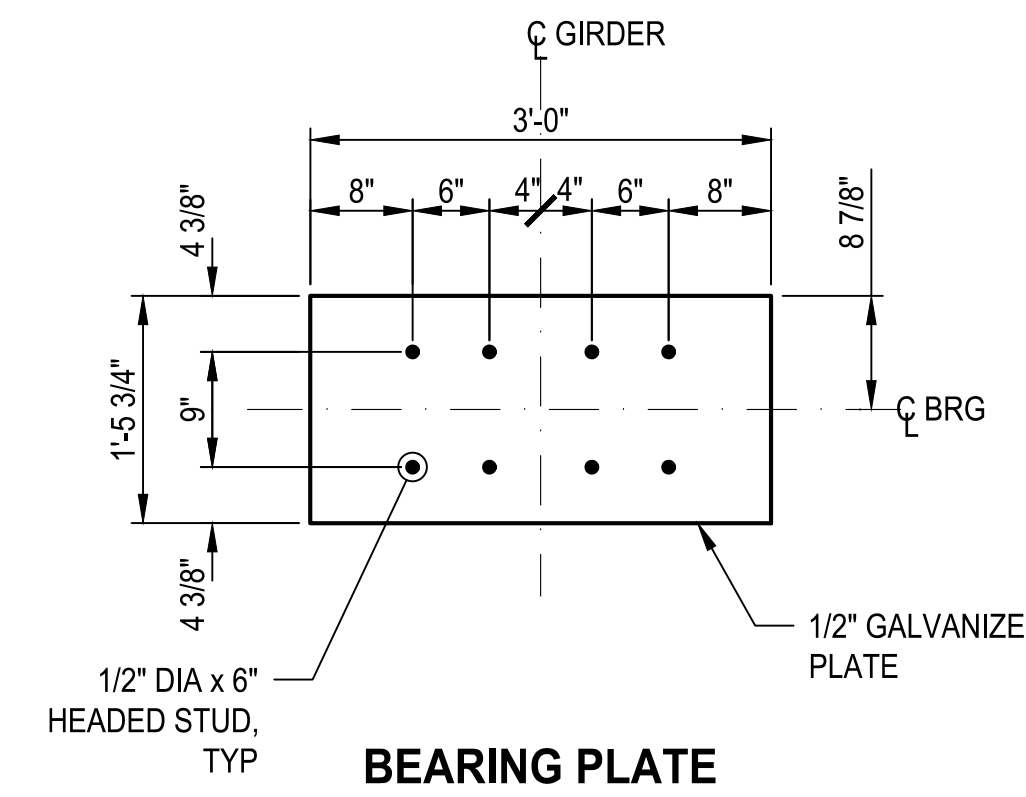
STRAND PATTERN



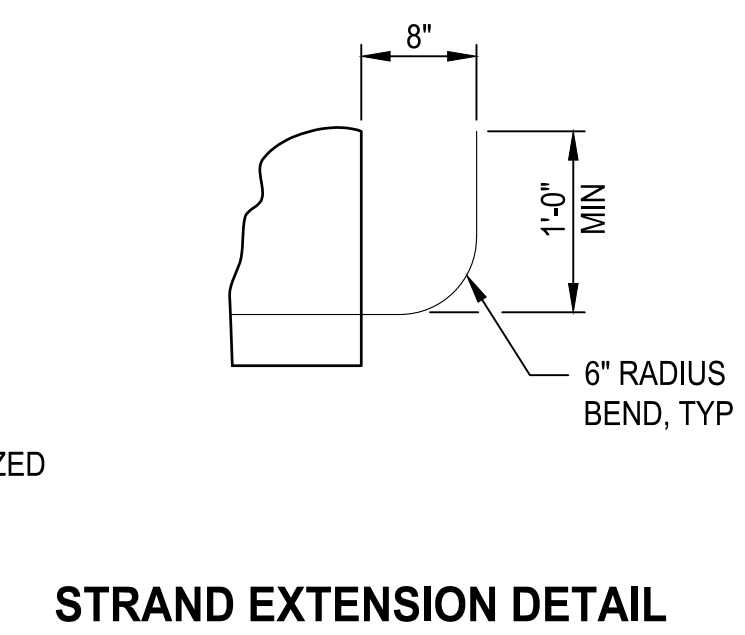
GIRDER ELEVATION



GIRDER CAMBER DIAGRAM



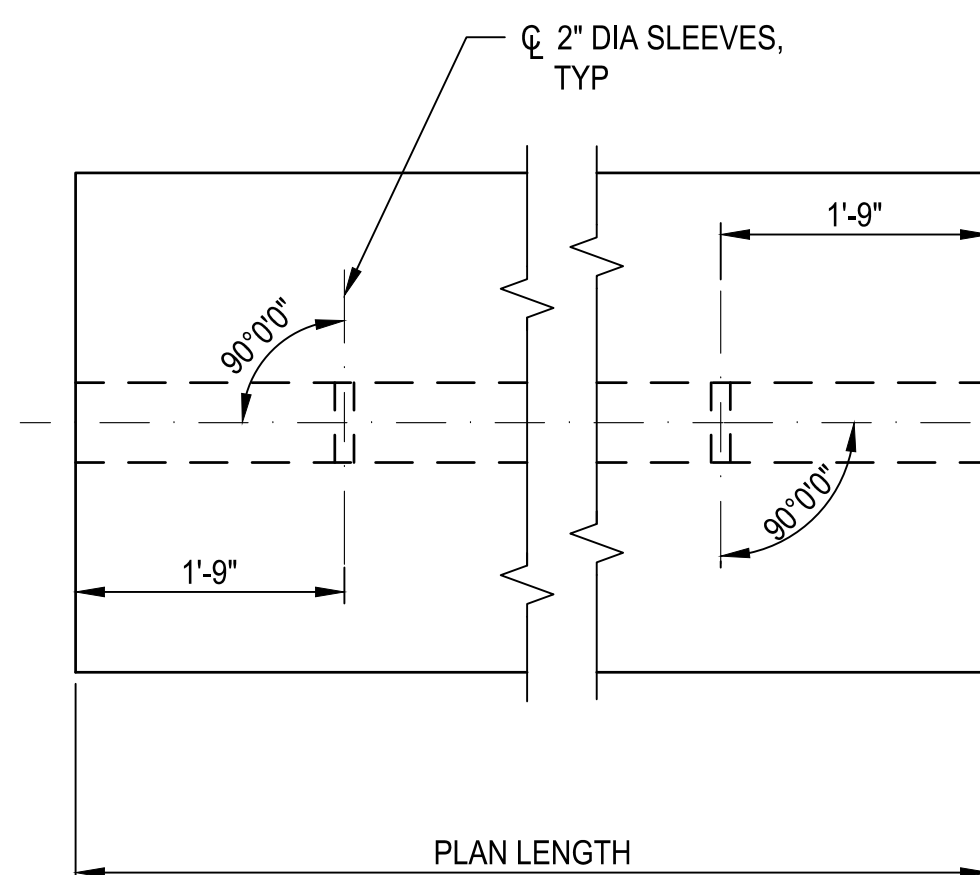
BEARING PLATE



STRAND EXTENSION DETAIL

NOTES

- INCREASE PLAN LENGTH AS NECESSARY TO COMPENSATE FOR SHORTENING DUE TO PRESTRESS AND SHRINKAGE.
- USE 0.6 INCH DIAMETER LOW RELAXATION STRANDS (AASHTO M 203 GRADE 270).
- DESIGN AND INSTALL LIFTING EMBEDMENTS IN ACCORDANCE WITH THE SPECIFICATIONS.
- BRACE GIRDERS TO PREVENT TIPPING AND TO CONTROL LATERAL BENDING DURING SHIPPING. BRACE GIRDERS LATERALLY TO PREVENT TIPPING UNTIL THE DIAPHRAGMS ARE INSTALLED.
- SEE "FRAMING PLAN" FOR INTERMEDIATE DIAPHRAGM LOCATIONS AND SEE "INTERMEDIATE DIAPHRAGM DETAILS FOR PRESTRESSED GIRDERS" FOR CONNECTION REQUIREMENTS.
- USE INSERTS OR SLEEVES FOR GRAFFITI COVER PLATE. ADJUST HOLES TO AVOID CONFLICT WITH GIRDER STRANDS. SEE "GRAFFITI COVER AND ELASTOMERIC PAD - INTEGRAL UBT GIRDER" FOR LOCATION OF INSERTS OR SLEEVES.
- NOTIFY ENGINEER IF ACTUAL CAMBER VARIES FROM THEORETICAL CAMBER BY MORE THAN 1/8 INCH.
- EXTEND 8 STRANDS AS SHOWN FOR END OF GIRDER AT BENT ONLY. CUT ALL OTHER STRANDS FLUSH. OFFSET EXTENDED STRANDS FROM SPAN TO SPAN. FABRICATOR TO SELECT AND SHOW EXTENDED STRANDS IN SHOP DRAWINGS.
- USE STRUCTURAL STEEL CONFORMING TO AASHTO M 270 GRADE 36 FOR BEARING PLATE. GALVANIZE ACCORDING TO AASHTO M 111 AFTER FABRICATION.



END DETAILS

CLIP TOP FLANGE ONLY

| SPAN | GIRDER | GIRDER TYPE | THETA 1 | THETA 2 | PLAN LENGTH | f'ci | f'c | HARPED | | STRAIGHT | | PRESTRESSED FORCE AFTER ALL LOSSES | LOCATION OF CG STRANDS | | CAMBER | | | | | |
|------|--------|-------------|---------|---------|-------------|------|-----|-------------------|--------------------|-------------------|--------------------|------------------------------------|------------------------|------|----------------|----------------|-----|----------------|----------------|------|
| | | | | | | | | NUMBER OF STRANDS | JACKING FORCE KIPS | NUMBER OF STRANDS | JACKING FORCE KIPS | | Ee | Ecl | A _R | A _D | B | C _C | C _F | |
| | | | | | | | | | | | | | | | | | | | | INCH |
| 2 | 1 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1317.4 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |
| 2 | 2 TO 6 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1330.0 | 11.67 | 3.33 | 2.2 | 3.9 | 1.8 | 2.1 | 2.9 | |
| 2 | 7 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1321.1 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |
| 3 | 1 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1317.0 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |
| 3 | 2 TO 6 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1329.8 | 11.67 | 3.33 | 2.2 | 3.9 | 1.8 | 2.1 | 2.9 | |
| 3 | 7 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1320.9 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |
| 4 | 1 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1317.2 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |
| 4 | 2 TO 6 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1330.0 | 11.67 | 3.33 | 2.2 | 3.9 | 1.8 | 2.1 | 2.9 | |
| 4 | 7 | UBT58 | N/A | N/A | 109'-0" | 6.0 | 9.5 | 6 | 263.7 | 30 | 1318.3 | 1321.0 | 11.67 | 3.33 | 2.2 | 3.9 | 1.6 | 2.3 | 3.1 | |

CAMBER NOTES: A_R = GIRDER CAMBER AT RELEASE
 A_D = GIRDER CAMBER JUST BEFORE DECK PLACEMENT (USE 40 DAYS)
 B = DEFLECTION DUE TO ALL DEAD LOADS APPLIED TO THE GIRDER (NO FUTURE WEARING SURFACE)
 C_C = GIRDER CAMBER AT CONSTRUCTION AFTER ALL DEAD LOADS APPLIED (NO FUTURE WEARING SURFACE)
 C_F = GIRDER CAMBER AT 20 YEARS (INCLUDES FUTURE WEARING SURFACE)

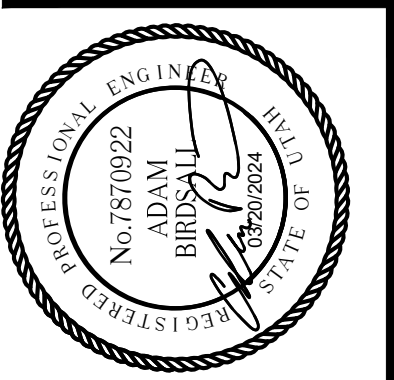
CG STRAND NOTES: Ee = CG OF ALL STRANDS AT END OF GIRDER
 Ecl = CG OF ALL STRANDS AT MIDSPAN OF GIRDER

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

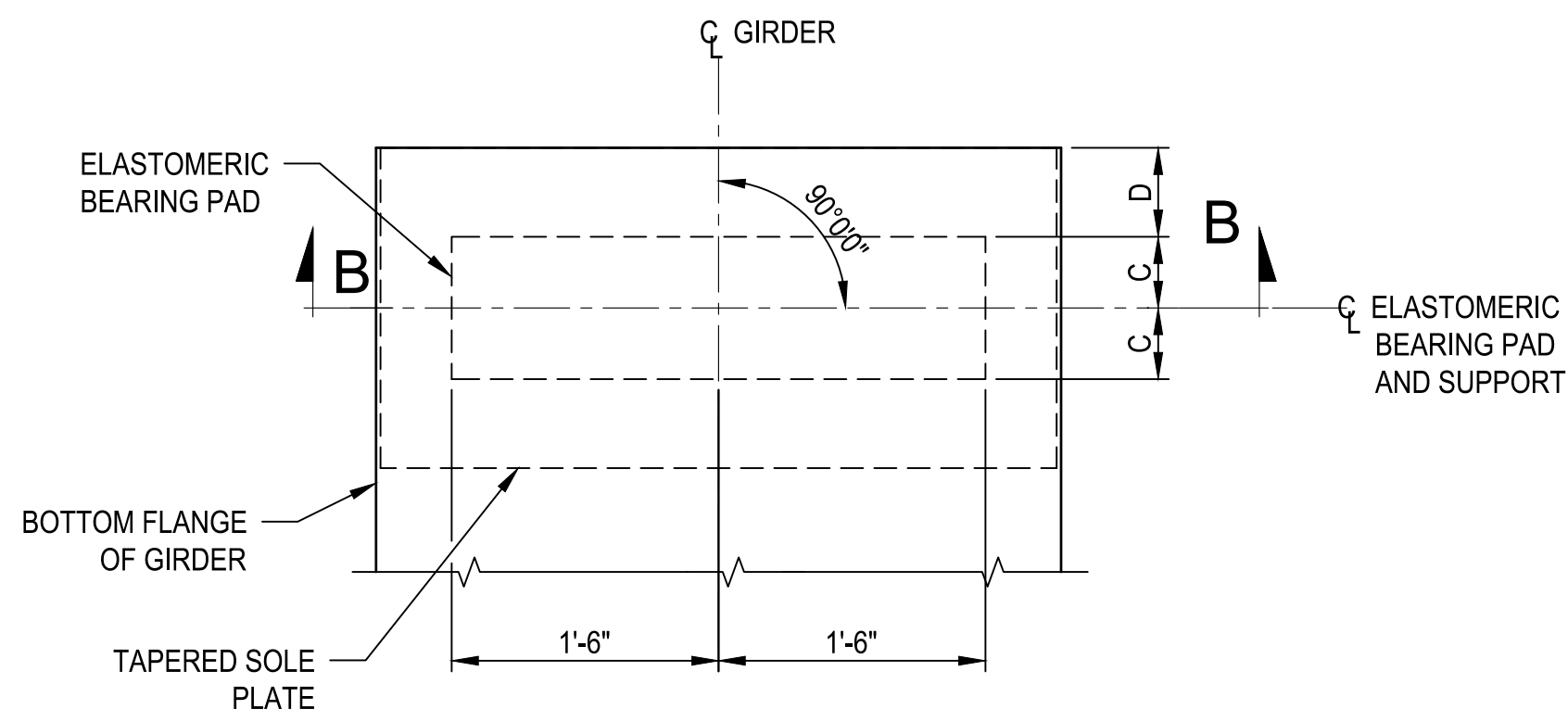
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 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
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 APPROVED: AUB



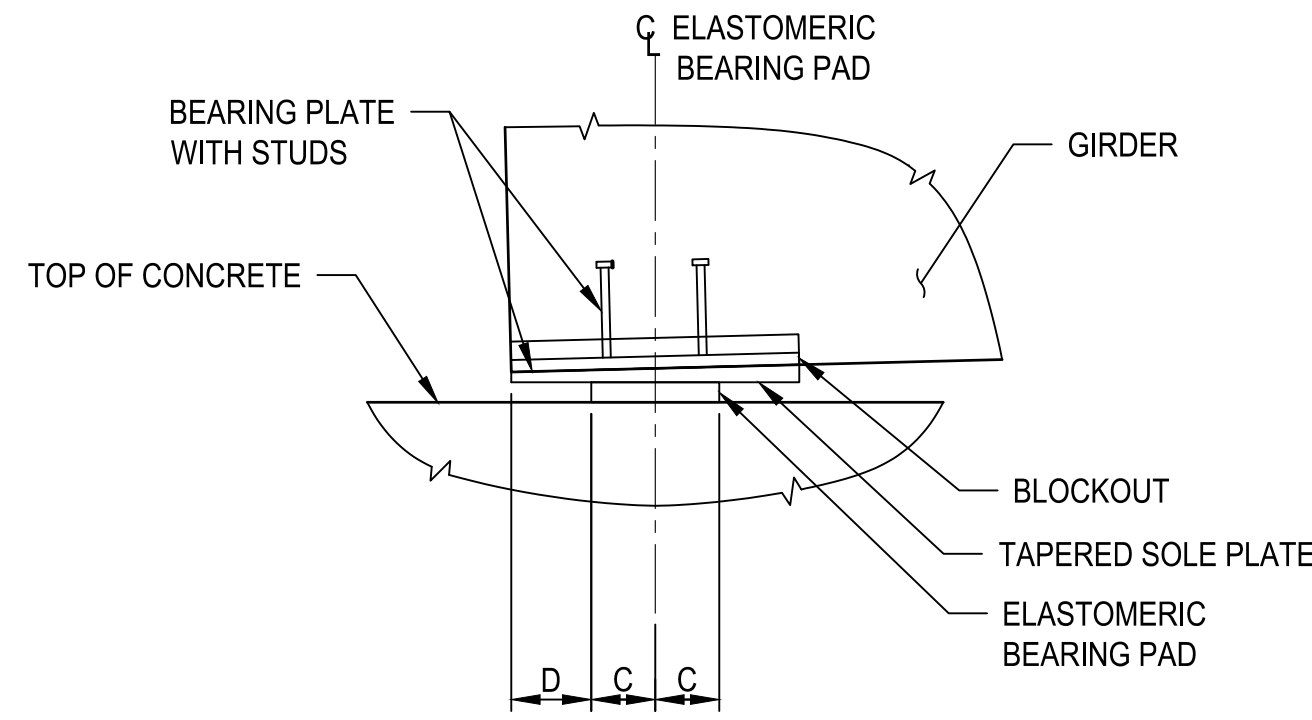
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

UBT58 GIRDER 110 FOOT SPAN

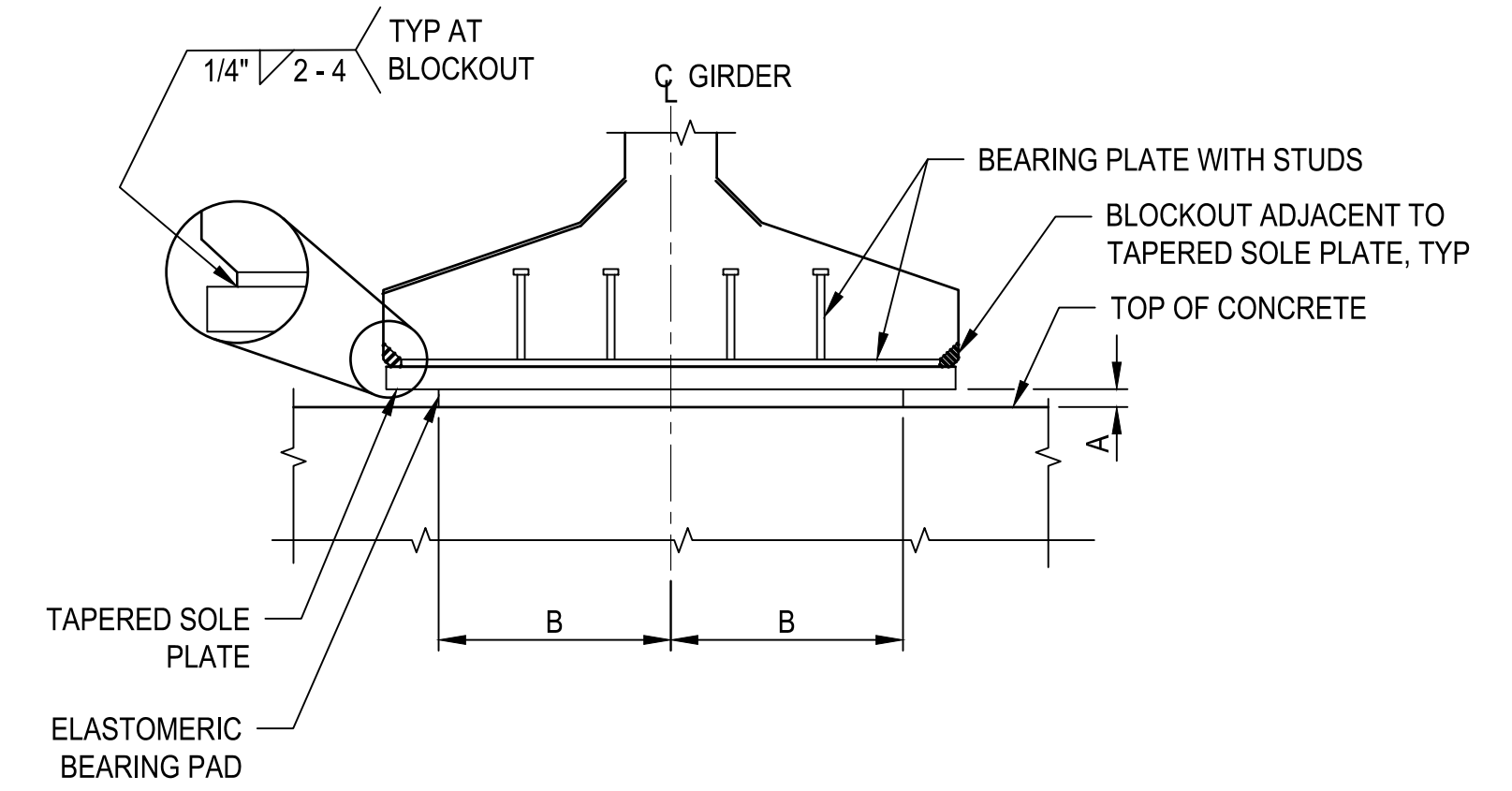
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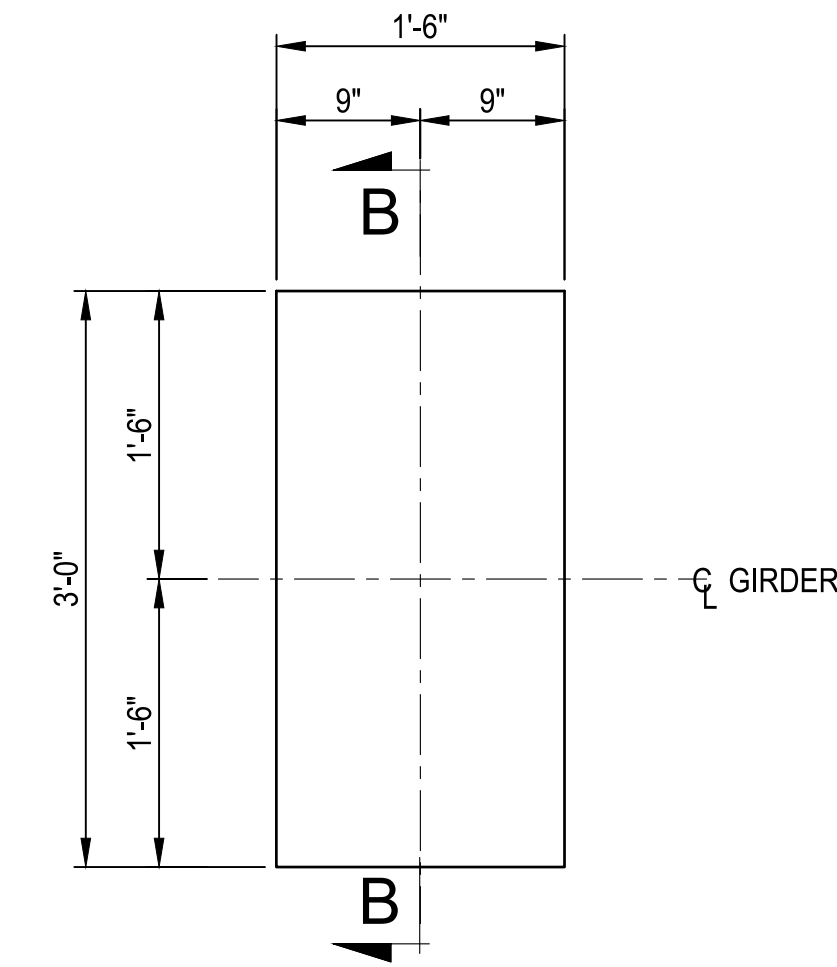
BEARING PLAN



BEARING DETAIL - SIDE VIEW

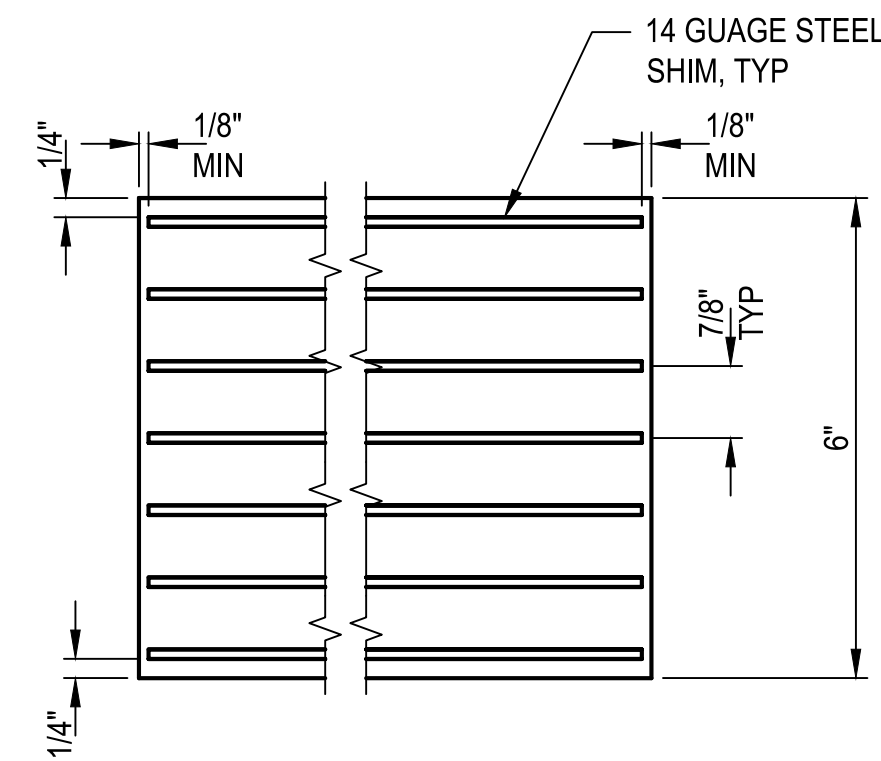


BEARING DETAIL - END VIEW

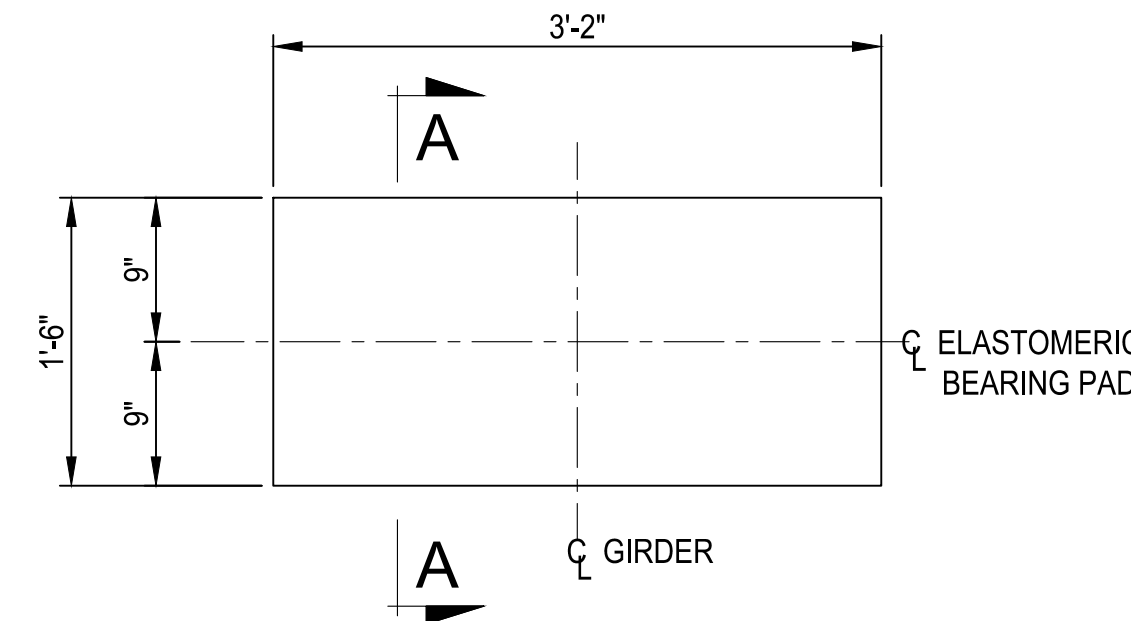


PLAN

ELASTOMERIC BEARING - ABUTMENT



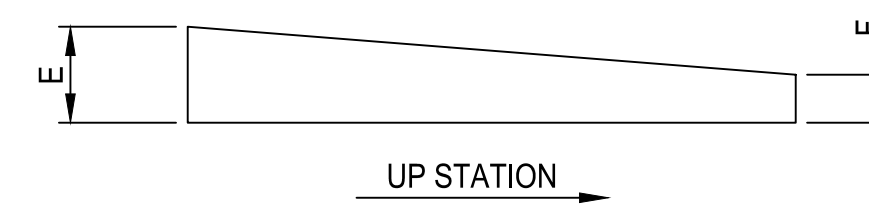
SECTION B-B



PLAN

TAPERED SOLE PLATE

| SPAN | BEAM END 1 | | BEAM END 2 | |
|------|------------|---------|------------|---------|
| | E | F | E | F |
| 1 | 1 1/2" | 3" | 2 5/8" | 1'-1/2" |
| 2 | 1 1/2" | 2'-3/4" | 2" | 1 1/2" |
| 3 | 1 1/2" | 2'-1/4" | 1 1/2" | 1 1/2" |
| 4 | 1 5/8" | 1'-1/2" | 1 1/2" | 2" |
| 5 | 2" | 1'-1/2" | 1 1/2" | 2 1/2" |



SECTION A-A

NOTES

- USE AASHTO M 270 GRADE 36 STEEL FOR GRAFFITI COVER. USE 7/8 INCH DIA ASTM A 307 GRADE A BOLTS WITH ASTM A 563 NUTS AND ASTM F 436 WASHERS. GALVANIZE ALL NUTS, BOLTS AND WASHERS PAINT GRAFFITI COVERS TO MATCH THE COLOR OF THE GIRDERS.
- USE 60 DUROMETER HARDNESS (SHORE A), GRADE 3 ELASTOMER. MINIMUM AND MAXIMUM SHEAR MODULUS ARE 0.130 KSI AND 0.200 KSI, RESPECTIVELY. BEARING PADS ARE DESIGNED USING METHOD A.
- USE AASHTO M 270 GRADE 50 STEEL FOR TAPERED SOLE PLATE.

| LOCATION | DC $\gamma = 1.00$ | DW $\gamma = 1.00$ | LL WITHOUT IMPACT $\gamma = 1.00$ | TEMPERATURE MOVEMENT $\gamma = 1.00$ | | SEISMIC MOVEMENT $\gamma = 1.00$ | | NUMBER OF BEARINGS |
|-------------|-----------------------|-----------------------|---|---|--------------|-------------------------------------|--------------|--------------------------|
| | | | | LATERAL | LONGITUDINAL | LATERAL | LONGITUDINAL | |
| | | | | KIPS | KIPS | KIPS | KIPS | |
| ABUTMENT #1 | 139 | 10 | 42 | NA | 2.68 | 5.09 | 6.96 | 7 |
| ABUTMENT #6 | 139 | 10 | 42 | NA | 2.68 | 5.09 | 6.96 | 7 |

| GIRDER | ELASTOMERIC BEARING PAD DIMENSIONS | | | | ALLOWABLE * LOAD | LOAD ** | NUMBER OF BEARINGS |
|----------------------|------------------------------------|-------|--------|--------|---------------------|----------|-----------------------|
| | A | B | C | D | | | |
| UBT58 AT ABUTS 1 & 6 | 6" | 1'-6" | 9" | 0" | 250 KIPS | 140 KIPS | 14 |
| UBT58 AT BENTS 2-5 | 1 5/8" | 1'-6" | 6 1/2" | 2 1/2" | 178 KIPS | 143 KIPS | 56 |

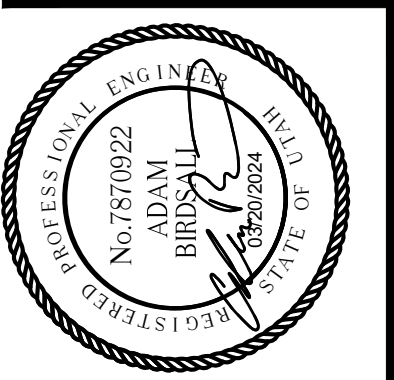
* MAXIMUM ALLOWABLE UNFACTORED NONCOMPOSITE DEAD LOAD
** UNFACTORED NONCOMPOSITE DEAD LOAD

| REVISIONS | DATE | BY |
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ONE INCH
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Parametrix

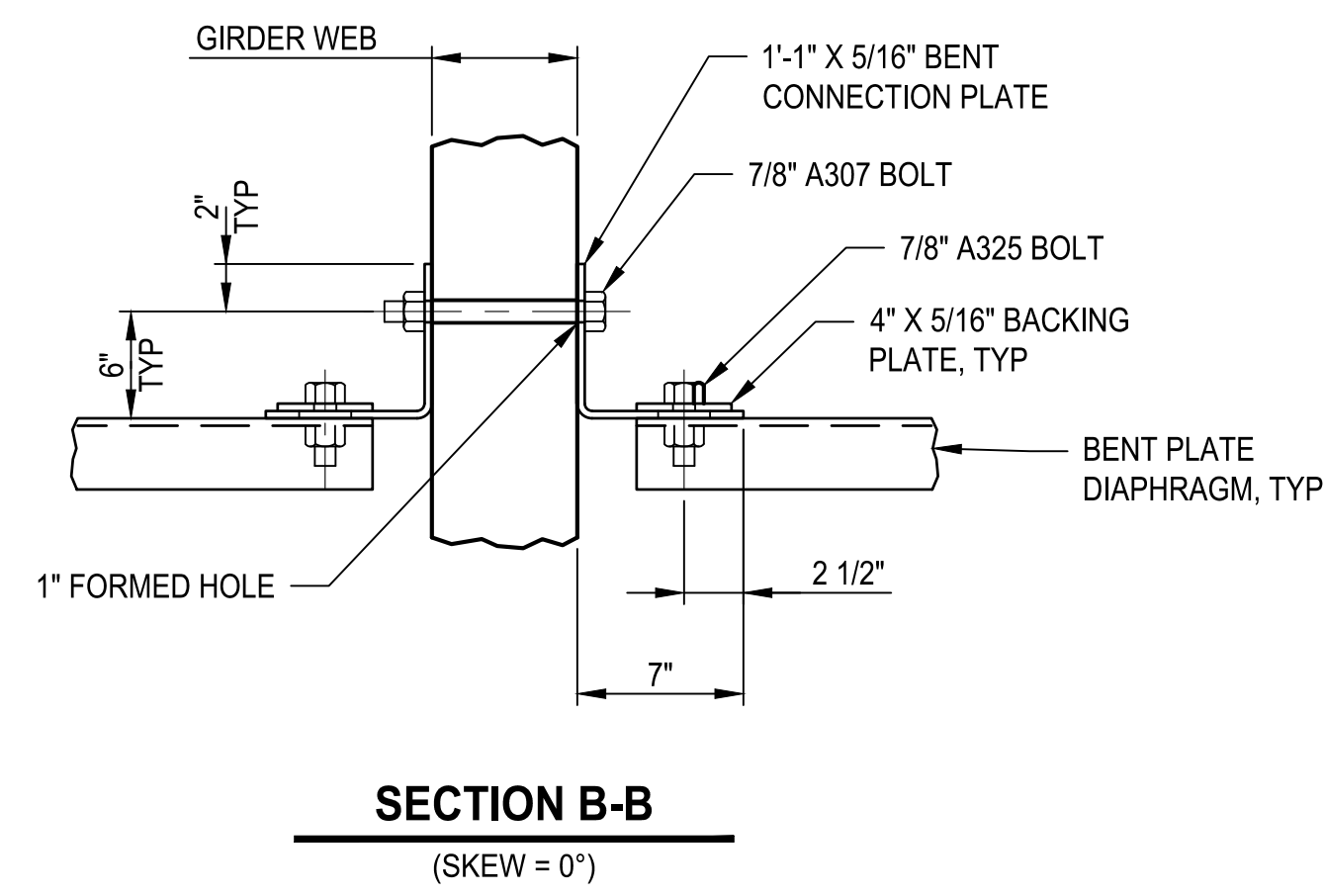
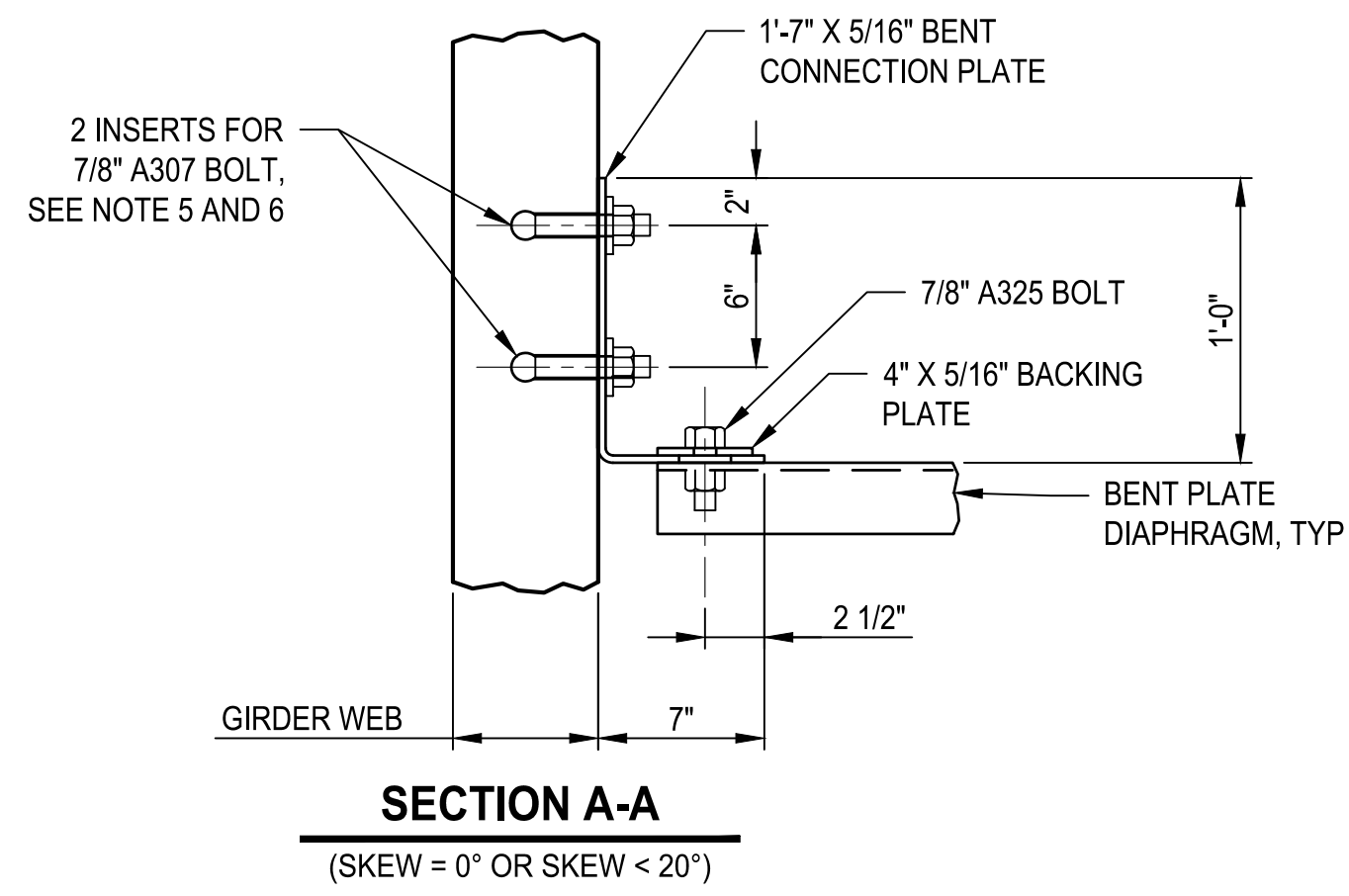
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JOB No.: 344-8541-002
DESIGNED: TWP
DRAWN: SLO
CHECKED: NICC
APPROVED: AUB



PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

**ELASTOMERIC
BEARING PAD
DETAILS**

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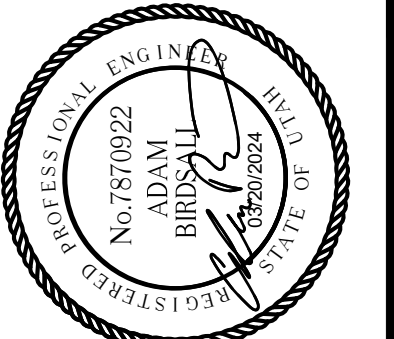
NOTES

- USE AASHTO M270 GRADE 36 STEEL FOR ALL CONNECTION ANGLES AND PLATES. GALVANIZE ALL CONNECTION ANGLES AND PLATES IN ACCORDANCE WITH AASHTO M111.
- USE 7/8 INCH DIAMETER ASTM F3125, GRADE A325, TYPE 1, BOLTS, ASTM A563 NUTS AND ASTM F959 WASHERS FOR STEEL TO STEEL CONNECTIONS. GALVANIZE ALL NUTS, BOLTS AND WASHERS. BOLTS ARE SLIP CRITICAL. PROVIDE A CLASS C SURFACE CONDITION.
- USE 7/8 INCH DIAMETER ASTM A307, GRADE A BOLTS, ASTM A563 NUTS AND ASTM F436 WASHERS FOR STEEL TO CONCRETE CONNECTIONS. GALVANIZE ALL NUTS, BOLTS AND WASHERS.
- USE THREADED INSERTS FOR DIAPHRAGM CONNECTIONS TO EXTERIOR GIRDERS. PLACE INSERTS PERPENDICULAR TO WEB. PROVIDE INSERTS WITH A MINIMUM ULTIMATE TENSILE CAPACITY OF 11.0 KIPS.
- ADJUST LOCATION OF INSERTS AND HOLES IN WEB TO AVOID INTERFERENCE WITH PRESTRESSING STRAND. COORDINATE CHANGES WITH BENT CONNECTION PLATE SUPPLIER.
- FIELD DRILLING OF BENT CONNECTION PLATE IS PERMITTED. FIELD WELDS ARE NOT PERMITTED.
- INCLUDE GIRDER DIAPHRAGMS IN COST OF GIRDER.

| | |
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ONE INCH
AT FULL
SCALE IF
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OTHERWISE
ACCORDINGLY

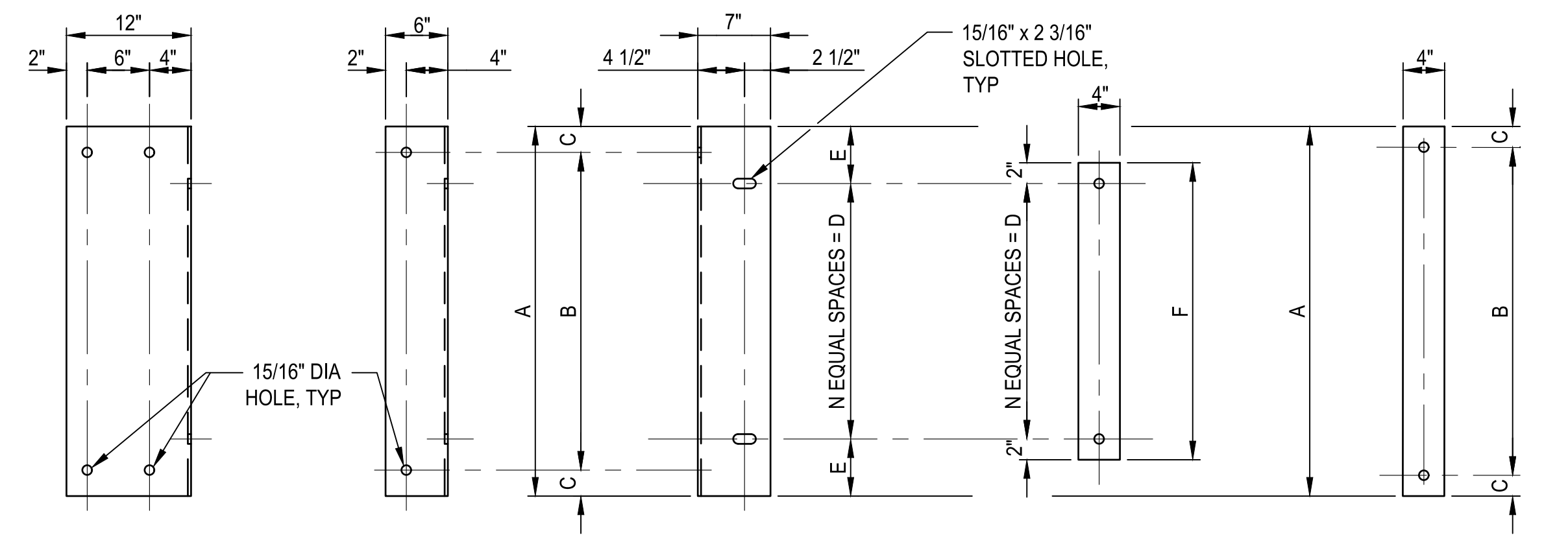
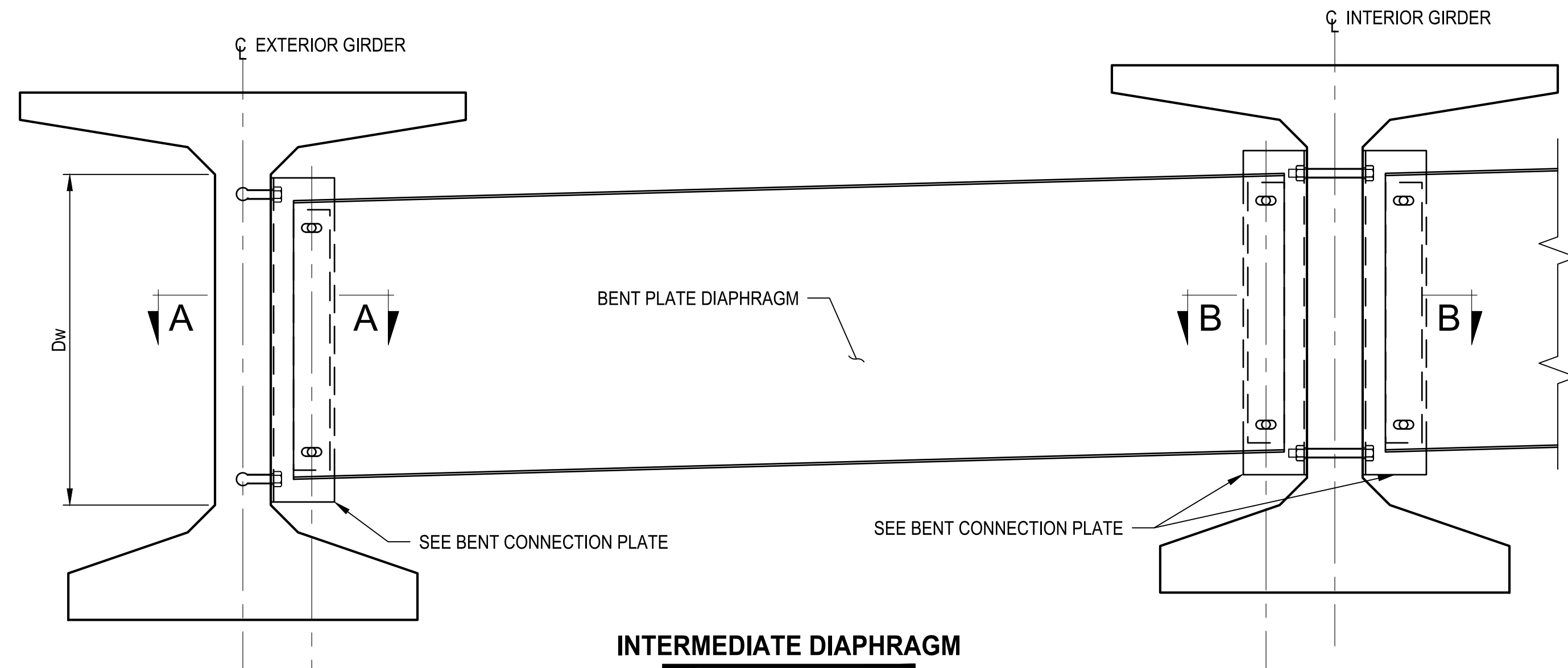
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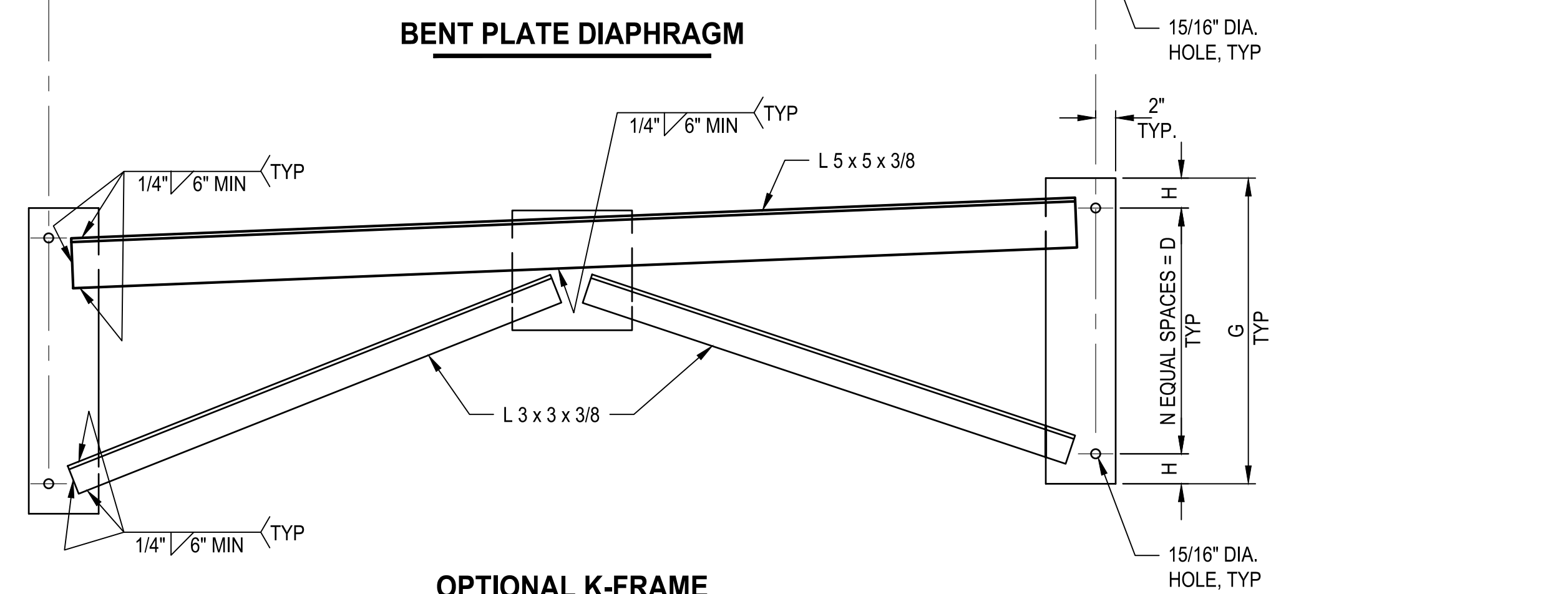
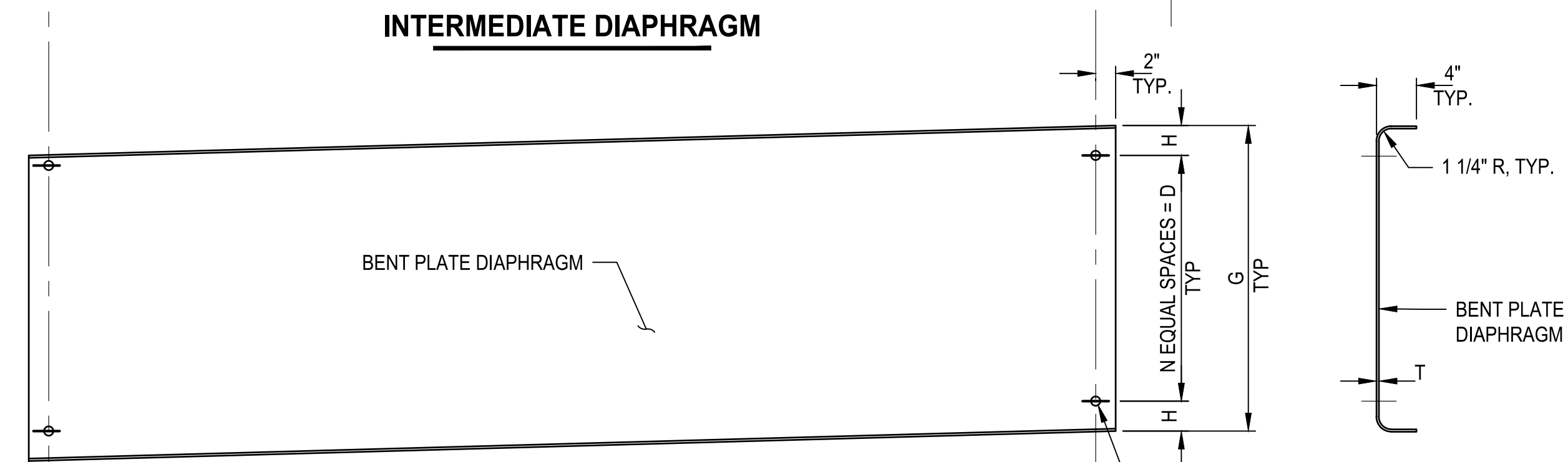
PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

**INTERMEDIATE
 DIAPHRAGM DETAILS
 FOR PRESTRESSED
 GIRDERS**

DRAWING NO.
 34 OF 59
S34



SIDE VIEW SECTION A-A **SIDE VIEW SECTION B-B** **FRONT VIEW**
BENT CONNECTION PLATE **BACKING PLATE** **BEARING PLATE**

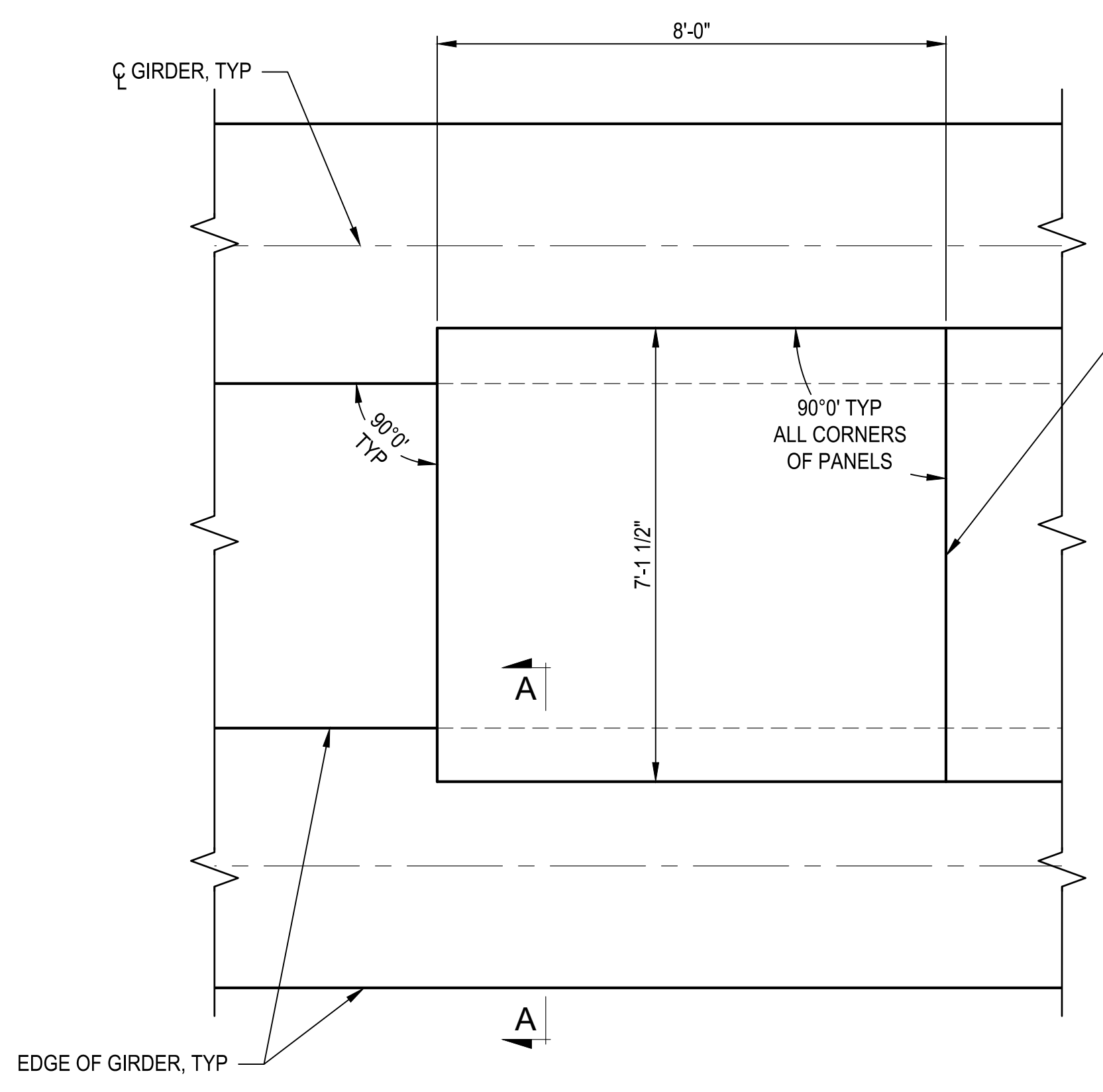


| DIAPHRAGM DATA TABLE | | | | | | | | | | | |
|----------------------|--------|--------|--------|-----|--------|-----|--------|--------|-----|---|--------|
| GIRDER | Dw | A | B | C | D | E | F | G | H | N | T |
| UBT42 | 20.375 | 19.625 | 14.625 | 2.5 | 9.625 | 5.0 | 13.625 | 14.625 | 2.5 | 2 | 0.3125 |
| UBT50 | 28.375 | 27.625 | 22.625 | 2.5 | 17.625 | 5.0 | 21.625 | 22.625 | 2.5 | 3 | 0.3125 |
| UBT58 | 36.375 | 35.625 | 30.625 | 2.5 | 24.625 | 5.5 | 28.625 | 30.625 | 3.0 | 4 | 0.3125 |
| UBT66 | 44.375 | 43.625 | 38.625 | 2.5 | 30.625 | 6.5 | 34.625 | 38.625 | 4.0 | 5 | 0.3125 |
| UBT74 | 52.375 | 51.625 | 46.625 | 2.5 | 34.625 | 8.5 | 38.625 | 46.625 | 6.0 | 5 | 0.3125 |
| UDBT42 | 18.375 | 17.625 | 12.625 | 2.5 | 7.625 | 5.0 | 11.625 | 12.625 | 2.5 | 2 | 0.3125 |
| UDBT50 | 26.375 | 25.625 | 20.625 | 2.5 | 15.625 | 5.0 | 19.625 | 20.625 | 2.5 | 3 | 0.3125 |
| UDBT58 | 34.375 | 33.625 | 28.625 | 2.5 | 22.625 | 5.5 | 26.625 | 28.625 | 3.0 | 4 | 0.3125 |
| UDBT66 | 42.375 | 41.625 | 36.625 | 2.5 | 28.625 | 6.5 | 32.625 | 36.625 | 4.0 | 5 | 0.3125 |
| UDBT74 | 50.375 | 49.625 | 44.625 | 2.5 | 32.625 | 8.5 | 36.625 | 44.625 | 6.0 | 5 | 0.3125 |

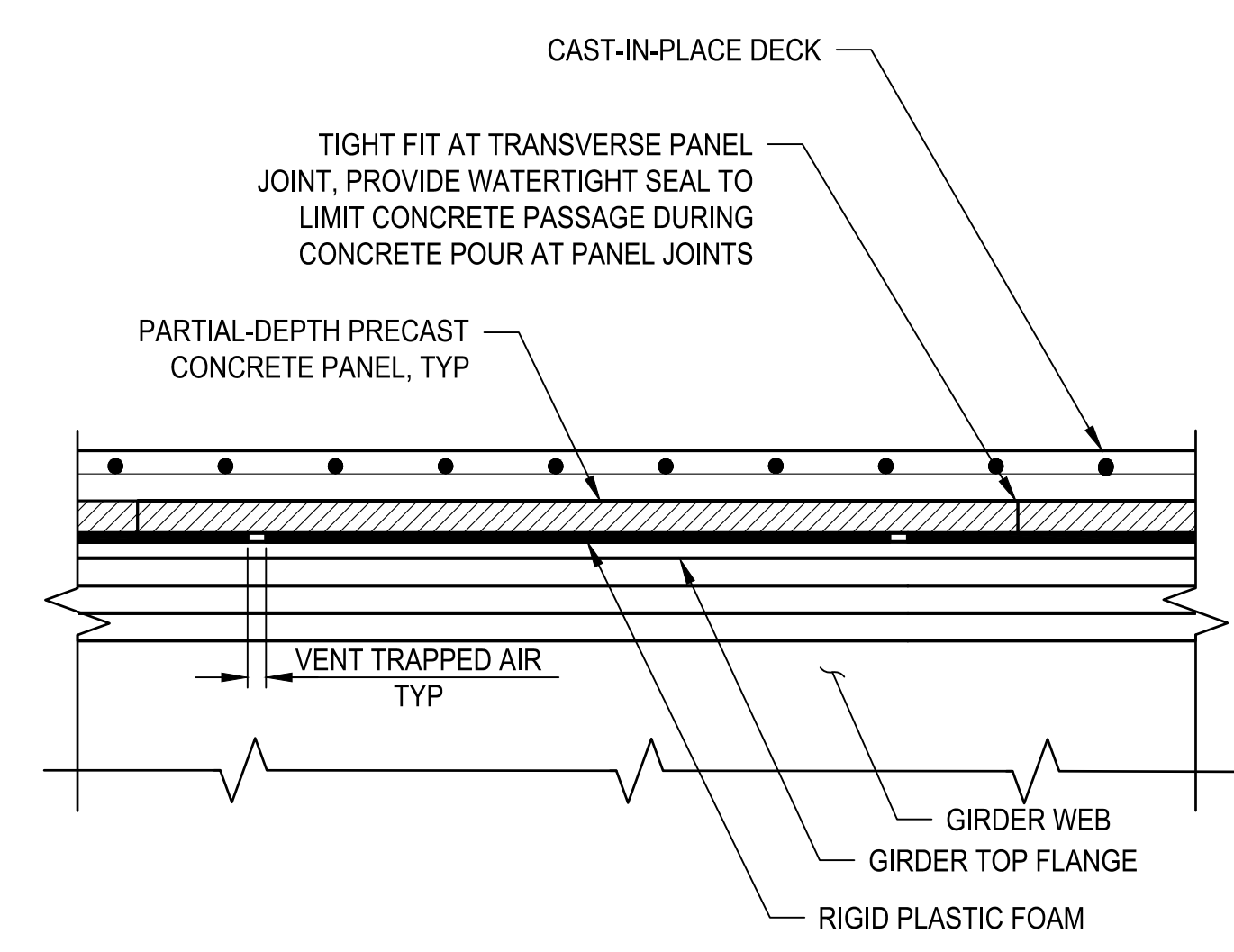
DIMENSIONS ARE SHOWN IN INCHES

| ITEM | LOCATION | EST QTY | UNIT |
|------------------------------|-------------------------|---------|------|
| UBT58 DIAPHRAGM (SEE NOTE 7) | SPANS 1, 2, 3, 4, AND 5 | 48 | EACH |

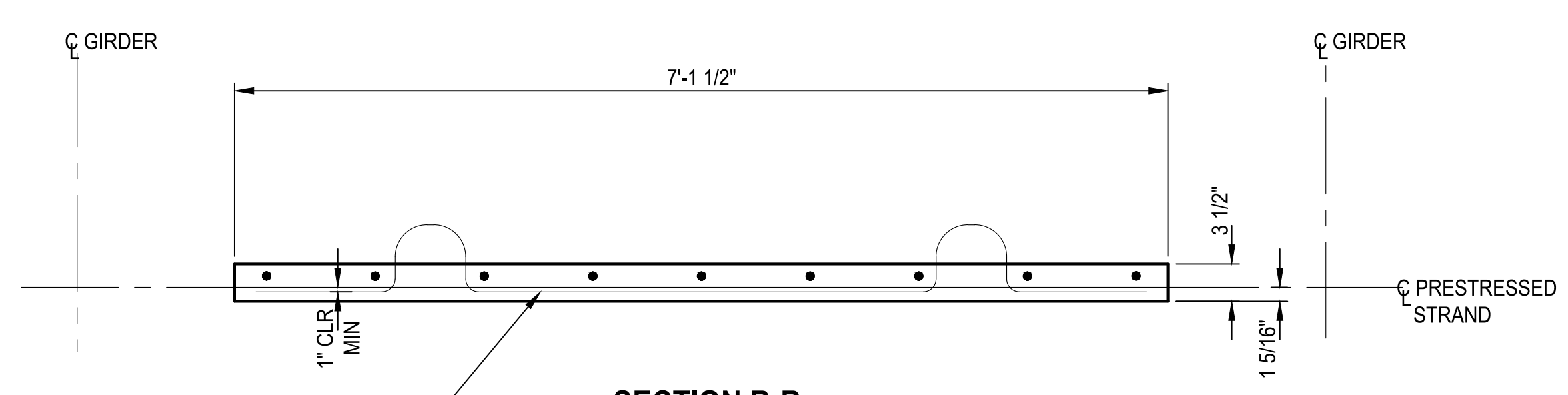
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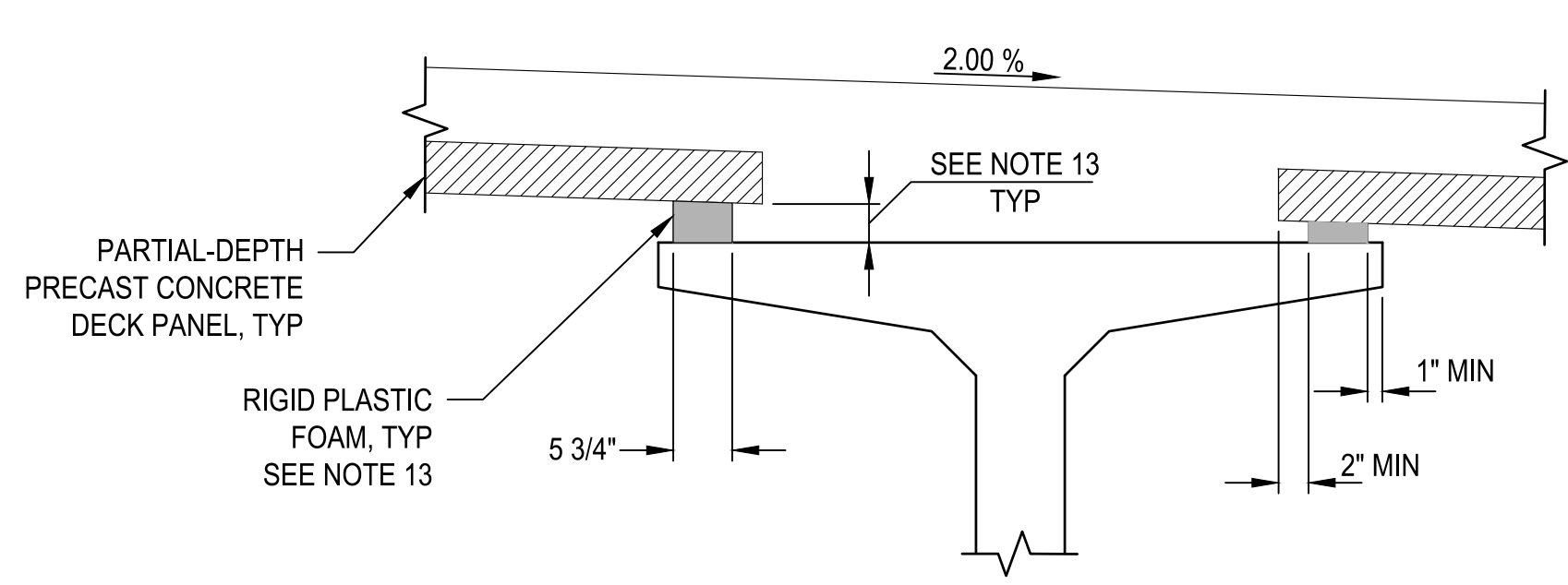
PANEL SET DETAIL



PANEL SET DETAIL



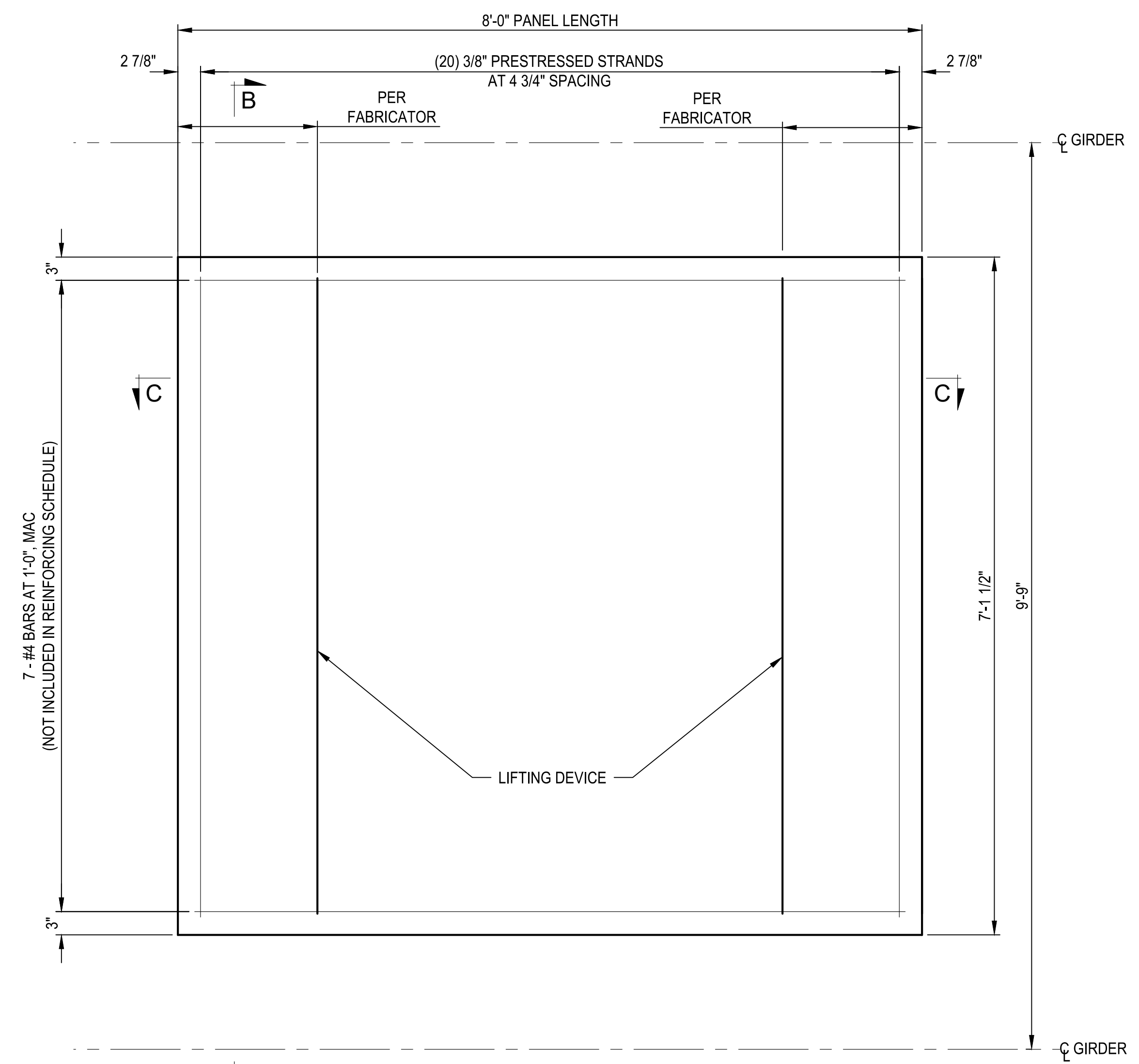
SECTION B-B



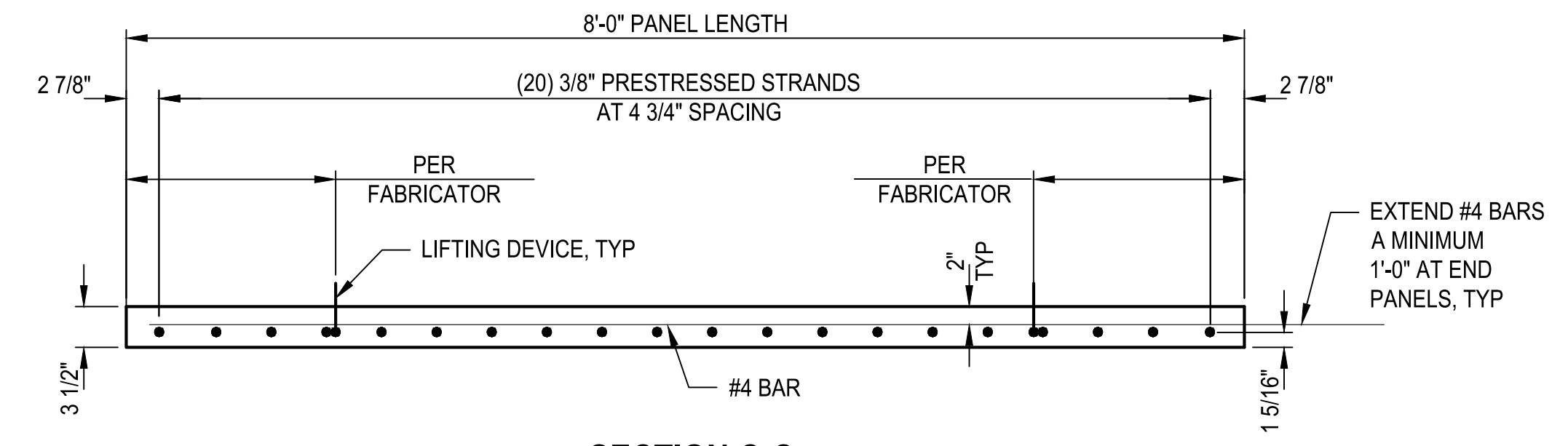
SECTION A-A

NOTES

- USE CLASS AAA(AE) f'_{ci} = 4500 PSI AND MINIMUM 28 DAYS STRENGTH f'_c = 6000 PSI. USE f'_c min = 5000 PSI AT TIME OF THE DECK POUR.
- USE 3/8 INCH DIA LOW RELAXATION STRANDS MEETING THE REQUIREMENTS OF AASHTO M 203 GRADE 270. MINIMUM JACKING FORCE PER STRAND (F_j) IS 17.2 KIPS. ESTIMATED FINAL FORCE PER STRAND (F_f) IS 14.3 KIPS.
- INSTALLATION OF LIFTING DEVICE IS MANDATORY. USE ALL FOUR LIFTING DEVICES SIMULTANEOUSLY FOR LIFTING THE PANELS. PROVIDE DETAILS OF LIFTING DEVICES ON THE WORKING DRAWINGS.
- REMOVE ALL CONSTRUCTION DEBRIS FROM GIRDERS AND PANELS PRIOR TO DECK POUR. PROVIDE ADEQUATE VIBRATION TO FILL VOIDS UNDER PANELS.
- MAINTAIN THE STABILITY OF THE PANELS ON THE GIRDERS. SUPPORT ERECTED PANELS UNIFORMLY ALONG THE LENGTH OF THE PANEL. PROVIDE THE TOTAL SLAB THICKNESS SHOWN ON THE SUPERSTRUCTURE DETAILS.
- DO NOT WELD REBAR.
- SEE "DECK PLAN 1 OF 2", "DECK PLAN 2 OF 2" AND "DECK SECTIONS" FOR LONGITUDINAL AND TRANSVERSE REINFORCEMENT OVER PANELS.



PLAN



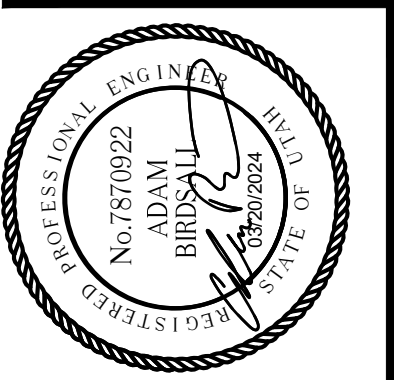
SECTION C-C

- SAWING OF PANELS IS ACCEPTABLE.
- STRAND PLACEMENT TOLERANCE IS +/- 1/4 INCH. PANEL THICKNESS TOLERANCE IS +1/4 INCH TO 1/8 INCH.
- CONCENTRATED CONSTRUCTION LOADS MUST NOT EXCEED 700 LBS UNLESS THE LOAD IS DISTRIBUTED TO LESS THAN 117 PSF. TOTAL LOADS APPLIED TO ANY PANEL DURING CONSTRUCTION MUST NOT EXCEED 117 PSF.
- BOTTOM FLEXURAL CRACKS, SAGS GREATER THAN 1/2 INCH, OR CAMBERS GREATER THAN 1/2 INCH WILL BE CONSIDERED EVIDENCE OF MISHANDLING, OVERLOADING, OR EXCEEDING ALLOWABLE TOLERANCE, AND ARE CAUSE FOR REJECTING PANELS AT THE ENGINEER'S DISCRETION.
- USE COATED DEFORMED CARBON STEEL BARS CONFORMING TO ASTM A 775 OR AASHTO M 111 AND AASHTO M 31 GRADE 60, RESPECTIVELY.
- THE ANTICIPATED HEIGHT OF THE RIGID PLASTIC FOAM VARIES FROM 1 3/4 INCH MIN TO 5 3/4 INCH MAX. NOTIFY THE ENGINEER WHEN THE THICKNESS OF THE RIGID PLASTIC FOAM EXCEEDS THESE LIMITS.

| REVISIONS | DATE | BY |
|-----------|------|----|
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ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix
 CHECKED: NICC
 APPROVED: AUB
 DESIGNED: TWP
 DRAWN: SLO
 DATE: 03/20/2024
 JOB No.: 344-8541-002



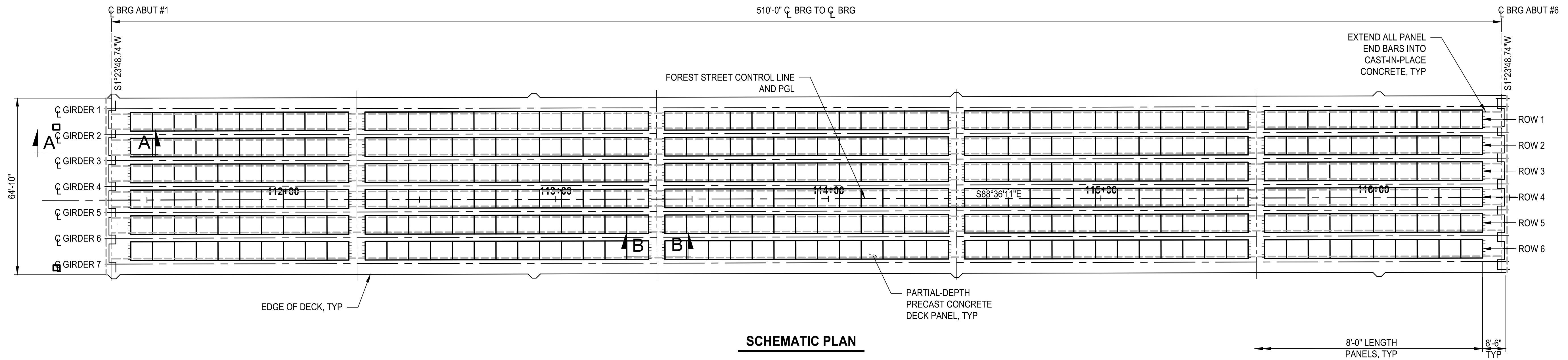
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

PARTIAL-DEPTH PRECAST PANEL DETAILS
1 OF 2

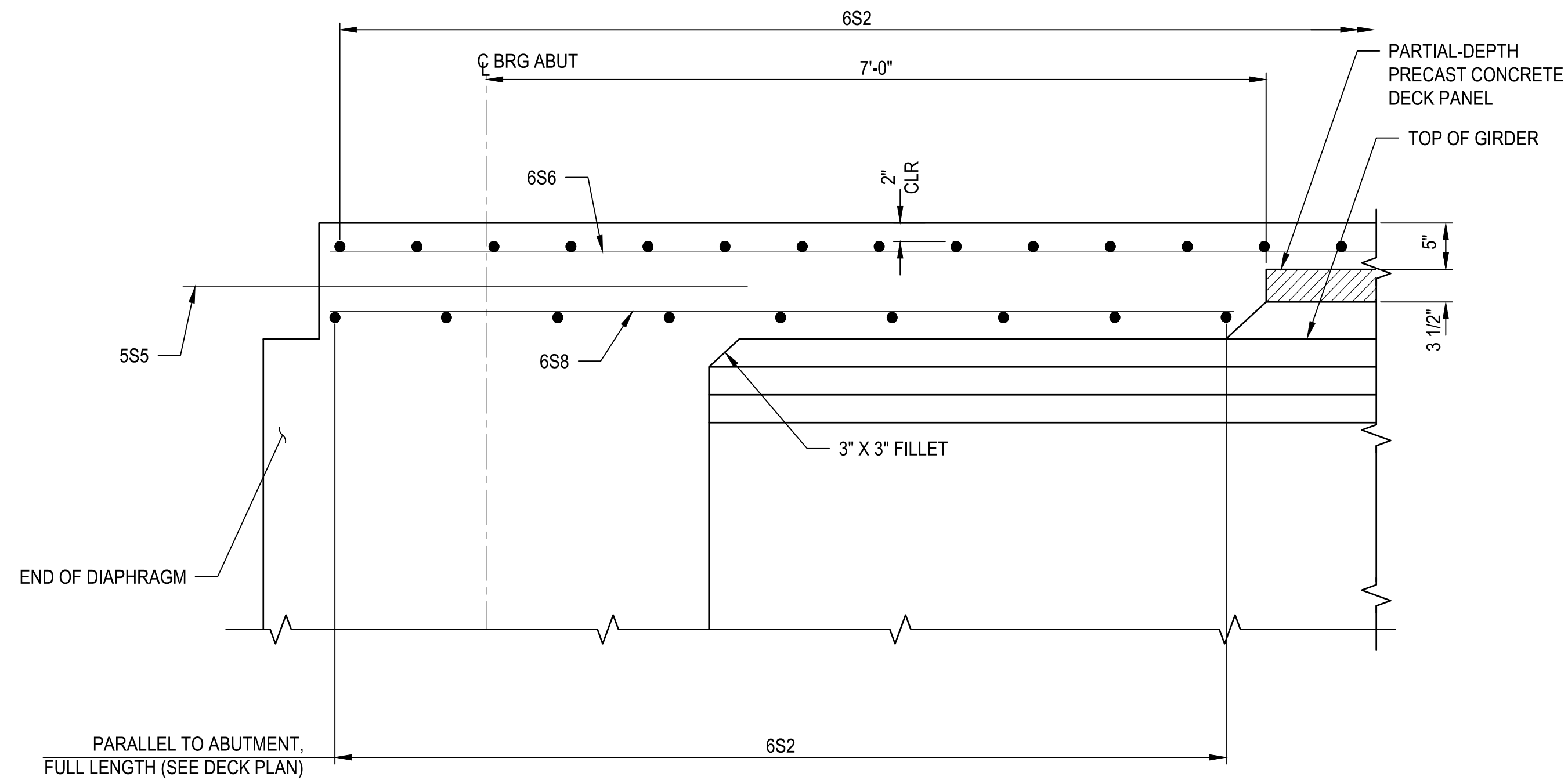
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 35 OF 59

S35

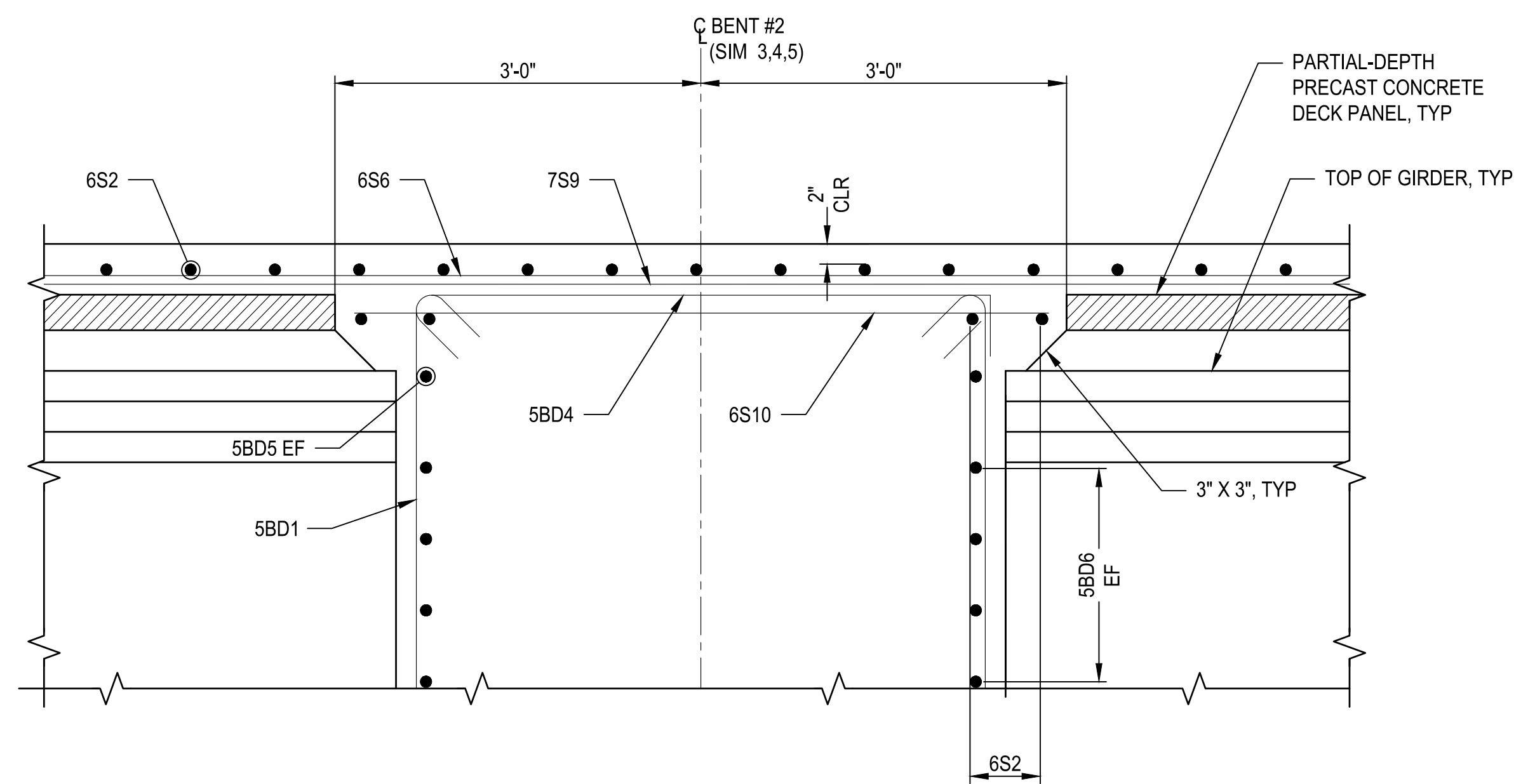
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SCHEMATIC PLAN



SECTION A-A



SECTION B-B

NOTES

- SEE "PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL 1 OF 2" FOR ADDITIONAL DETAILS.

| PRECAST CONCRETE DECK PANEL SCHEDULE | | |
|--------------------------------------|-----------------|------------|
| ROW | INTERIOR PANELS | END PANELS |
| 1 THRU 6 | 49 | 10 |
| TOTAL | 294 | 60 |

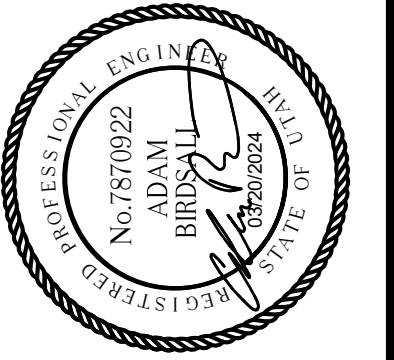
| ITEM | LOCATION | EST QTY | UNIT |
|---|----------|---------|------|
| PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL | DECK | 20,178 | SF |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB

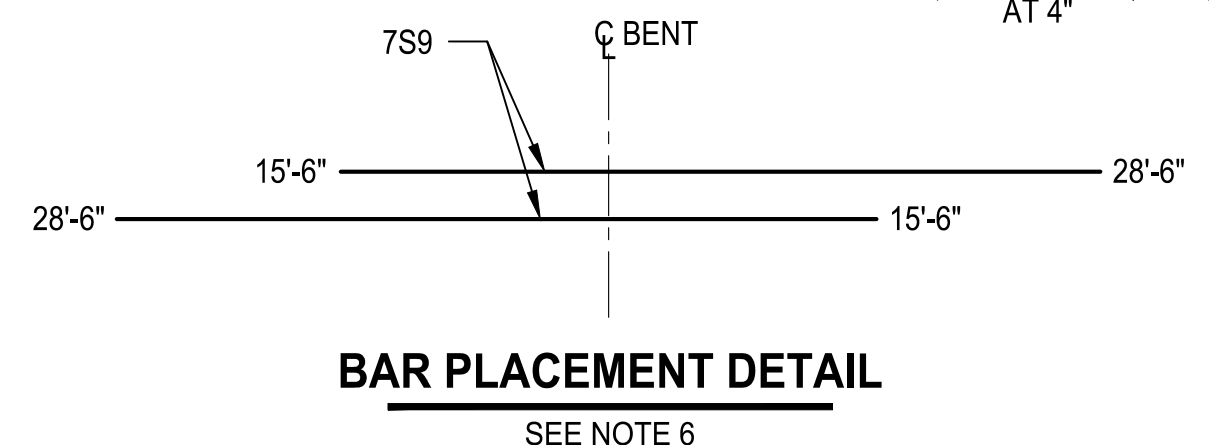
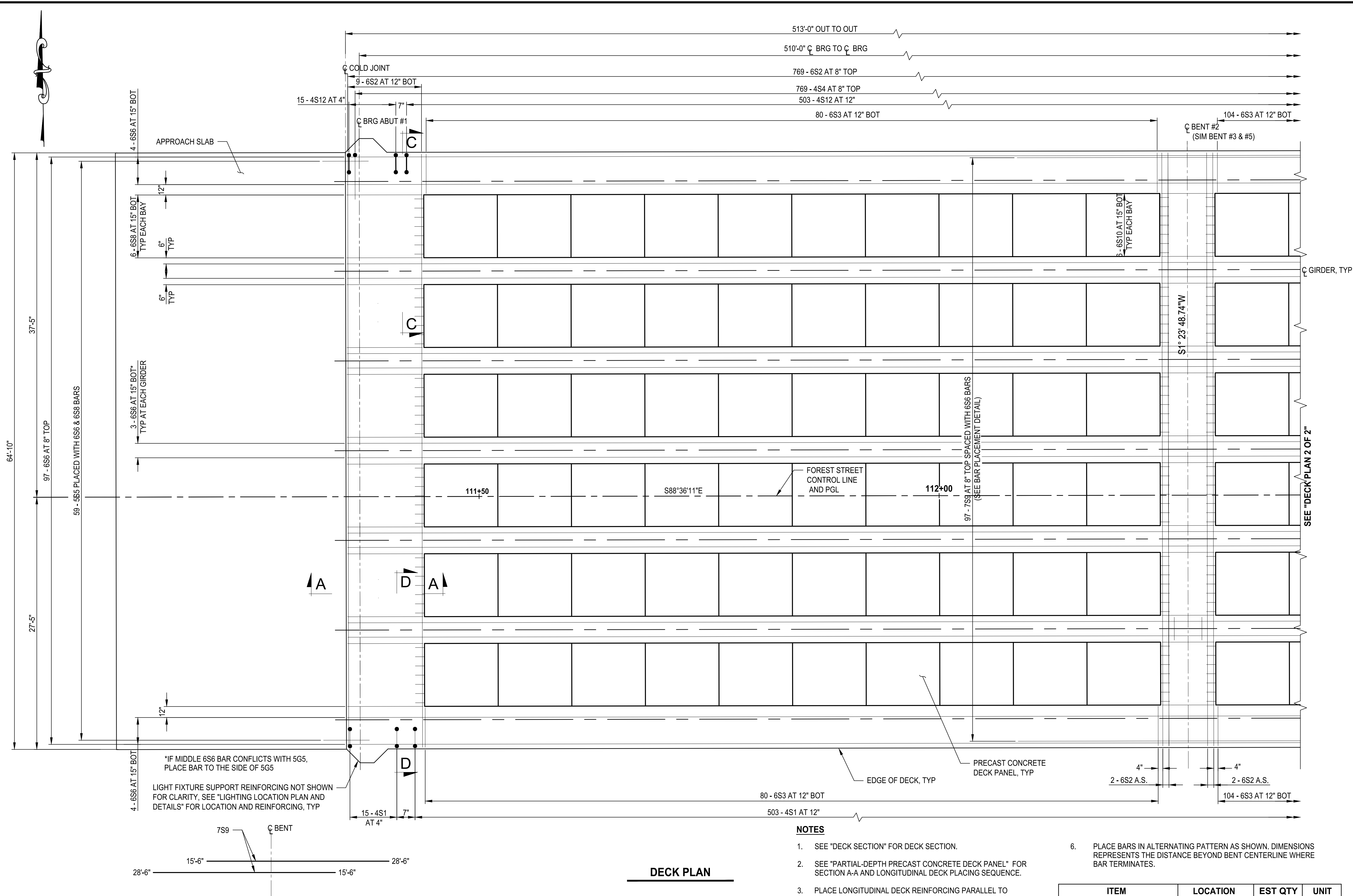


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL 2 OF 2

DRAWING NO.
 36 OF 59
S36

PATH: U:\Snl\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:49:10 AM LAYOUT: DP 1



DECK PLAN

NOTES

- SEE "DECK SECTION" FOR DECK SECTION.
- SEE "PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL" FOR SECTION A-A AND LONGITUDINAL DECK PLACING SEQUENCE.
- PLACE LONGITUDINAL DECK REINFORCING PARALLEL TO GIRDERS UNLESS NOTED OTHERWISE.
- SEE "DECK SECTIONS" FOR SECTION C-C AND SECTION D-D.
- USE ASTM A1035-CS/AASHTO M334 FOR ALL BARS IN TOP MAT OF DECK BARS (S2, S6, AND S9BARS). USE ASTM A1035-CM FOR ALL OTHER BARS IN DECK, PARAPETS, AND LIGHT PEDESTALS.
- PLACE BARS IN ALTERNATING PATTERN AS SHOWN. DIMENSIONS REPRESENTS THE DISTANCE BEYOND BENT CENTERLINE WHERE BAR TERMINATES.

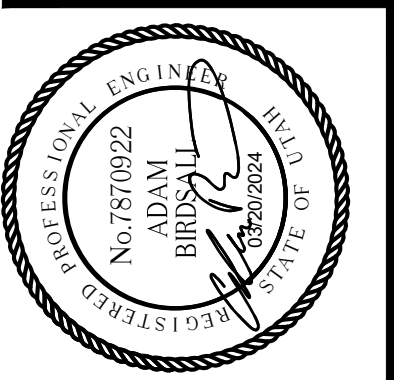
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|-----------------------------|---------------------------------|---------|-------|
| STRUCTURAL CONCRETE - FIBER | DECK, HAUNCH AND END DIAPHRAGMS | 864 | CU YD |
| STRUCTURAL CONCRETE - FIBER | SIDEWALK | 111 | CU YD |

| REVISIONS | DATE | BY | QUANTITIES |
|-----------|-------|-----|------------|
| 1 | 03/24 | AUB | |

ONE INCH AT FULL SCALE IF NOTED OTHERWISE ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB

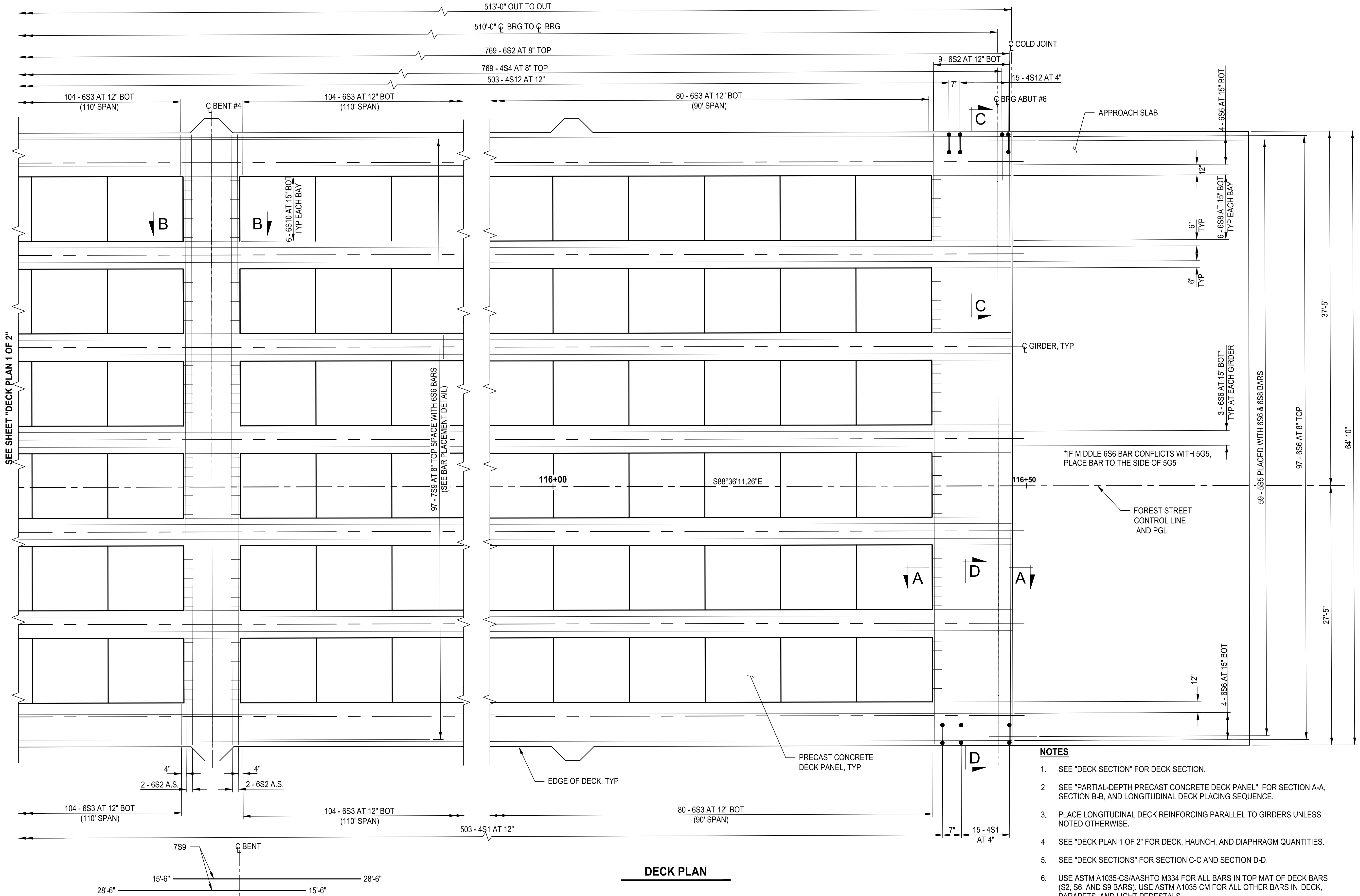


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DECK PLAN 1 OF 2

DRAWING NO. 37 OF 59
S37

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BAR PLACEMENT DETAIL
SEE NOTE 7

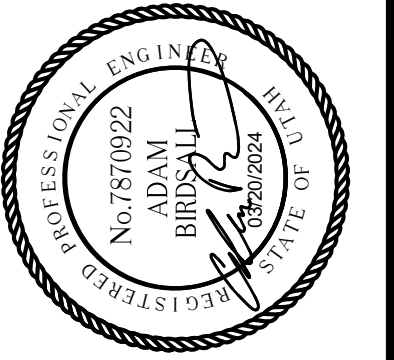
DECK PLAN

- NOTES**
1. SEE "DECK SECTION" FOR DECK SECTION.
 2. SEE "PARTIAL-DEPTH PRECAST CONCRETE DECK PANEL" FOR SECTION A-A, SECTION B-B, AND LONGITUDINAL DECK PLACING SEQUENCE.
 3. PLACE LONGITUDINAL DECK REINFORCING PARALLEL TO GIRDERS UNLESS NOTED OTHERWISE.
 4. SEE "DECK PLAN 1 OF 2" FOR DECK, HAUNCH, AND DIAPHRAGM QUANTITIES.
 5. SEE "DECK SECTIONS" FOR SECTION C-C AND SECTION D-D.
 6. USE ASTM A1035-CS/AASHTO M334 FOR ALL BARS IN TOP MAT OF DECK BARS (S2, S6, AND S9 BARS). USE ASTM A1035-CM FOR ALL OTHER BARS IN DECK, PARAPETS, AND LIGHT PEDESTALS.
 7. PLACE BARS IN ALTERNATING PATTERN AS SHOWN. DIMENSIONS REPRESENTS THE DISTANCE BEYOND BENT CENTERLINE WHERE BAR TERMINATES.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |
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ONE INCH AT FULL SCALE IF NOTED ACCORDINGLY

| | | |
|-------------------|--------------|-----|
| Parametrix | CHECKED | NCC |
| | APPROVED | AJB |
| DATE | DESIGNED | TWP |
| 03/20/2024 | DRAWN | SLO |
| JOB No. | 344-8541-002 | |



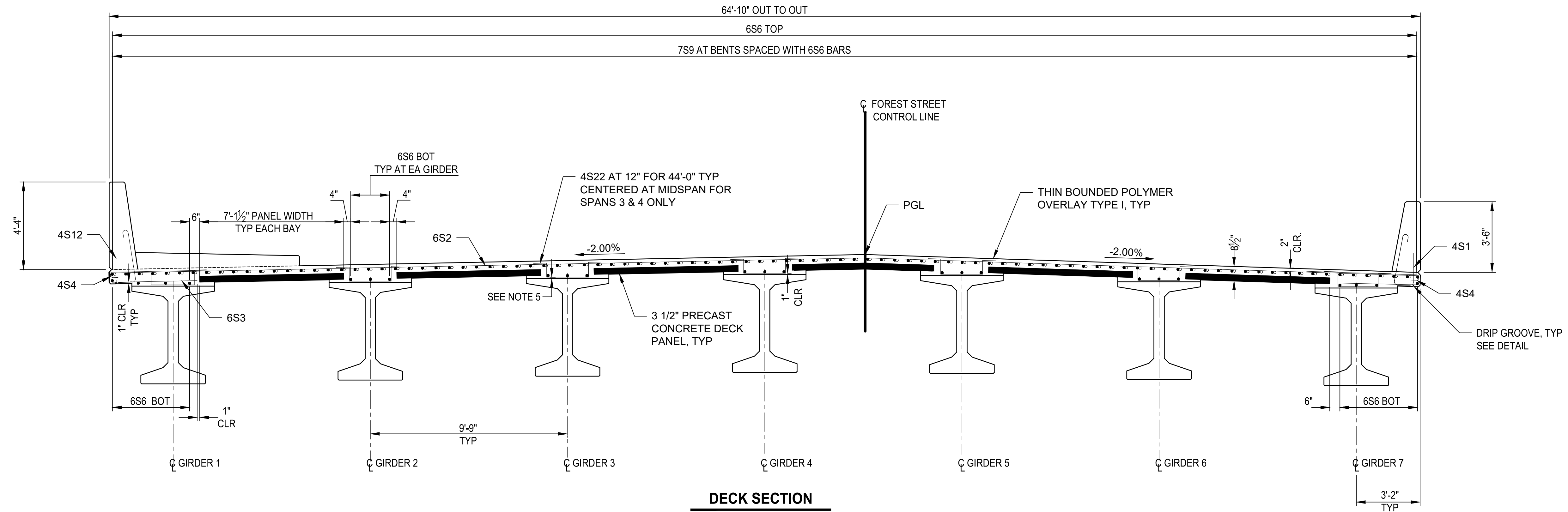
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DECK PLAN 2 OF 2

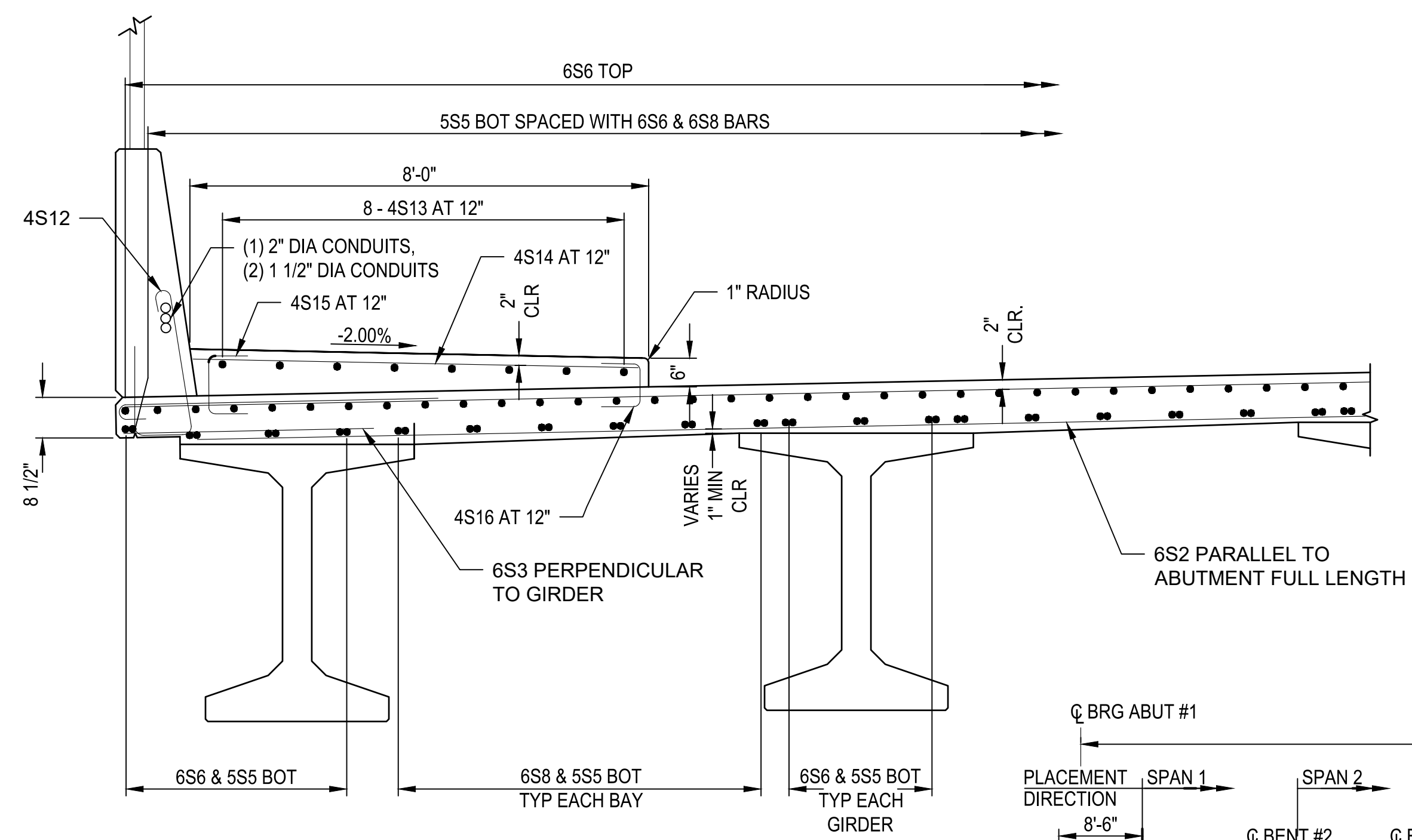
DRAWING NO.
38 OF 59

S38

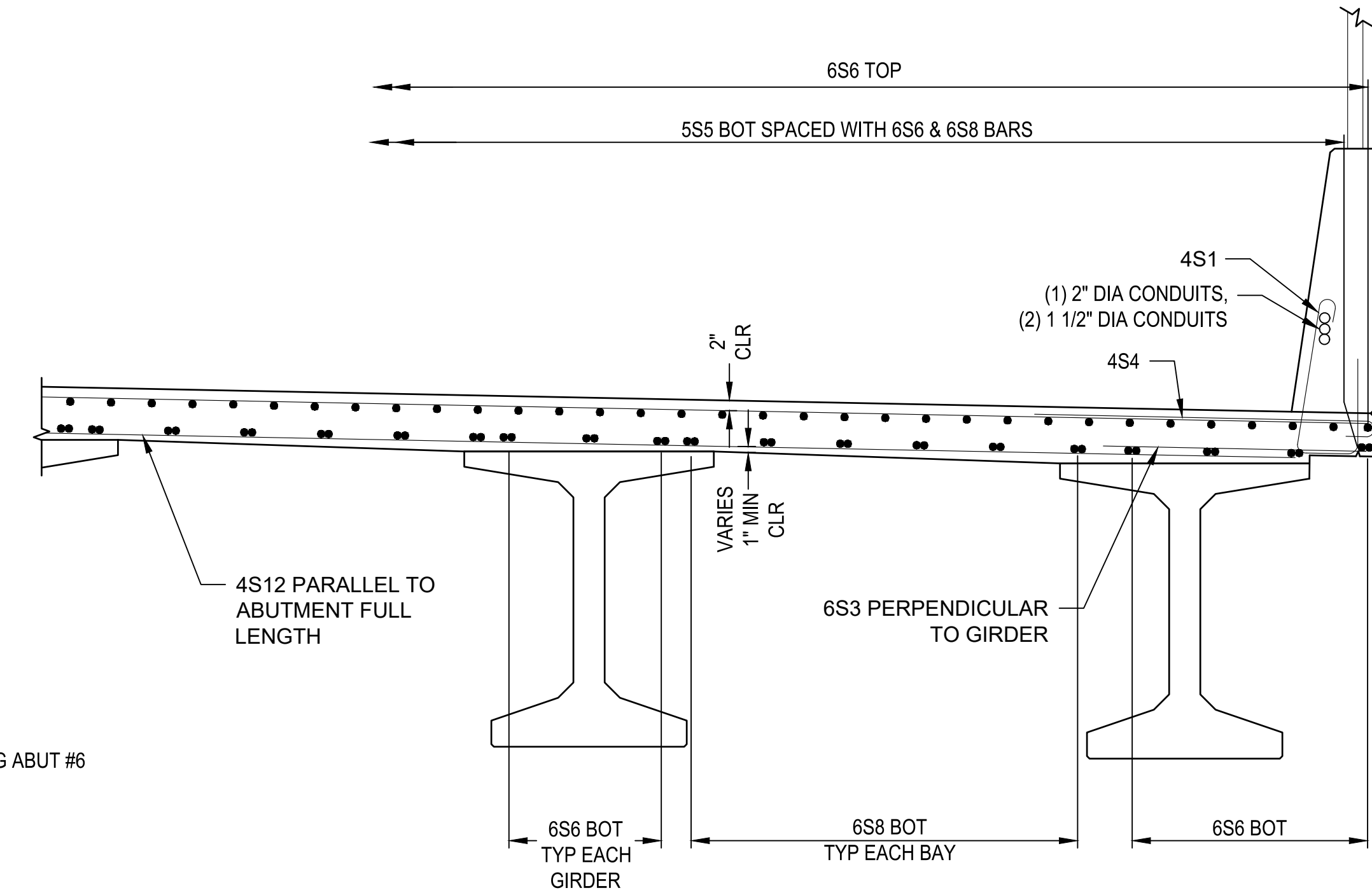
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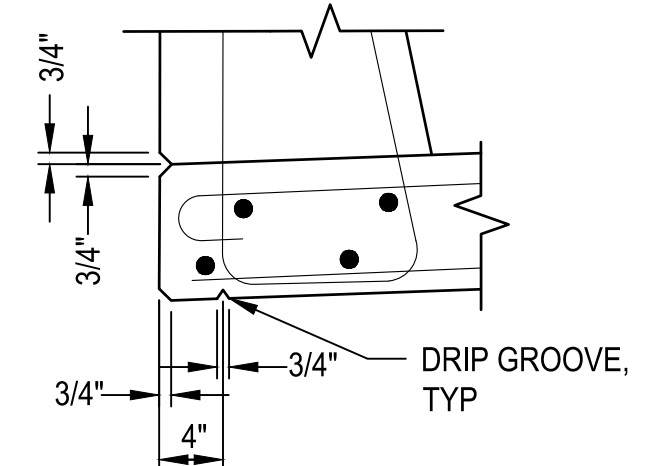
DECK SECTION



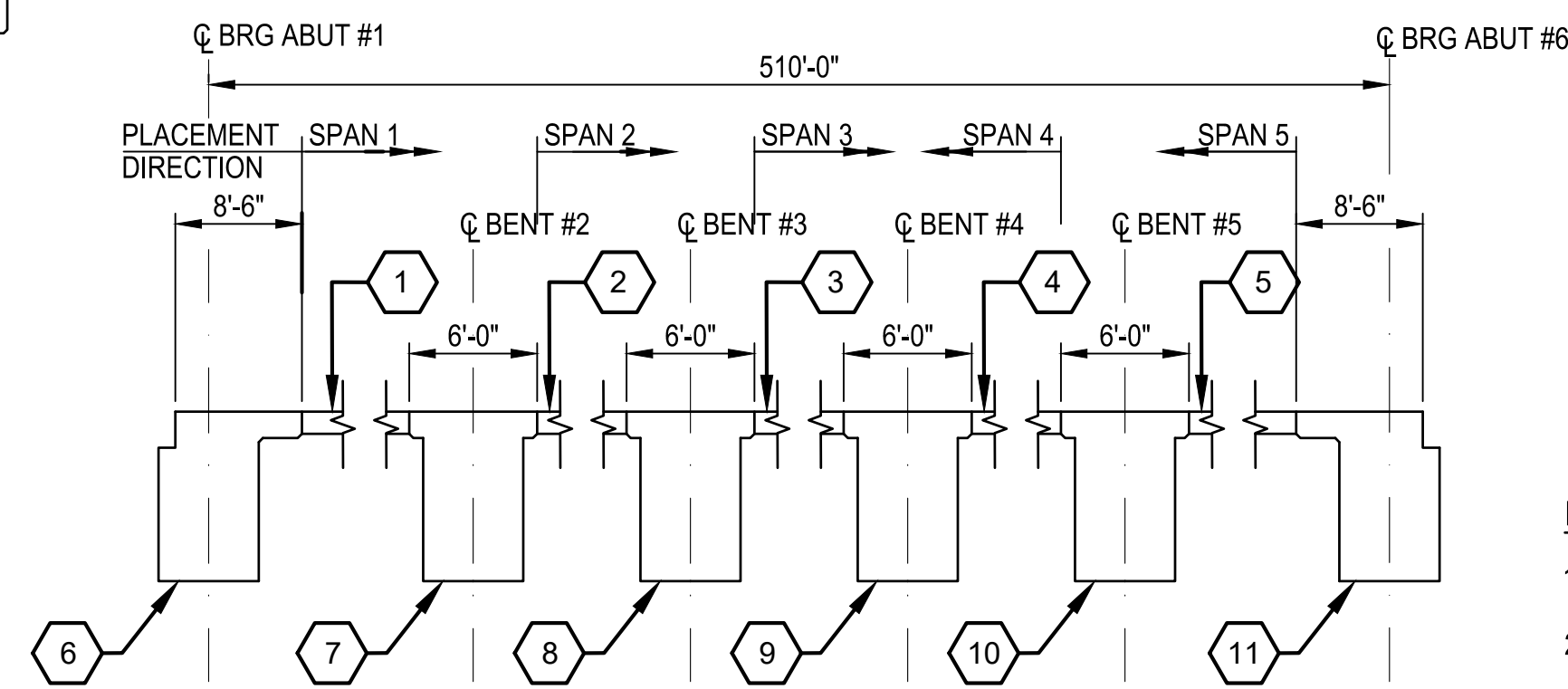
SECTION C-C



SECTION D-D



DRIP GROOVE DETAIL



LONGITUDINAL DECK PLACING SEQUENCE

- NOTES**
- ② DESIGNATES PLACING SEQUENCE.
 - ARROWS DESIGNATE DIRECTION OF PLACEMENT.
 - SEQUENCES ① THRU ⑪ MAY BE COMBINED AT THE CONTRACTOR'S DISCRETION.
 - IF SEQUENCES ⑥ THRU ⑪ ARE NOT COMBINED WITH ① THRU ⑤, DO NOT PLACE ⑥ THRU ⑪ UNTIL DECK SLAB ① THRU ⑤ HAVE CURED FOR 3 DAYS.

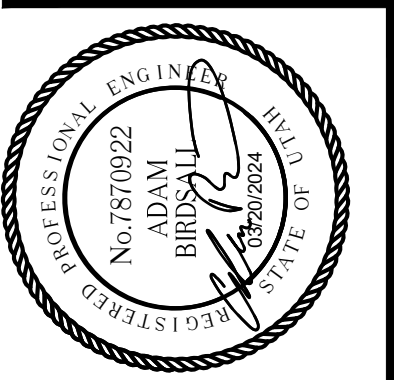
- NOTES**
- SEE "PRECAST CONCRETE DECK PANEL-HALF DEPTH" FOR PRECAST CONCRETE DECK PANEL DETAILS.
 - SEE "DECK AND ABUTMENT DIAPHRAGM DETAILS" FOR SIDEWALK REINFORCEMENT.
 - SEE "DECK PLAN 1 OF 2" AND "DECK PLAN 2 OF 2" FOR LOCATION OF SECTIONS C-C AND D-D.
 - SEE "42-INCH SINGLE SLOPE PEDESTRIAN PARAPET W/SIDEWALK AND 42-INCH SINGLE SLOPE PEDESTRIAN PARAPET" FOR PARAPET REINFORCEMENT.
 - VARY CLEAR DISTANCE BETWEEN TOP OF GIRDER AND 4S22 BAR AS NEEDED TO AVOID REINFORCING CONFLICTS.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY

Parametrix

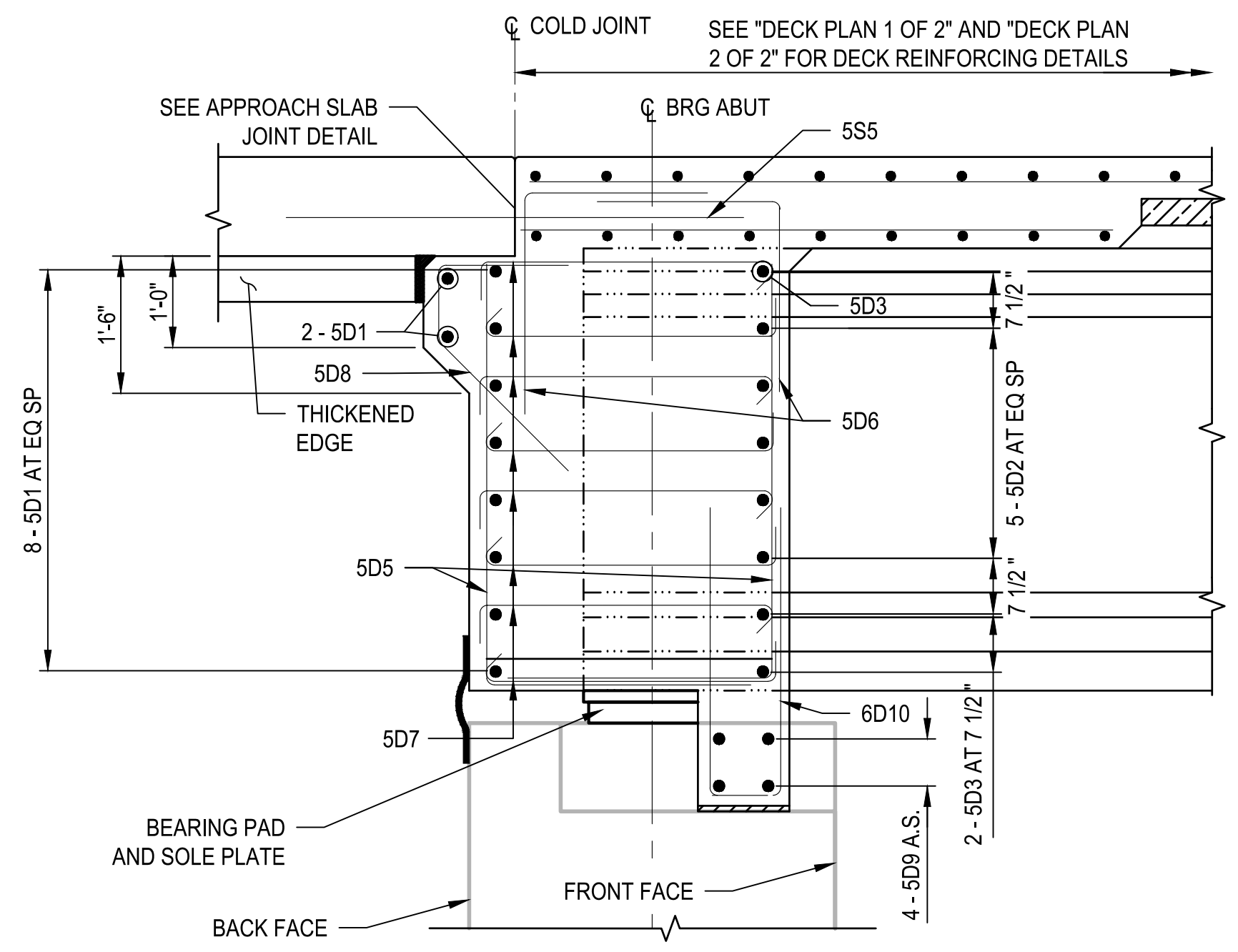
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 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
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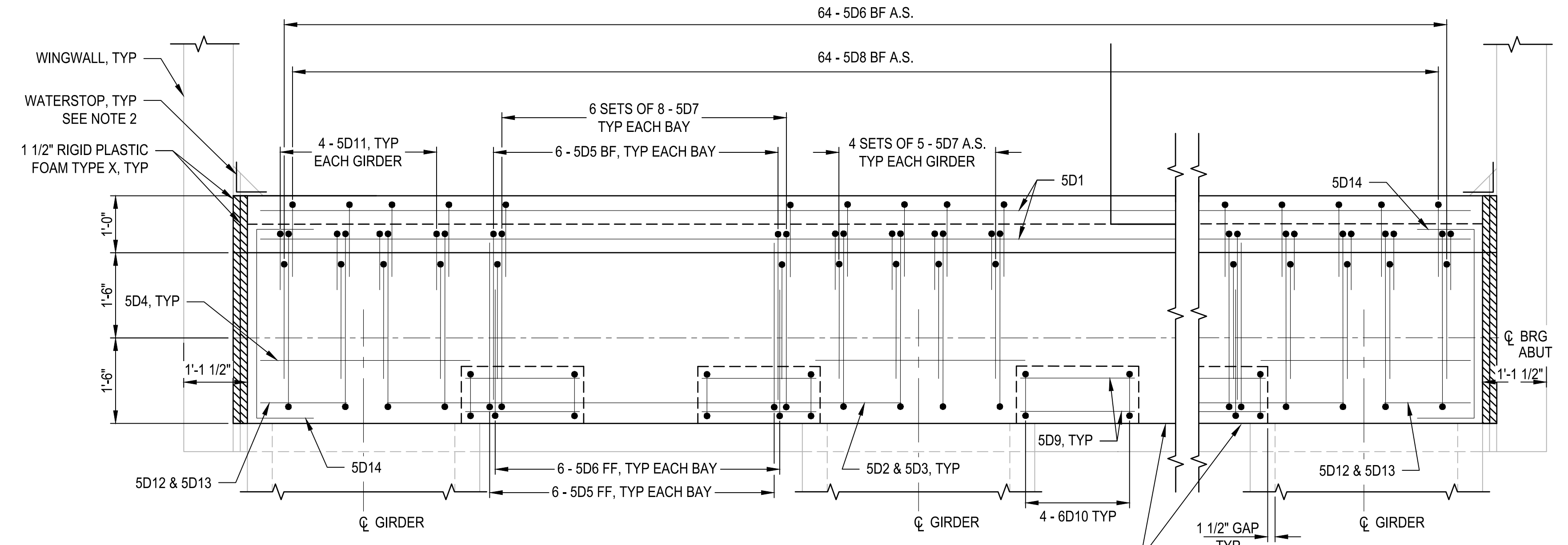
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DECK SECTIONS

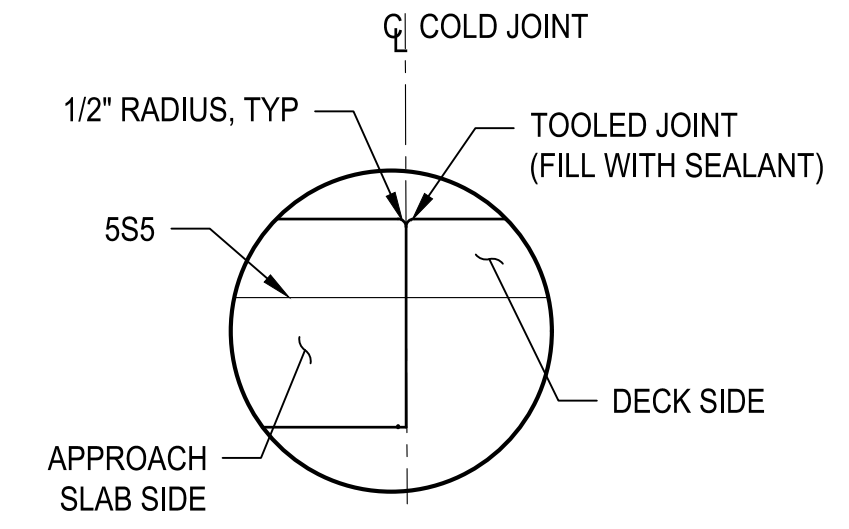
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 PLOTTED BY: OlweSto DATE: Friday, March 22, 2024 9:50:47 AM
 LAYOUT: abut_det



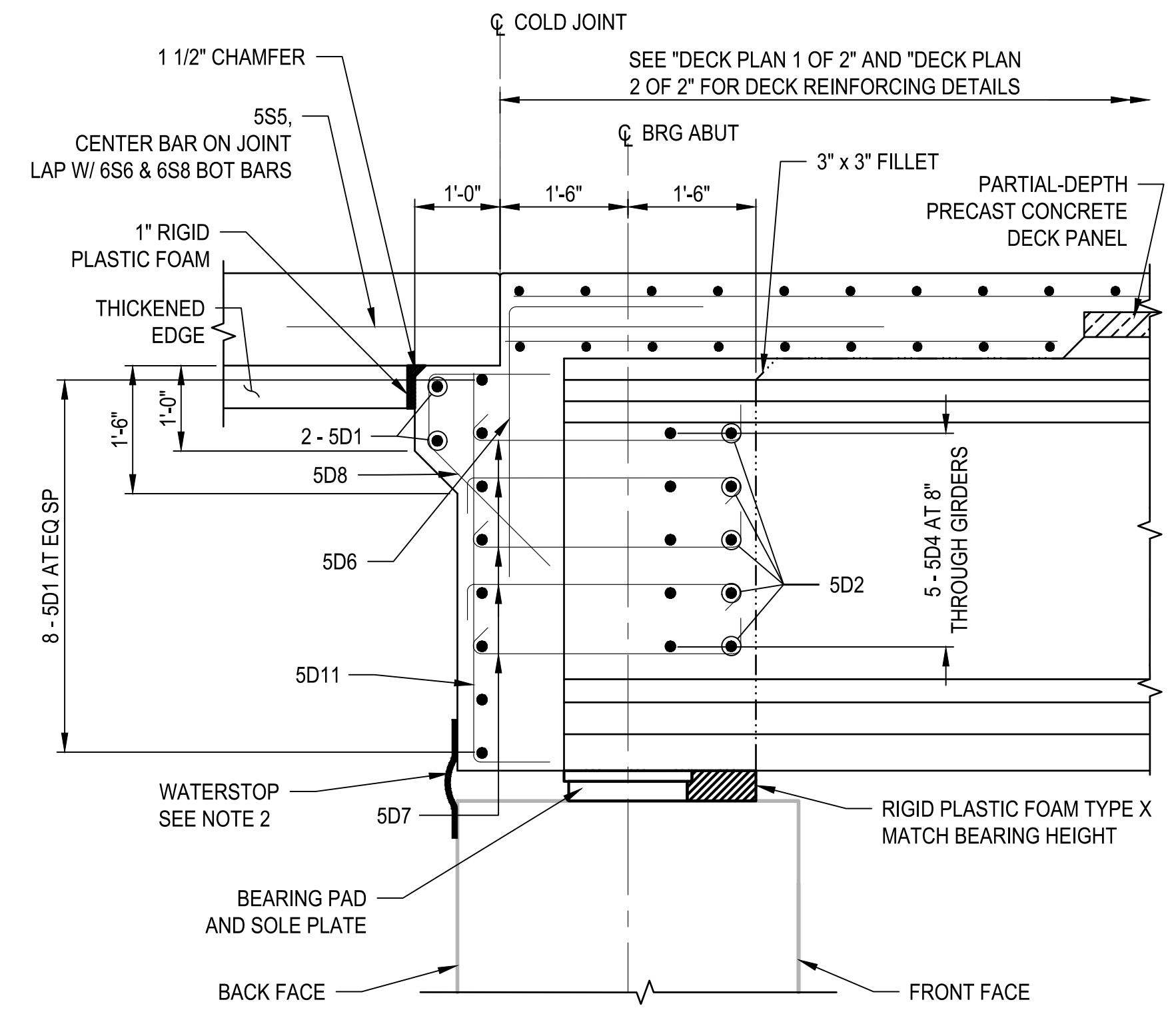
SECTION A-A



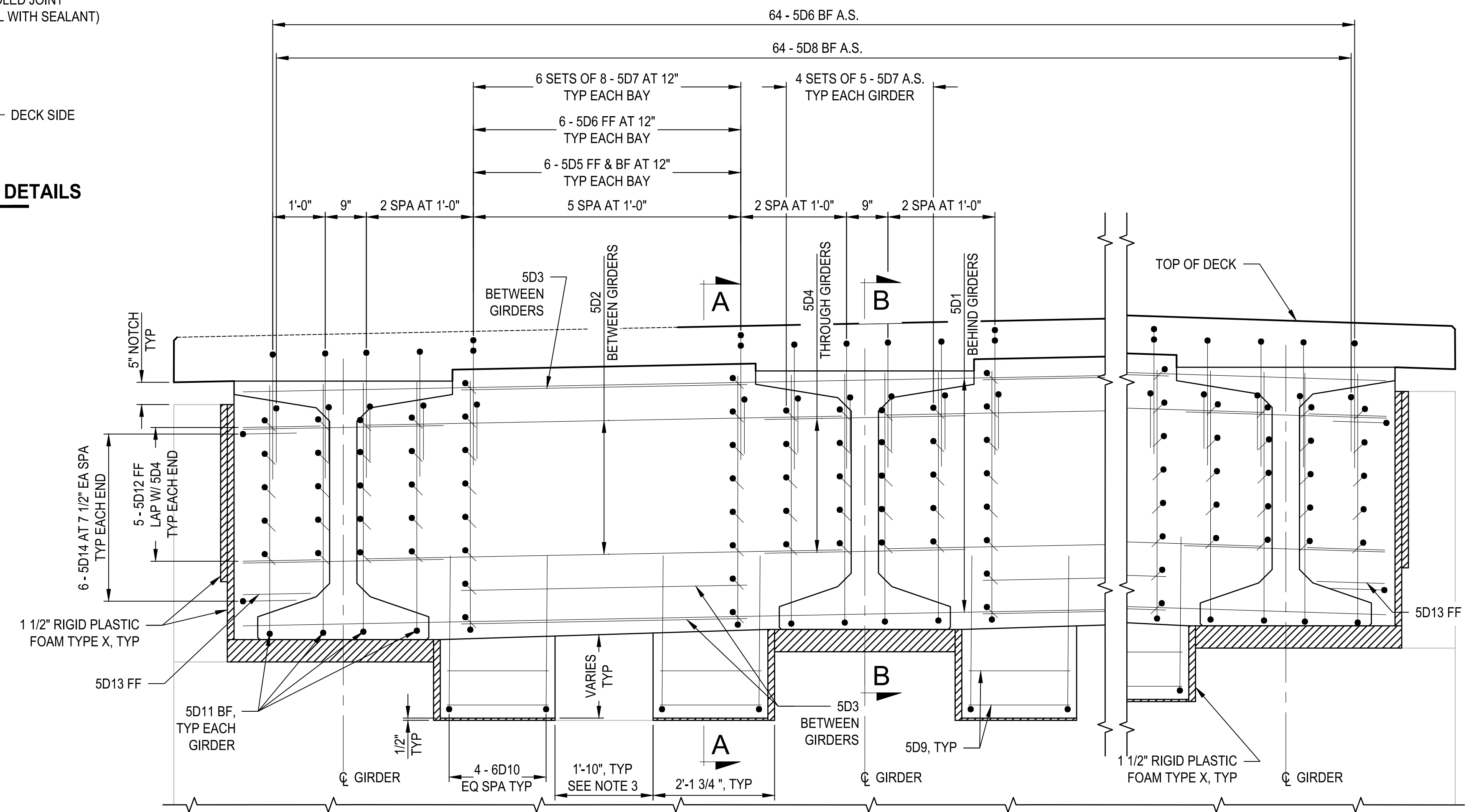
PLAN



APPROACH SLAB JOINT DETAILS



SECTION B-B



ELEVATION

NOTES

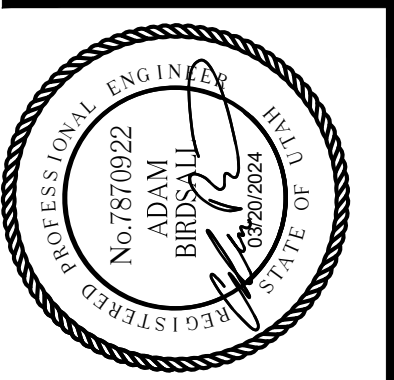
1. SEE "DECK PLAN 1 OF 2" AND "DECK PLAN 2 OF 2" FOR LOCATION OF A-A.
2. SEE "ABUTMENT DETAILS" FOR WATERSTOP DETAILS.
3. JACKING POCKET FOR FUTURE BEARING REPLACEMENT.

| ITEM | LOCATION | EST QTY | UNIT |
|------------------|---------------------|---------|-------|
| CONCRETE COATING | ABUTMENT DIAPHRAGMS | 738 | SQ FT |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED

| | | |
|-----------------------|---------------|---------------|
| DATE: 03/20/2024 | DESIGNED: TWP | CHECKED: NICC |
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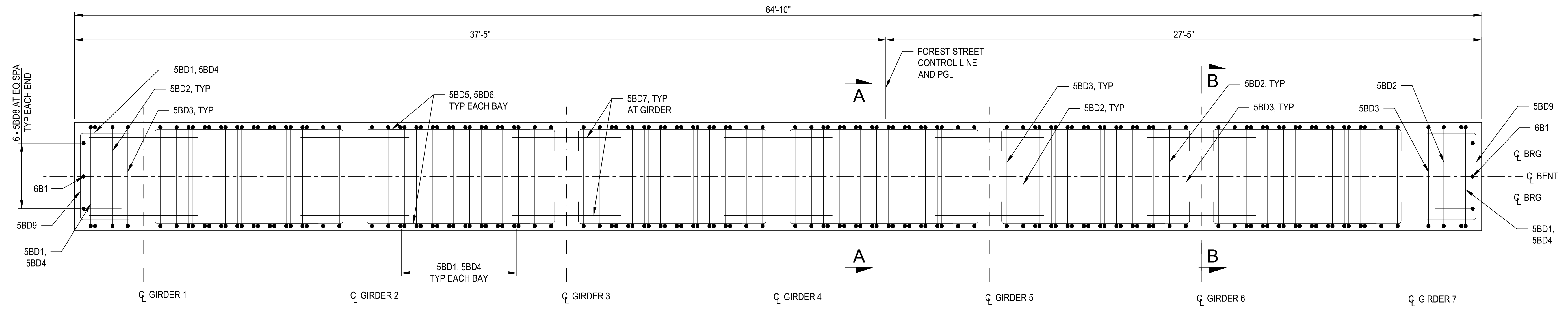
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

DECK AND ABUTMENT DIAPHRAGM DETAILS

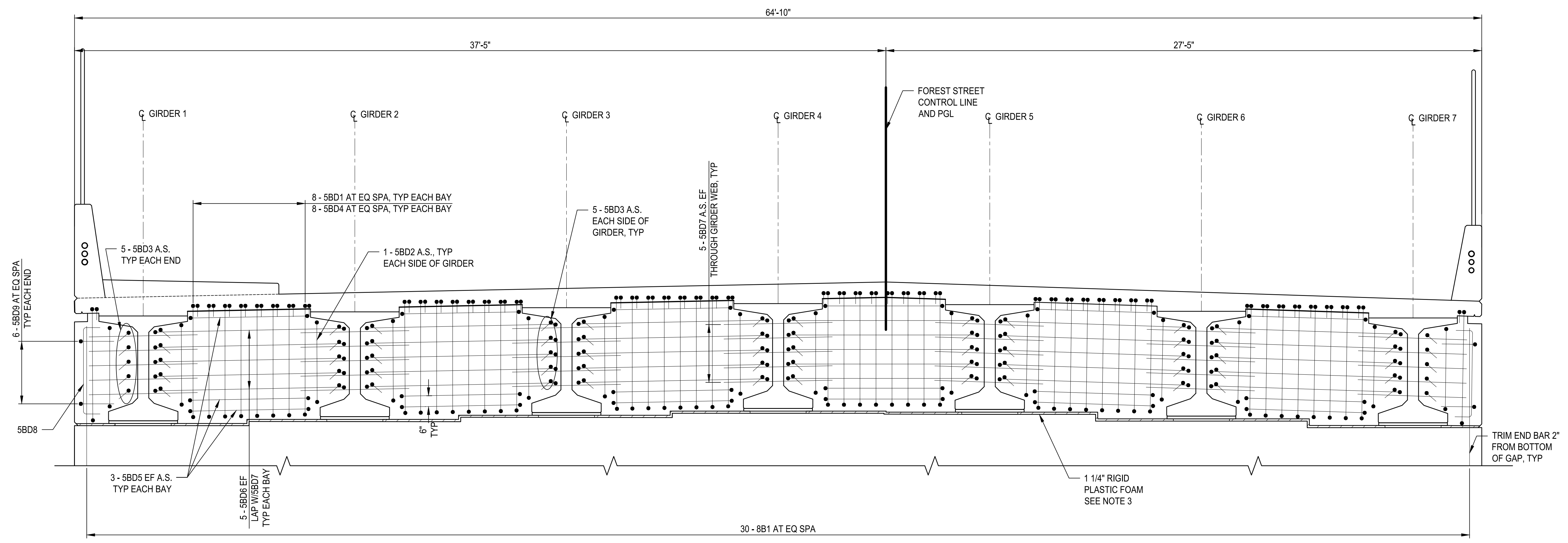
DRAWING NO.
 40 OF 59

S40

LAYOUT: Bent Det PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995scs\CADD\DWG\Structure PLOTTED BY: OrlwaSta DATE: Friday, March 22, 2024 9:51:05 AM



BENT DIAPHRAGM PLAN
PARAPET, SIDEWALK AND DECK NOT SHOWN FOR CLARITY



BENT DIAPHRAGM ELEVATION

NOTES

- SEE "BENT DIAPHRAGM DETAILS 2 OF 2" FOR SECTIONS A-A, B-B AND DETAIL A.
- STEEL REINFORCING IN DECK, SIDEWALK, AND PARAPET NOT SHOWN FOR CLARITY.
- PLACE RIGID PLASTIC FOAM BETWEEN BENT CAP AND BENT DIAPHRAGM. SEE "BENT DIAPHRAGM DETAILS 2 OF 2" FOR EXTENT.
- CONTINUE JOINTS THROUGH SIDEWALK WHERE THEY OCCUR IN THE PARAPET.

| ITEM | LOCATION | EST QTY | UNIT |
|------|-----------------------------|---------|-------|
| 1 | STRUCTURAL CONCRETE - FIBER | 228 | CU YD |

| REVISIONS | DATE | BY |
|-----------|-------|-----|
| 1 | 03/24 | AUB |

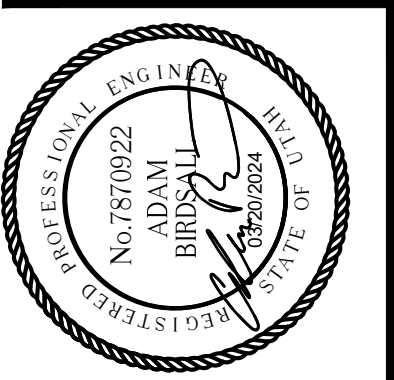
ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED

Parametrix

DATE: 03/20/2024
JOB No.: 344-8541-002

DESIGNED: TWP
DRAWN: SLO

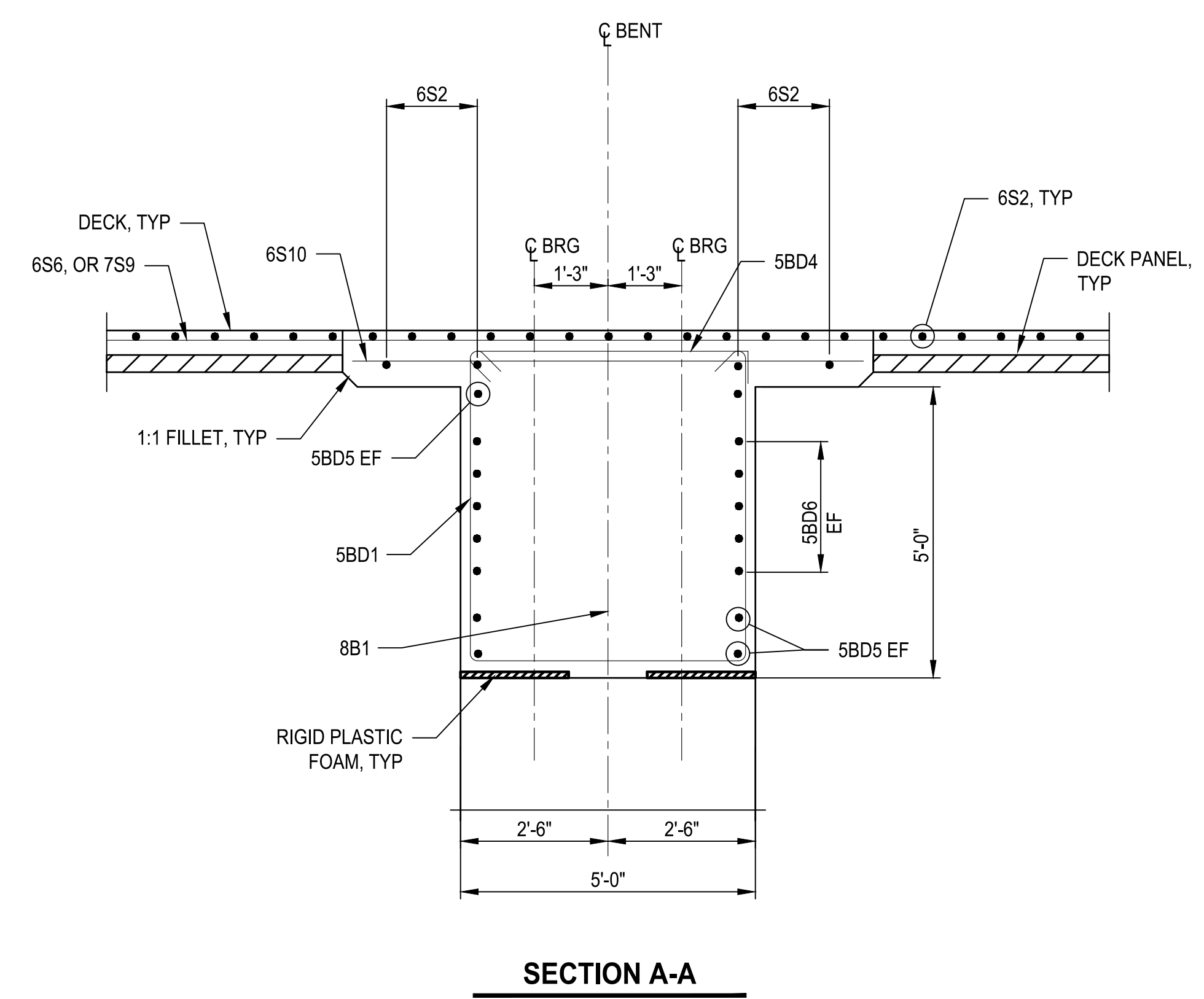
CHECKED: EA
APPROVED: AUB



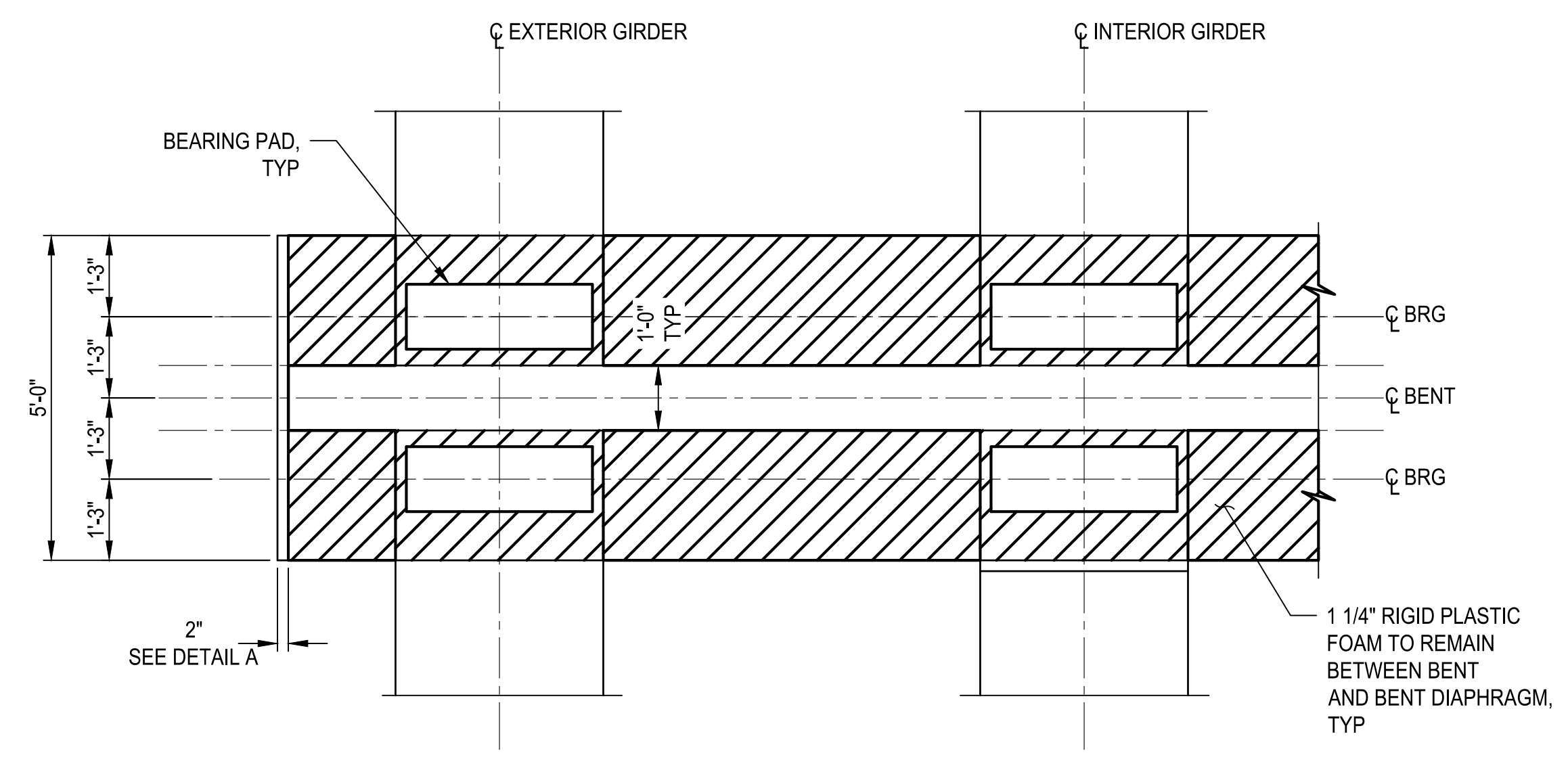
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

BENT DIAPHRAGM DETAILS 1 OF 2

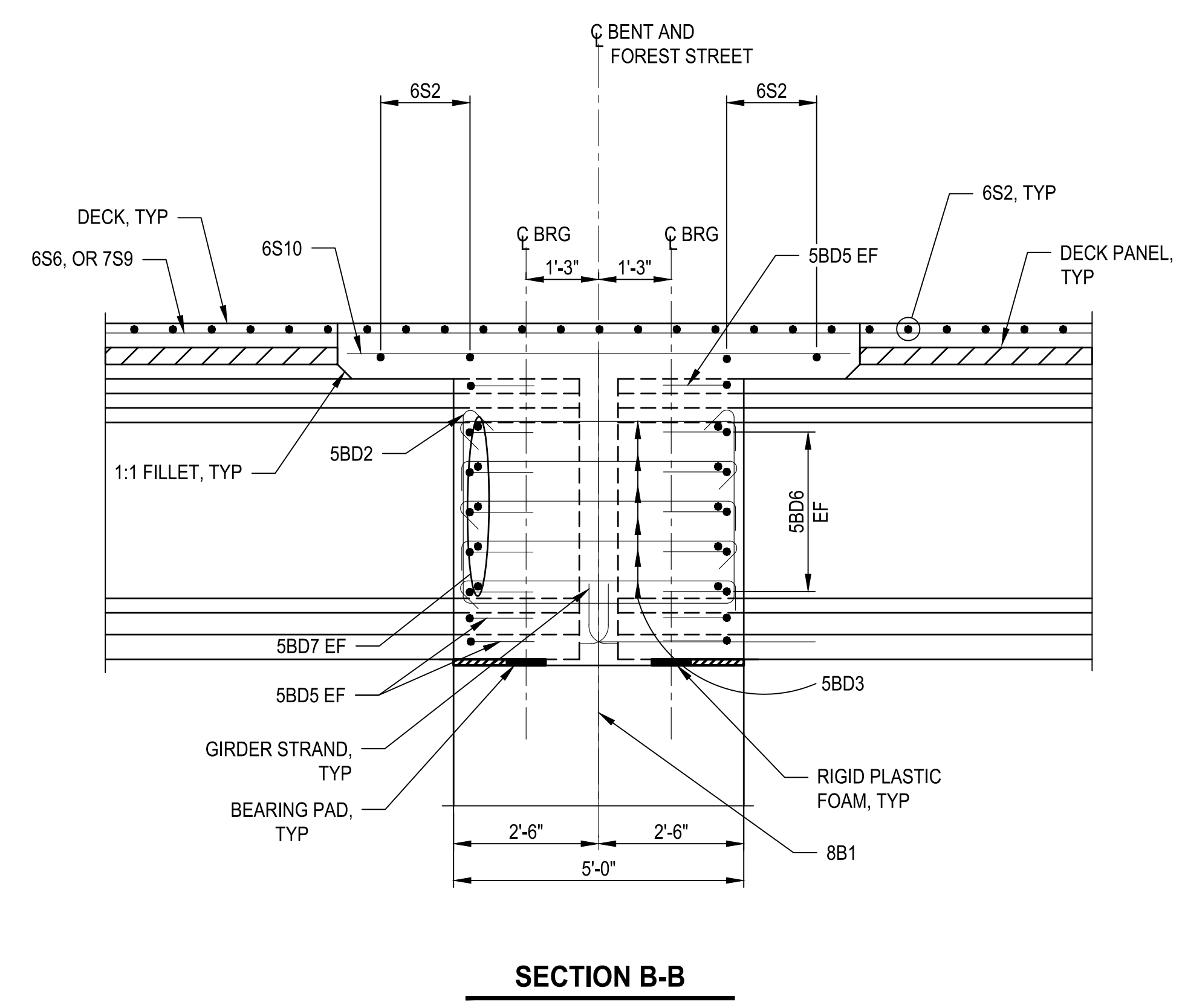
LAYOUT: bent.dwg 2 of 2
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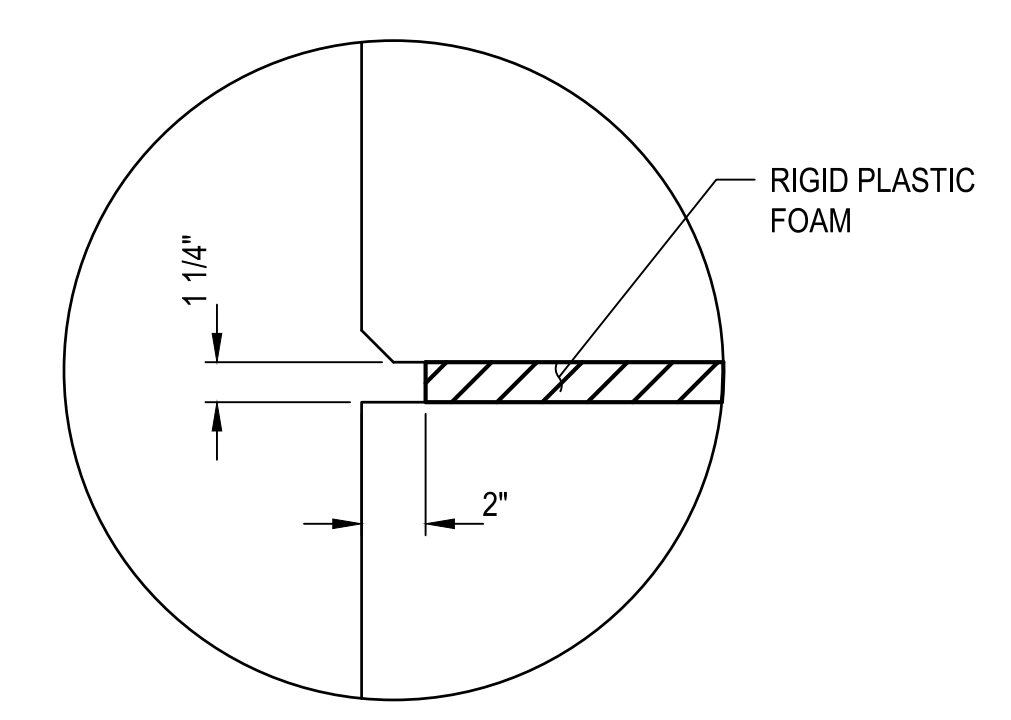
SECTION A-A



TYPICAL PLAN VIEW BETWEEN GIRDERS
 GIRDER STRAND NOT SHOWN



SECTION B-B



DETAIL A

- NOTES**
- SEE "BENT DIAPHRAGM DETAILS 1 OF 2" FOR LOCATIONS OF SECTIONS A-A, B-B, AND DETAIL A.

| REVISIONS | DATE | BY |
|-----------|------|----|
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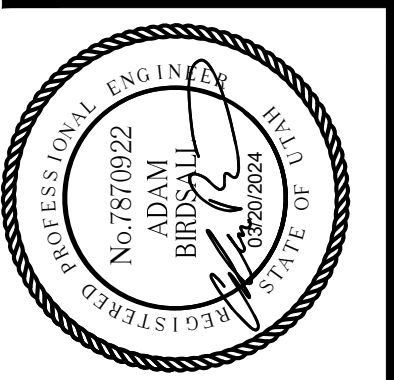
ONE INCH AT FULL SCALE IF NOT SHOWN OTHERWISE

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: TWP
 DRAWN: SLO

CHECKED: EA
 APPROVED: AUB



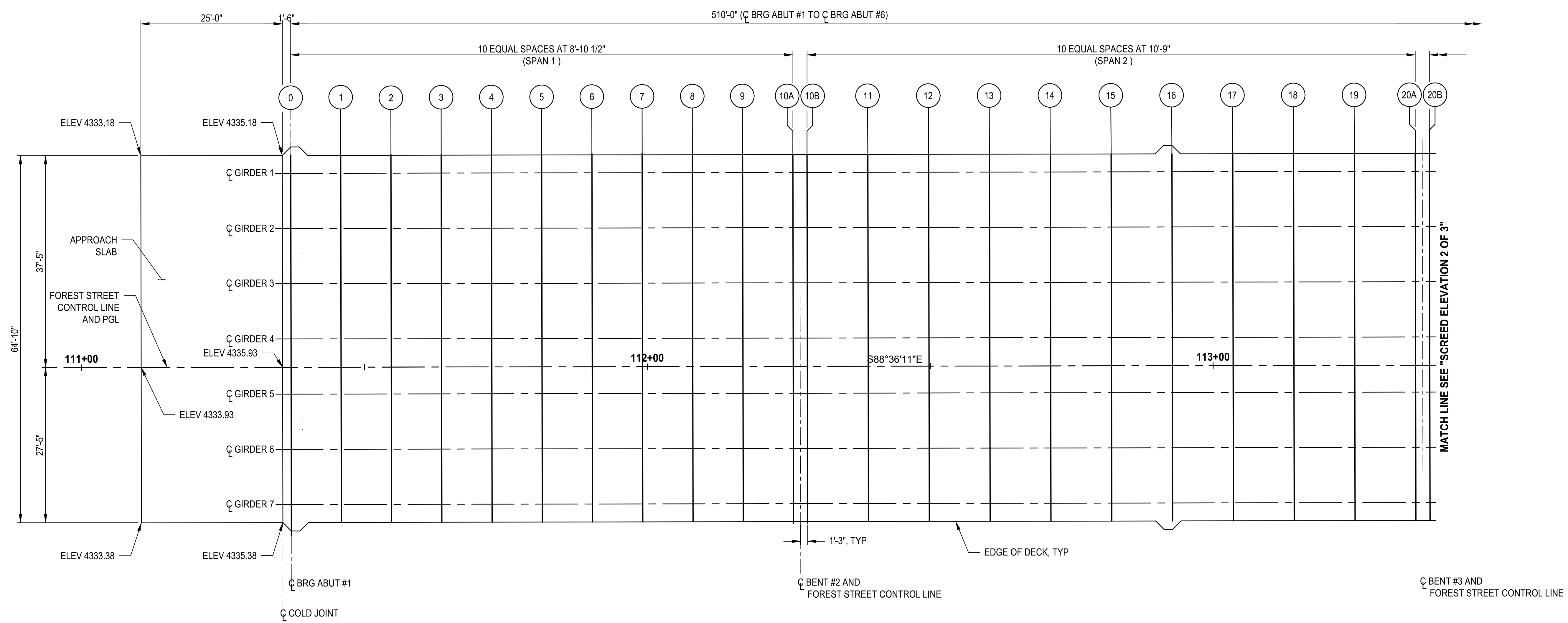
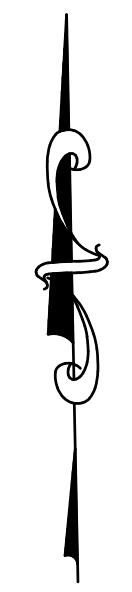
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

BENT DIAPHRAGM DETAILS 2 OF 2

DRAWING NO.
 42 OF 59

S42

PATH: U:\Sart\Projects\Clients\8541-Brigham City\344-8541-002_Forest St Final Design\99Sves\CADD\DWG\Structure PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:51:43 AM LAYOUT: screed 1



SCREED ELEVATIONS LOCATION DIAGRAM

| | | SCREED ELEVATION TABLE | | | | | | | | | | | | | | | | | | | | | |
|----------|---------|------------------------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| | | SPAN 1 | | | | | | | | | | SPAN 2 | | | | | | | | | | | |
| | | 0 | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10A | 10B | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | 19 | 20A |
| GIRDER 1 | ELEV | 4335.36 | 4336.07 | 4336.78 | 4337.49 | 4338.20 | 4338.91 | 4339.60 | 4340.27 | 4340.92 | 4341.54 | 4342.15 | 4342.31 | 4343.00 | 4343.66 | 4344.29 | 4344.89 | 4345.45 | 4345.98 | 4346.47 | 4346.94 | 4347.37 | 4347.77 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.05 | 0.06 | 0.07 | 0.06 | 0.05 | 0.04 | 0.02 | 0.00 | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 |
| GIRDER 2 | ELEV | 4335.56 | 4336.27 | 4336.98 | 4337.69 | 4338.40 | 4339.11 | 4339.80 | 4340.47 | 4341.11 | 4341.74 | 4342.34 | 4342.51 | 4343.20 | 4343.86 | 4344.49 | 4345.08 | 4345.64 | 4346.17 | 4346.67 | 4347.13 | 4347.57 | 4347.96 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 3 | ELEV | 4335.75 | 4336.46 | 4337.17 | 4337.88 | 4338.59 | 4339.30 | 4339.99 | 4340.66 | 4341.31 | 4341.93 | 4342.54 | 4342.70 | 4343.39 | 4344.05 | 4344.68 | 4345.28 | 4345.84 | 4346.37 | 4346.86 | 4347.33 | 4347.76 | 4348.16 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 4 | ELEV | 4335.95 | 4336.66 | 4337.37 | 4338.08 | 4338.79 | 4339.50 | 4340.19 | 4340.86 | 4341.50 | 4342.13 | 4342.73 | 4342.90 | 4343.59 | 4344.25 | 4344.88 | 4345.47 | 4346.03 | 4346.56 | 4347.06 | 4347.52 | 4347.96 | 4348.35 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| PGL | ELEV | 4336.05 | 4336.76 | 4337.47 | 4338.18 | 4338.89 | 4339.60 | 4340.29 | 4340.96 | 4341.60 | 4342.23 | 4342.83 | 4343.00 | 4343.69 | 4344.35 | 4344.98 | 4345.57 | 4346.13 | 4346.66 | 4347.16 | 4347.62 | 4348.06 | 4348.45 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 5 | ELEV | 4335.95 | 4336.66 | 4337.37 | 4338.08 | 4338.79 | 4339.50 | 4340.19 | 4340.86 | 4341.51 | 4342.13 | 4342.74 | 4342.90 | 4343.59 | 4344.25 | 4344.88 | 4345.48 | 4346.04 | 4346.57 | 4347.06 | 4347.53 | 4347.96 | 4348.36 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 6 | ELEV | 4335.76 | 4336.47 | 4337.18 | 4337.89 | 4338.60 | 4339.31 | 4340.00 | 4340.67 | 4341.31 | 4341.94 | 4342.54 | 4342.71 | 4343.40 | 4344.06 | 4344.69 | 4345.28 | 4345.84 | 4346.37 | 4346.87 | 4347.33 | 4347.77 | 4348.16 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.07 | 0.07 | 0.07 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 7 | ELEV | 4335.56 | 4336.27 | 4336.98 | 4337.69 | 4338.40 | 4339.11 | 4339.80 | 4340.47 | 4341.12 | 4341.74 | 4342.35 | 4342.51 | 4343.20 | 4343.86 | 4344.49 | 4345.09 | 4345.65 | 4346.18 | 4346.67 | 4347.14 | 4347.57 | 4347.97 |
| | DL DEFL | 0.00 | 0.02 | 0.04 | 0.06 | 0.06 | 0.07 | 0.06 | 0.05 | 0.04 | 0.02 | 0.00 | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 |

NOTES

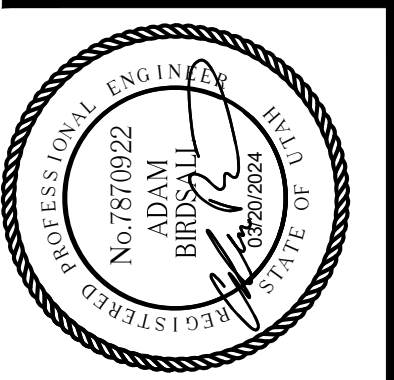
- FINAL TOP OF CONCRETE DECK ELEVATIONS ARE SHOWN. ADD DEAD LOAD DEFLECTIONS TO THE TOP OF CONCRETE DECK ELEVATIONS TO OBTAIN SCREED ELEVATIONS.
- ALL ELEVATIONS AND DEAD LOAD DEFLECTIONS VALUES ARE SHOWN IN FEET.
- SCREED ELEVATIONS ARE PROVIDED AT INTERSECTION OF CENTERLINE OF GIRDER AND SCREED LINE.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



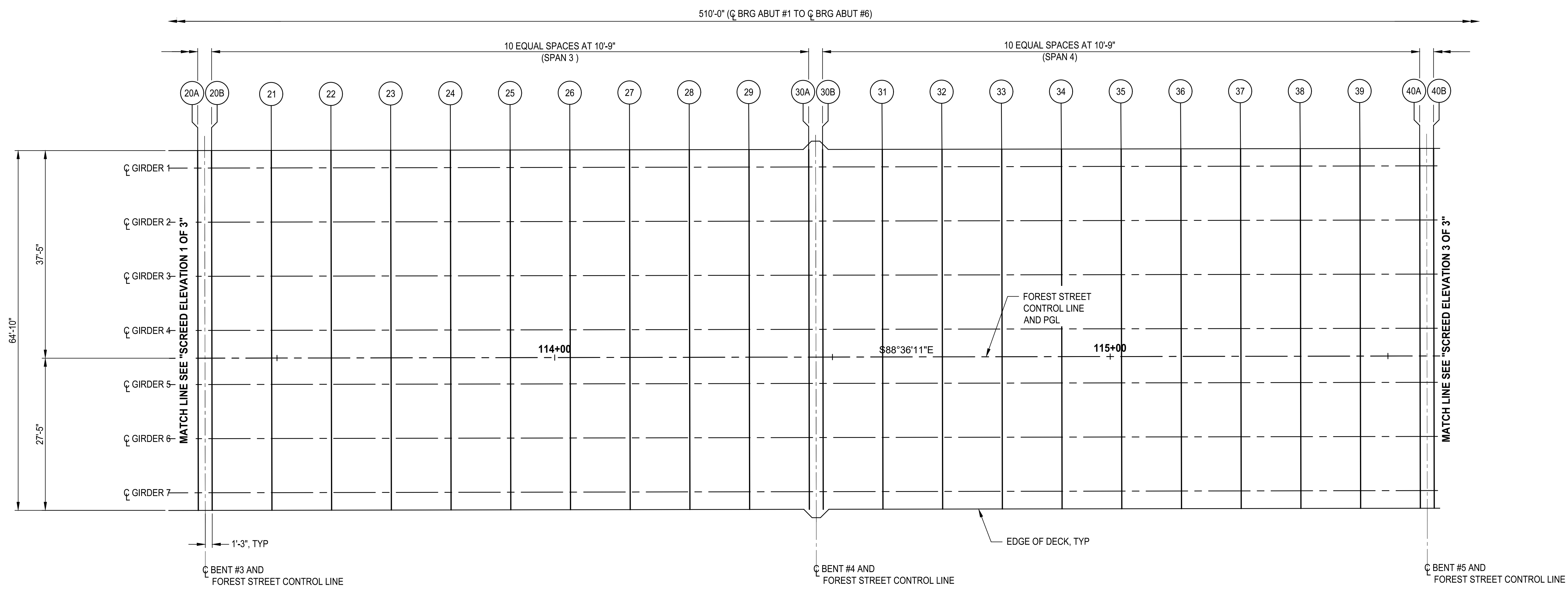
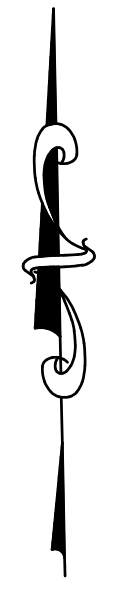
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SCREED ELEVATION 1 OF 3

DRAWING NO.
 43 OF 59

S43

LAYOUT: Screenshot 2
 PATH: U:\Sait\Projects\Clients\8541-Brigham City\344-8541-Brigham City\344-8541-002-Forest St Final Design\995\Structure\CADD\DWG\Structure
 PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:51:54 AM



SCREED ELEVATIONS LOCATION DIAGRAM

| | | SCREED ELEVATION TABLE | | | | | | | | | | | | | | | | | | | | | |
|----------|---------|------------------------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| | | SPAN 3 | | | | | | | | | | SPAN 4 | | | | | | | | | | | |
| | | 20B | 21 | 22 | 23 | 24 | 25 | 26 | 27 | 28 | 29 | 30A | 30B | 31 | 32 | 33 | 34 | 35 | 36 | 37 | 38 | 39 | 40A |
| GIRDER 1 | ELEV | 4347.86 | 4348.22 | 4348.54 | 4348.84 | 4349.10 | 4349.33 | 4349.52 | 4349.69 | 4349.82 | 4349.92 | 4349.98 | 4349.99 | 4350.02 | 4350.01 | 4349.97 | 4349.90 | 4349.80 | 4349.66 | 4349.49 | 4349.29 | 4349.05 | 4348.78 |
| | DL DEFL | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 |
| GIRDER 2 | ELEV | 4348.05 | 4348.41 | 4348.74 | 4349.03 | 4349.29 | 4349.52 | 4349.72 | 4349.88 | 4350.01 | 4350.11 | 4350.18 | 4350.19 | 4350.21 | 4350.21 | 4350.17 | 4350.10 | 4349.99 | 4349.85 | 4349.68 | 4349.48 | 4349.25 | 4348.98 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 3 | ELEV | 4348.25 | 4348.61 | 4348.93 | 4349.23 | 4349.49 | 4349.72 | 4349.91 | 4350.08 | 4350.21 | 4350.31 | 4350.37 | 4350.38 | 4350.41 | 4350.40 | 4350.36 | 4350.29 | 4350.19 | 4350.05 | 4349.88 | 4349.68 | 4349.44 | 4349.17 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 4 | ELEV | 4348.44 | 4348.80 | 4349.13 | 4349.42 | 4349.68 | 4349.91 | 4350.11 | 4350.27 | 4350.40 | 4350.50 | 4350.57 | 4350.58 | 4350.60 | 4350.60 | 4350.56 | 4350.49 | 4350.38 | 4350.24 | 4350.07 | 4349.87 | 4349.64 | 4349.37 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| PGL | ELEV | 4348.54 | 4348.90 | 4349.23 | 4349.52 | 4349.78 | 4350.01 | 4350.21 | 4350.37 | 4350.50 | 4350.60 | 4350.67 | 4350.68 | 4350.70 | 4350.70 | 4350.66 | 4350.59 | 4350.48 | 4350.34 | 4350.17 | 4349.97 | 4349.74 | 4349.47 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 5 | ELEV | 4348.45 | 4348.81 | 4349.13 | 4349.43 | 4349.69 | 4349.92 | 4350.11 | 4350.28 | 4350.41 | 4350.51 | 4350.57 | 4350.58 | 4350.61 | 4350.60 | 4350.56 | 4350.49 | 4350.39 | 4350.25 | 4350.08 | 4349.88 | 4349.64 | 4349.37 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 6 | ELEV | 4348.25 | 4348.61 | 4348.94 | 4349.23 | 4349.49 | 4349.72 | 4349.92 | 4350.08 | 4350.21 | 4350.31 | 4350.38 | 4350.39 | 4350.41 | 4350.41 | 4350.37 | 4350.30 | 4350.19 | 4350.05 | 4349.88 | 4349.68 | 4349.45 | 4349.18 |
| | DL DEFL | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 | 0.00 | 0.05 | 0.09 | 0.12 | 0.14 | 0.15 | 0.14 | 0.12 | 0.09 | 0.05 | 0.00 |
| GIRDER 7 | ELEV | 4348.06 | 4348.42 | 4348.74 | 4349.04 | 4349.30 | 4349.53 | 4349.72 | 4349.89 | 4350.02 | 4350.12 | 4350.18 | 4350.19 | 4350.22 | 4350.21 | 4350.17 | 4350.10 | 4350.00 | 4349.86 | 4349.69 | 4349.49 | 4349.25 | 4348.98 |
| | DL DEFL | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 | 0.00 | 0.04 | 0.08 | 0.11 | 0.13 | 0.13 | 0.13 | 0.11 | 0.08 | 0.04 | 0.00 |

NOTES

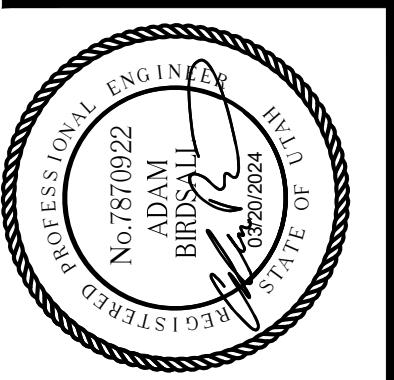
1. FINAL TOP OF CONCRETE DECK ELEVATIONS ARE SHOWN. ADD DEAD LOAD DEFLECTIONS TO THE TOP OF CONCRETE DECK ELEVATIONS TO OBTAIN SCREED ELEVATIONS.
2. ALL ELEVATIONS AND DEAD LOAD DEFLECTIONS VALUES ARE SHOWN IN FEET.
3. SCREED ELEVATIONS ARE PROVIDED AT INTERSECTION OF CENTERLINE OF GIRDER AND SCREED LINE.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



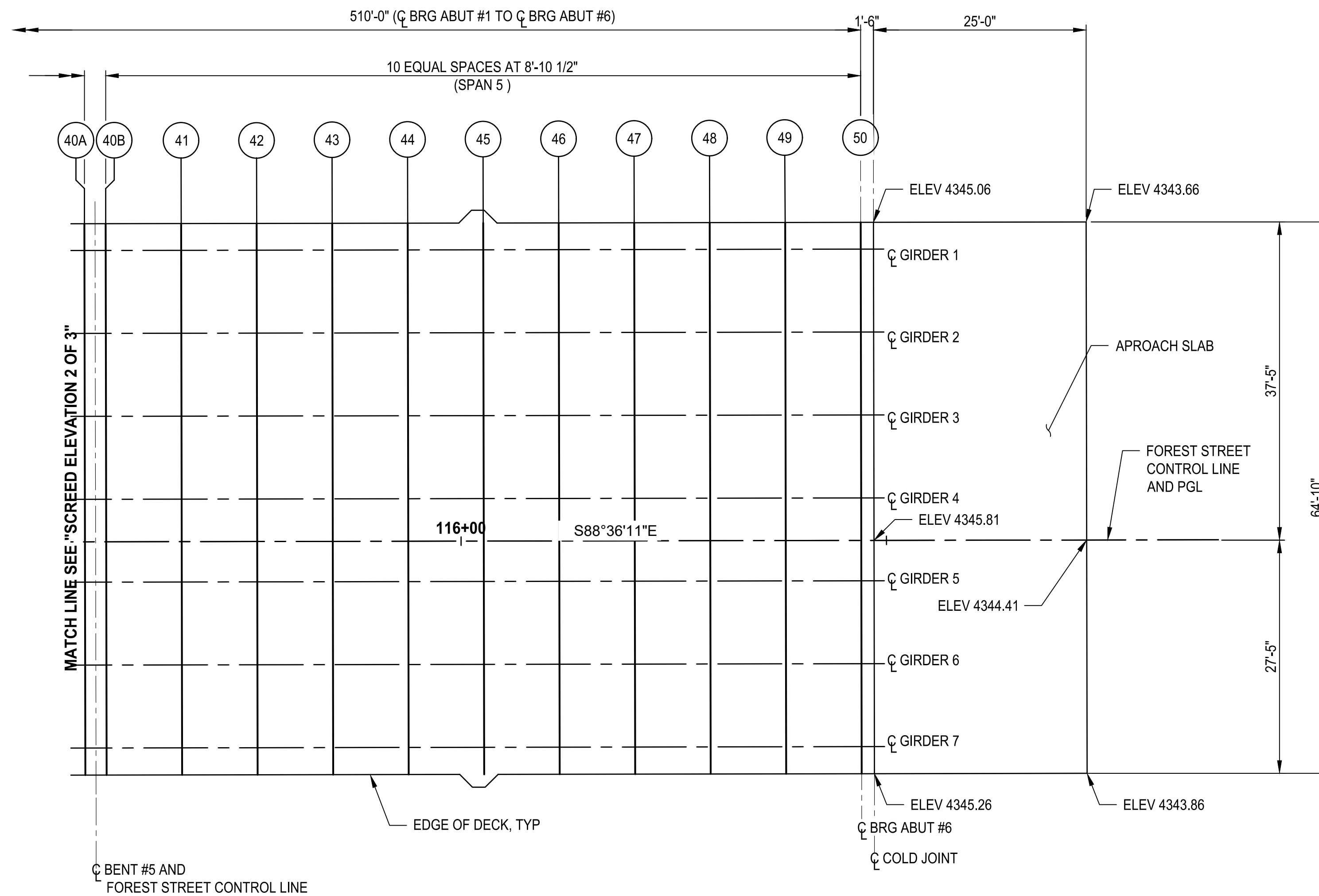
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SCREED ELEVATION 2 OF 3

DRAWING NO.
 44 OF 59

S44

LAYOUT: Screed 3 PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995socs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:52:11 AM



SCREED ELEVATIONS LOCATION DIAGRAM

| SCREED ELEVATION TABLE | | | | | | | | | | | | |
|------------------------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| | | SPAN 5 | | | | | | | | | | |
| | | 40B | 41 | 42 | 43 | 44 | 45 | 46 | 47 | 48 | 49 | 50 |
| GIRDER 1 | ELEV | 4348.72 | 4348.47 | 4348.19 | 4347.90 | 4347.58 | 4347.24 | 4346.88 | 4346.49 | 4346.08 | 4345.66 | 4345.20 |
| | DL DEFL | 0.00 | 0.02 | 0.05 | 0.06 | 0.08 | 0.08 | 0.08 | 0.06 | 0.05 | 0.02 | 0.00 |
| GIRDER 2 | ELEV | 4348.91 | 4348.66 | 4348.39 | 4348.09 | 4347.77 | 4347.43 | 4347.07 | 4346.69 | 4346.28 | 4345.85 | 4345.40 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| GIRDER 3 | ELEV | 4349.11 | 4348.86 | 4348.58 | 4348.29 | 4347.97 | 4347.63 | 4347.27 | 4346.88 | 4346.47 | 4346.05 | 4345.59 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| GIRDER 4 | ELEV | 4349.30 | 4349.05 | 4348.78 | 4348.48 | 4348.16 | 4347.82 | 4347.46 | 4347.08 | 4346.67 | 4346.24 | 4345.79 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| PGL | ELEV | 4349.40 | 4349.15 | 4348.88 | 4348.58 | 4348.26 | 4347.92 | 4347.56 | 4347.18 | 4346.77 | 4346.34 | 4345.89 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| GIRDER 5 | ELEV | 4349.31 | 4349.06 | 4348.78 | 4348.49 | 4348.17 | 4347.83 | 4347.47 | 4347.08 | 4346.67 | 4346.25 | 4345.79 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| GIRDER 6 | ELEV | 4349.11 | 4348.86 | 4348.59 | 4348.29 | 4347.97 | 4347.63 | 4347.27 | 4346.89 | 4346.48 | 4346.05 | 4345.60 |
| | DL DEFL | 0.00 | 0.03 | 0.05 | 0.07 | 0.08 | 0.09 | 0.08 | 0.07 | 0.05 | 0.03 | 0.00 |
| GIRDER 7 | ELEV | 4348.92 | 4348.67 | 4348.39 | 4348.10 | 4347.78 | 4347.44 | 4347.08 | 4346.69 | 4346.28 | 4345.86 | 4345.40 |
| | DL DEFL | 0.00 | 0.02 | 0.05 | 0.06 | 0.08 | 0.08 | 0.08 | 0.06 | 0.05 | 0.02 | 0.00 |

NOTES

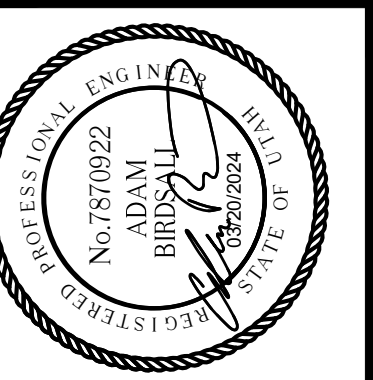
- FINAL TOP OF CONCRETE DECK ELEVATIONS ARE SHOWN. ADD DEAD LOAD DEFLECTIONS TO THE TOP OF CONCRETE DECK ELEVATIONS TO OBTAIN SCREED ELEVATIONS.
- ALL ELEVATIONS AND DEAD LOAD DEFLECTIONS VALUES ARE SHOWN IN FEET.
- SCREED ELEVATIONS ARE PROVIDED AT INTERSECTION OF CENTERLINE OF GIRDER AND SCREED LINE.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



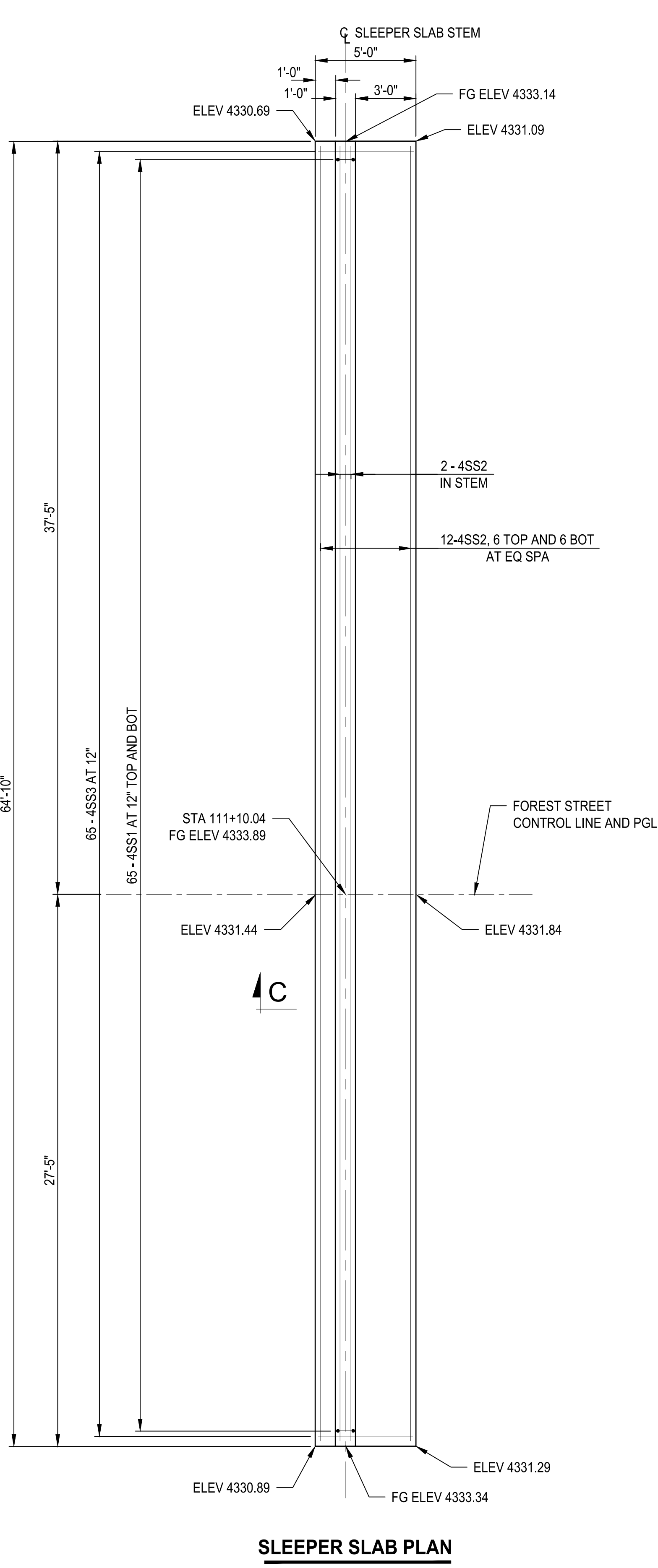
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SCREED ELEVATION 3 OF 3

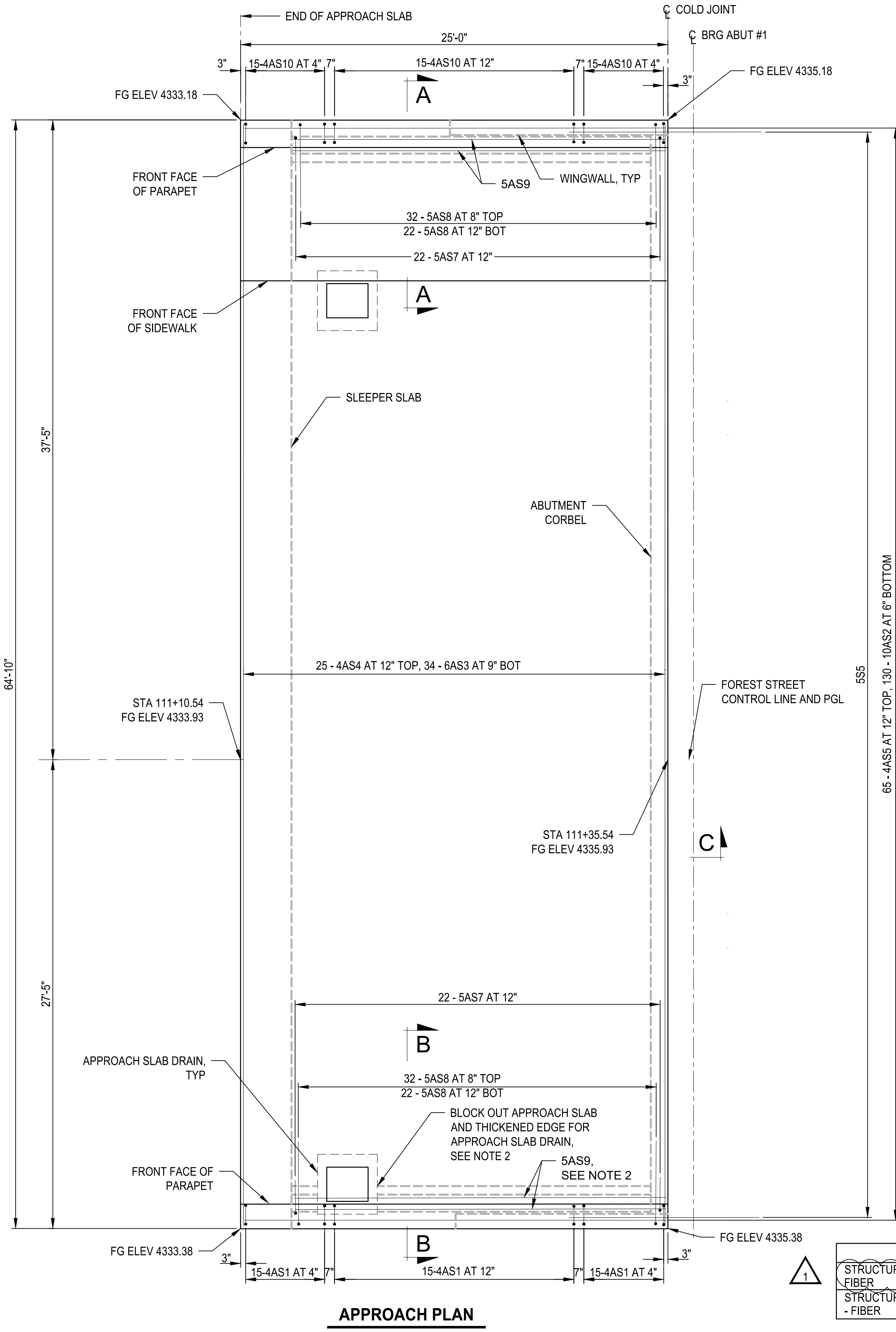
DRAWING NO.
 45 OF 59

S45

LAYOUT: APP SLAB W PATH: U:\Salt\Projects\Clients\B541-Brigham City\344-8541-002 Forest St Final Design\995\cadd\DWG\Structure PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:52:27 AM



SLEEPER SLAB PLAN



APPROACH PLAN

NOTES

- SLEEPER SLAB ELEVATIONS SHOWN CORRESPOND TO BOTTOM OF SLEEPER SLAB UNLESS INDICATED BY FG ELEV (FINISHED GRADE ELEVATION).
- FIELD CUT OR BEND APPROACH SLAB REINFORCING TO CLEAR APPROACH SLAB DRAINS.
- SEE "APPROACH SLAB DRAIN DETAILS" FOR LOCATIONS OF DRAINS.
- SEE "APPROACH SLAB DETAILS" FOR SECTION A-A AND B-B.

| ITEM | LOCATION | EST QTY | UNIT |
|-----------------------------|-----------------------------|---------|-------|
| STRUCTURAL CONCRETE - FIBER | CLASS AA(LSF) SLEEPER SLABS | 18 | CU YD |
| STRUCTURAL CONCRETE - FIBER | CLASS AA(LSF) APPROACH SLAB | 66 | CU YD |

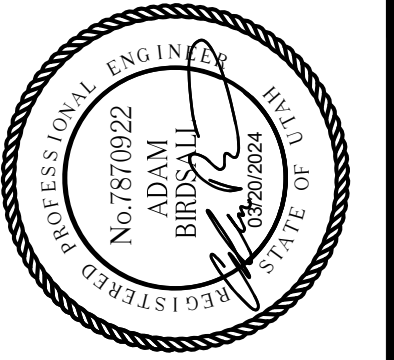
| REVISIONS | DATE | BY |
|-----------|-------|-----|
| △ | 03/24 | AUB |

REVISOR: CONCRETE TYPE

ONE INCH AT FULL SCALE IF NOT SHOWN OTHERWISE

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB

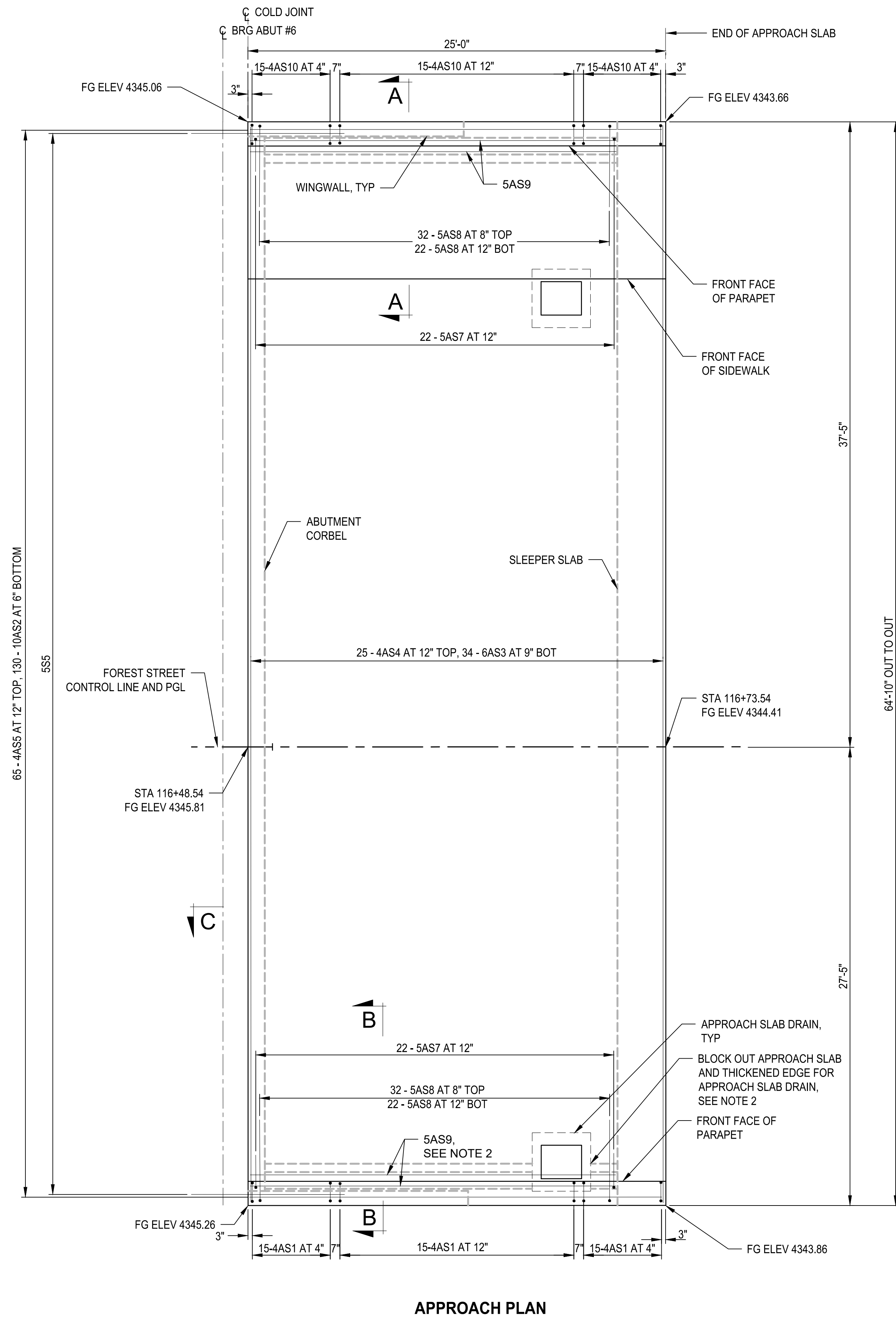


PROJECT NAME: **BRIGHAM CITY CONNECTION PROJECT**

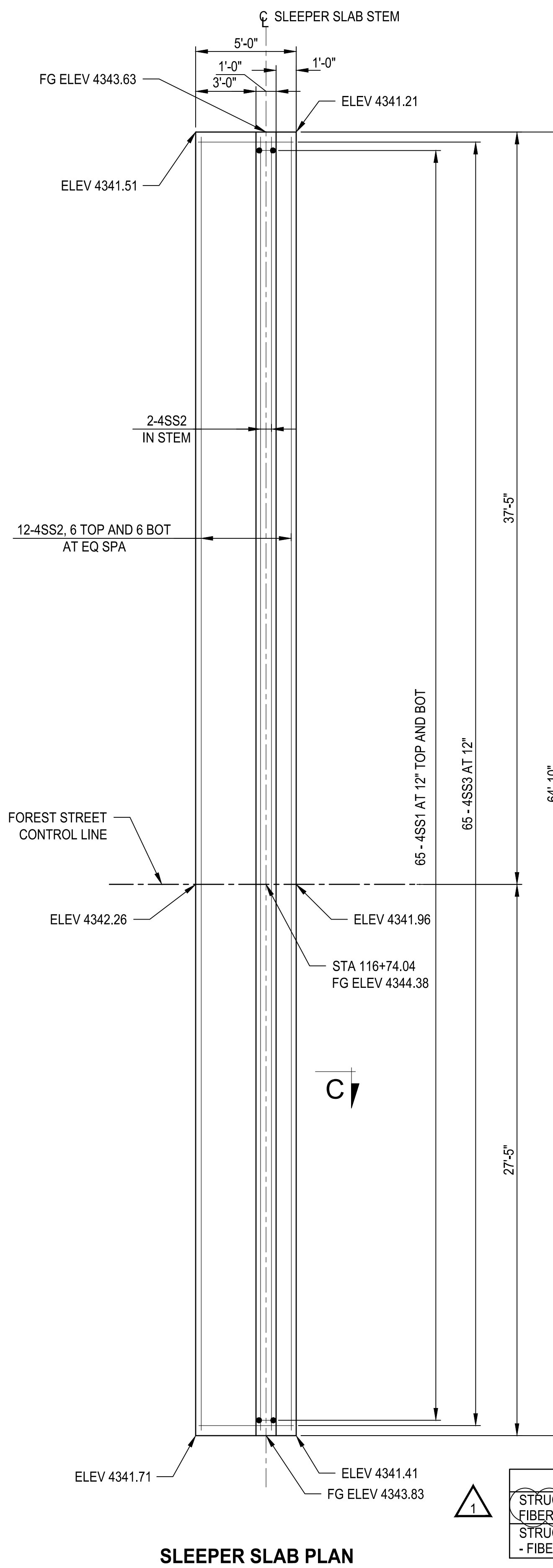
APPROACH SLAB PLAN 1 OF 2

DRAWING NO. 46 OF 59
S46

LAYOUT: APP SLAB E. PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vca\CADD\DWG\Structure. PLOTTED BY: OliveSta. DATE: Friday, March 22, 2024 9:52:45 AM



APPROACH PLAN



SLEEPER SLAB PLAN

NOTES

- SLEEPER SLAB ELEVATIONS SHOWN CORRESPOND TO BOTTOM OF SLEEPER SLAB UNLESS INDICATED BY FG ELEV (FINISHED GRADE ELEVATION).
- FIELD CUT OR BEND APPROACH SLAB REINFORCING TO CLEAR APPROACH SLAB DRAINS.
- SEE "APPROACH SLAB DRAIN DETAILS" FOR LOCATIONS OF DRAINS.
- SEE "APPROACH SLAB DETAILS" FOR SECTION A-A AND B-B.

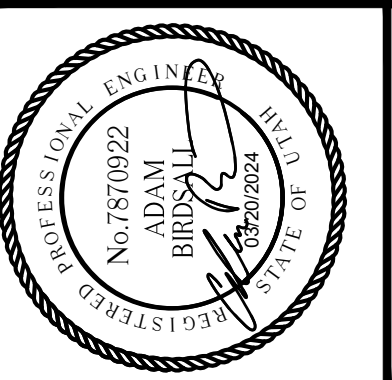
| ITEM | LOCATION | EST QTY | UNIT |
|-----------------------------|-----------------------------|---------|-------|
| STRUCTURAL CONCRETE - FIBER | CLASS AA(LSF) SLEEPER SLABS | 18 | CU YD |
| STRUCTURAL CONCRETE - FIBER | CLASS AA(LSF) APPROACH SLAB | 66 | CU YD |

| REVISIONS | DATE | BY |
|-----------|-------|-----|
| △ | 03/24 | AJB |

ONE INCH AT FULL SCALE IF NOT INDICATED OTHERWISE ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AJB

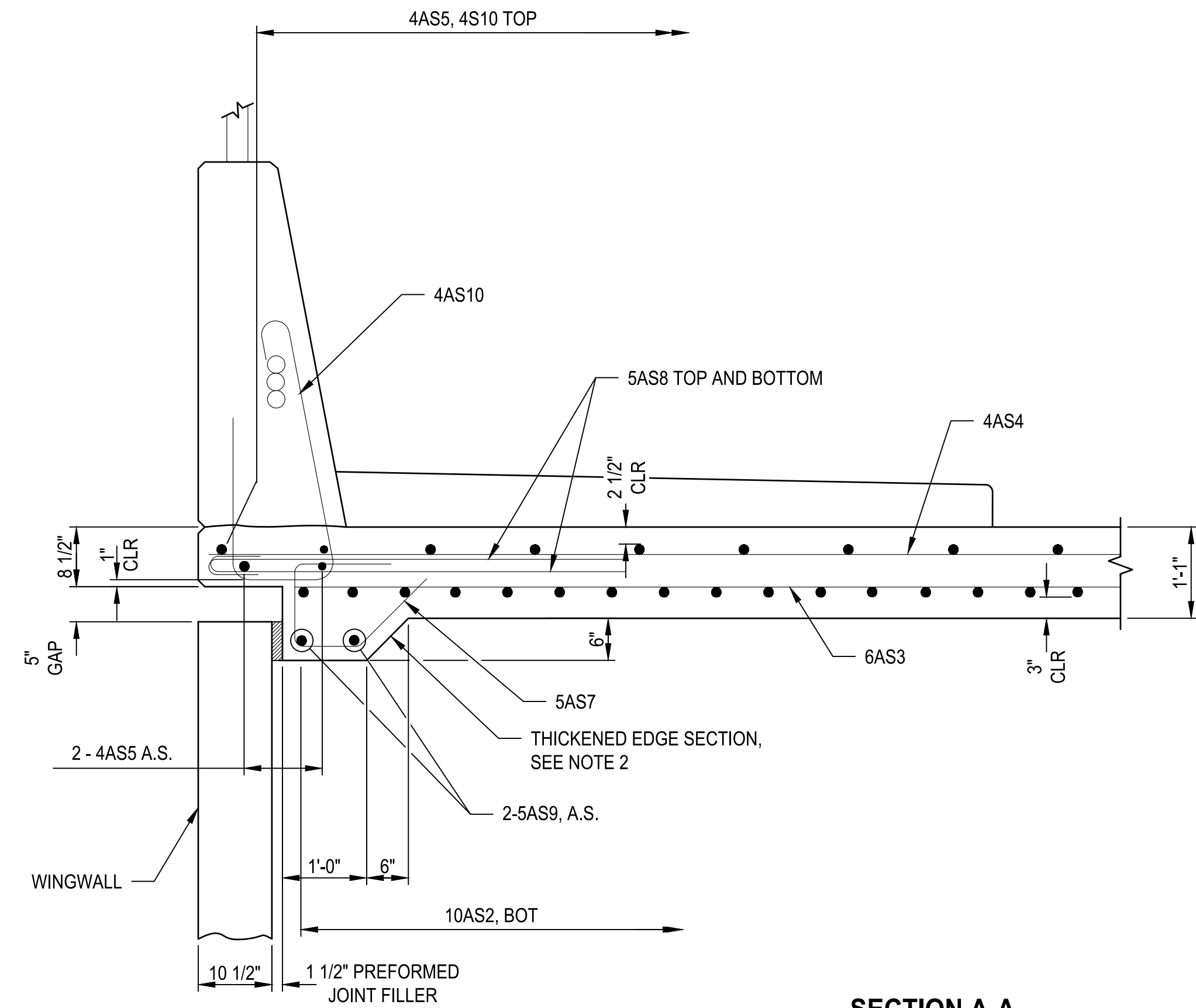


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

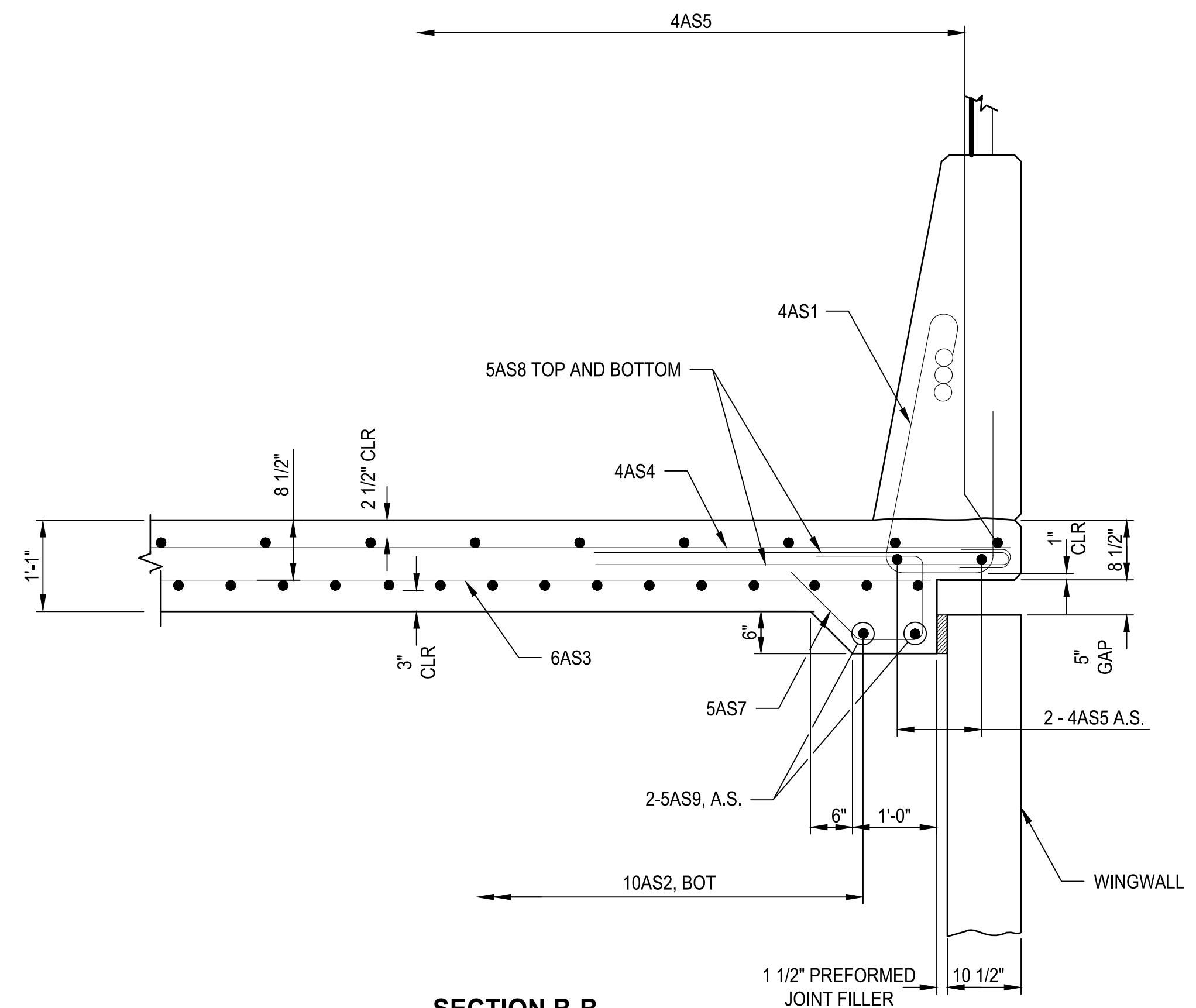
APPROACH SLAB PLAN 2 OF 2

DRAWING NO.
 47 OF 59
S47

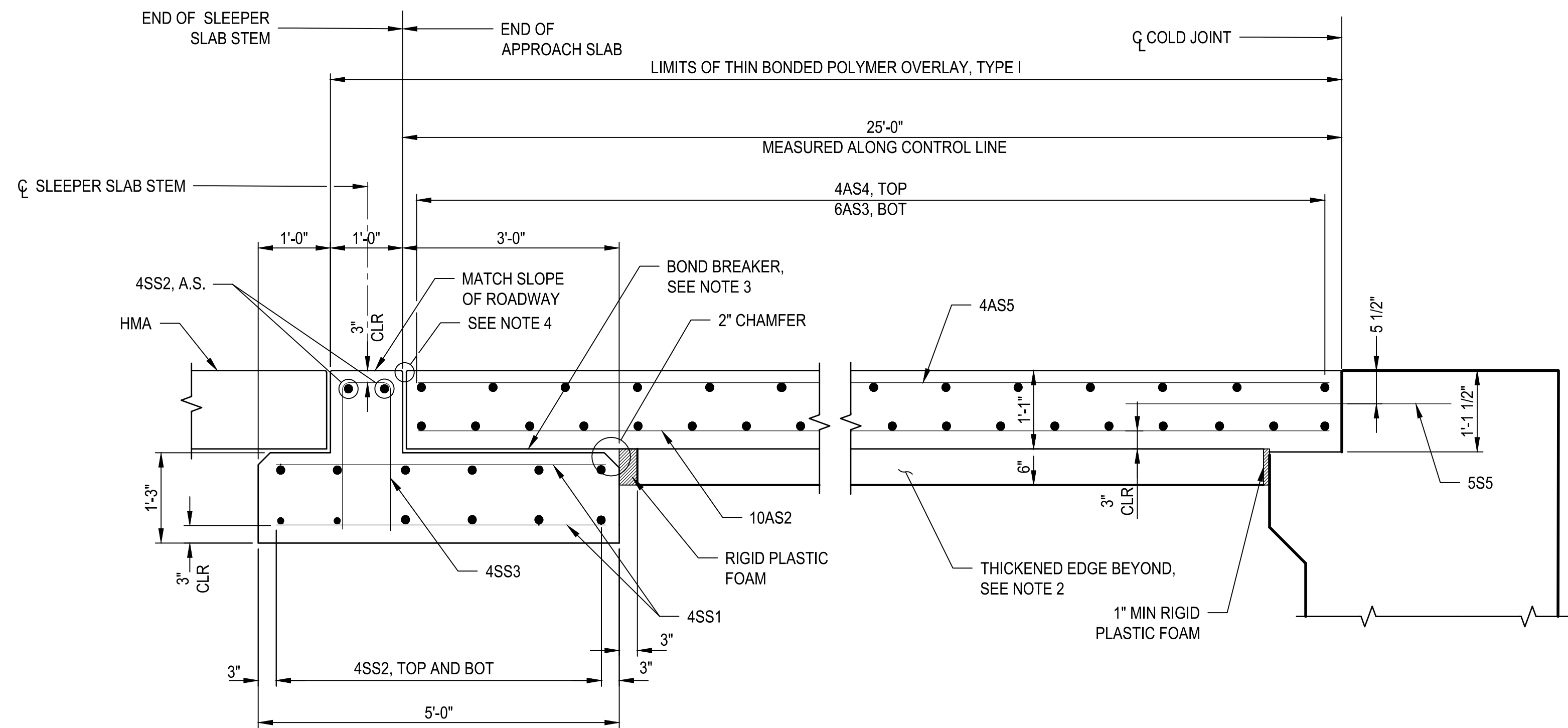
LAYOUT: App Slab det. PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:52:55 AM



SECTION A-A



SECTION B-B



SECTION C-C

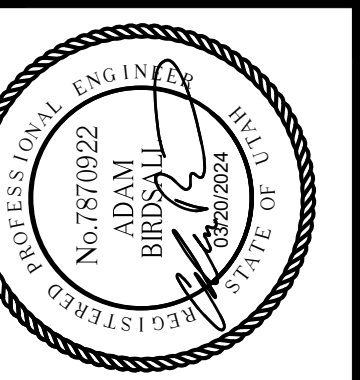
NOTES

- SEE "APPROACH SLAB PLAN 1 OF 2" AND "APPROACH SLAB PLAN 2 OF 2" FOR LOCATIONS OF SECTIONS A-A AND B-B.
- EXTEND THICKENED EDGE AS SHOWN WHEN NO APPROACH SLAB DRAIN IS PRESENT. EXTEND THICKENED EDGE TO APPROACH SLAB DRAIN WHEN THE APPROACH SLAB DRAIN IS PRESENT.
- PLACE BOND BREAKER OVER ENTIRE SURFACE BETWEEN APPROACH SLAB AND SLEEPER SLAB.
- SEE "COMPRESSION JOINT DETAILS" FOR JOINT DETAILS.

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

| | | | | |
|-------------------|----------|--------------|----------|-----|
| Parametrix | DESIGNED | TWP | CHECKED | NCC |
| | DATE | 03/20/2024 | DRAWN | AJB |
| | JOB No. | 344-8541-002 | APPROVED | SLO |



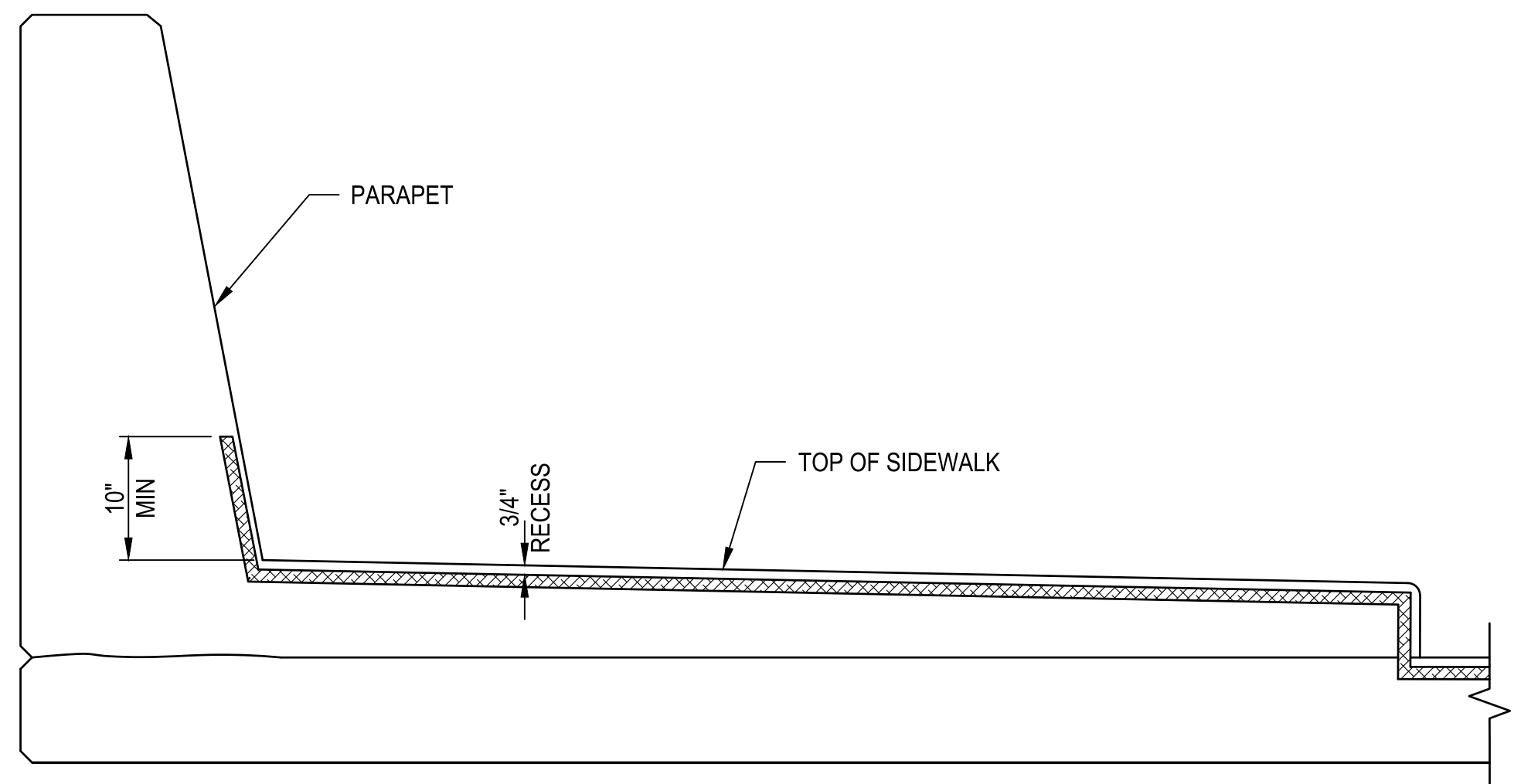
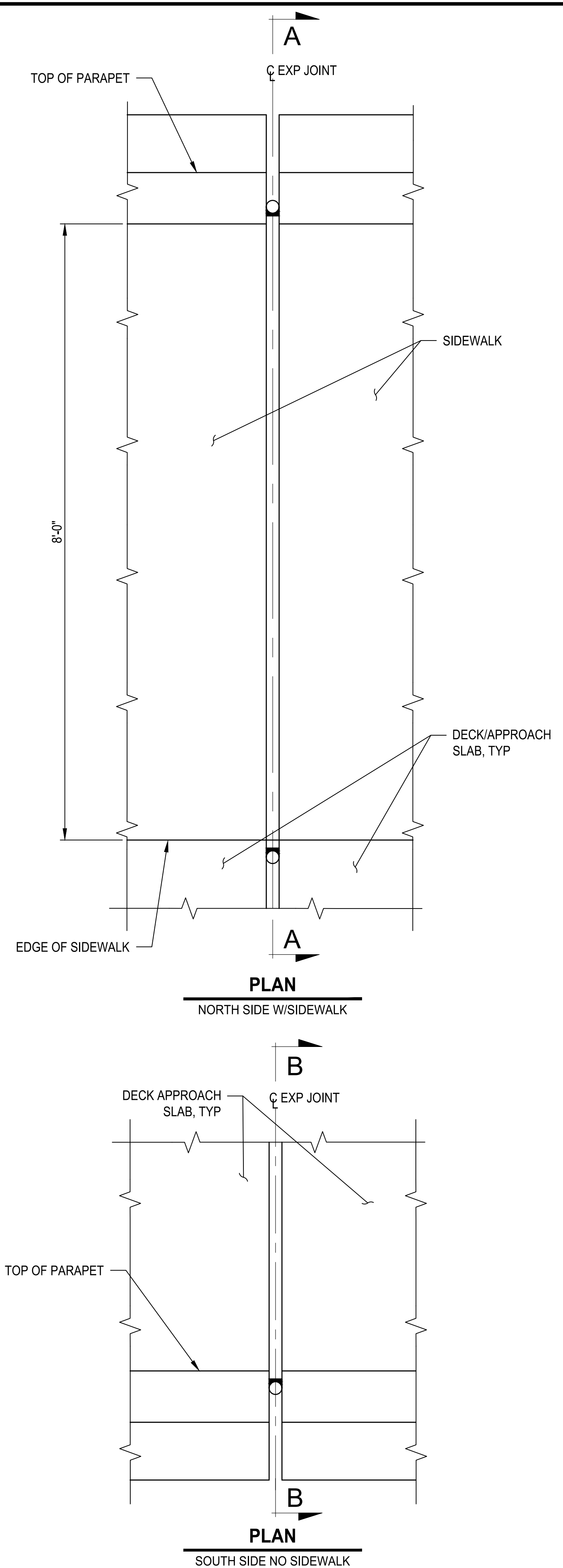
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

APPROACH SLAB DETAILS

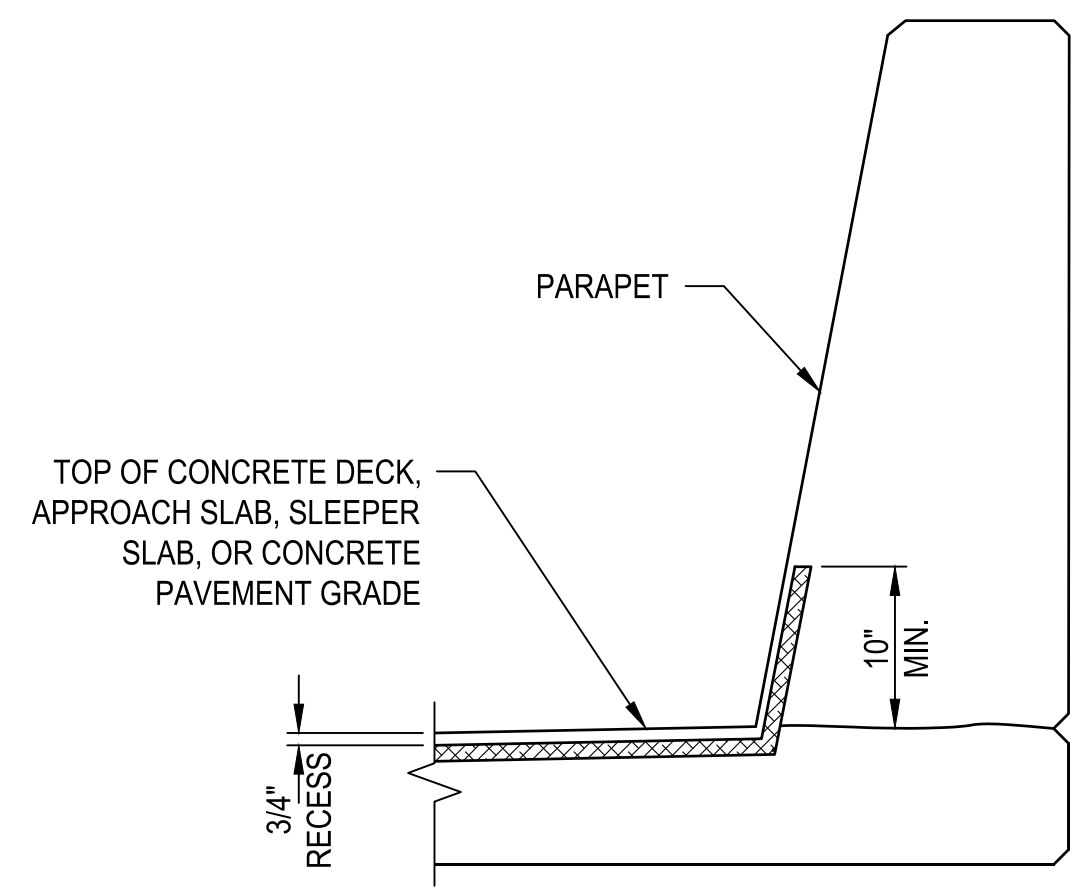
DRAWING NO.
48 OF 59

S48

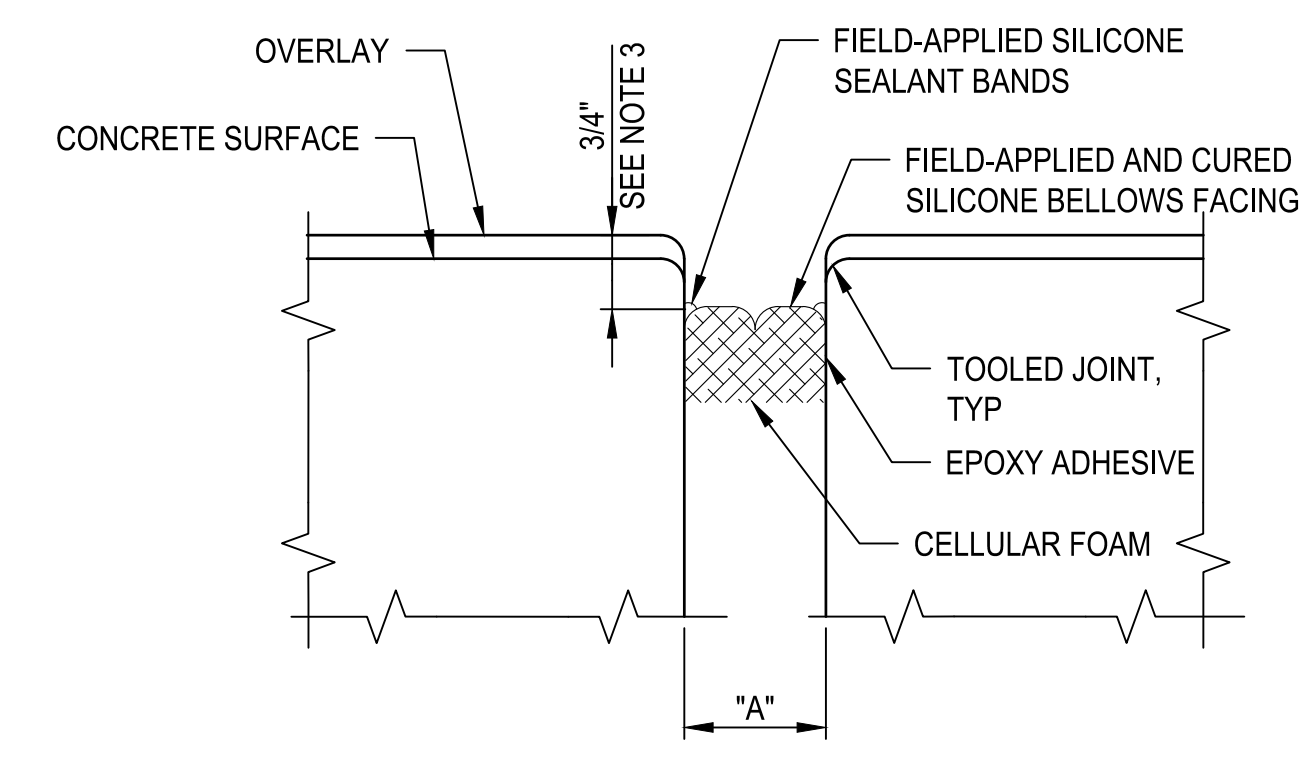
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 LAYOUT: COM JT



SECTION A-A



SECTION B-B



COMPRESSION JOINT SEAL TYPE A

NOTES

1. INSTALL THE EXPANSION DEVICE BELOW FINISHED CONCRETE SURFACE AS SHOWN, PARALLEL TO THE SLOPE AND GRADE OF THE DECK.
2. ROUND INSTALLATION TEMPERATURE TO NEAREST 10 DEGREE INCREMENT.
3. INSTALL JOINT SEALER BELOW THE TRAFFIC SURFACE AS SHOWN OR ACCORDING TO MANUFACTURER'S INSTALLATION INSTRUCTIONS, WHICHEVER IS LARGER.

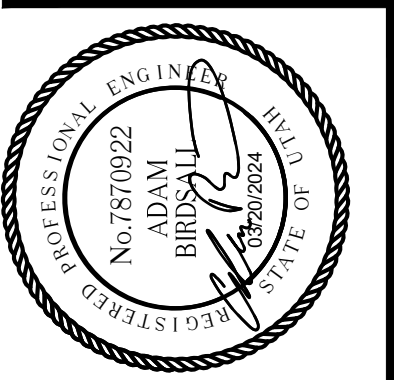
| JOINT DATA TABLE | | | | | | | | | | | | | | | |
|------------------|---------------|------|----------------------------|--|---|--|--------------------------------|----------------------------------|----------------------------------|---------------------------------|----------|----------|----------|----------|----------|
| STRUCTURE NUMBER | LOCATION | TYPE | FACTORY DESIGN MOVEMENT | | | FACTORY DESIGN WIDTHS PERPENDICULAR TO JOINT | | INSTALLATION LIMITS | | JOINT WIDTH "A" AT INSTALLATION | | | | | |
| | | | IN THE DIRECTION OF TRAVEL | PERPENDICULAR TO THE CENTERLINE OF THE JOINT | PARALLEL TO THE CENTERLINE OF THE JOINT | MAXIMUM WIDTH A ^{MAX} | MINIMUM WIDTH A ^{MIN} | MINIMUM INSTALLATION TEMPERATURE | MAXIMUM INSTALLATION TEMPERATURE | T = 40°F | T = 50°F | T = 60°F | T = 70°F | T = 80°F | T = 90°F |
| | | | INCH | INCH | INCH | INCH | INCH | F° | F° | INCH | INCH | INCH | INCH | INCH | INCH |
| 003087F | ABUTMENT 1, 6 | A | 2.80 | 2.80 | 0.00 | 3.875 | 1.00 | 40 | 90 | 2.625 | 2.375 | 2.125 | 1.875 | 1.625 | 1.375 |

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



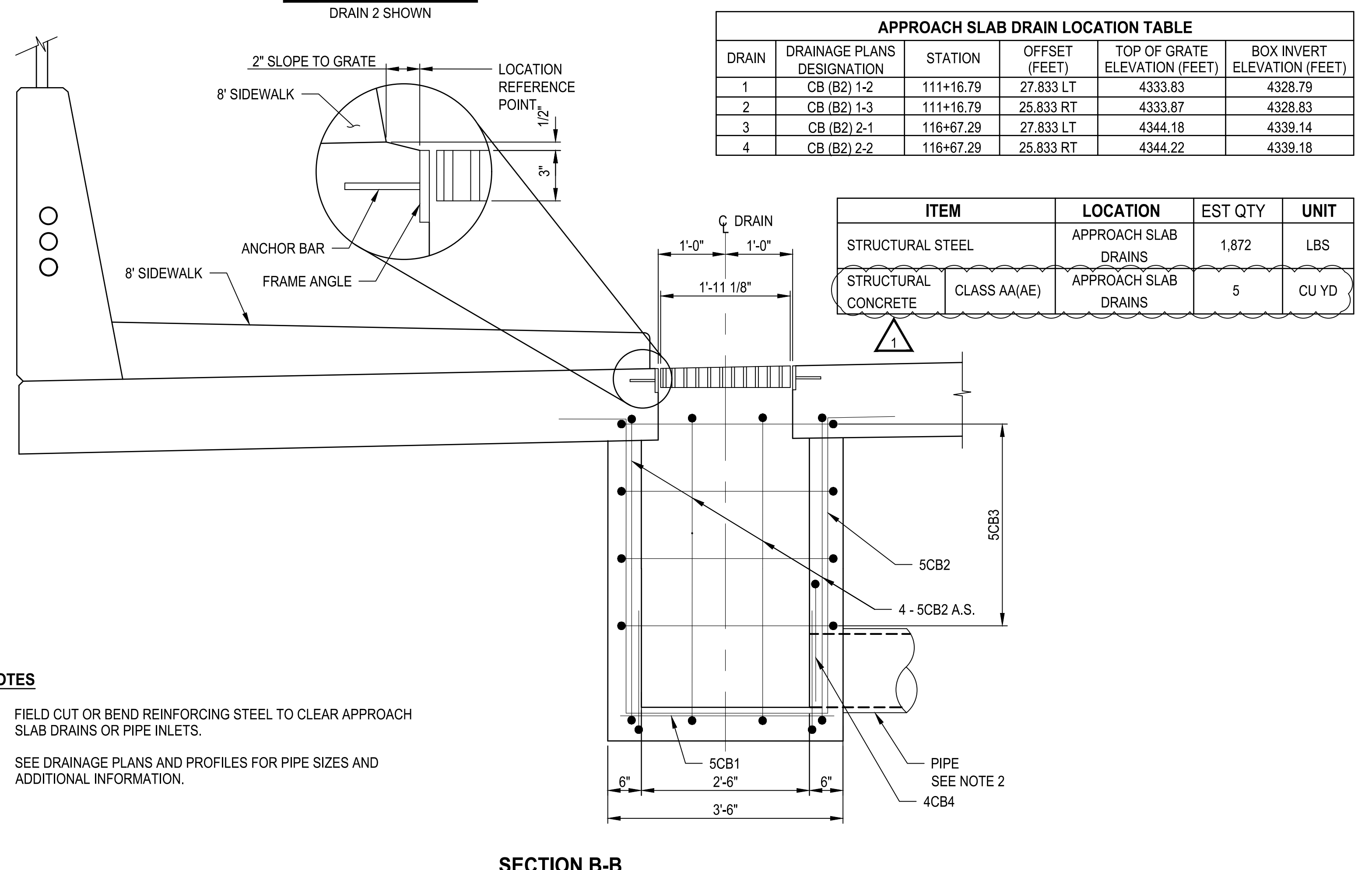
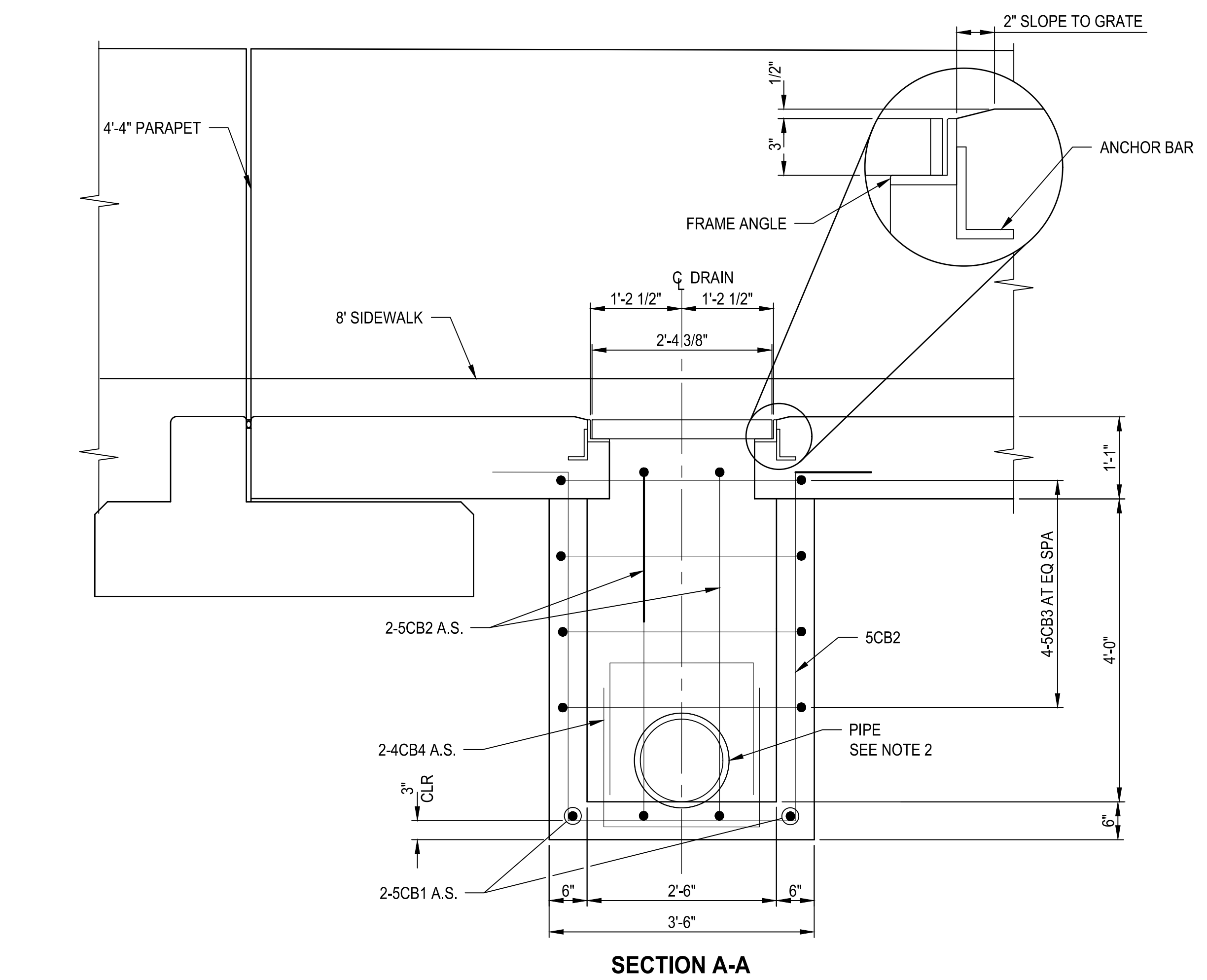
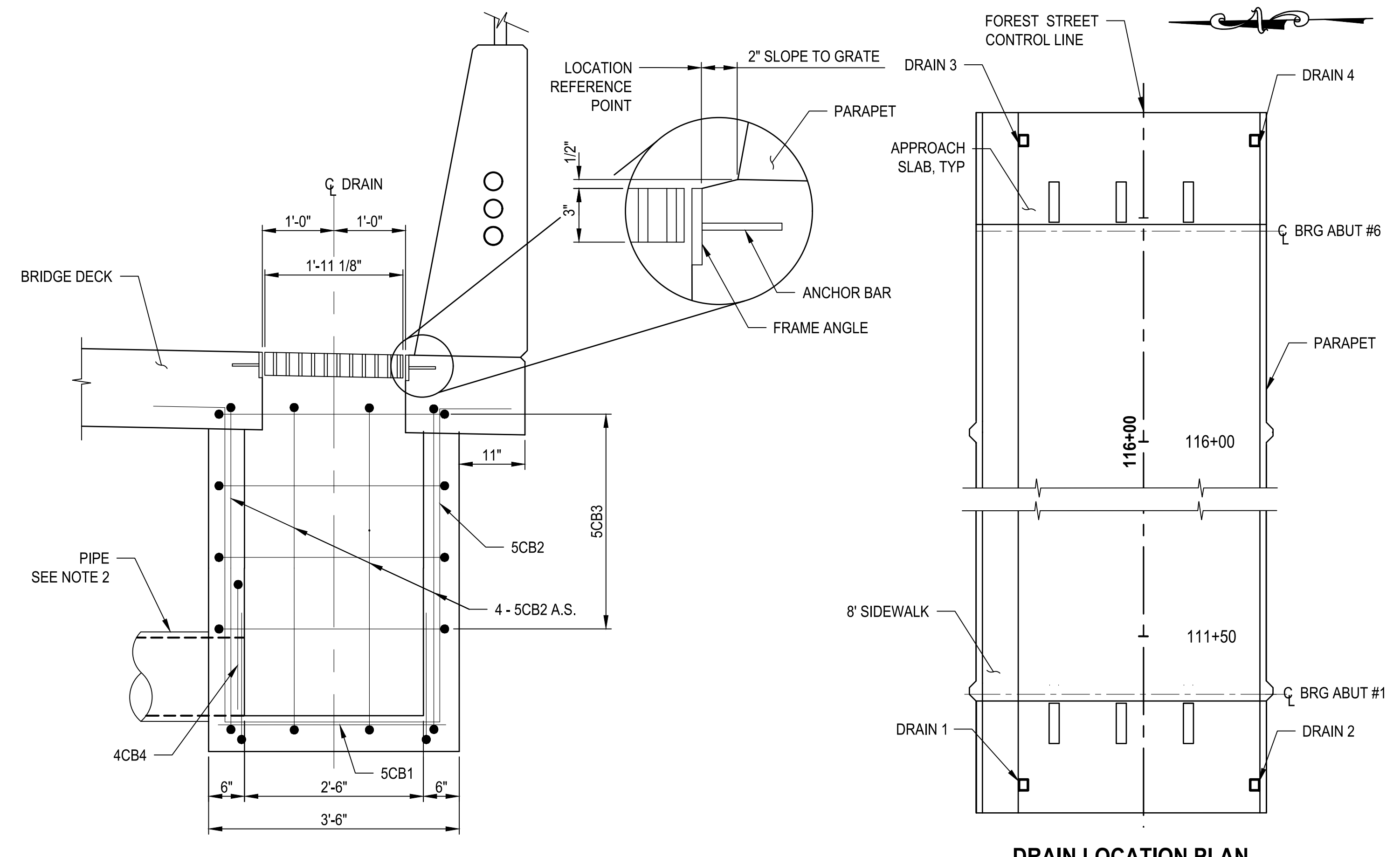
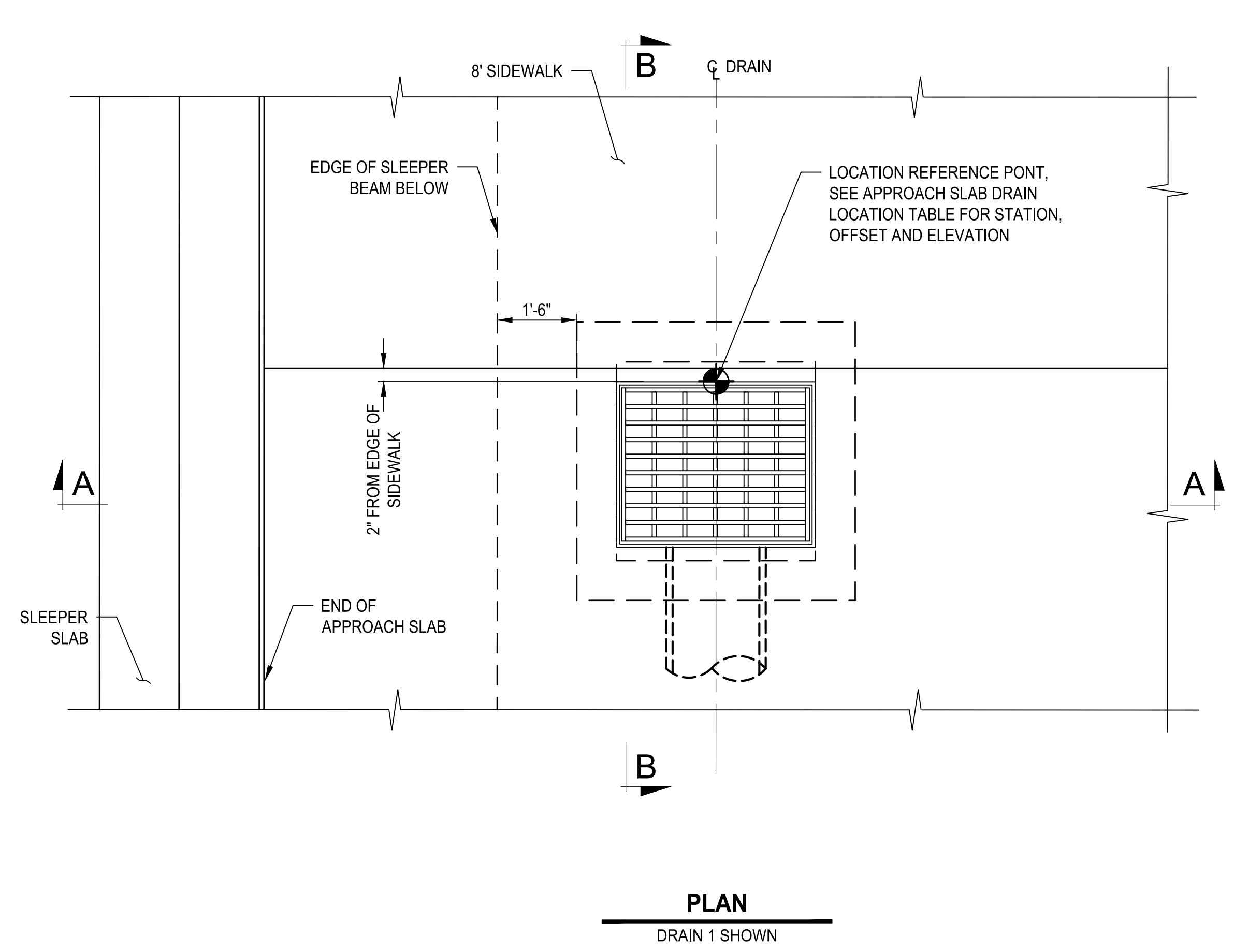
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

COMPRESSION JOINT DETAILS

DRAWING NO.
49 OF 59

S49

PATH: U:\S&B\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995vcs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:53:19 AM
 LAYOUT: drain



APPROACH SLAB DRAIN LOCATION TABLE

| DRAIN | DRAINAGE PLANS DESIGNATION | STATION | OFFSET (FEET) | TOP OF GRATE ELEVATION (FEET) | BOX INVERT ELEVATION (FEET) |
|-------|----------------------------|-----------|---------------|-------------------------------|-----------------------------|
| 1 | CB (B2) 1-2 | 111+16.79 | 27.833 LT | 4333.83 | 4328.79 |
| 2 | CB (B2) 1-3 | 111+16.79 | 25.833 RT | 4333.87 | 4328.83 |
| 3 | CB (B2) 2-1 | 116+67.29 | 27.833 LT | 4344.18 | 4339.14 |
| 4 | CB (B2) 2-2 | 116+67.29 | 25.833 RT | 4344.22 | 4339.18 |

| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|-----------------------------------|---------|-------|
| STRUCTURAL STEEL | APPROACH SLAB DRAINS | 1,872 | LBS |
| STRUCTURAL CONCRETE | CLASS AA(AE) APPROACH SLAB DRAINS | 5 | CU YD |

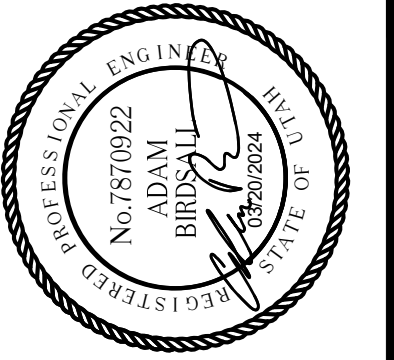
- NOTES**
- FIELD CUT OR BEND REINFORCING STEEL TO CLEAR APPROACH SLAB DRAINS OR PIPE INLETS.
 - SEE DRAINAGE PLANS AND PROFILES FOR PIPE SIZES AND ADDITIONAL INFORMATION.

| REVISIONS | DATE | BY | QUANTITY |
|-----------|-------|-----|----------|
| 1 | 03/24 | AUB | |

ONE INCH AT FULL SCALE, IF NOT OTHERWISE NOTED ACCORDINGLY

Parametrix

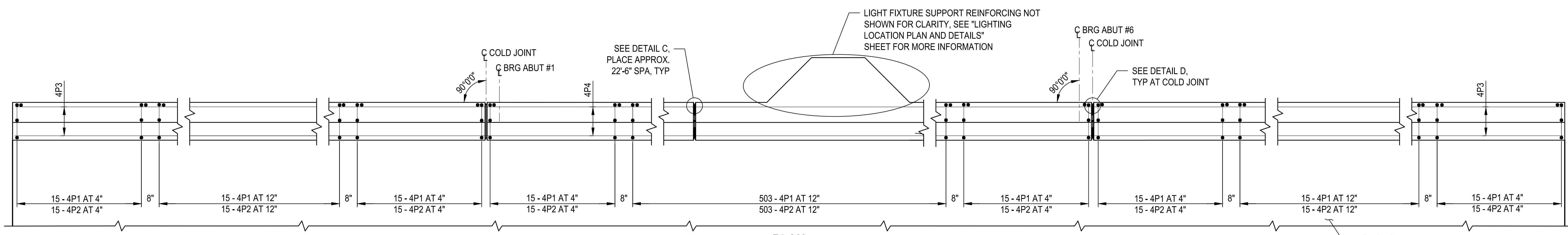
DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

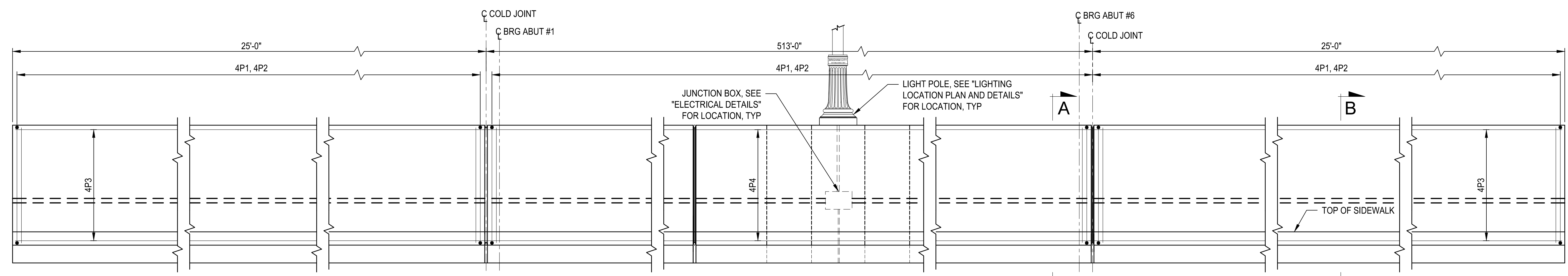
APPROACH SLAB DRAIN DETAILS 2X2 GRATE

LAYOUT: PARAPET SW PATH: U:\Self\Projects\Clients\B541-Brigham City\344-8541-002 Forest St Final Design\995\cadd\DWG\Structure PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:53:37 AM



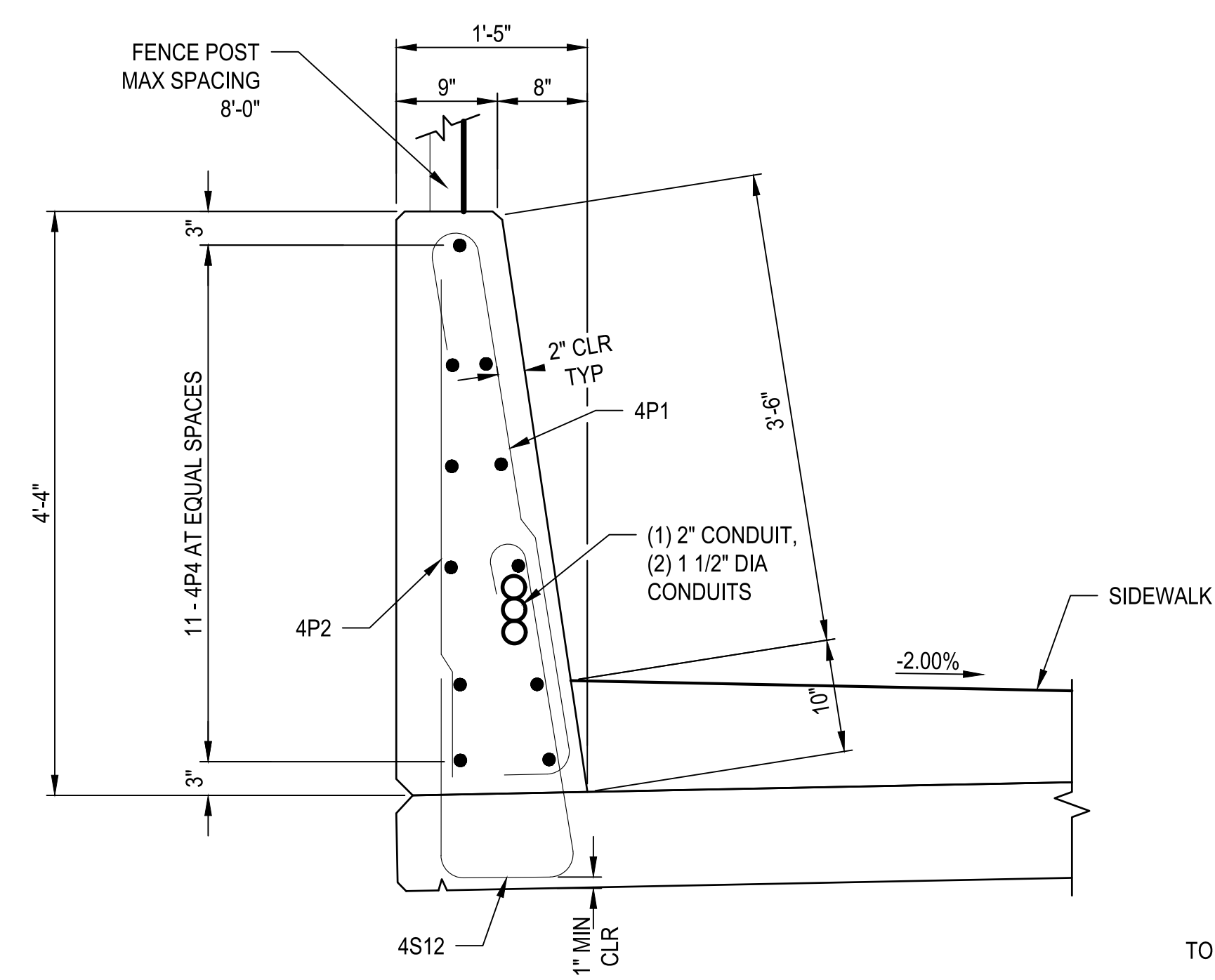
PLAN

DECK AND APPROACH SLAB REINFORCEMENT NOT SHOWN

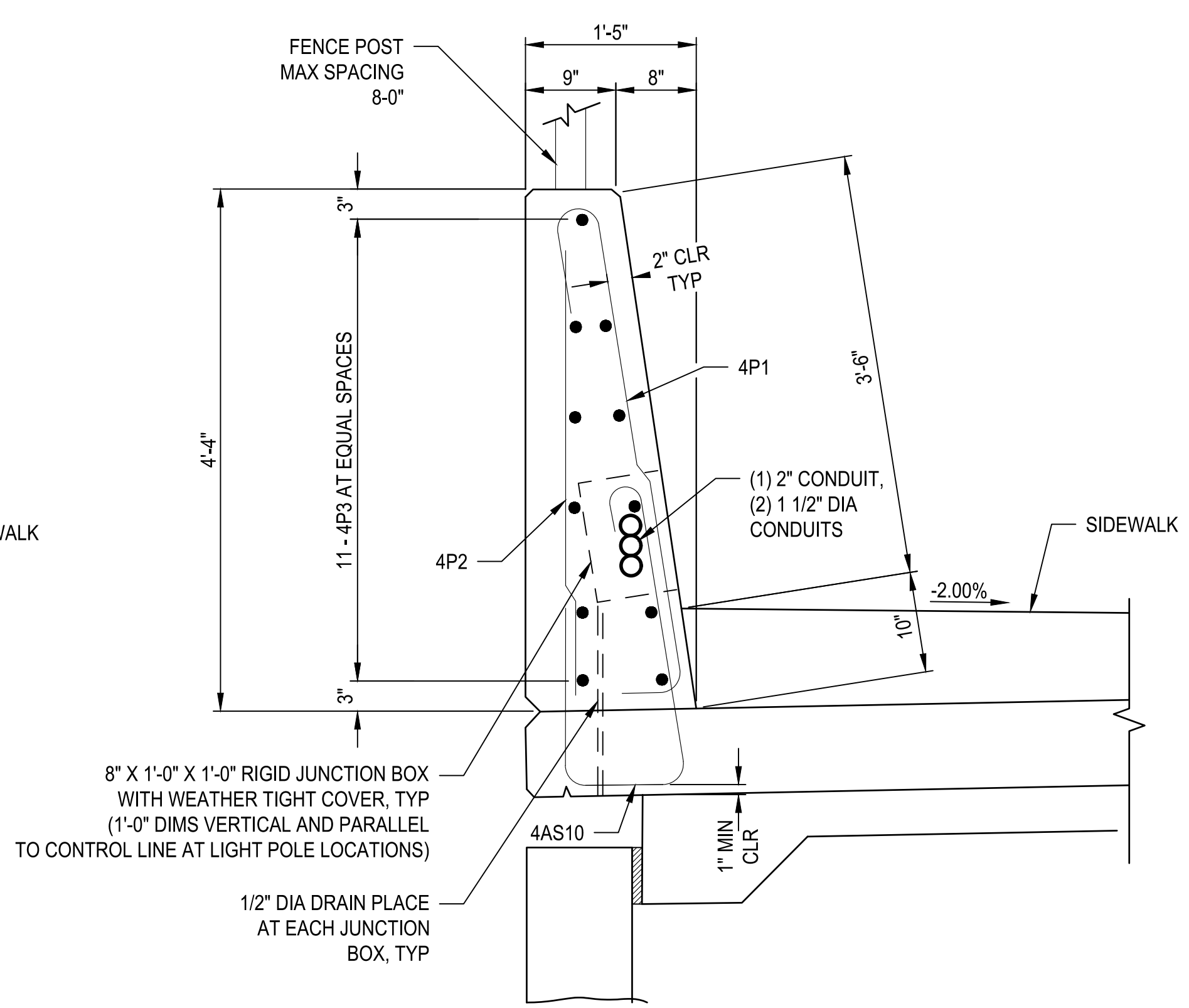


ELEVATION

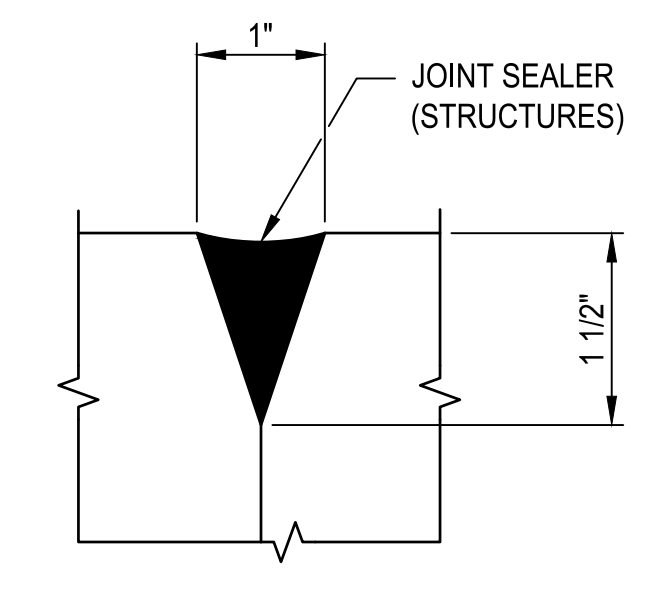
DECK AND APPROACH SLAB REINFORCEMENT NOT SHOWN



SECTION A-A

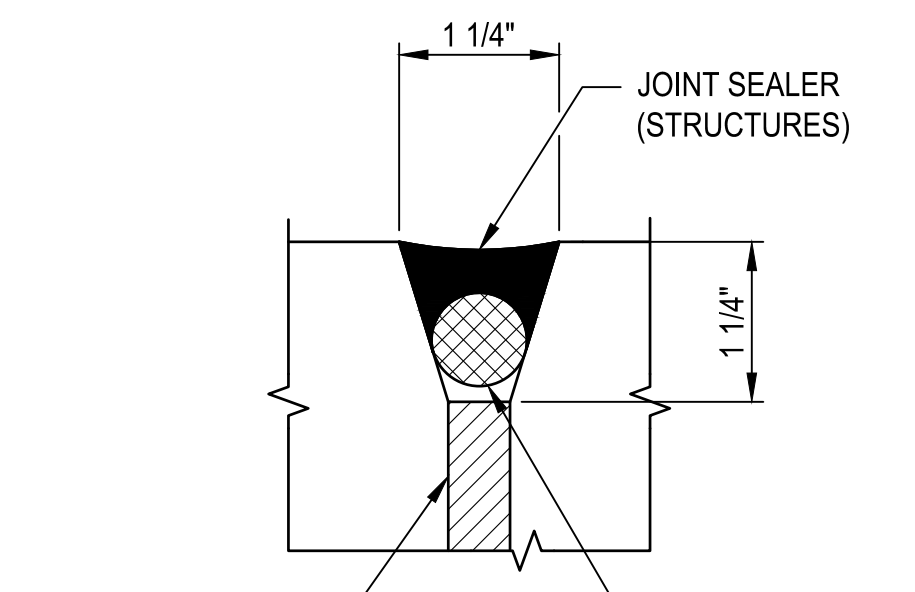


SECTION B-B



DETAIL C

CONTRACTION JOINT



DETAIL D

SEE NOTE 8

NOTES

1. ALTERNATE ALL REINFORCING SPLICES.
2. PROVIDE 2 INCH MIN COVER TO REINFORCING UNLESS NOTED OTHERWISE.
3. PLACE CONTRACTION JOINT ON SIDES AND ON TOP OF PARAPET.
4. EXTEND SEALANT AND FOAM BACKER ROD FROM DECK TO TOP OF PARAPET ON THE INSIDE PARAPET FACE AND ACROSS TOP OF PARAPET.
5. LOCATE STRUCTURE NUMBER ON RIGHT-HAND SIDE OF APPROACH PARAPET. SEE "PARAPET END DETAILS" FOR STRUCTURE NUMBER DETAILS.
6. PARAPET MEETS TL-4 REQUIREMENTS.
7. PROVIDE DETAIL D JOINT, PERPENDICULAR TO EDGE OF DECK, ABOVE BENT CENTERLINES AT BENTS 2, 3, AND 5.
8. CONTINUE JOINTS THROUGH SIDEWALK WHERE THEY OCCUR IN THE PARAPET.

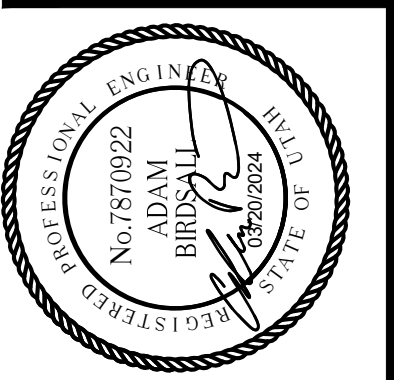
| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|--------------|---------|-------|
| STRUCTURAL CONCRETE | CLASS AA(AE) | 101 | CU YD |

| REVISIONS | DATE | BY | QUANTITY |
|-----------|-------|-----|----------|
| 1 | 03/24 | AUB | |

ONE INCH AT FULL SCALE IF NOTED OTHERWISE

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB

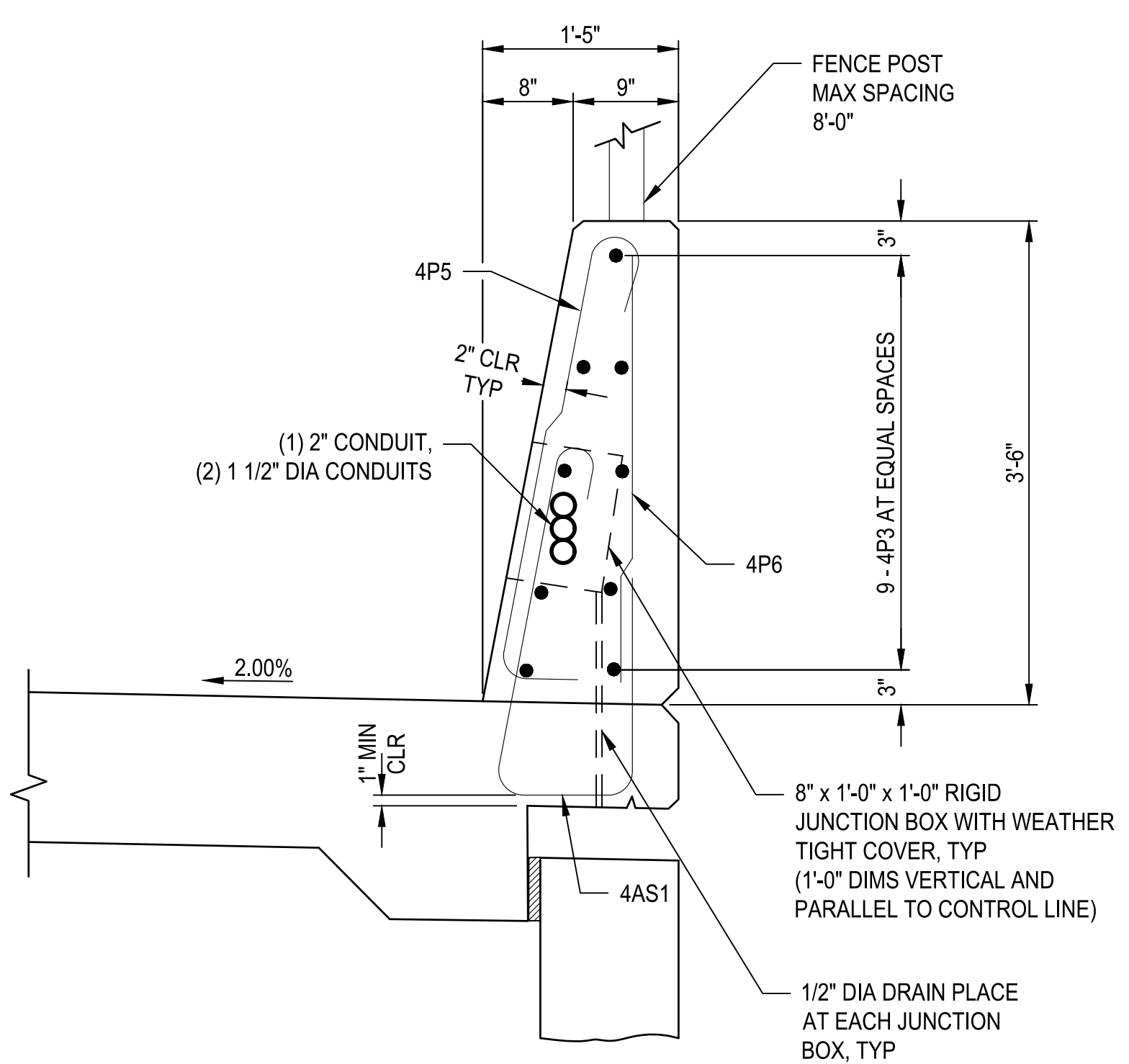
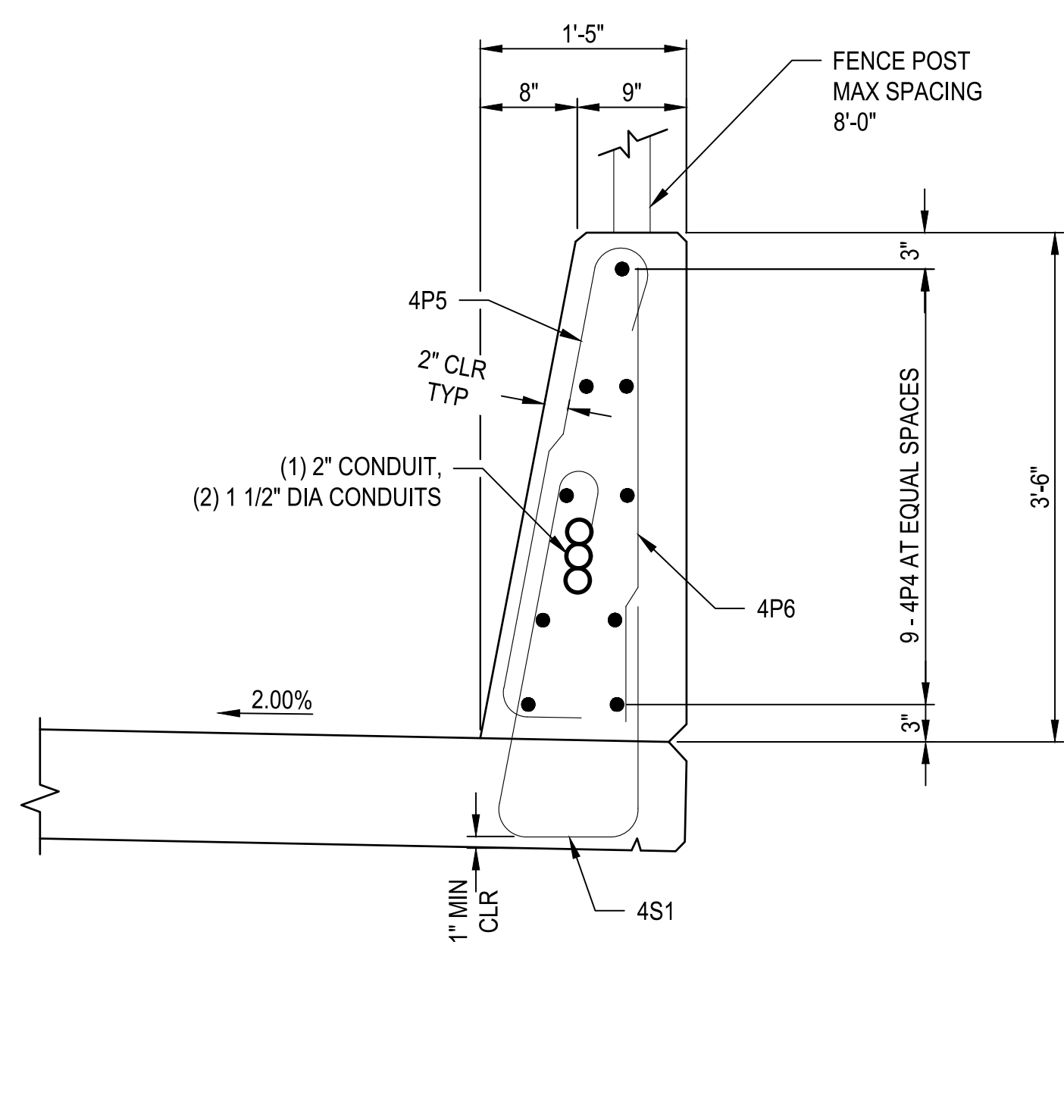
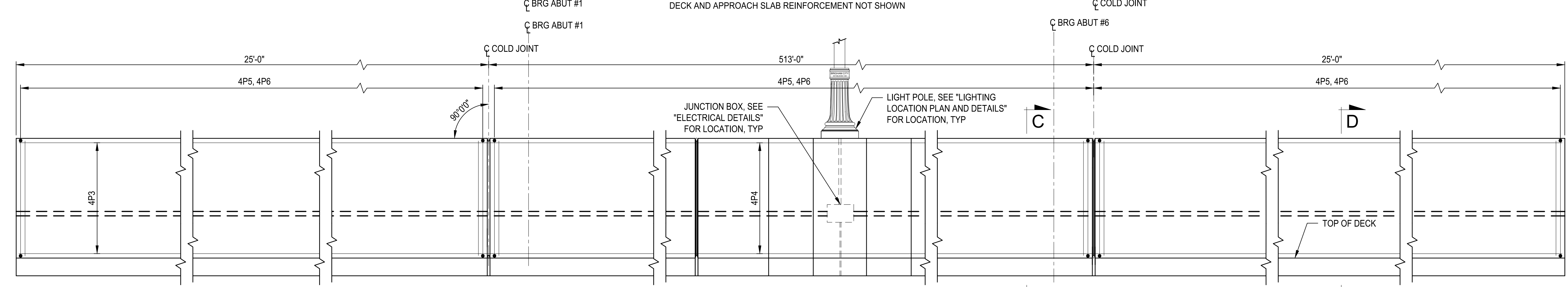
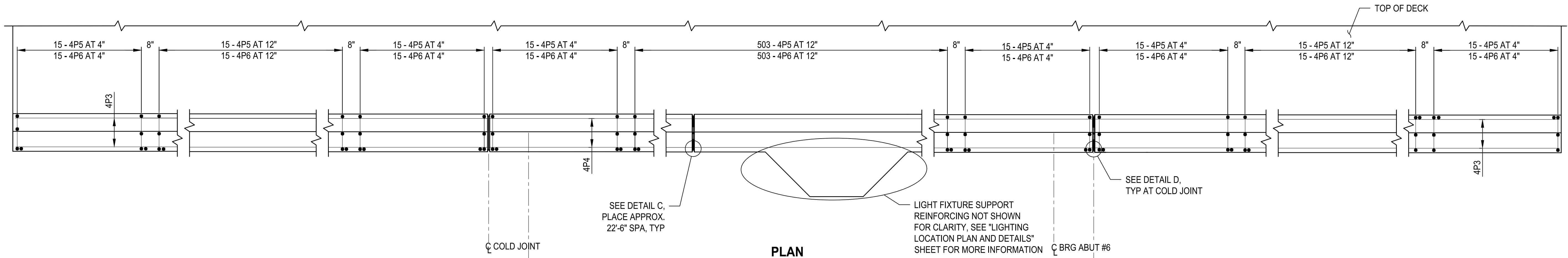


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

42-INCH SINGLE SLOPE PEDESTRIAN PARAPET W/ SIDEWALK

DRAWING NO. 51 OF 59
S51

PATH: U:\S01\Projects\Clients\8541-Brigham City\344-8541-002-Forest St Final Design\995sves\CADD\DWG\Structure
 PLOTTED BY: OrlwaSto DATE: Friday, March 22, 2024 11:27:21 AM
 LAYOUT: PARAPET



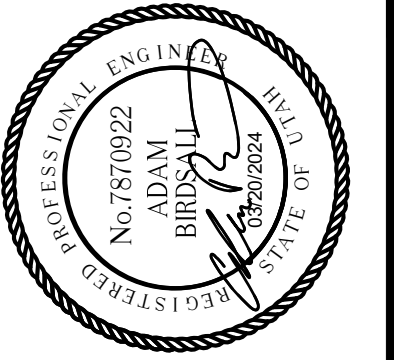
- NOTES**
1. ALTERNATE ALL REINFORCING SPLICES.
 2. PROVIDE 2 INCH MIN COVER TO REINFORCING UNLESS NOTED OTHERWISE.
 3. PLACE CONTRACTION JOINT ON SIDES AND ON TOP OF PARAPET.
 4. EXTEND SEALANT AND FOAM BACKER ROD FROM DECK TO TOP OF PARAPET ON THE INSIDE PARAPET FACE AND ACROSS TOP OF PARAPET.
 5. LOCATE STRUCTURE NUMBER ON RIGHT-HAND SIDE OF APPROACH PARAPET. SEE "PARAPET END DETAILS" FOR STRUCTURE NUMBER DETAILS.
 6. PARAPET MEETS TL-4 REQUIREMENTS.
 7. PROVIDE DETAIL D JOINT, PERPENDICULAR TO EDGE OF DECK, ABOVE BENT CENTERLINES AT BENTS 2, 3, AND 5.
 8. CONTINUE JOINTS THROUGH SIDEWALK WHERE THEY OCCUR IN THE PARAPET.
 9. SEE "42-INCH SINGLE SLOPE PEDESTRIAN PARAPET W/ SIDEWALK" FOR DETAIL A.

| ITEM | LOCATION | EST QTY | UNIT |
|---------------------|--------------|---------|-------|
| STRUCTURAL CONCRETE | CLASS AA(AE) | 82 | CU YD |

| REVISIONS | DATE | BY | DESCRIPTION |
|-----------|-------|-----|---------------------------|
| 1 | 03/24 | AUB | UPDATED CONCRETE QUANTITY |

ONE INCH AT FULL SCALE IF NOTED OTHERWISE ACCORDINGLY

| | | |
|-----------------------|---------------|---------------|
| DATE: 03/20/2024 | DESIGNED: TWP | CHECKED: NICC |
| JOB No.: 344-8541-002 | DRAWN: SLO | APPROVED: AUB |



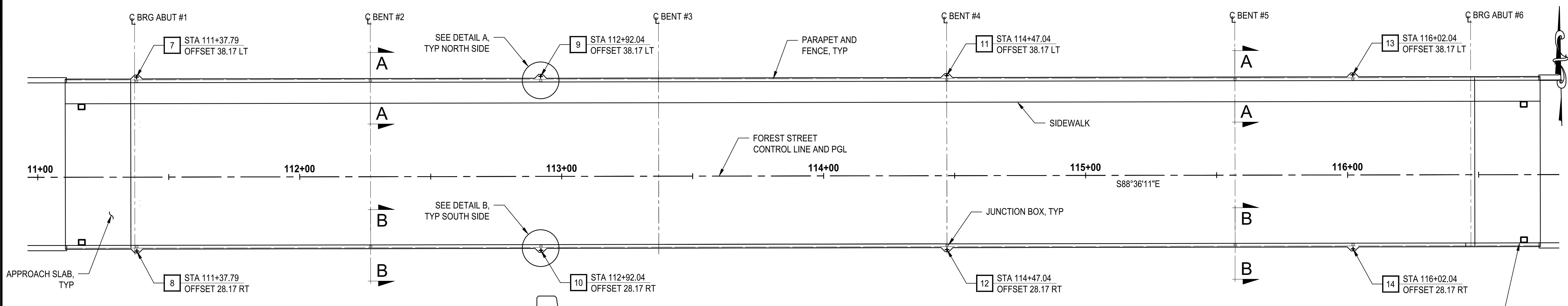
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

42-INCH SINGLE SLOPE PEDESTRIAN PARAPET

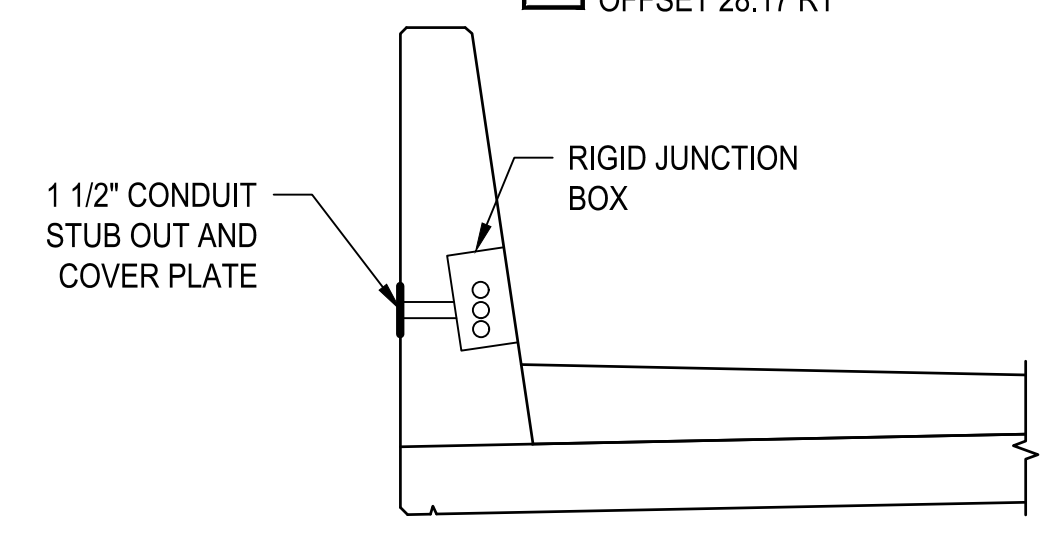
DRAWING NO. 52 OF 59

S52

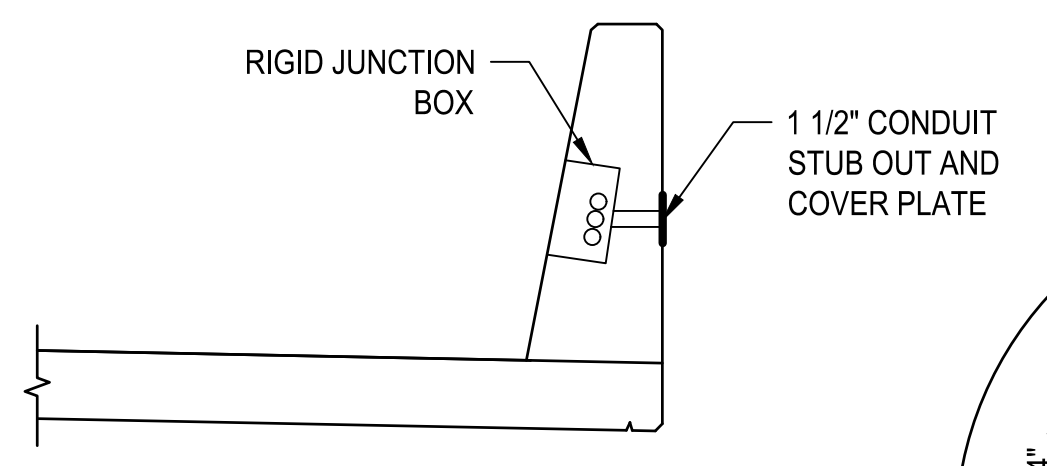
PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:54:44 AM
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 LAYOUT: LT



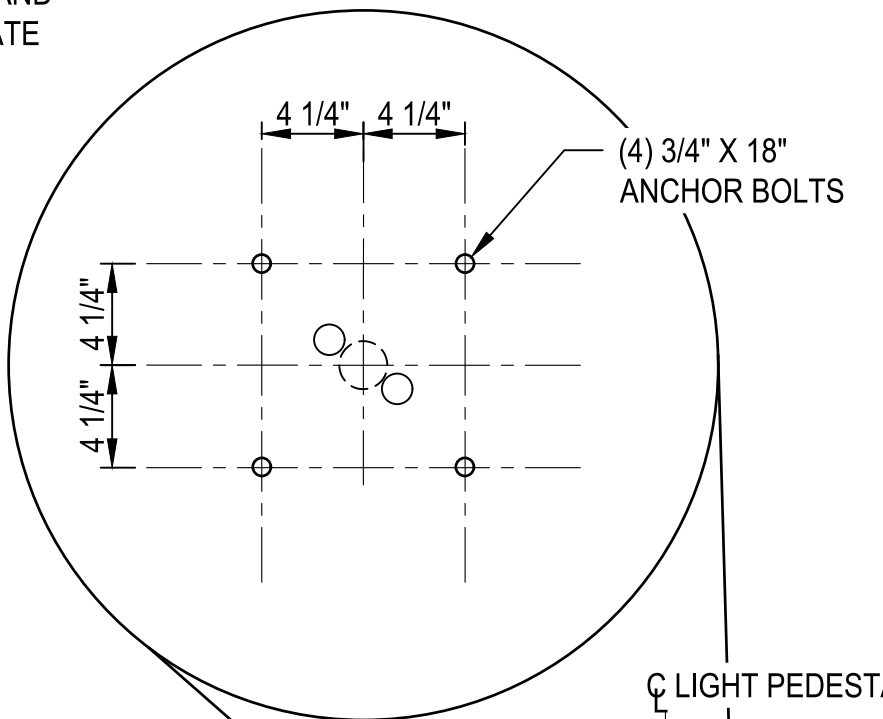
PLAN
SCALE 1" = 20'



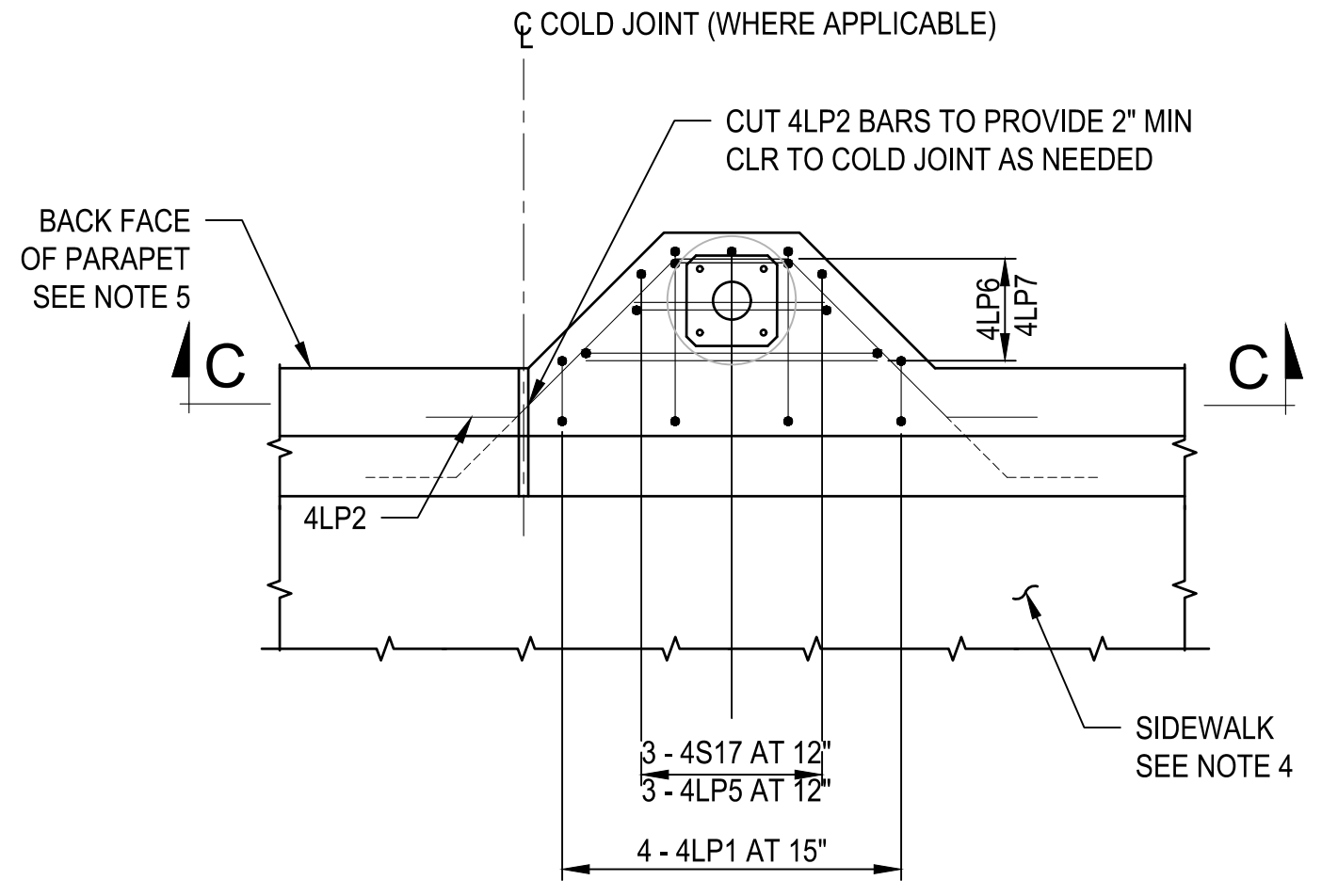
SECTION A-A



SECTION B-B

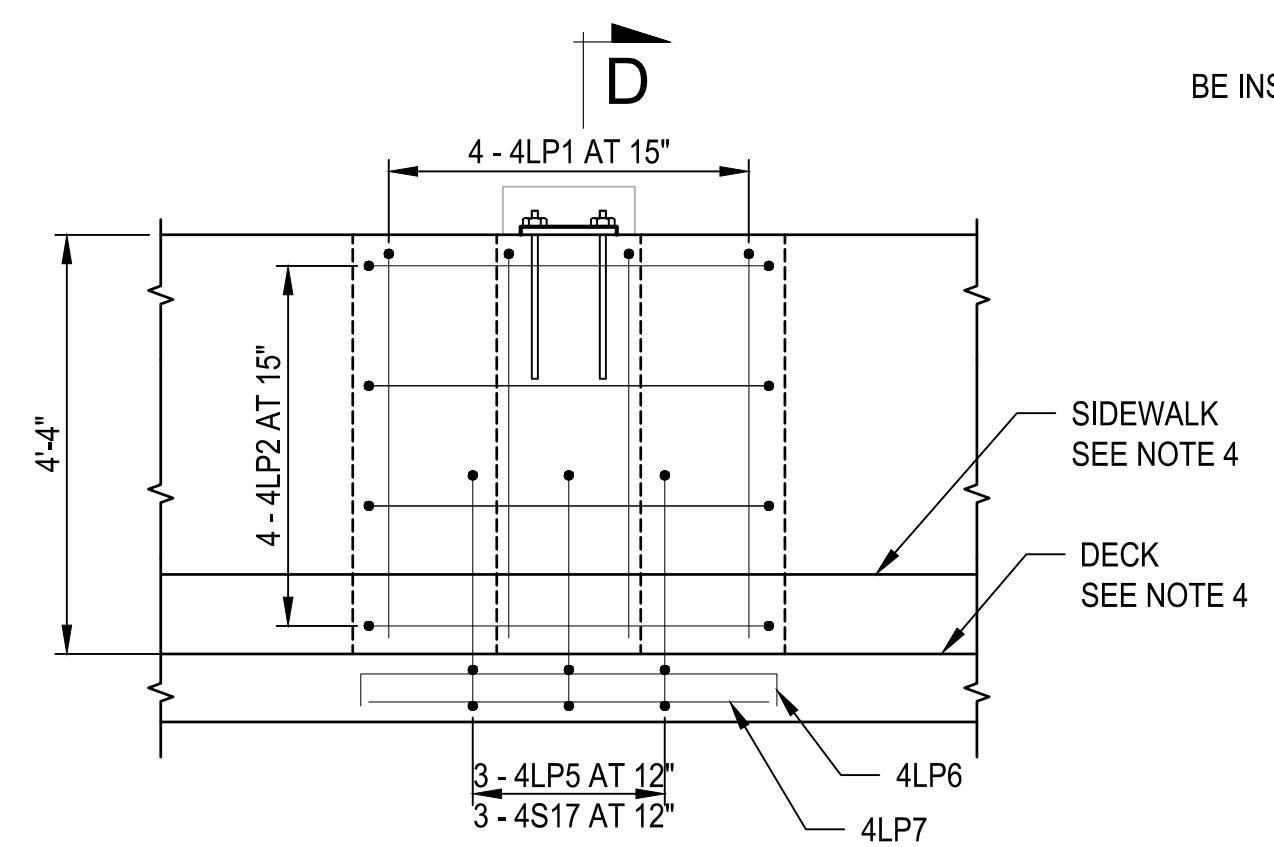


TYPICAL PLAN



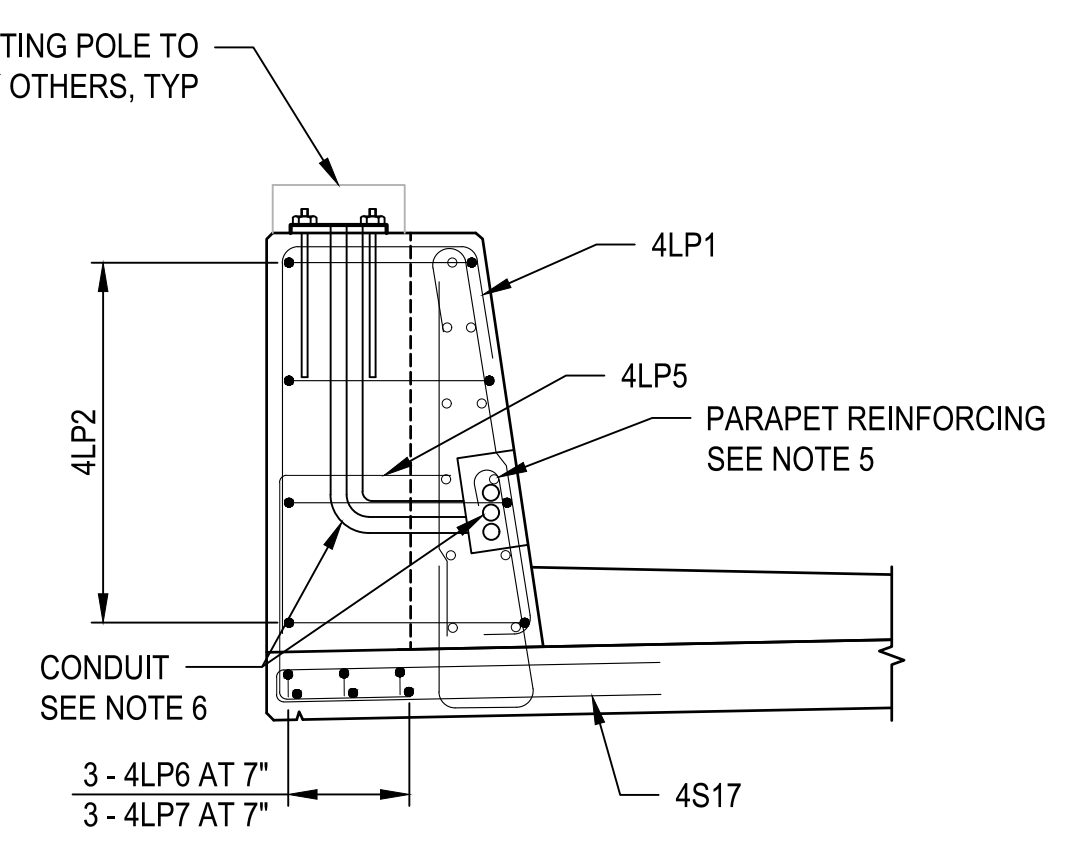
DETAIL A

CONDUIT, DECK, AND PARAPET REINFORCING NOT SHOWN FOR CLARITY



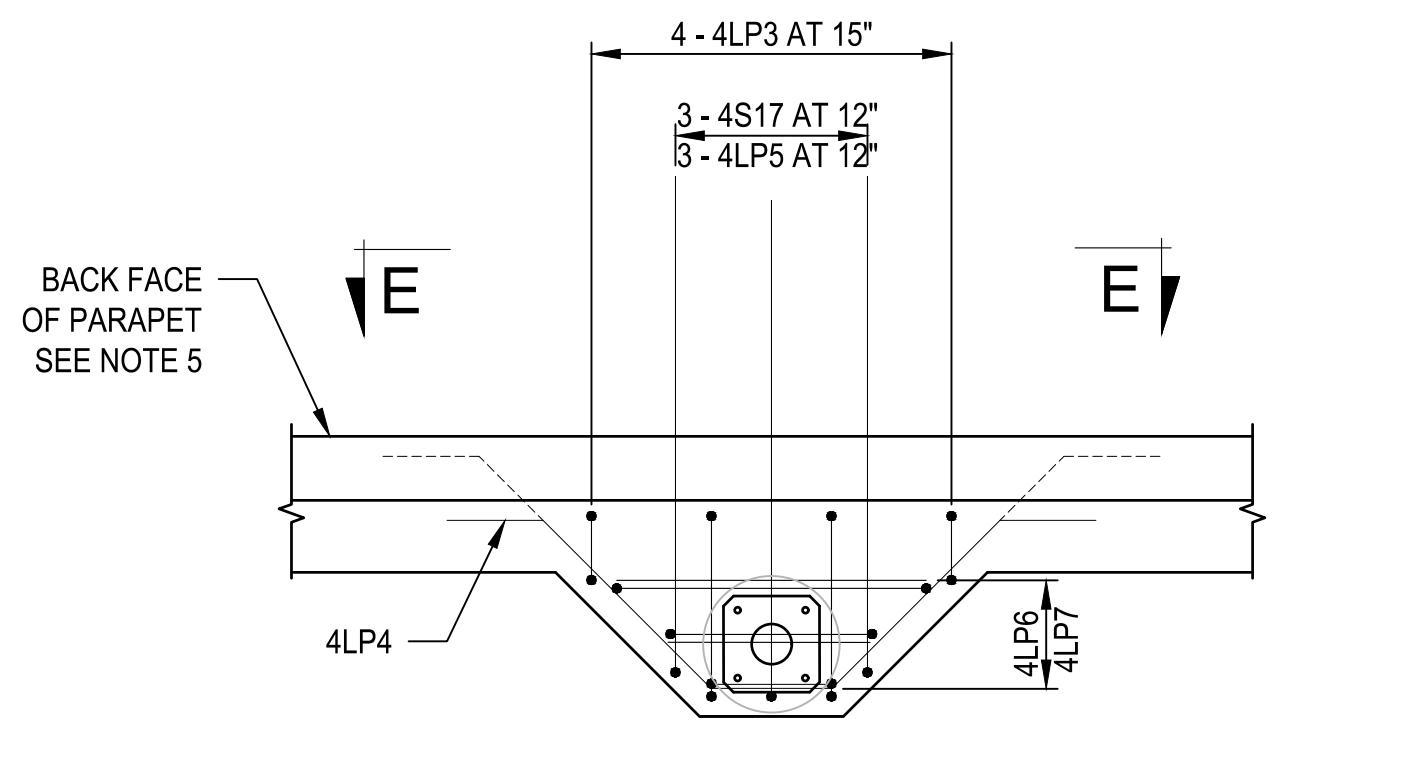
SECTION C-C

CONDUIT, DECK, AND PARAPET REINFORCING NOT SHOWN FOR CLARITY



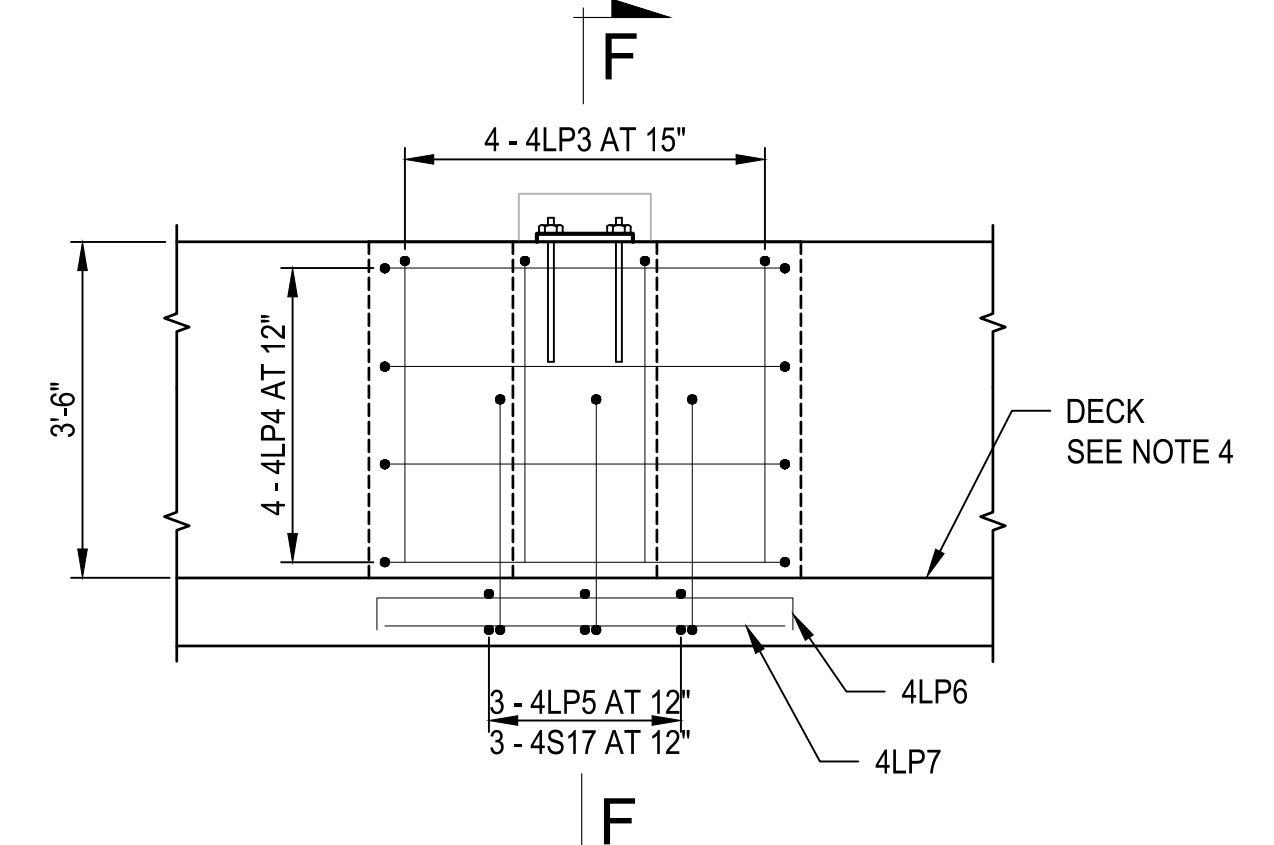
SECTION D-D

LOCATION REFERENCE POINT
SEE PLAN FOR STATION AND OFFSET



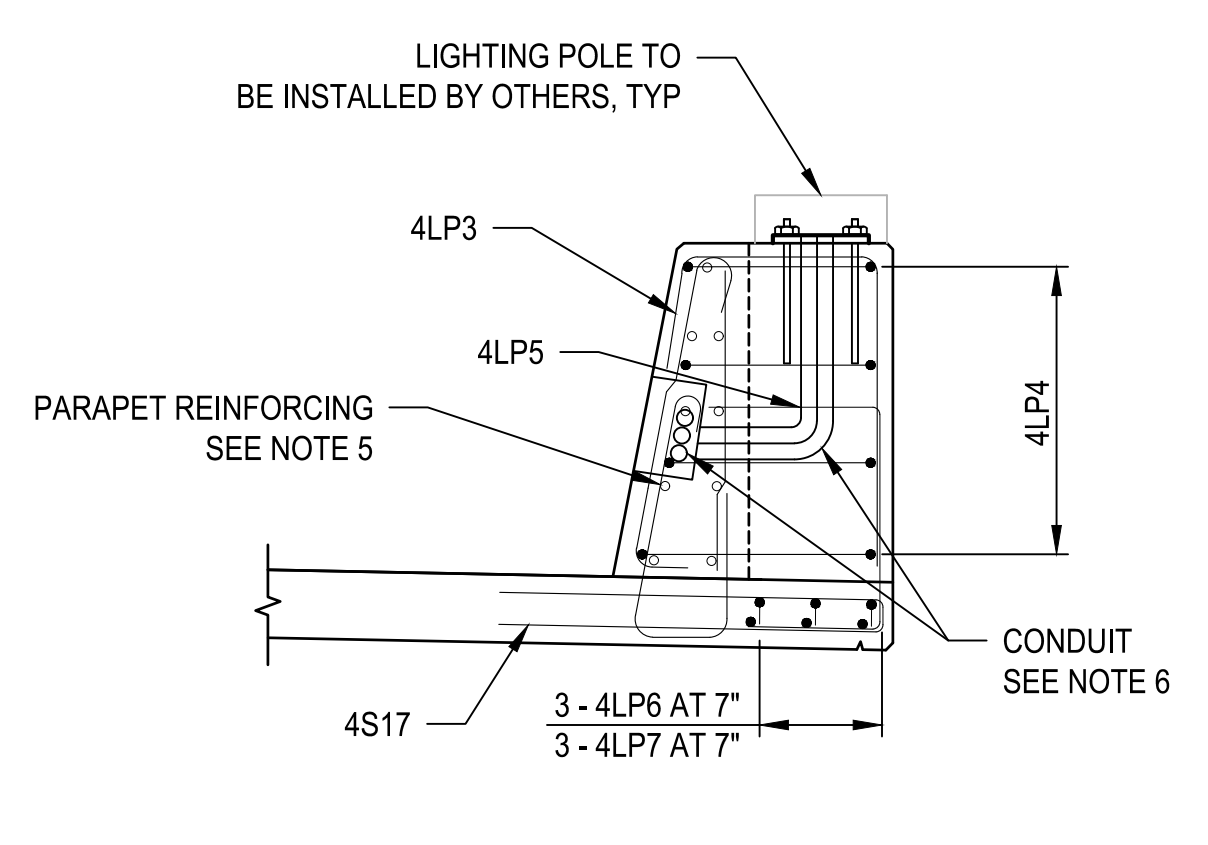
DETAIL B

CONDUIT, DECK, AND PARAPET REINFORCING NOT SHOWN FOR CLARITY



SECTION E-E

CONDUIT, DECK, AND PARAPET REINFORCING NOT SHOWN FOR CLARITY



SECTION F-F

NOTES

- ILLUMINATION FIXTURES, WIRING, AND OTHER APPURTENANCES RELATED TO LIGHTING SERVICE AND CONNECTION TO BE INSTALLED BY OTHERS.
- THE CONDUIT LAYOUT SHOWN IS SCHEMATIC. CONTRACTOR SHALL COORDINATE LAYOUT WITH BRIGHAM CITY PUBLIC POWER DIRECTOR PRIOR TO INSTALLATION.
- REFER TO APWA STANDARD PLANS AND SPECIFICATIONS FOR CONDUIT CONSTRUCTION DETAILS AND INSTALLATION REQUIREMENTS.
- SEE "DECK PLAN 1 OF 2", "DECK PLAN 2 OF 2", AND "DECK SECTION" SHEETS FOR DECK REINFORCING DETAILS.
- SEE "42-INCH SINGLE SLOPE PARAPET" AND "42-INCH SINGLE SLOPE PARAPET W/ SIDEWALK" SHEETS FOR PARAPET REINFORCING DETAILS.
- SEE "ELECTRICAL AND STRUCTURE NUMBER DETAILS" SHEET FOR ADDITIONAL CONDUIT AND JUNCTION BOX DETAILS.

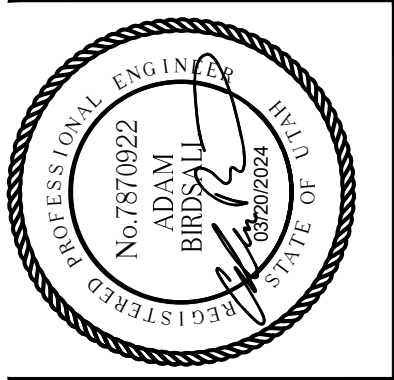
| NO. | DATE | BY | REVISIONS |
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| | | | |

ONE INCH AT FULL SCALE. IF NOT SCALE ACCORDINGLY.

Parametrix

DESIGNED: SLO
DRAWN: SLO
CHECKED: AJB
APPROVED: AJB

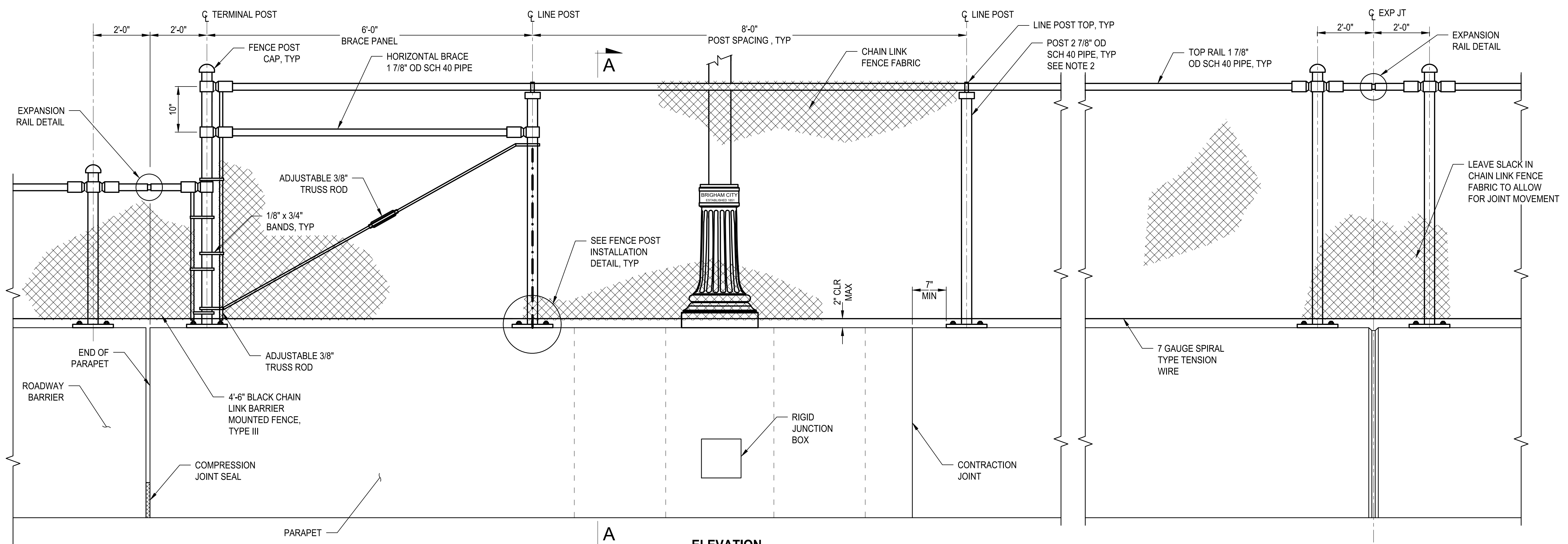
DATE: 03/20/2024
JOB No.: 344-8541-002



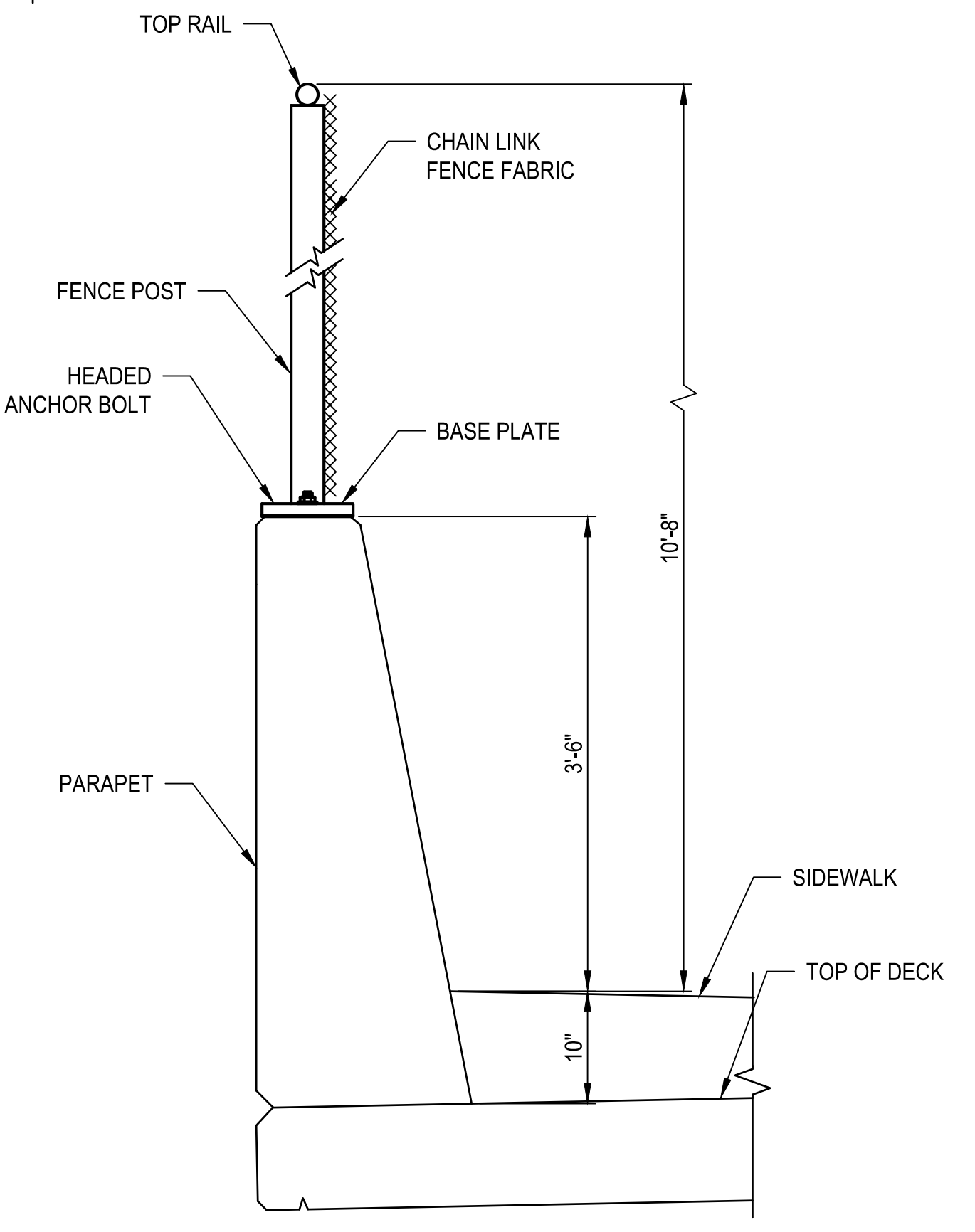
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

LIGHTING LOCATION PLAN AND DETAILS

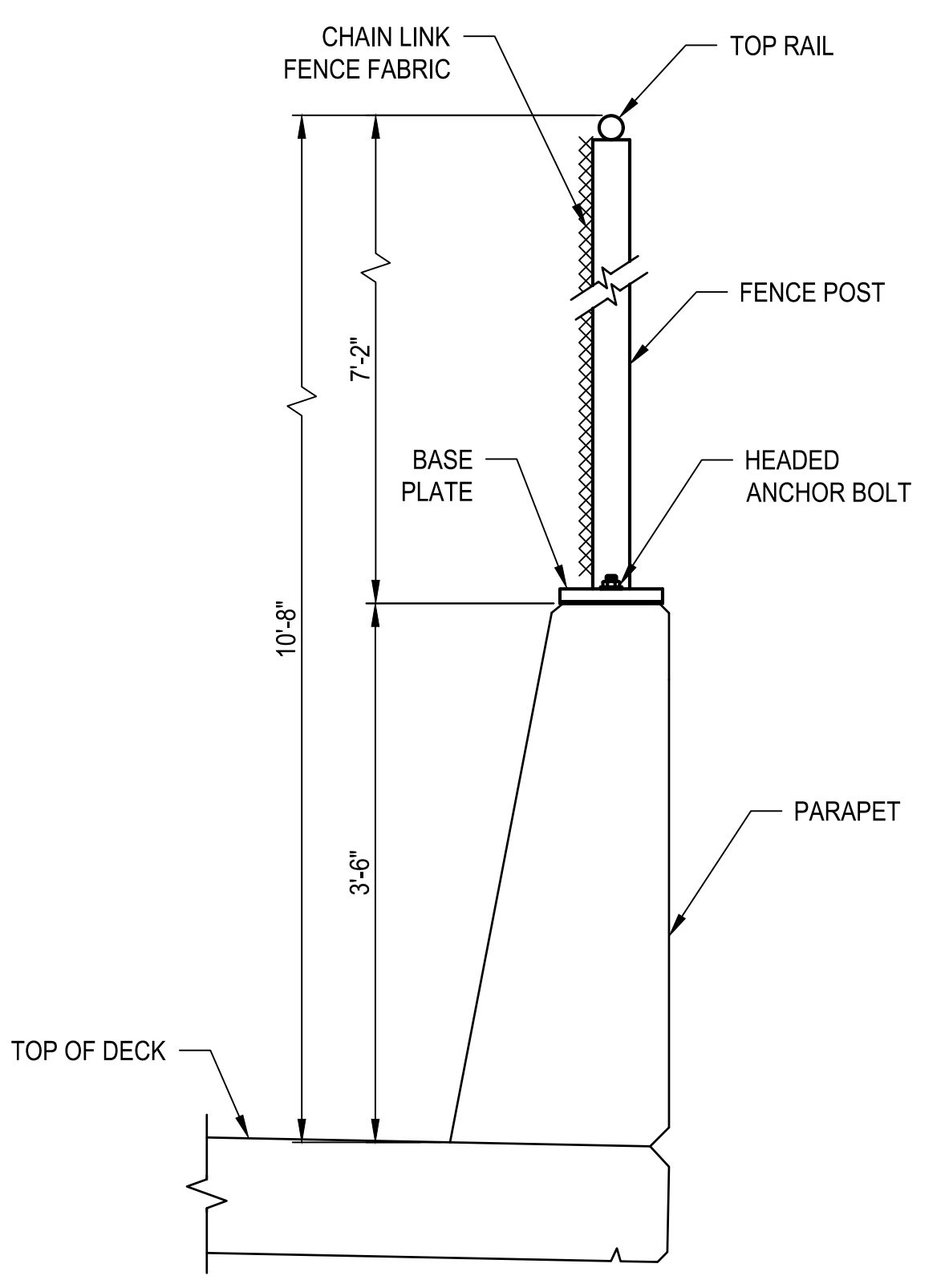
LAYOUT: Fence PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Sves\CADD\DWG\Structure PLOTTED BY: OliveSto DATE: Friday, March 22, 2024 9:55:12 AM



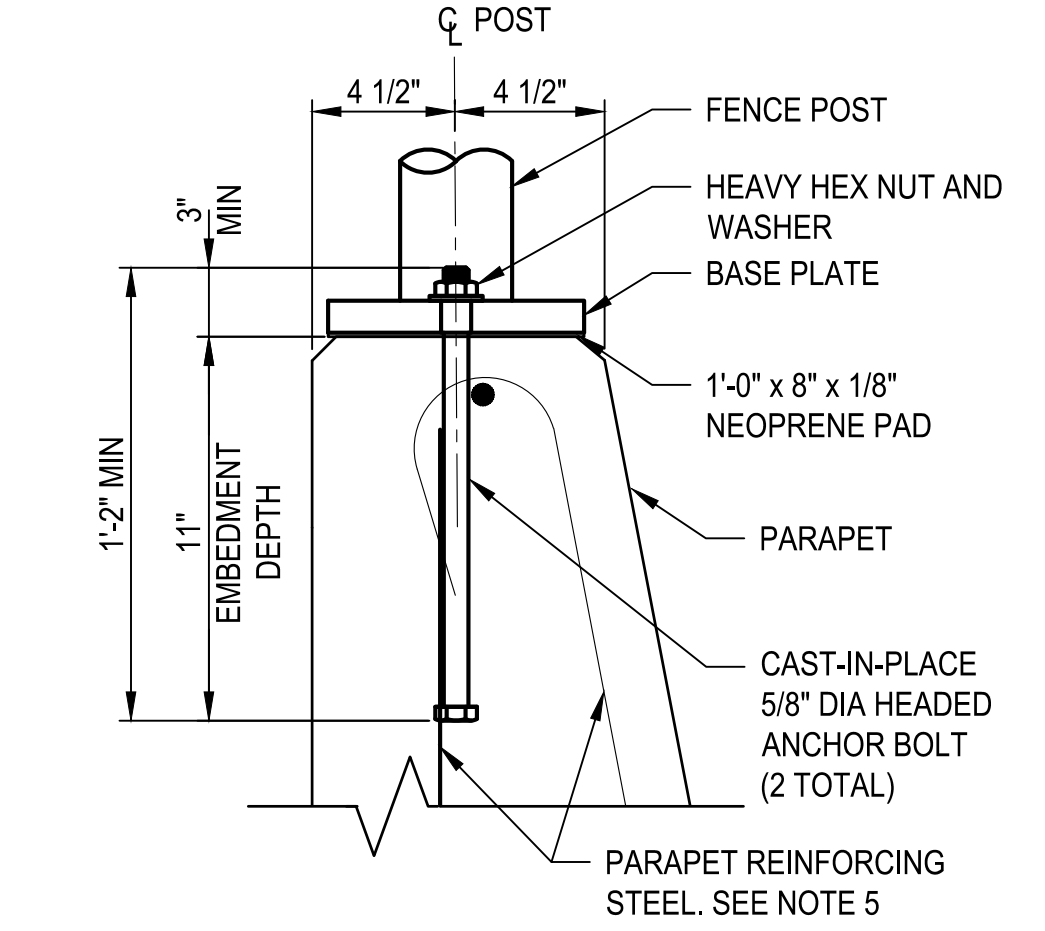
ELEVATION



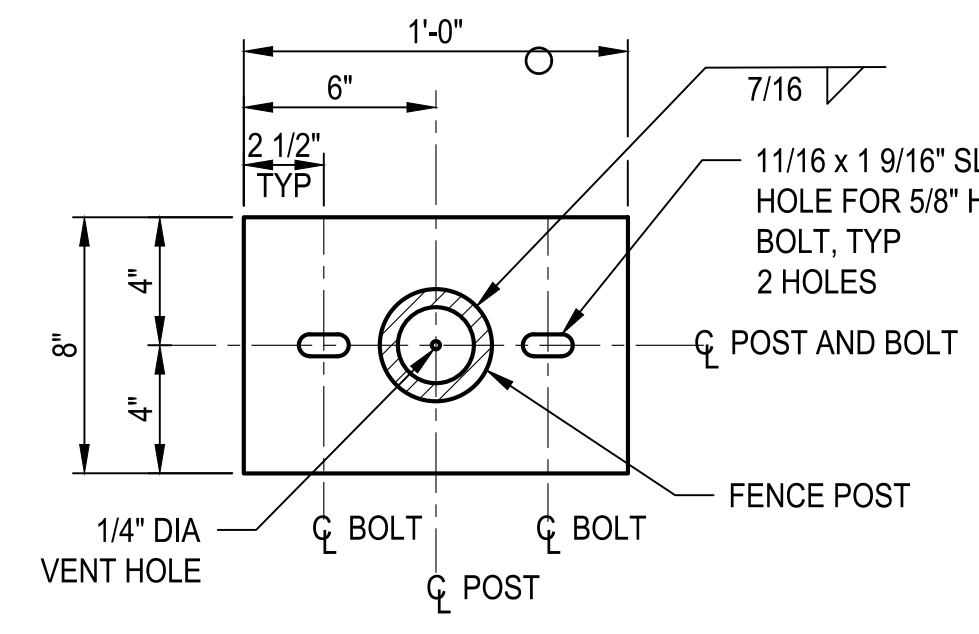
SECTION A-A
NORTH SIDE W/SIDEWALK



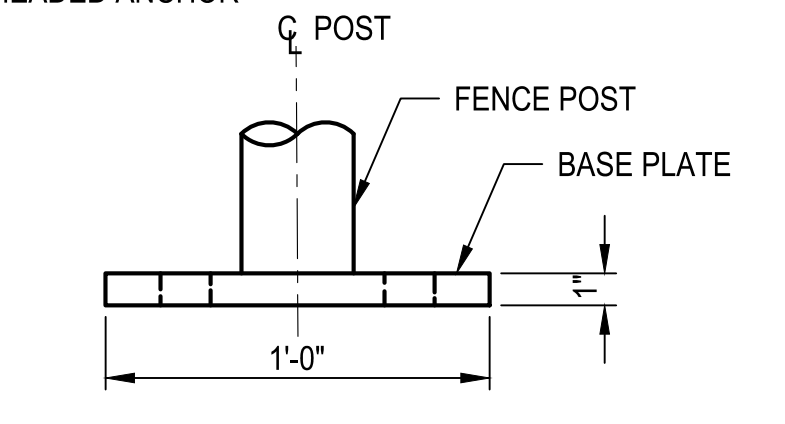
SECTION A-A
SOUTH SIDE NO SIDEWALK



FENCE POST INSTALLATION DETAIL



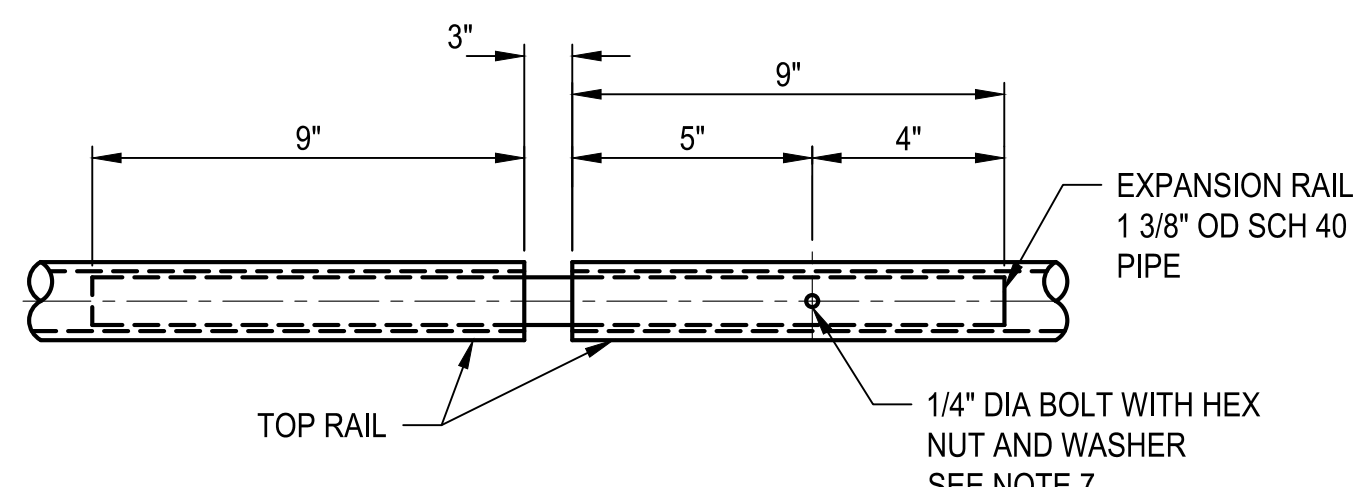
BASE PLATE PLAN



BASE PLATE ELEVATION

NOTES

1. PLACE FENCE FABRIC ON TRAFFIC SIDE OF POST.
2. FENCE POSTS ARE PLUMB.
3. FOR DETAILS NOT SHOWN SEE UDOT STD DWG FG 6.
4. CONTRACTOR IS RESPONSIBLE FOR LOCATION AND PLUMBNESS OF ANCHOR BOLTS.
5. ADJUST ANCHOR BOLT LOCATIONS TO AVOID PARAPET REINFORCING STEEL. MAINTAIN MAXIMUM POST SPACING.
6. LOCATE TERMINAL POSTS AT DISCONTINUOUS ENDS OF FENCE AND AT LOCATIONS OF DISCRETE ANGLE CHANGE GREATER THAN 15° HORIZONTALLY OR VERTICALLY. PROVIDE BRACE PANELS ON BOTH SIDES OF TERMINAL POST AS APPLICABLE.
7. INSTALL NUTS FOR EXPANSION RAILS FINGER-TIGHT. NUTS FULLY ENGAGE BOLT WITH A MINIMUM OF ONE BOLT THREAD EXTENDING BEYOND THE NUT. DISTORT THE FIRST THREAD ON THE OUTSIDE OF THE NUT TO PREVENT LOOSENING.
8. PAINT FENCE COMPONENTS AFTER FABRICATION. FENCE COLOR ACCORDING TO FEDERAL STANDARD COLOR SYSTEM NO. 27038.
9. FENCE POST CAN NOT BE INSTALLED IN FRONT OF LIGHT FIXTURE.

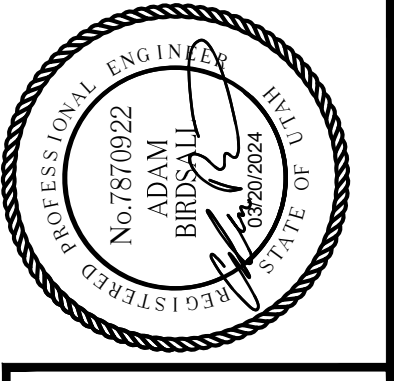


EXPANSION RAIL DETAIL

| | |
|-----------|--|
| BY | |
| DATE | |
| REVISIONS | |

ONE INCH
 AT FULL
 SCALE IF
 NOT
 ACCORDINGLY

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| DATE | 03/20/2024 | DESIGNED | TWP | CHECKED | NCC |
| JOB No. | 344-8541-002 | DRAWN | SLO | APPROVED | AJB |

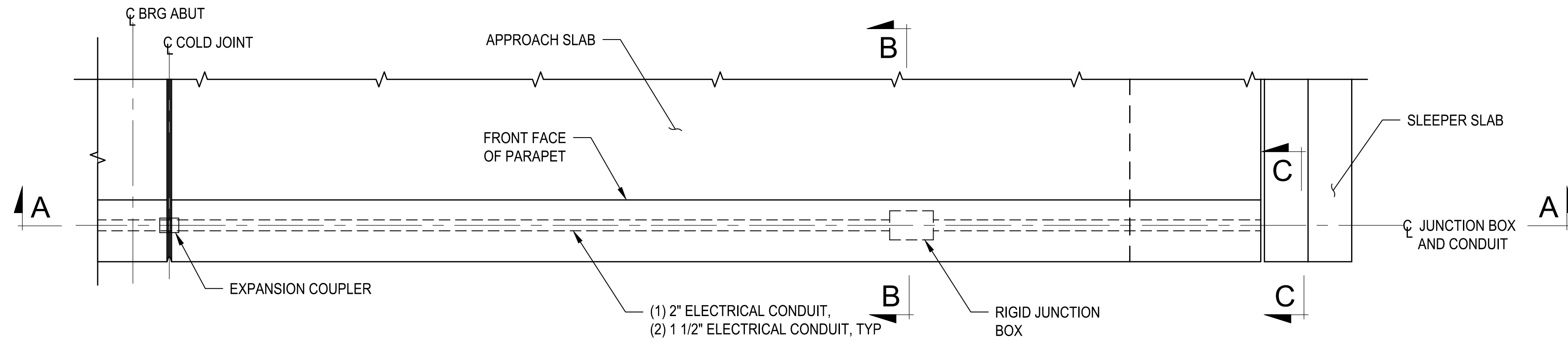


PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

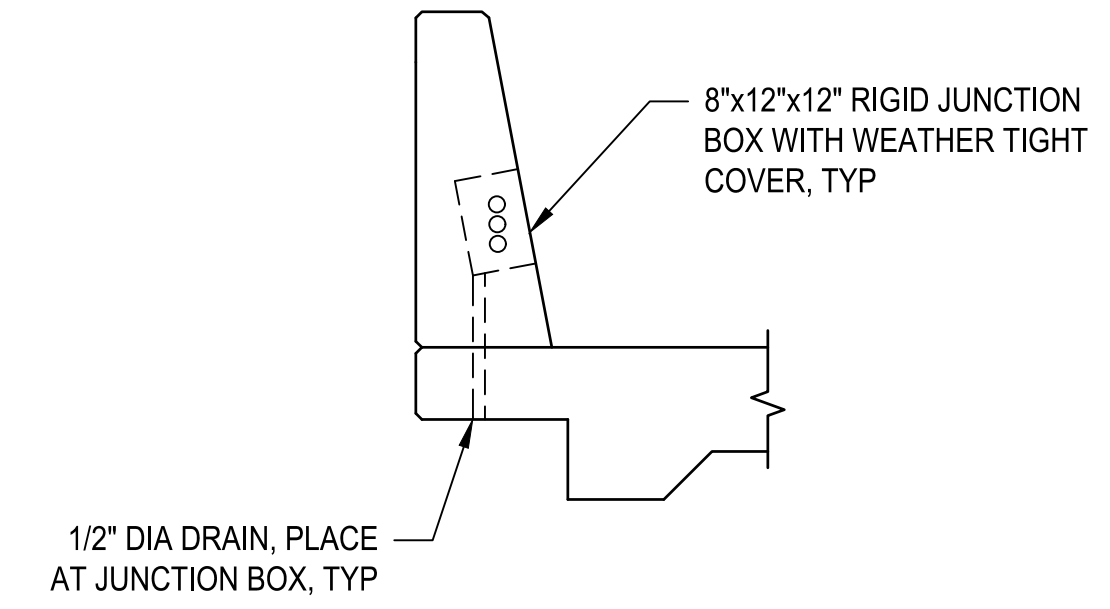
**CHAIN LINK FENCE
 ON STRUCTURE**

DRAWING NO. 54 OF 59

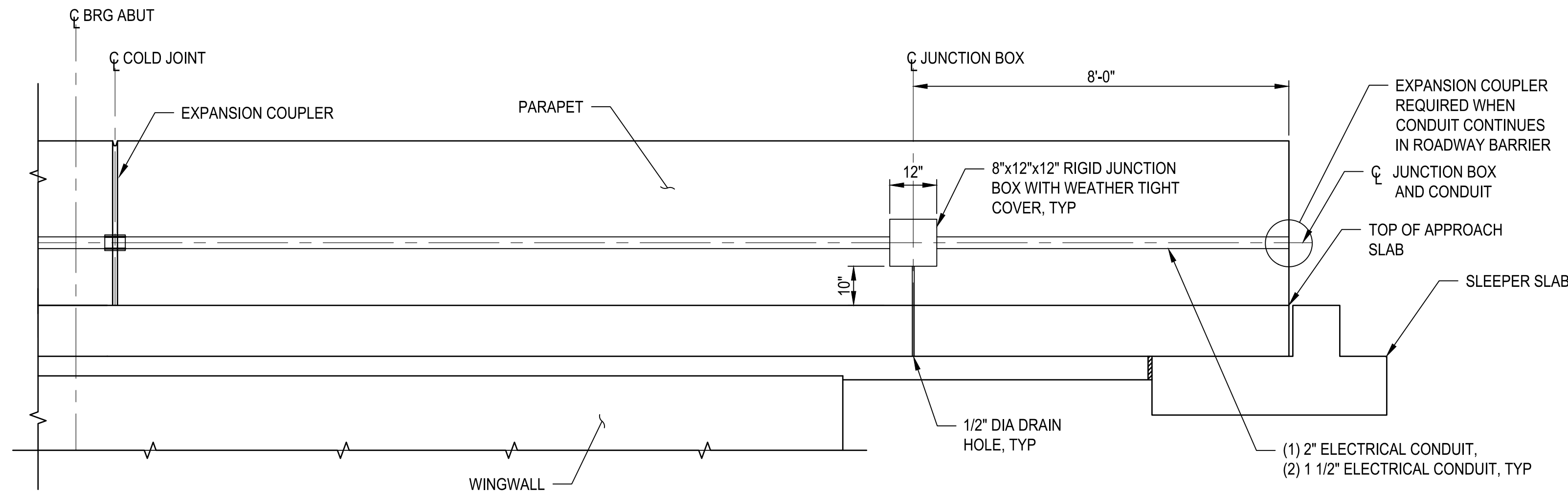
S54



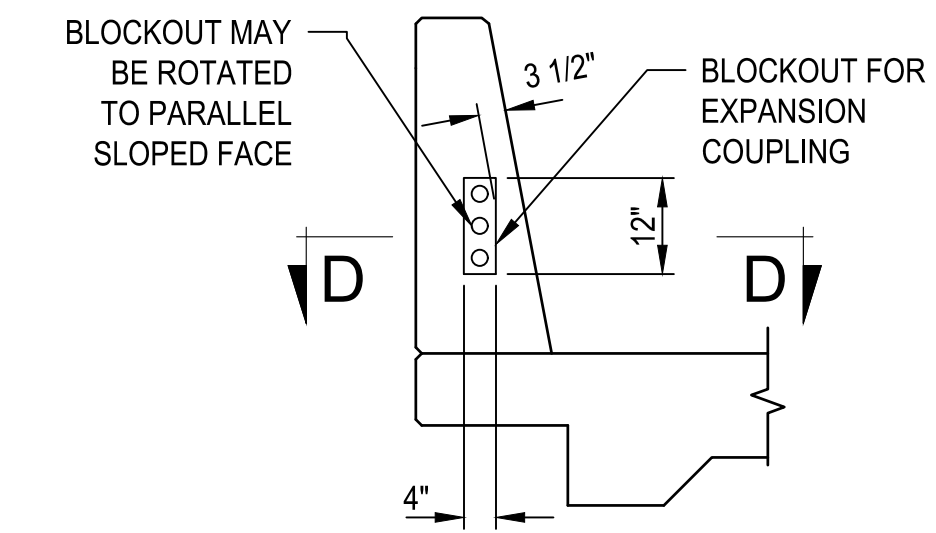
TYPICAL END - PLAN



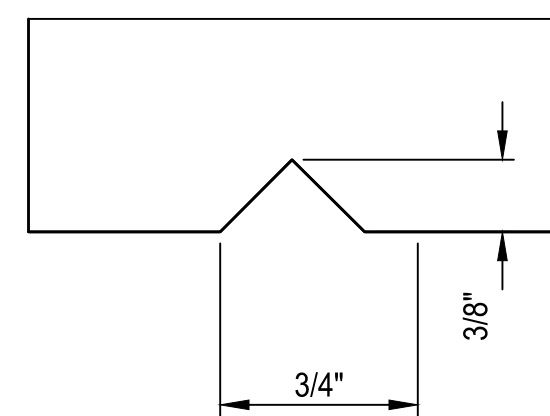
SECTION B-B



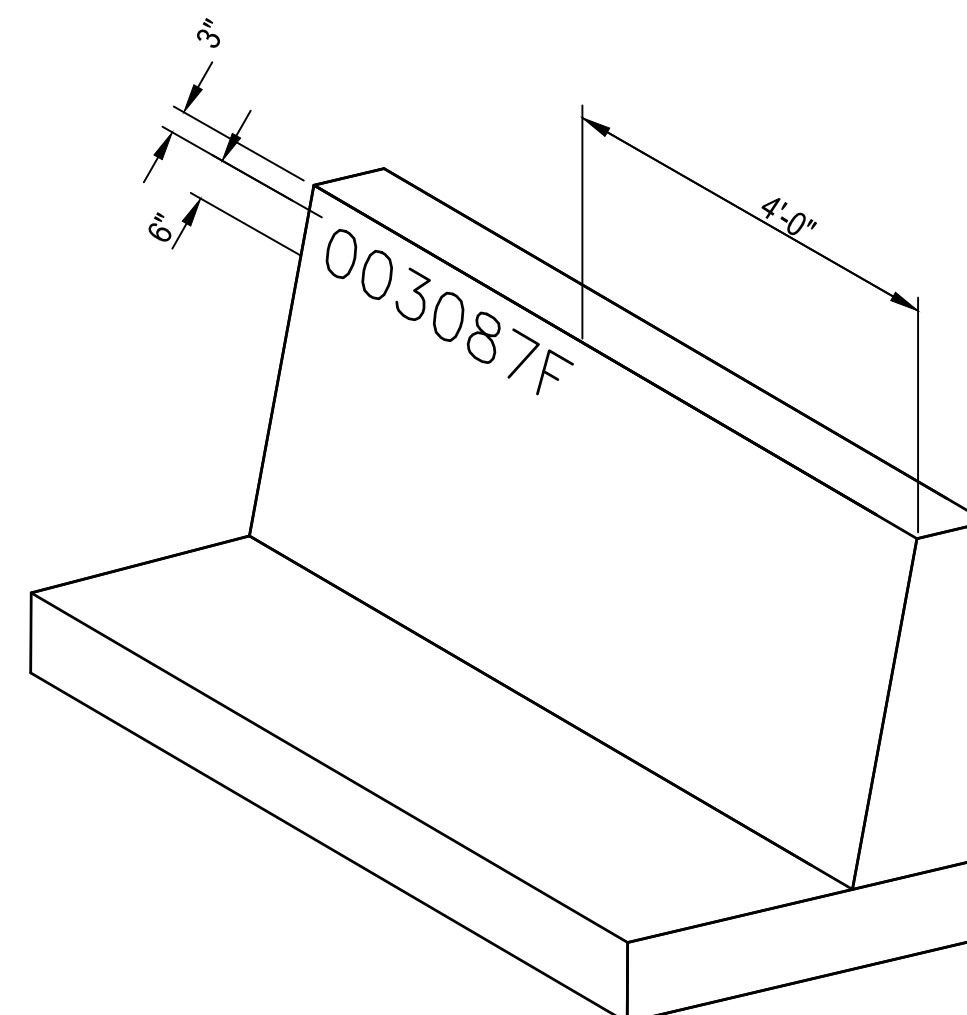
SECTION A-A



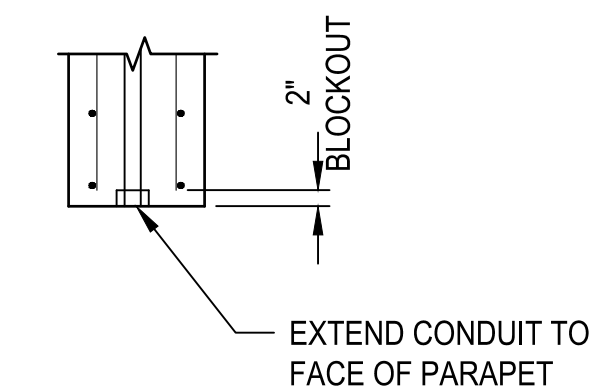
VIEW C-C



SECTION THRU STRUCTURE NUMBER



STANDARD END ISOMETRIC



SECTION D-D

NOTES

- LOCK ELECTRICAL CONDUIT AT JUNCTION BOXES WITH DOUBLE LOCK NUTS.
- PROVIDE ALL WORK CONFORMING WITH THE LATEST EDITIONS OF THE NATIONAL ELECTRICAL CODE AND UNDERWRITERS LABORATORIES INC. STANDARDS WHERE APPLICABLE.
- CUT OR BEND REBAR IN OUTSIDE FACE OF PARAPET TO ALLOW INSTALLATION OF JUNCTION BOX ON VERTICAL FACE OF PARAPET. FIELD ADJUST AS REQUIRED SO CONDUITS DO NOT ENCR OACH INTO WINGWALL.

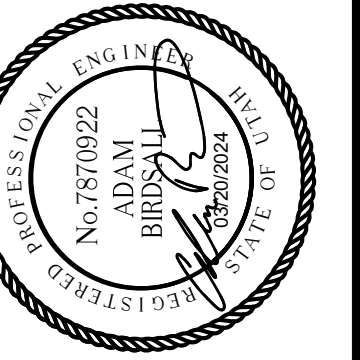
LAYOUT: ELEC DET PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:55:20 AM

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

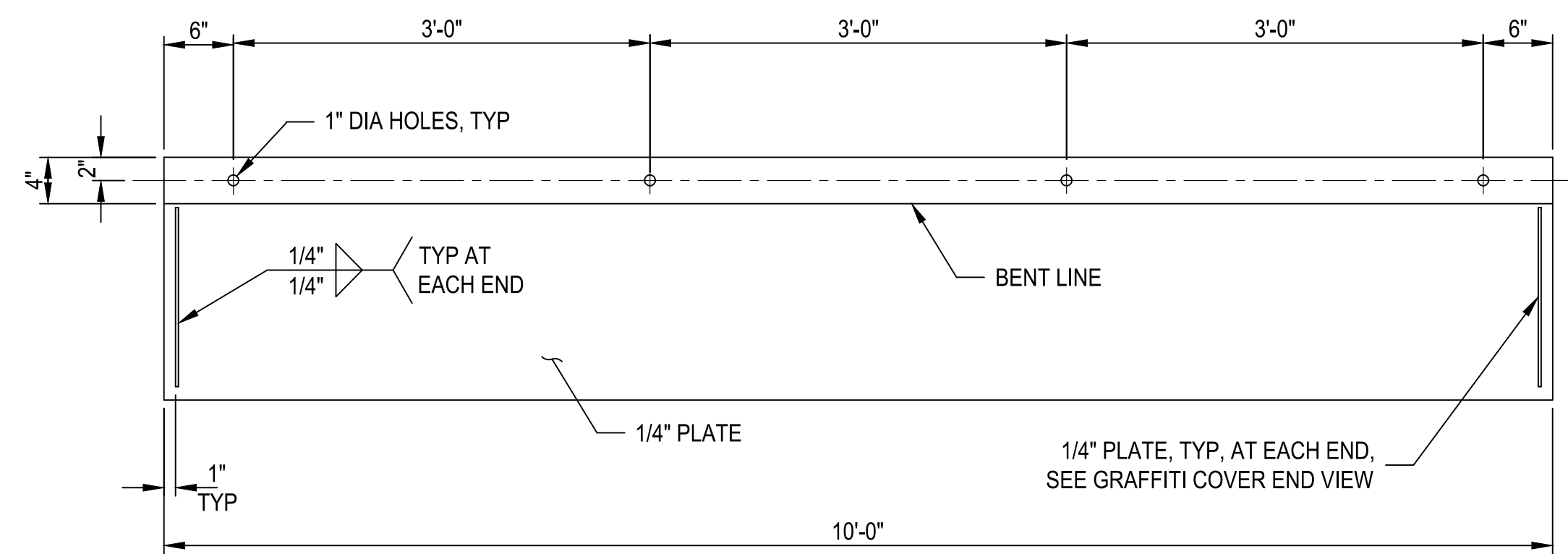
DATE: 03/20/2024 DESIGNED: TWP DRAWN: SLO
 JOB No.: 344-8541-002 CHECKED: NICC APPROVED: AUB



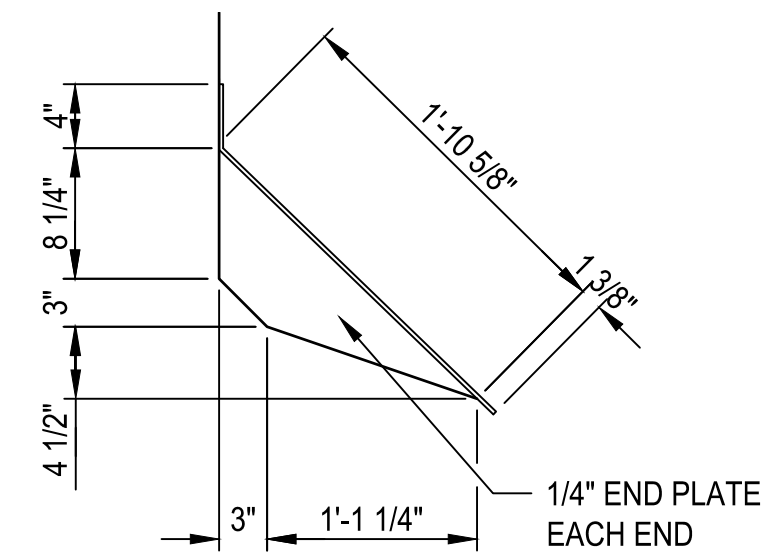
PROJECT NAME: BRIGHAM CITY CONNECTION PROJECT

ELECTRICAL AND STRUCTURE NUMBER DETAILS

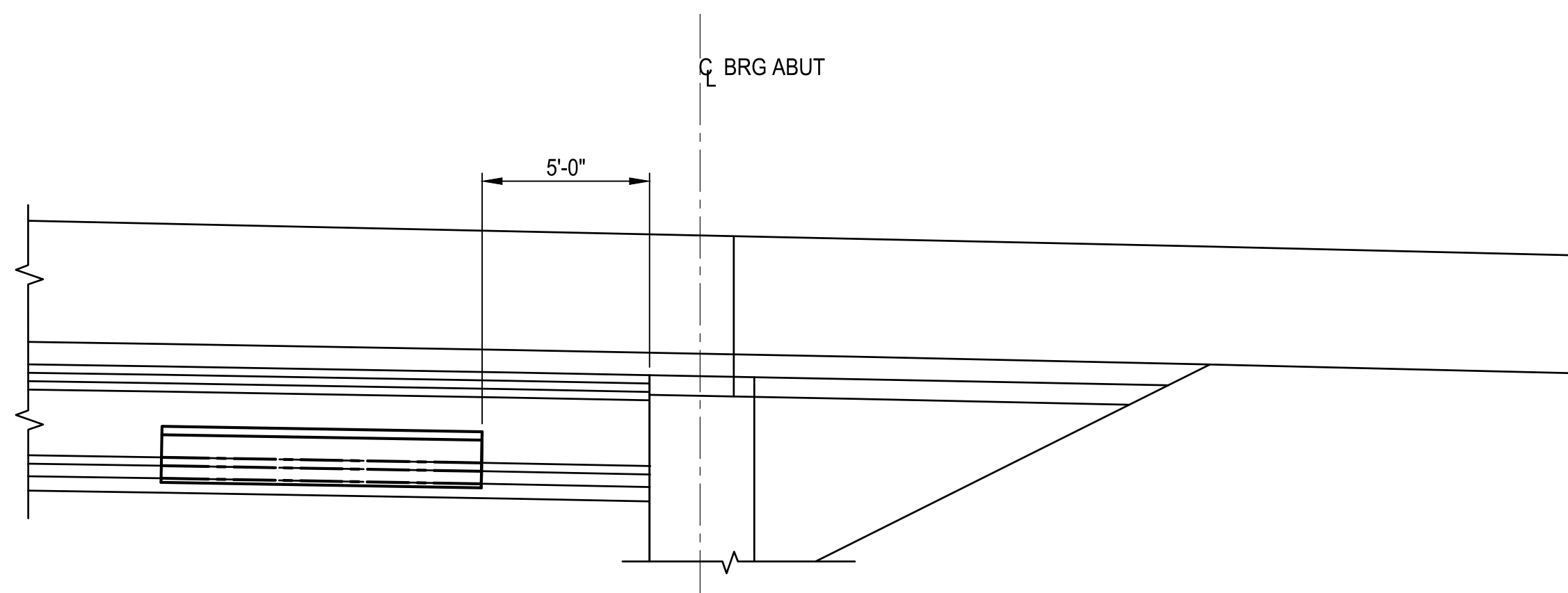
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GRAFFITI COVER ELEVATION

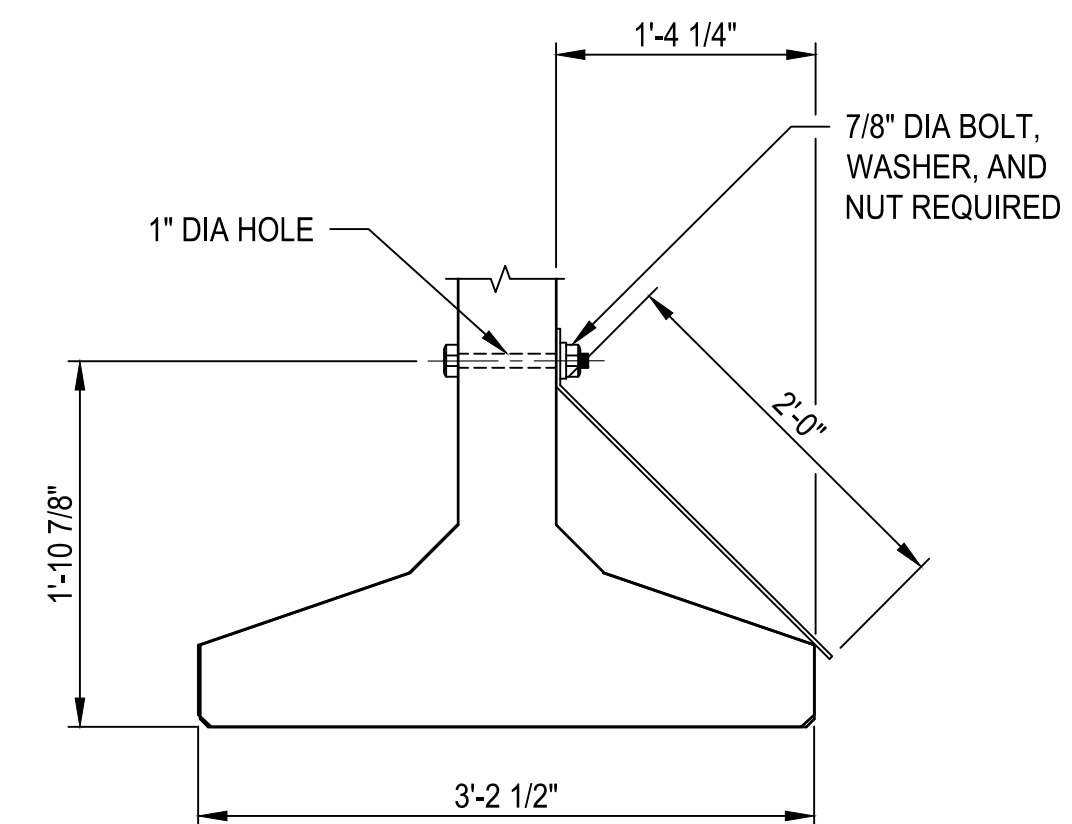


GRAFFITI COVER END VIEW



GRAFFITI COVER LOCATION

PLACE GRAFFITI COVERS ON BOTH ENDS OF EXTERIOR FACE OF EXTERIOR GIRDERS ONLY

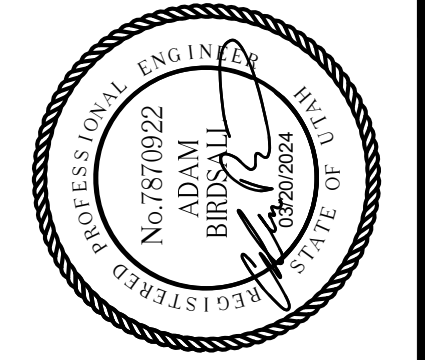


GRAFFITI COVER SECTION

| REVISIONS | DATE | BY |
|-----------|------|----|
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| | | |

ONE INCH AT FULL SCALE, IF NOT OTHERWISE ACCORDINGLY

| | | |
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| Parametrix | CHECKED | NICC |
| | DESIGNED | TWP |
| DATE | 03/20/2024 | DRAWN |
| JOB No. | 344-8541-002 | APPROVED |
| | | AJB |



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

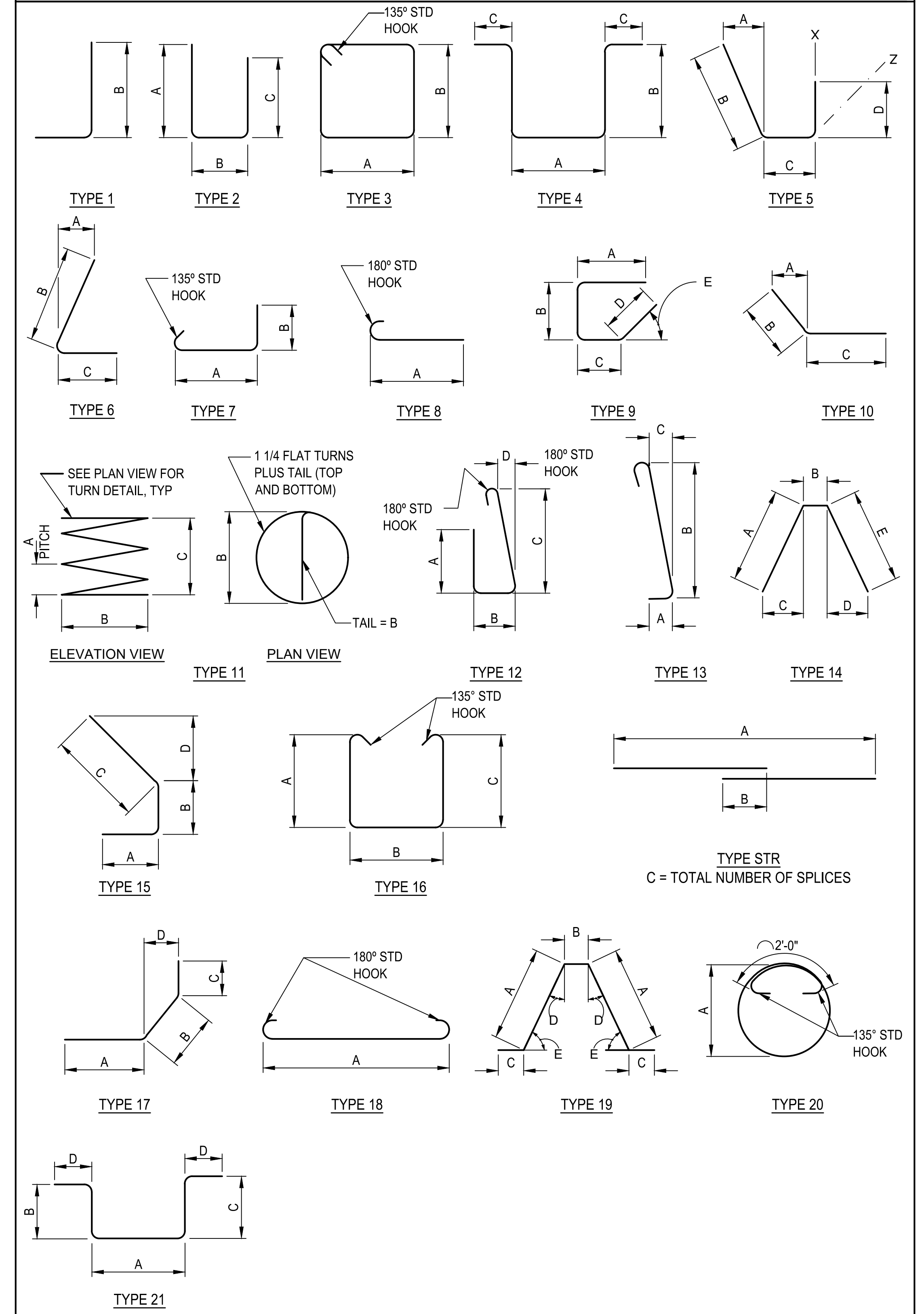
GRAFFITI COVER

DRAWING NO.
56 OF 59
S56

PATH: U:\Soil\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:55:35 AM LAYOUT: REIN SCH

| MARK | LOCATION | SIZE | NO. BARS | TYPE | LENGTH | TOTAL LENGTH | DIMENSIONS | | | | | SERIES INC. | REMARKS |
|------|----------------|------|----------|------|---------|--------------|------------|-----------|-----------|-----------|-----------|-------------|---------|
| | | | | | | | A | B | C | D | E | | |
| AS1 | APPROACH SLAB | 4 | 90 | 12 | 5'-9" | 514'-6" | 1'-6" | 1'-0" | 2'-8" | 0'-6 1/4" | | | |
| AS2 | APPROACH SLAB | 10 | 260 | STR. | 24'-8" | 6413'-4" | 24'-8" | | | | | | |
| AS3 | APPROACH SLAB | 6 | 68 | STR. | 65'-1" | 4425'-8" | 62'-3" | 2'-10" | 1 | | | | |
| AS4 | APPROACH SLAB | 4 | 50 | STR. | 67'-4" | 3366'-8" | 64'-6" | 2'-10" | 1 | | | | |
| AS5 | APPROACH SLAB | 4 | 138 | STR. | 24'-8" | 3404'-0" | 24'-8" | | | | | | |
| AS6 | NOT USED | | | | | | | | | | | | |
| AS7 | APPROACH SLAB | 5 | 88 | 9 | 3'-8" | 322'-8" | 1'-0" | 1'-0" | 0'-8" | 1'-0" | 45° | | |
| AS8 | APPROACH SLAB | 5 | 216 | 8 | 5'-5" | 1170'-0" | 4'-10" | | | | | | |
| AS9 | APPROACH SLAB | 5 | 8 | STR. | 21'-10" | 174'-8" | 21'-10" | | | | | | |
| AS10 | APPROACH SLAB | 4 | 90 | 12 | 7'-6" | 678'-1" | 2'-4" | 1'-2" | 3'-6" | 0'-6 1/4" | | | |
| SS1 | SLEEPER SLAB | 4 | 260 | STR. | 4'-6" | 1170'-0" | 4'-6" | | | | | | |
| SS2 | SLEEPER SLAB | 4 | 28 | STR. | 67'-4" | 1885'-4" | 64'-6" | 2'-10" | 1 | | | | |
| SS3 | SLEEPER SLAB | 4 | 130 | 2 | 4'-6" | 585'-0" | 1'-11" | 0'-8" | 1'-11" | | | | |
| CB1 | CATCH BASIN | 5 | 8 | STR. | 3'-2" | 25'-4" | 3'-2" | | | | | | |
| CB2 | CATCH BASIN | 5 | 24 | 4 | 13'-1" | 313'-6" | 2'-10 3/4" | 4'-1" | 1'-0" | | | | |
| CB3 | CATCH BASIN | 5 | 16 | 3 | 13'-5" | 214'-8" | 3'-1 1/4" | 3'-1 1/4" | | | | | |
| CB4 | CATCH BASIN | 4 | 8 | 2 | 4'-1" | 32'-8" | 1'-3" | 1'-7" | 1'-3" | | | | |
| S1 | DECK | 4 | 533 | 12 | 5'-8" | 3020'-4" | 1'-6" | 1'-0" | 2'-8" | | | A1035 CM | |
| S2 | DECK | 6 | 803 | STR. | 67'-4" | 54068'-8" | 64'-6" | 2'-10" | 1 | | | A1035 CS | |
| S3 | DECK | 6 | 944 | STR. | 4'-2" | 3933'-4" | 4'-2" | | | | | A1035 CM | |
| S4 | DECK | 4 | 769 | 8 | 5'-4" | 4101'-4" | 4'-10" | | | | | A1035 CS | |
| S5 | DECK | 5 | 118 | STR. | 5'-0" | 590'-0" | 5'-0" | | | | | A1035 CM | |
| S6 | DECK | 6 | 120 | STR. | 535'-4" | 64240'-0" | 512'-8" | 2'-10" | 8 | | | A1035 CS | |
| S7 | NOT USED | | | | | | | | | | | | |
| S8 | DECK | 6 | 72 | STR. | 8'-0" | 576'-0" | 8'-0" | | | | | A1035 CM | |
| S9 | DECK | 7 | 388 | STR. | 44'-0" | 17072'-0" | 44'-0" | | | | | A1035 CM | |
| S10 | DECK | 6 | 144 | STR. | 5'-8" | 816'-0" | 5'-8" | | | | | A1035 CM | |
| S11 | NOT USED | | | | | | | | | | | | |
| S12 | DECK | 4 | 533 | 12 | 7'-6" | 4015'-7" | 2'-4" | 1'-2" | 3'-6" | 0'-6 1/4" | | A1035 CM | |
| S13 | DECK | 4 | 8 | STR. | 528'-8" | 4229'-4" | 512'-8" | 2'-0" | 8 | | | A1035 CM | |
| S14 | DECK | 4 | 513 | STR. | 7'-8" | 3933'-0" | 7'-8" | | | | | A1035 CM | |
| S15 | DECK | 4 | 513 | 2 | 3'-1" | 1590'-4" | 1'-0" | 1'-1 1/4" | 1'-0" | | | A1035 CM | |
| S16 | DECK | 4 | 513 | 2 | 2'-8" | 1368'-0" | 1'-0" | 0'-8" | 1'-0" | | | A1035 CM | |
| S17 | DECK | 4 | 24 | 2 | 10'-9" | 257'-0" | 5'-2" | 0'-4 1/2" | 5'-2" | | | A1035 CM | |
| S18 | NOT USED | | | | | | | | | | | A1035 CM | |
| S19 | NOT USED | | | | | | | | | | | A1035 CM | |
| S20 | NOT USED | | | | | | | | | | | A1035 CM | |
| S21 | NOT USED | | | | | | | | | | | A1035 CM | |
| S22 | DECK | 4 | 630 | 21 | 4'-8" | 2940'-0" | 1'-6" | 0'-6 3/4" | 0'-7 1/4" | 1'-0" | | A1035 CS | |
| P1 | PARAPET | 4 | 623 | 13 | 4'-11" | 3088'-4" | 0'-5 3/4" | 3'-11" | 0'-8 1/8" | | | A1035 CM | |
| P2 | PARAPET | 4 | 623 | STR. | 3'-10" | 2388'-2" | 3'-10" | | | | | A1035 CM | |
| P3 | PARAPET | 4 | 40 | STR. | 24'-8" | 986'-8" | 24'-8" | | | | | A1035 CM | |
| P4 | PARAPET | 4 | 20 | STR. | 528'-8" | 10573'-4" | 512'-8" | 2'-0" | 8 | | | A1035 CM | |
| P5 | PARAPET | 4 | 623 | 13 | 4'-2" | 2570'-0" | 0'-5 3/4" | 3'-1" | 0'-7 1/2" | | | A1035 CM | |
| P6 | PARAPET | 4 | 623 | STR. | 3'-0" | 1869'-0" | 3'-0" | | | | | A1035 CM | |
| LP1 | LIGHT PEDESTAL | 4 | SER. OF | 15 | TO | 101'-8" | 4'-0" | TO | 1'-0" | 0'-2 1/4" | 1'-1 1/2" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP2 | LIGHT PEDESTAL | 4 | SER. OF | 19 | TO | 146'-8" | TO | 1'-3" | 1'-0" | | 0'-7 3/8" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP3 | LIGHT PEDESTAL | 4 | SER. OF | 15 | TO | 88'-4" | 3'-2" | TO | 1'-0" | 0'-2 1/4" | 1'-1 1/2" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP4 | LIGHT PEDESTAL | 4 | SER. OF | 19 | TO | 146'-8" | TO | 1'-3" | 1'-0" | | 0'-7 3/8" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP5 | LIGHT PEDESTAL | 4 | 24 | 2 | 5'-5" | 129'-11" | 1'-4" | 2'-4" | 1'-9" | | | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP6 | LIGHT PEDESTAL | 4 | SER. OF | 2 | TO | 80'-0" | 0'-4" | TO | 0'-4" | | 1'-8" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| LP7 | LIGHT PEDESTAL | 4 | SER. OF | STR. | TO | 60'-0" | TO | | | | 1'-8" | A1035 CM | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |
| | | | | | | | | | | | | | |

BENDING DIAGRAMS



NOTES

- ALL BARS ARE COATED.
- BAR SIZES ARE U.S. UNITS.
- REINFORCING STEEL DIMENSIONS ARE OUT TO OUT OF BARS UNLESS OTHERWISE NOTED.
- TYPE 'STR' INDICATES A STRAIGHT BAR. TYPE 'SER OF' INDICATES SERIES (CUT SET) BARS AND COLUMN TITLE 'SERIES INC' IS ABBREVIATED FOR 'SERIES INCREMENT'.
- SERIES BARS - EACH BAR VARIES BY TABULATED AMOUNT.
- SPLICES MAY BE OMITTED AT FABRICATOR'S OPTION, HOWEVER, IN SUCH CASE, FABRICATOR ASSUMES RESPONSIBILITY FOR FIT.
- USE THE CRSI MANUAL OF PRACTICE, LATEST EDITION FOR HOOKS, BENDS, AND FABRICATION OF REINFORCING STEEL UNLESS NOTED OTHERWISE.

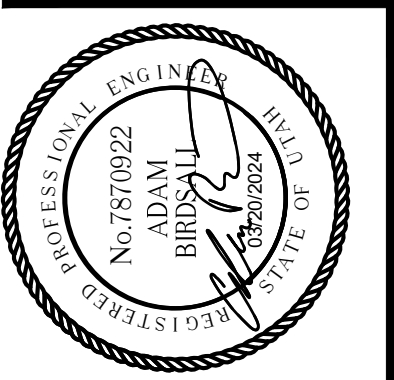
| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DESIGNED: TWP
DRAWN: SLO
CHECKED: NICC
APPROVED: AUB

DATE: 03/20/2024
JOB No.: 344-8541-002



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

REINFORCING SCHEDULE
1 OF 3

U:\Set\Projects\Clients\8541-8541-002 Forest St Final Design\995ves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:55:45 AM

| MARK | LOCATION | SIZE | NO. BARS | TYPE | LENGTH | TOTAL LENGTH | DIMENSIONS | | | | | SERIES INC. | REMARKS |
|------|------------|------|----------|---------|---------|--------------|-------------|-------------|--------|---|---|-------------|---------|
| | | | | | | | A | B | C | D | E | | |
| W1 | WINGWALL 1 | 7 | 1 | SER. OF | 10 | 4'-6" | 0'-9 5/8" | 0'-8" | | | | 0'-3 3/8" | |
| | | | 18 | | | 14'-0" | 12'-3" | 10'-2 1/4" | | | | | |
| W2 | WINGWALL 1 | 7 | 4 | 10 | 18'-4" | 73'-5" | 12'-1" | 14'-6 1/4" | 3'-10" | | | | |
| | | | 1 | | | 2'-1" | 2'-0 3/4" | | | | | | |
| W3 | WINGWALL 1 | 6 | 1 | SER. OF | STR. | TO | 152'-6" | TO | | | | 0'-4 1/2" | |
| | | | 24 | | | 10'-8" | 10'-7 3/4" | | | | | | |
| | | | 1 | | | 10'-10" | 10'-10 1/4" | | | | | | |
| W4 | WINGWALL 1 | 6 | 1 | SER. OF | STR. | TO | 87'-11" | TO | | | | 0'-0 1/2" | |
| | | | 8 | | | 11'-2" | 11'-1 5/8" | | | | | | |
| | | | 1 | | | 10'-7" | 10'-7" | | | | | | |
| W5 | WINGWALL 1 | 7 | 1 | SER. OF | STR. | TO | 21'-5" | TO | | | | 0'-3 1/4" | |
| | | | 2 | | | 10'-10" | 10'-10 3/8" | | | | | | |
| W6 | WINGWALL 1 | 7 | 2 | STR. | | 16'-0" | 15'-11 5/8" | | | | | | |
| W7 | WINGWALL 1 | 4 | 1 | STR. | | 13'-8" | 13'-8" | | | | | | |
| W8 | WINGWALL 1 | 4 | 2 | STR. | | 16'-6" | 16'-6" | | | | | | |
| | | | 1 | | | 5'-4" | 5'-4" | | | | | | |
| W9 | WINGWALL 1 | 4 | 1 | SER. OF | STR. | TO | 68'-10" | TO | | | | 1'-6" | |
| | | | 7 | | | 14'-4" | 14'-4" | | | | | | |
| | | | 1 | | | 2'-5" | 2'-5 1/4" | | | | | | |
| W10 | WINGWALL 1 | 4 | 1 | SER. OF | STR. | TO | 78'-6" | TO | | | | 0'-9" | |
| | | | 12 | | | 10'-8" | 10'-7 3/4" | | | | | | |
| | | | 1 | | | 10'-11" | 10'-10 3/4" | | | | | | |
| W11 | WINGWALL 1 | 4 | 1 | SER. OF | STR. | TO | 44'-1" | TO | | | | 0'-1" | |
| | | | 4 | | | 11'-2" | 11'-1 5/8" | | | | | | |
| W12 | WINGWALL 1 | 4 | 1 | 10 | 18'-4" | 18'-4" | 12'-1" | 14'-6 1/4" | 3'-10" | | | | |
| W13 | WINGWALL 1 | 4 | 68 | 2 | 153'-0" | 153'-0" | 1'-0" | 0'-3" | 1'-0" | | | | |
| | | | 1 | | | 4'-0" | 0'-3" | 0'-2 1/2" | | | | 0'-3 5/8" | |
| W14 | WINGWALL 2 | 7 | 18 | SER. OF | 10 | TO | 164'-6" | TO | | | | | |
| | | | 18 | | | 14'-3" | 12'-6" | 10'-4 3/4" | | | | | |
| W15 | WINGWALL 2 | 7 | 4 | 10 | 18'-8" | 74'-7" | 12'-4" | 14'-9 7/8" | 3'-10" | | | | |
| | | | 1 | | | 1'-10" | 1'-10 1/4" | | | | | 0'-4 1/2" | |
| W16 | WINGWALL 2 | 6 | 1 | SER. OF | STR. | TO | 158'-4" | TO | | | | | |
| | | | 25 | | | 10'-10" | 10'-9 3/4" | | | | | | |
| | | | 1 | | | 11'-0" | 11'-0 1/4" | | | | | | |
| W17 | WINGWALL 2 | 6 | 1 | SER. OF | STR. | TO | 89'-3" | TO | | | | 0'-0 1/2" | |
| | | | 8 | | | 11'-4" | 11'-3 5/8" | | | | | | |
| | | | 1 | | | 11'-0" | 11'-0 3/8" | | | | | | |
| W18 | WINGWALL 2 | 7 | 1 | SER. OF | STR. | TO | 21'-9" | TO | | | | | |
| | | | 2 | | | 10'-9" | 10'-9" | | | | | | |
| W19 | WINGWALL 2 | 7 | 2 | STR. | | 16'-3" | 32'-5" | 16'-2 5/8" | | | | | |
| W20 | WINGWALL 2 | 4 | 1 | STR. | | 14'-0" | 14'-0" | 14'-0" | | | | | |
| W21 | WINGWALL 2 | 4 | 1 | STR. | | 16'-9" | 16'-9" | 16'-9" | | | | | |
| | | | 1 | | | 5'-4" | 5'-4" | | | | | | |
| W22 | WINGWALL 2 | 4 | 1 | SER. OF | STR. | TO | 102'-0" | TO | | | | 1'-6" | |
| | | | 9 | | | 17'-4" | 17'-4" | | | | | | |
| | | | 1 | | | 1'-10" | 1'-10 1/4" | | | | | | |
| W23 | WINGWALL 2 | 4 | 1 | SER. OF | STR. | TO | 82'-4" | TO | | | | 0'-9" | |
| | | | 13 | | | 10'-10" | 10'-9 3/4" | | | | | | |
| | | | 1 | | | 11'-1" | 11'-0 3/4" | | | | | | |
| W24 | WINGWALL 2 | 4 | 1 | SER. OF | STR. | TO | 44'-9" | TO | | | | 0'-1" | |
| | | | 4 | | | 11'-4" | 11'-3 5/8" | | | | | | |
| W25 | WINGWALL 2 | 4 | 1 | 10 | 18'-8" | 18'-8" | 12'-4" | 14'-9 7/8" | 3'-10" | | | | |
| | | | 1 | | | 4'-6" | 0'-10" | 0'-8 3/8" | | | | | |
| W26 | WINGWALL 3 | 7 | 1 | SER. OF | 10 | TO | 169'-6" | TO | | | | 0'-3 1/2" | |
| | | | 18 | | | 14'-4" | 12'-7" | 10'-5 5/8" | | | | | |
| W27 | WINGWALL 3 | 7 | 4 | 10 | 18'-9" | 75'-0" | 12'-5" | 14'-11 1/8" | 3'-10" | | | | |
| | | | 1 | | | 1'-10" | 1'-10 1/8" | | | | | | |
| W28 | WINGWALL 3 | 6 | 1 | SER. OF | STR. | TO | 155'-2" | TO | | | | 0'-4 3/8" | |
| | | | 25 | | | 10'-7" | 10'-6 7/8" | | | | | | |
| | | | 1 | | | 10'-9" | 10'-9 1/8" | | | | | | |
| W29 | WINGWALL 3 | 6 | 1 | SER. OF | STR. | TO | 87'-0" | TO | | | | 0'-0 3/8" | |
| | | | 8 | | | 11'-0" | 10'-11 3/4" | | | | | | |
| | | | 1 | | | 10'-9" | 10'-9 1/4" | | | | | | |
| W30 | WINGWALL 3 | 7 | 1 | SER. OF | STR. | TO | 21'-3" | TO | | | | | |
| | | | 2 | | | 10'-6" | 10'-6 1/8" | | | | | | |
| W31 | WINGWALL 3 | 7 | 2 | STR. | | 16'-3" | 32'-7" | 16'-3 1/4" | | | | | |
| W32 | WINGWALL 3 | 4 | 1 | STR. | | 12'-11" | 12'-11" | 12'-10 5/8" | | | | | |
| W33 | WINGWALL 3 | 4 | 1 | STR. | | 16'-3" | 16'-3" | 16'-3" | | | | | |
| | | | 1 | | | 5'-4" | 5'-4" | | | | | | |
| W34 | WINGWALL 3 | 4 | 1 | SER. OF | STR. | TO | 84'-8" | TO | | | | 1'-6" | |
| | | | 8 | | | 15'-10" | 15'-10" | | | | | | |

| MARK | LOCATION | SIZE | NO. BARS | TYPE | LENGTH | TOTAL LENGTH | DIMENSIONS | | | | | SERIES INC. | REMARKS |
|------|----------------|------|----------|---------|---------|--------------|-------------|-------------|------------|---|---|-------------|---------|
| | | | | | | | A | B | C | D | E | | |
| W35 | WINGWALL 3 | 4 | 1 | SER. OF | STR. | 1'-8" | 1'-9 1/8" | | | | | 0'-8 3/4" | |
| | | | 13 | | | 10'-6" | TO | | | | | | |
| W36 | WINGWALL 3 | 4 | 1 | SER. OF | STR. | TO | 10'-10" | TO | | | | 0'-0 3/4" | |
| | | | 4 | | | 11'-0" | 10'-11 3/4" | | | | | | |
| W37 | WINGWALL 3 | 4 | 1 | 10 | 18'-9" | 18'-9" | 12'-5" | 14'-11 1/8" | 3'-10" | | | | |
| | | | 1 | | | 4'-2" | 0'-5" | 0'-4 1/8" | | | | | |
| W38 | WINGWALL 4 | 7 | 1 | SER. OF | 10 | TO | 168'-3" | TO | | | | 0'-3 5/8" | |
| | | | 18 | | | 14'-6" | 12'-10" | 10'-8 1/8" | | | | | |
| W39 | WINGWALL 4 | 7 | 4 | 10 | 19'-1" | 76'-3" | 12'-8" | 15'-2 5/8" | 3'-10" | | | | |
| | | | 1 | | | 2'-1" | 2'-1 1/8" | | | | | | |
| W40 | WINGWALL 4 | 6 | 1 | SER. OF | STR. | TO | 161'-5" | TO | | | | 0'-4 3/8" | |
| | | | 25 | | | 10'-10" | 10'-9 7/8" | | | | | | |
| | | | 1 | | | 11'-0" | 11'-0 1/8" | | | | | | |
| W41 | WINGWALL 4 | 6 | 1 | SER. OF | STR. | TO | 89'-0" | TO | | | | 0'-0 3/8" | |
| | | | 8 | | | 11'-3" | 11'-2 3/4" | | | | | | |
| | | | 1 | | | 11'-0" | 11'-0 1/4" | | | | | | |
| W42 | WINGWALL 4 | 7 | 1 | SER. OF | STR. | TO | 21'-9" | TO | | | | | |
| | | | 2 | | | 10'-9" | 10'-9 1/8" | | | | | | |
| W43 | WINGWALL 4 | 7 | 2 | STR. | | 16'-6" | 33'-1" | 16'-6 3/8" | | | | | |
| W44 | WINGWALL 4 | 4 | 2 | STR. | | 16'-6" | 33'-0" | 16'-6" | | | | | |
| | | | 1 | | | 5'-4" | 5'-4" | | | | | | |
| W45 | WINGWALL 4 | 4 | 1 | SER. OF | STR. | TO | 102'-0" | TO | | | | 1'-6" | |
| | | | 9 | | | 17'-4" | 17'-4" | | | | | | |
| | | | 1 | | | 2'-0" | 2'-0 1/8" | | | | | | |
| W46 | WINGWALL 4 | 4 | 1 | SER. OF | STR. | TO | 82'-10" | TO | | | | 0'-8 3/4" | |
| | | | 13 | | | 10'-9" | 10'-8 7/8" | | | | | | |
| | | | 1 | | | 11'-1" | 11'-0 1/2" | | | | | | |
| W47 | WINGWALL 4 | 4 | 1 | SER. OF | STR. | TO | 44'-6" | TO | | | | 0'-0 3/4" | |
| | | | 4 | | | 11'-3" | 11'-2 3/4" | | | | | | |
| W48 | WINGWALL 4 | 4 | 1 | 10 | 19'-1" | 19'-1" | 12'-8" | 15'-2 5/8" | 3'-10" | | | | |
| C1 | BENT 2 COLUMNS | 10 | 72 | 1 | 36'-1" | 2598'-10" | 2'-0" | 34'-1 1/8" | | | | A706 | |
| C2 | BENT 3 COLUMNS | 10 | 72 | 1 | 41'-9" | 3004'-3" | 2'-0" | 39'-8 3/4" | | | | A706 | |
| C3 | BENT 4 COLUMNS | 10 | 72 | 1 | 37'-5" | 2696'-9" | 2'-0" | 35'-5 1/2" | | | | A706 | |
| C4 | BENT 5 COLUMNS | 10 | 72 | 1 | 35'-8" | 2569'-4" | 2'-0" | 33'-8 1/4" | | | | A706 | |
| C5 | ALL COLUMNS | 6 | 12 | 11 | 161'-4" | 1935'-9" | 0'-4" | 3'-6" | 3'-10 1/8" | | | A706 | |
| C6 | ALL COLUMNS | 6 | 96 | 20 | 14'-4" | 1375'-7" | 3'-6" | | | | | A706 | |
| C7 | BENT 2 COLUMNS | 6 | 6 | 11 | 90'-10" | 545'-2" | 0'-4" | 3'-6" | 1'-8 1/2" | | | A706 | |
| C8 | BENT 3 COLUMNS | 6 | 6 | 11 | 98'-5" | 590'-7" | 0'-4" | 3'-6" | 1'-11 1/4" | | | A706 | |
| C9 | BENT 4 COLUMNS | 6 | 6 | 11 | 92'-9" | 556'-3" | 0'-4" | 3'-6" | 1'-9 1/8" | | | A706 | |
| C10 | BENT 5 COLUMNS | 6 | 6 | 11 | 86'-3" | 517'-8" | 0'-4" | 3'-6" | 1'-6 7/8" | | | A706 | |
| C11 | BENT 2 COLUMNS | 6 | 3 | 11 | 464'-6" | 1393'-6" | 0'-6" | 3'-6" | 19'-6 3/8" | | | A706 | |
| C12 | BENT 3 COLUMNS | 6 | 3 | 11 | 578'-4" | 1735'-0" | 0'-6" | 3'-6" | 24'-8 1/2" | | | A706 | |
| C13 | BENT 4 COLUMNS | 6 | 3 | 11 | 492'-0" | 1475'-11" | 0'-6" | 3'-6" | 20'-9 3/8" | | | A706 | |
| C14 | BENT 5 COLUMNS | 6 | 3 | 11 | 461'-7" | 1384'-9" | 0'-6" | 3'-6" | 19'-4 7/8" | | | A706 | |
| C15 | ALL COLUMNS | 6 | 12 | 11 | 157'-6" | 1890'-2" | 0'-4" | 3'-6" | 3'-8 3/4" | | | A706 | |
| BD1 | BENT DIAPHRAGM | 5 | 200 | 16 | 14'-11" | 2975'-0" | 5'-1 3/4" | 4'-7" | 5'-1 3/4" | | | | |
| BD2 | BENT DIAPHRAGM | 5 | 56 | 16 | 11'-7" | 648'-8" | 3'-6" | 4'-7" | 3'-6" | | | | |
| BD3 | BENT DIAPHRAGM | 5 | 280 | 7 | 5'-7" | 1563'-4" | 4'-7" | 0'-6" | | | | | |
| BD4 | BENT DIAPHRAGM | 5 | 200 | 7 | 5'-7" | 1116'-8" | 4'-7" | 0'-6" | | | | | |
| BD5 | BENT DIAPHRAGM | 5 | 144 | 2 | 9'-4" | 1344'-0" | 2'-0" | 5'-4" | 2'-0" | | | | |
| BD6 | BENT DIAPHRAGM | 5 | 240 | 2 | 12'-10" | 3080'-0" | 2'-0" | 8'-10" | 2'-0" | | | | |
| BD7 | BENT DIAPHRAGM | 5 | 280 | STR. | 5'-0" | 1400'-0" | 5'-0" | | | | | | |
| BD8 | BENT DIAPHRAGM | 5 | 48 | 2 | 6'-3" | 300'-0" | 1'-0" | 4'-3" | 1'-0" | | | | |
| BD9 | BENT DIAPHRAGM | 5 | 48 | 2 | 8'-4" | 400'-0" | 2'-2" | 4'-0" | 2'-2" | | | | |
| B1 | BENT CAP | 8 | 120 | STR. | 2'-6" | 300'-0" | 2'-6" | | | | | | |
| B2A | BENT CAP | 9 | 88 | 1 | 60'-0" | 5280'-0" | 1'-8" | 58'-4" | | | | | |
| B2B | BENT CAP | 9 | 88 | 1 | 13'-10" | 1217'-4" | 1'-8" | 12'-2" | | | | | |
| B3 | BENT CAP | 6 | 40 | STR. | 69'-6" | 2780'-0" | 64'-6" | 5'-0" | 1 | | | | |
| B4 | BENT CAP | 9 | 44 | STR. | 70'-6" | 3102'-0" | 64'-6" | 6'-0" | 1 | | | | |
| B5 | BENT CAP | 7 | 808 | 7 | 5'-11" | 4780'-8" | 4'-8" | 0'-6" | | | | | |

LAYOUT: REIN SCH PATH: U:\Set\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995ves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 9:55:55 AM

| MARK | LOCATION | SIZE | NO. BARS | TYPE | LENGTH | TOTAL LENGTH | DIMENSIONS | | | | | SERIES INC. | REMARKS |
|------|--------------------|------|----------|------|---------|--------------|------------|--------|-----------|-------|---|-------------|---------|
| | | | | | | | A | B | C | D | E | | |
| B8 | BENT CAP | 7 | SER. OF | 16 | 12'-0" | 827'-1" | 4'-7" | 2'-10" | 4'-7" | | | 0 1/8" | |
| | | | | | TO | | TO | | TO | | | | |
| | | | | | 4 | | 4'-9" | | 4'-9" | | | | |
| B9 | BENT CAP | 7 | SER. OF | 16 | 12'-11" | 887'-6" | 5'-0 3/8" | 2'-10" | 5'-0 3/8" | | | 0 1/8" | |
| | | | | | TO | | TO | | TO | | | | |
| | | | | | 4 | | 5'-2 1/4" | | 5'-2 1/4" | | | | |
| B10 | BENT CAP | 7 | SER. OF | 16 | 11'-10" | 821'-8" | 4'-6" | 2'-10" | 4'-6" | | | 0 1/4" | |
| | | | | | TO | | TO | | TO | | | | |
| | | | | | 4 | | 4'-9" | | 4'-9" | | | | |
| F1 | FOOTING | 9 | 816 | 2 | 23'-0" | 18768'-0" | 2'-0" | 19'-0" | 2'-0" | | | | |
| | | | | | TO | | TO | | TO | | | | |
| F2 | FOOTING | 7 | 816 | 2 | 23'-0" | 18768'-0" | 2'-0" | 19'-0" | 2'-0" | | | | |
| F3 | FOOTING | 6 | 4656 | 7 | 5'-9" | 26772'-0" | 4'-7" | 0'-6" | | | | | |
| F4 | FOOTING | 7 | 288 | STR. | 19'-0" | 5472'-0" | 19'-0" | | | | | | |
| A1 | ABUTMENT CAP | 9 | 16 | STR. | 70'-6" | 1128'-0" | 64'-6" | 6'-0" | 1 | | | | |
| A2A | ABUTMENT CAP | 10 | 56 | 8 | 61'-5" | 3439'-4" | 60'-0" | | | | | | |
| A2B | ABUTMENT CAP | 10 | 56 | 8 | 13'-5" | 751'-4" | 12'-0" | | | | | | |
| A3 | ABUTMENT CAP | 6 | 910 | 7 | 4'-10" | 4398'-4" | 3'-8" | 0'-6" | | | | | |
| A4 | ABUTMENT CAP | 6 | 130 | 16 | 12'-10" | 1668'-4" | 4'-7" | 3'-8" | 4'-7" | | | | |
| A5 | ABUTMENT CAP | 6 | 84 | 2 | 5'-8" | 476'-1" | 2'-6" | 0'-8" | 2'-6" | | | | |
| A6 | ABUTMENT CAP | 6 | 70 | 2 | 7'-10" | 548'-5" | 2'-6" | 2'-10" | 2'-6" | | | | |
| A7 | ABUTMENT CAP | 6 | 60 | 2 | 8'-2" | 490'-0" | 2'-6" | 3'-2" | 2'-6" | | | | |
| A8 | ABUTMENT CAP | 6 | 24 | 2 | 5'-2" | 124'-0" | 2'-3" | 0'-8" | 2'-3" | | | | |
| A9 | ABUTMENT CAP | 4 | 32 | 17 | 5'-6" | 176'-0" | 1'-8" | 2'-2" | 1'-8" | 1'-6" | | | |
| A10 | ABUTMENT CAP | 4 | 4 | STR. | 23'-8" | 94'-8" | 23'-8" | | | | | | |
| A11 | ABUTMENT CAP | 5 | 120 | STR. | 4'-8" | 560'-0" | 4'-8" | | | | | | |
| A12 | ABUTMENT CAP | 9 | 72 | 1 | 14'-4" | 1032'-0" | 12'-8" | 1'-8" | | | | | |
| A13 | ABUTMENT CAP | 4 | 36 | 2 | 3'-8" | 132'-0" | 1'-0" | 1'-8" | 1'-0" | | | | |
| A14 | ABUTMENT CAP | 5 | 192 | 7 | 2'-8" | 512'-0" | 1'-8" | 0'-6" | | | | | |
| D1 | ABUTMENT DIAPHRAGM | 5 | 20 | STR. | 69'-6" | 1390'-0" | 64'-6" | 5'-0" | 1 | | | | |
| D2 | ABUTMENT DIAPHRAGM | 5 | 60 | STR. | 8'-10" | 530'-0" | 8'-10" | | | | | | |
| D3 | ABUTMENT DIAPHRAGM | 5 | 36 | STR. | 5'-4" | 191'-11" | 5'-4" | | | | | | |
| D4 | ABUTMENT DIAPHRAGM | 5 | 70 | STR. | 3'-9" | 262'-6" | 3'-9" | | | | | | |
| D5 | ABUTMENT DIAPHRAGM | 5 | 144 | 1 | 7'-6" | 1080'-0" | 3'-0" | 4'-6" | | | | | |
| D6 | ABUTMENT DIAPHRAGM | 5 | 200 | 1 | 4'-3" | 849'-4" | 1'-11" | 2'-4" | | | | | |
| D7 | ABUTMENT DIAPHRAGM | 5 | 856 | 7 | 4'-2" | 3566'-8" | 3'-2" | 0'-6" | | | | | |
| D8 | ABUTMENT DIAPHRAGM | 5 | 128 | 15 | 4'-1" | 522'-3" | 1'-4" | 0'-10" | 1'-11" | 1'-5" | | | |
| D9 | ABUTMENT DIAPHRAGM | 5 | 96 | STR. | 1'-10" | 174'-5" | 1'-9 3/4" | | | | | | |
| D10 | ABUTMENT DIAPHRAGM | 6 | 96 | 2 | 6'-6" | 624'-0" | 2'-11" | 0'-8" | 2'-11" | | | | |
| D11 | ABUTMENT DIAPHRAGM | 5 | 56 | 1 | 5'-5" | 303'-4" | 0'-11" | 4'-6" | | | | | |
| D12 | ABUTMENT DIAPHRAGM | 5 | 20 | STR. | 1'-5" | 28'-4" | 1'-5" | | | | | | |
| D13 | ABUTMENT DIAPHRAGM | 5 | 4 | STR. | 1'-3" | 5'-0" | 1'-3" | | | | | | |
| D14 | ABUTMENT DIAPHRAGM | 5 | 24 | 2 | 5'-2" | 124'-0" | 1'-0" | 3'-2" | 1'-0" | | | | |

SUMMARY OF COATED BARS - AASHTO M 31 GRADE 60

13,264'-0" OF NUMBER 4 BARS AT 0.668 LBS/FT = 8,861 LBS
 25,148'-3" OF NUMBER 5 BARS AT 1.043 LBS/FT = 26,230 LBS
 43,287'-5" OF NUMBER 6 BARS AT 1.502 LBS/FT = 65,018 LBS
 46,158'-1" OF NUMBER 7 BARS AT 2.044 LBS/FT = 94,348 LBS
 300'-0" OF NUMBER 8 BARS AT 2.044 LBS/FT = 801 LBS
 29,310'-0" OF NUMBER 9 BARS AT 3.400 LBS/FT = 103,793 LBS
 9,852'-8" OF NUMBER 10 BARS AT 4.300 LBS/FT = 45,598 LBS

SUMMARY OF COATED BARS - ASTM A 706 GRADE 60

13,400'-6" OF NUMBER 6 BARS AT 1.502 LBS/FT = 20,128 LBS
 10,869'-3" OF NUMBER 10 BARS AT 4.303 LBS/FT = 46,771 LBS

SUMMARY OF COATED BARS - ASTM A 1035 CM GRADE 100

40,642'-3" OF NUMBER 4 BARS AT 0.668 LBS/FT = 27,149 LBS
 590'-0" OF NUMBER 5 BARS AT 1.043 LBS/FT = 616 LBS
 5,325'-4" OF NUMBER 6 BARS AT 1.502 LBS/FT = 7,999 LBS
 17,072'-0" OF NUMBER 7 BARS AT 2.044 LBS/FT = 34,896 LBS

SUMMARY OF COATED BARS - ASTM A 1035 CS GRADE 100

7,041'-4" OF NUMBER 4 BARS AT 0.668 LBS/FT = 4,704 LBS
 118,308'-8" OF NUMBER 6 BARS AT 1.502 LBS/FT = 177,700 LBS

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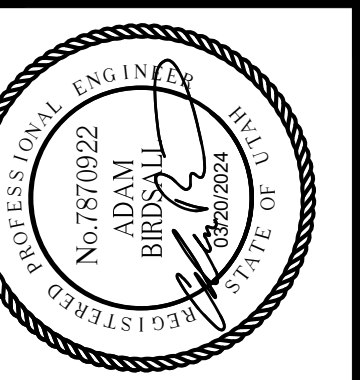
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 AT FULL
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 NOT
 OTHERWISE
 INDICATED
 ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002

DESIGNED: TWP
 DRAWN: SLO

CHECKED: EA
 APPROVED: AUB

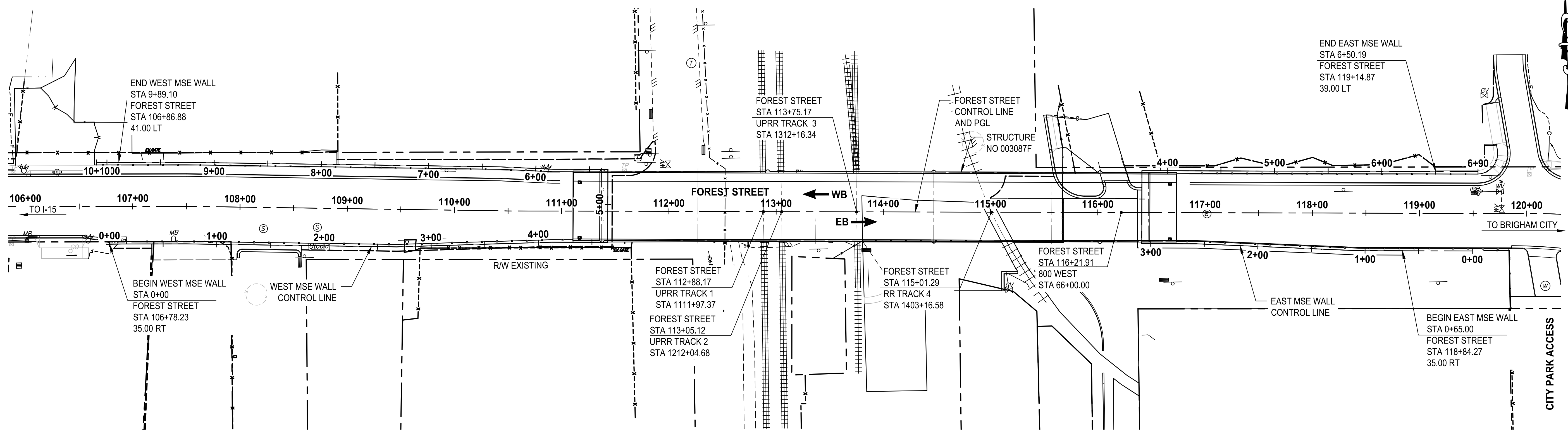


PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

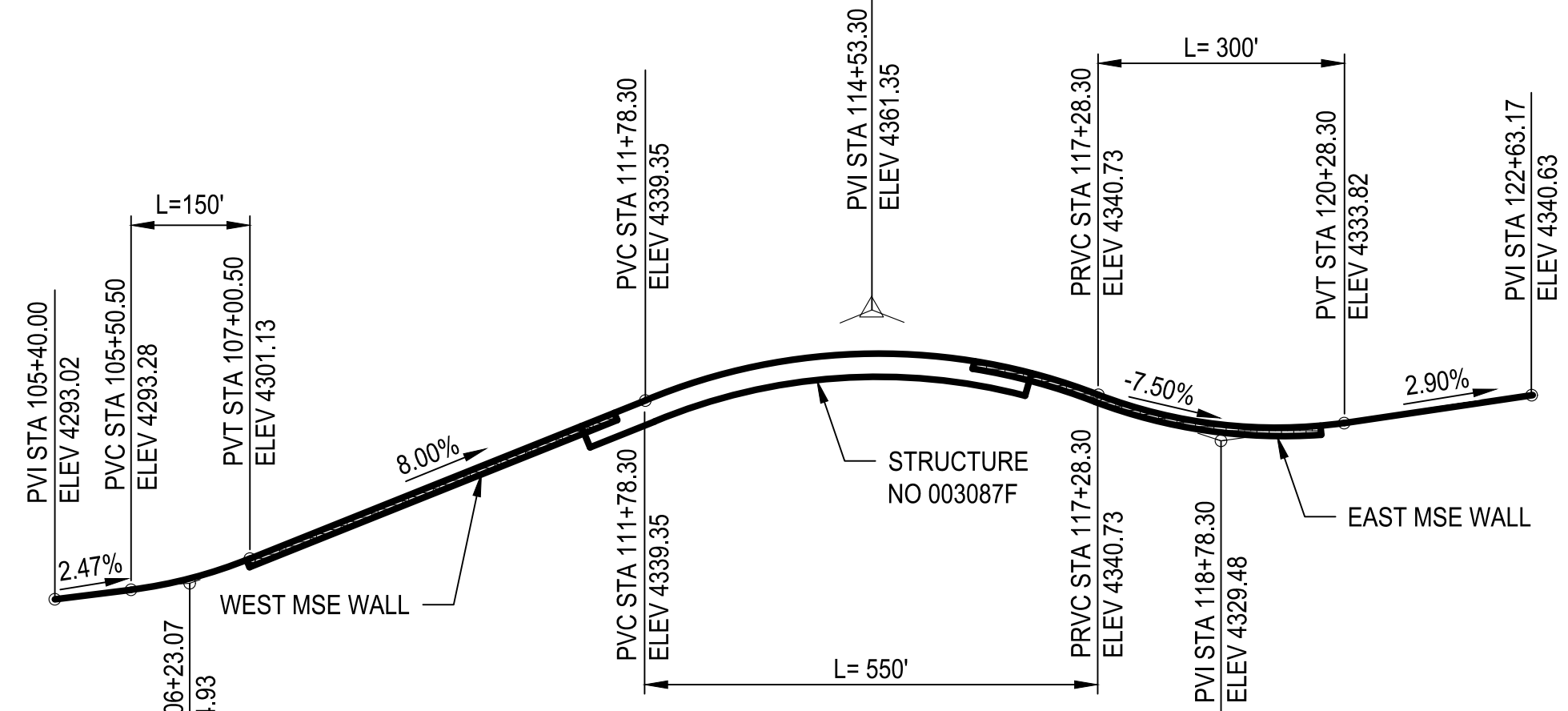
**REINFORCING
 SCHEDULE
 3 OF 3**

DRAWING NO.
 59 OF 59
S59

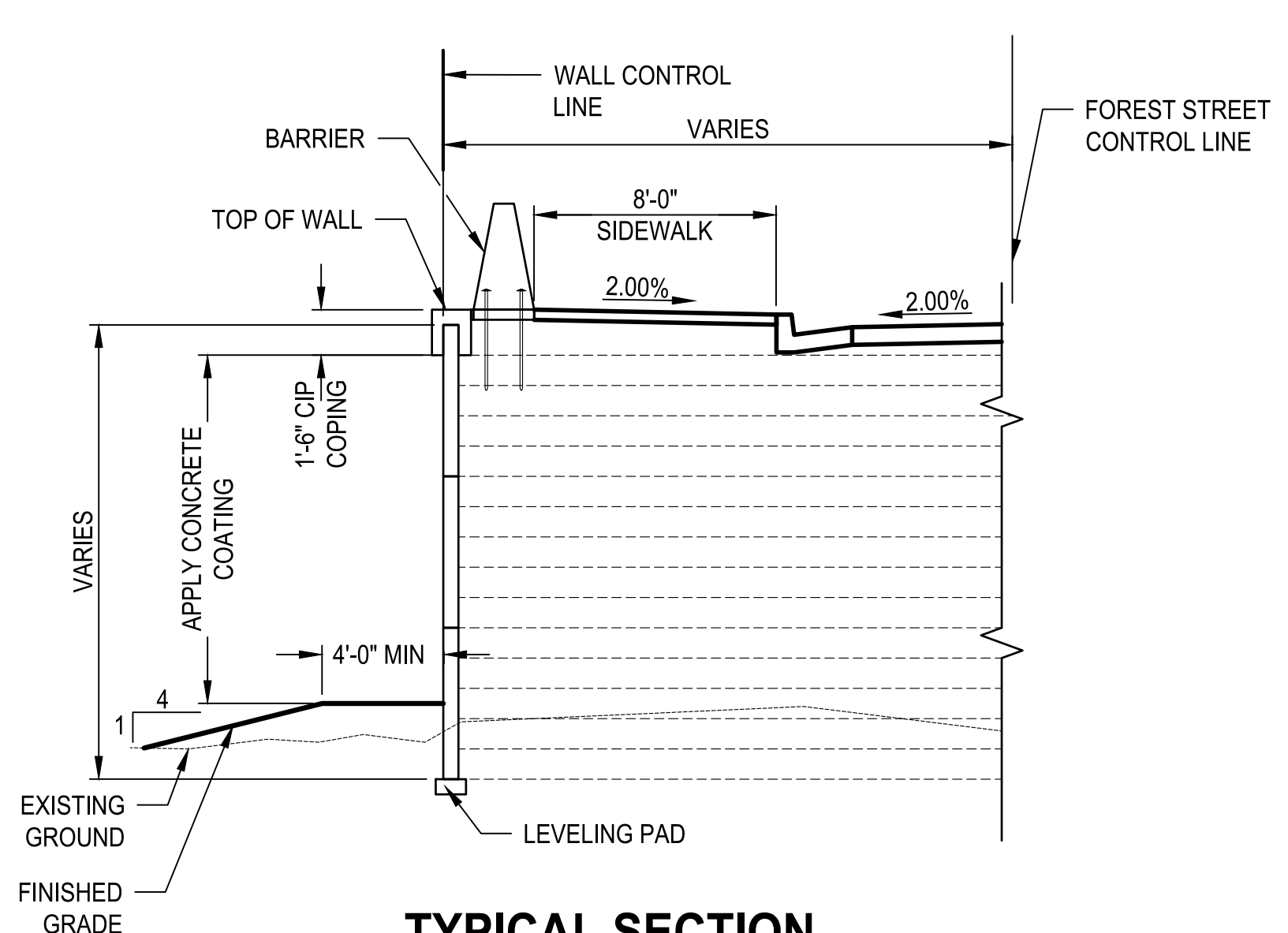
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LOCATION PLAN



FOREST STREET PROFILE



TYPICAL SECTION

GENERAL NOTES

1. USE COATED DEFORMED CARBON STEEL BARS CONFORMING TO ASTM A767 AND AASHTO M31 GRADE 60, UNLESS SHOWN OTHERWISE.
2. CHAMFER EXPOSED CONCRETE CORNERS 3/4" INCH UNLESS SHOWN OTHERWISE.
3. PROVIDE 2 INCH MINIMUM CONCRETE COVER TO REINFORCING STEEL UNLESS SHOWN OTHERWISE.
4. VERIFY UTILITY LOCATIONS BEFORE CONSTRUCTION. PROTECT EXISTING UTILITIES IN PLACE UNLESS SHOWN OTHERWISE.
5. COAT OR GALVANIZE MISCELLANEOUS STRUCTURAL STEEL PLACED IN STRUCTURAL CONCRETE, UNLESS SHOWN OTHERWISE.
6. DO NOT SCALE DRAWINGS. HORIZONTAL DIMENSIONS ARE PLAN. VERTICAL DIMENSIONS ARE PLUMB.

DESIGN DATA

HL-93 LOADING IN ACCORDANCE WITH AASHTO LRFD BRIDGE DESIGN SPECIFICATION, 9TH EDITION 2020 AND THE UDOT STRUCTURE DESIGN AND DETAILING MANUAL 2017.

CAST-IN-PLACE CONCRETE: $f'_c = 4.0$ KSI $n = 8$ CLASS AA (AE) f_y (REINF) = 60 KSI
 PRECAST WALL PANEL CONCRETE: $f'_c = 5.0$ KSI $n = 7$ CLASS AA (AE) f_y (REINF) = 60 KSI
 SEISMIC: 7% PROBABILITY OF EXCEEDANCE IN 75 YR DESIGN EVENT
 $PGA = 0.385g$ $SD_s = 0.961g$ $S_1 = 0.264g$
 $A_s = 0.364g$ $SD_{s1} = 0.910g$ $SD_1 = 0.776g$
 SITE CLASS E, SDC D
 WALL SPECIFICATION - NORMAL
 REFER TO THE GEOTECHNICAL REPORT FOR SOIL PROPERTIES USED IN DESIGN.

SOIL PROPERTIES:

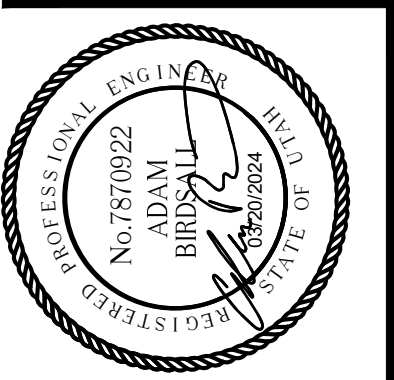
| QUANTITIES | | | |
|--|---------|-------|----------|
| ITEM | EST QTY | UNIT | AS CONST |
| MSE RETAINING WEST WALL (EST LUMP QTY: 18,293 SQ FT) | 1 | LUMP | 1 |
| MSE RETAINING EAST WALL (EST LUMP QTY: 8,466 SQ FT) | 1 | LUMP | 1 |
| CONCRETE COATING WEST WALL | 13,051 | SQ FT | |
| CONCRETE COATING EAST WALL | 5,663 | SQ FT | |

| REVISIONS | DATE | BY |
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| | | |

ONE INCH AT FULL SCALE IF NECESSARY

Parametrix

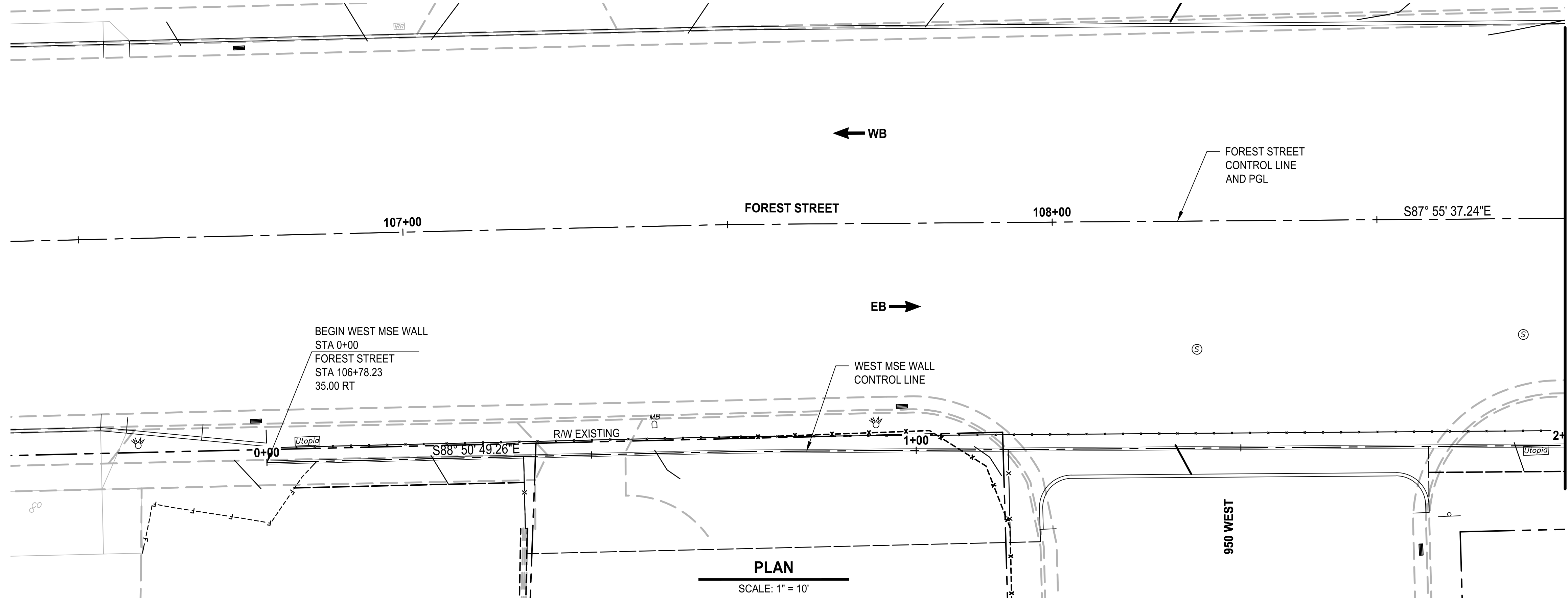
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 JOB No.: 344-8541-002
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 DRAWN: SLO
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 APPROVED: AJB



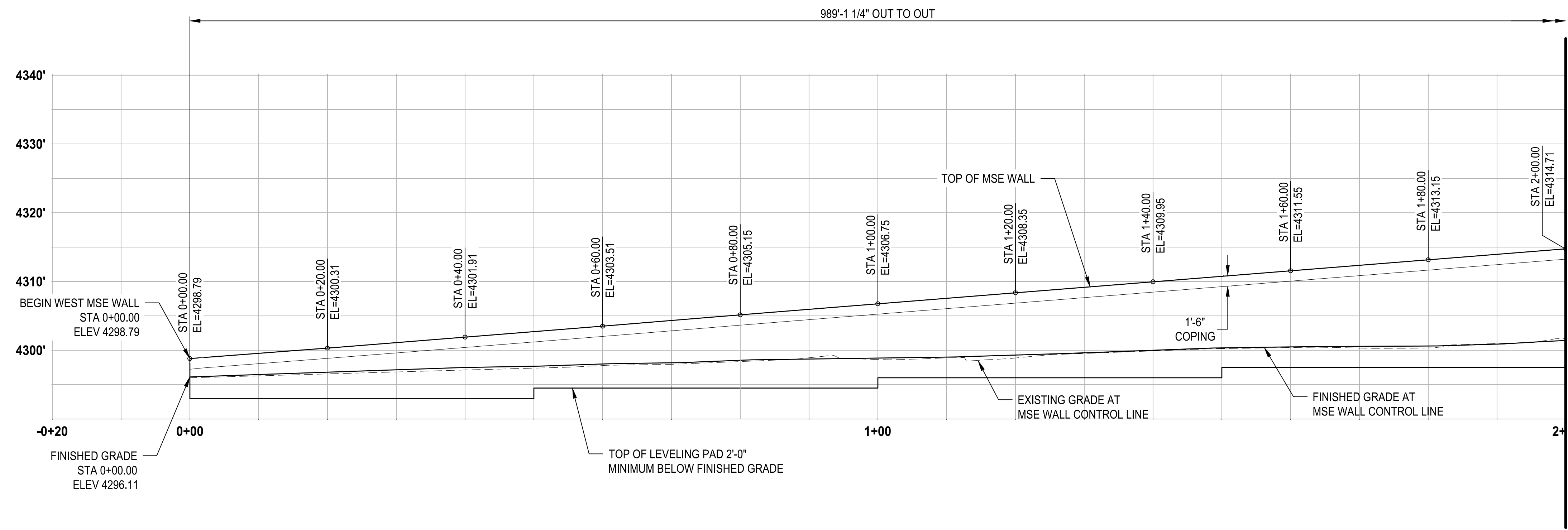
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

MSE WALLS LOCATION PLAN AND GENERAL NOTES

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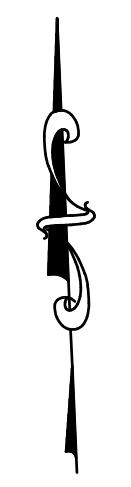
PLAN
SCALE: 1" = 10'



* MEASURED ALONG WEST MSE WALL CONTROL LINE
DEVELOPED ELEVATION
SCALE 1" = 10'

MATCH LINE STA 2+00.00
SEE "WEST MSE WALL SITUATION AND LAYOUT 2 OF 5"

MATCH LINE STA 2+00.00
SEE "WEST MSE WALL SITUATION AND LAYOUT 2 OF 5"



| REVISIONS | DATE | BY |
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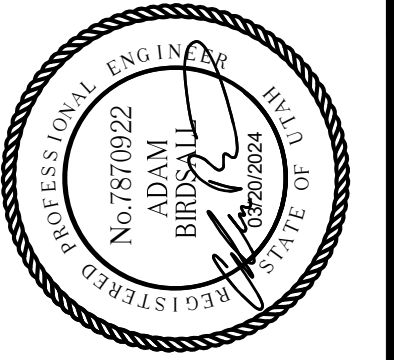
ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY

Parametrix

DATE: 03/20/2024
JOB No.: 344-8541-002

DESIGNED: TWP
DRAWN: SLO

CHECKED: NICC
APPROVED: AUB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

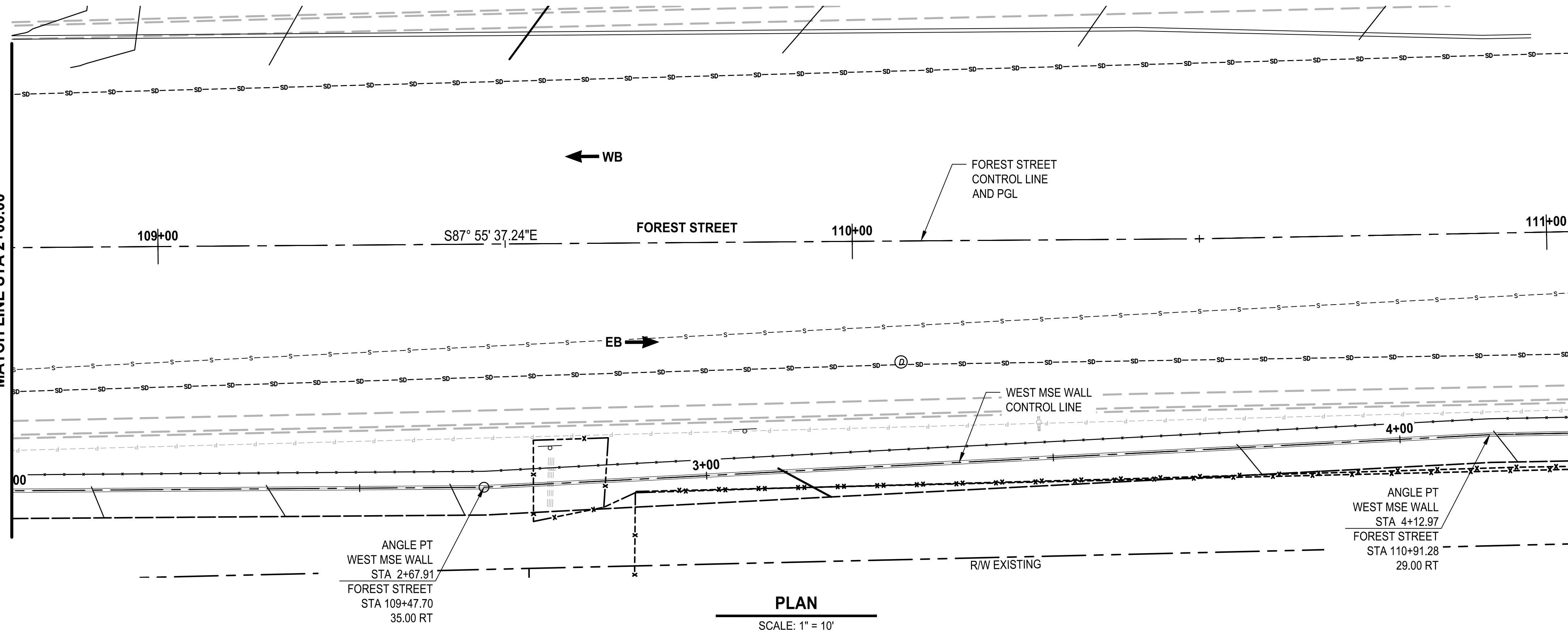
WEST MSE WALL SITUATION AND LAYOUT 1 OF 5

DRAWING NO.
2 OF 11
W02

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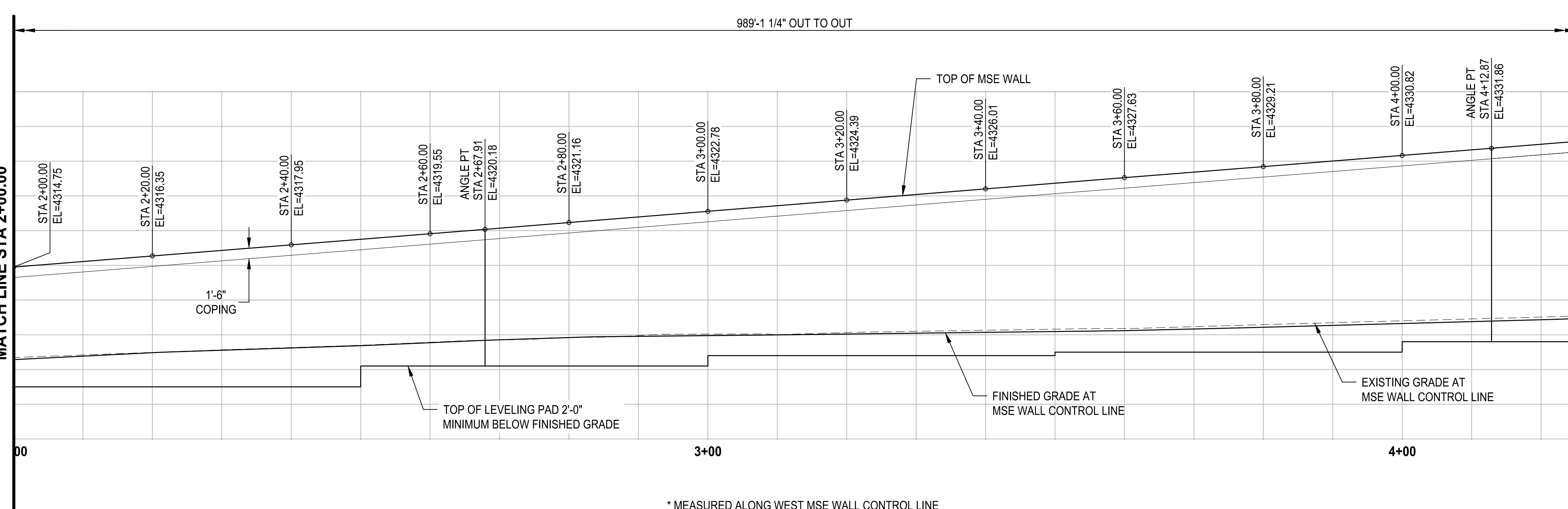
LAYOUT: 2W

SEE "WEST MSE WALL SITUATION AND LAYOUT 1 OF 5"
MATCH LINE STA 2+00.00



PLAN
SCALE: 1" = 10'

SEE "WEST MSE WALL SITUATION AND LAYOUT 1 OF 5"
MATCH LINE STA 2+00.00



* MEASURED ALONG WEST MSE WALL CONTROL LINE
DEVELOPED ELEVATION
SCALE 1" = 10'

MATCH LINE STA 4+25.00
SEE "WEST MSE WALL SITUATION AND LAYOUT 3 OF 5"

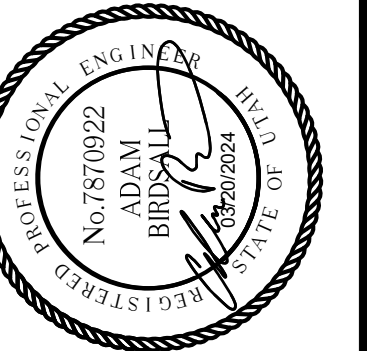
MATCH LINE STA 4+25.00
SEE "WEST MSE WALL SITUATION AND LAYOUT 3 OF 5"

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT OTHERWISE INDICATED

Parametrix

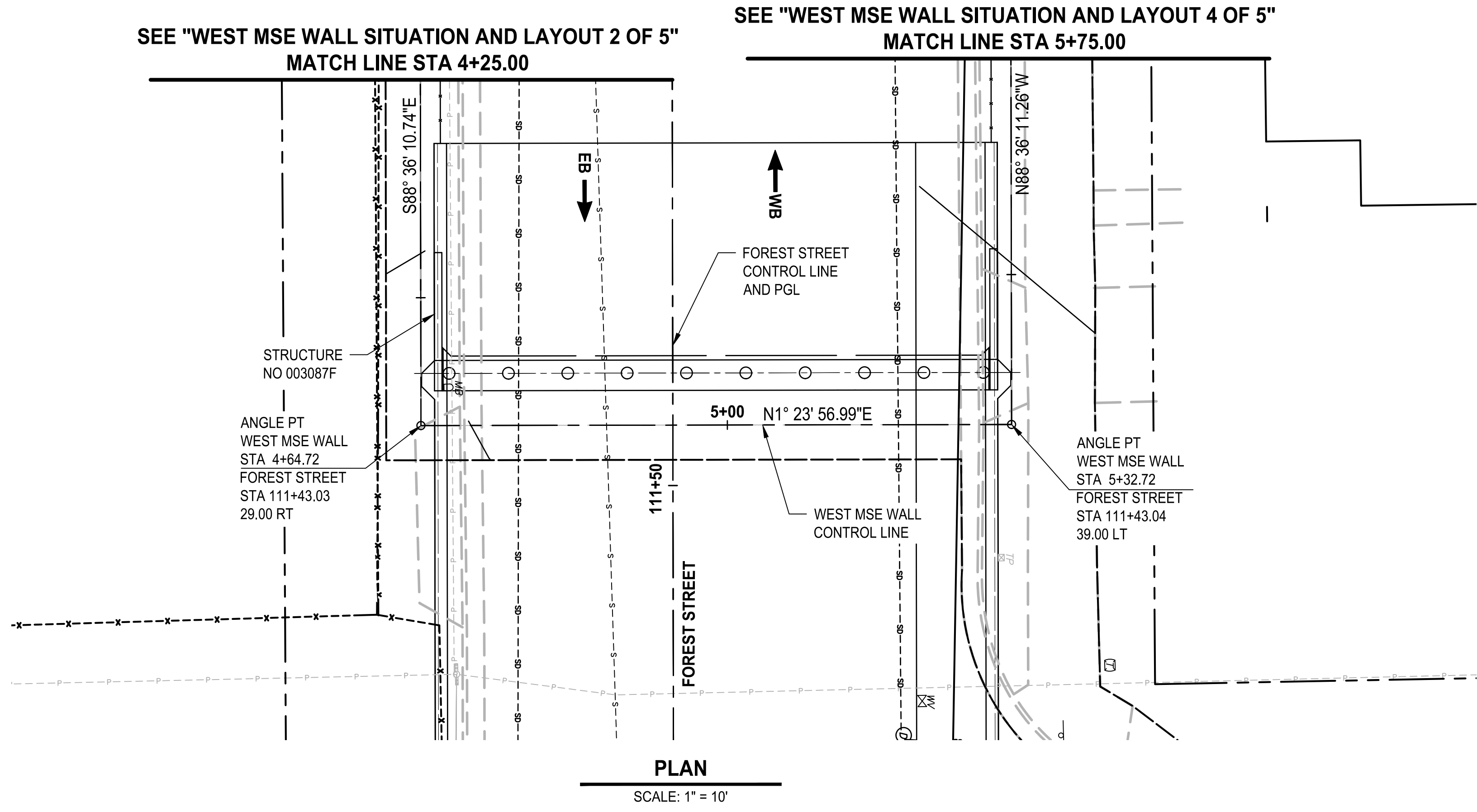
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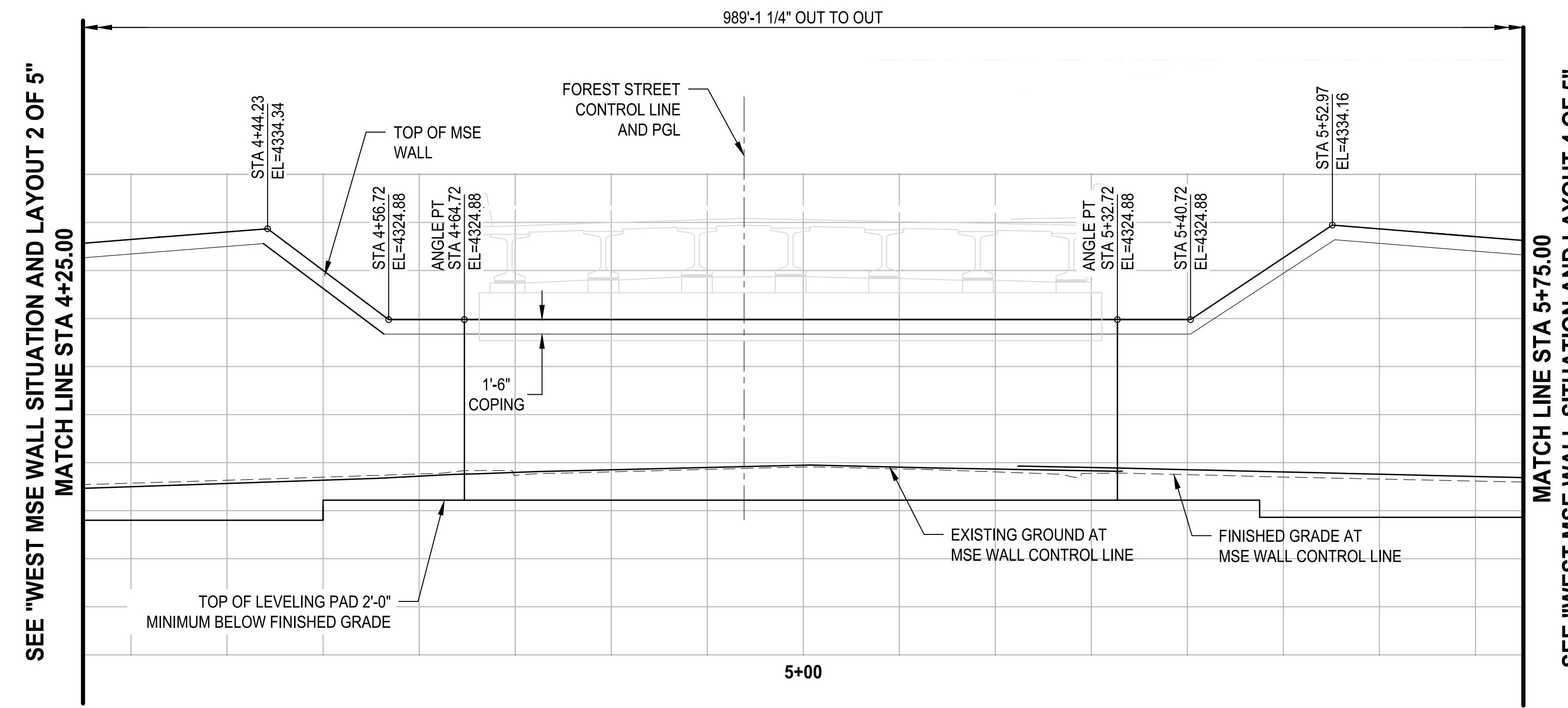
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

WEST MSE WALL SITUATION AND LAYOUT 2 OF 5

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PLAN
SCALE: 1" = 10'



* MEASURED ALONG WEST MSE WALL CONTROL LINE
DEVELOPED ELEVATION
SCALE 1" = 10'

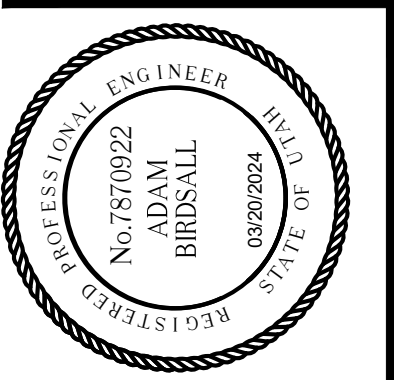


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ONE INCH
AT FULL
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ACCORDINGLY

Parametrix

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| DATE 03/20/2024 | DESIGNED TWP | CHECKED NICC |
| JOB No. 344-8541-002 | DRAWN SLO | APPROVED AUB |



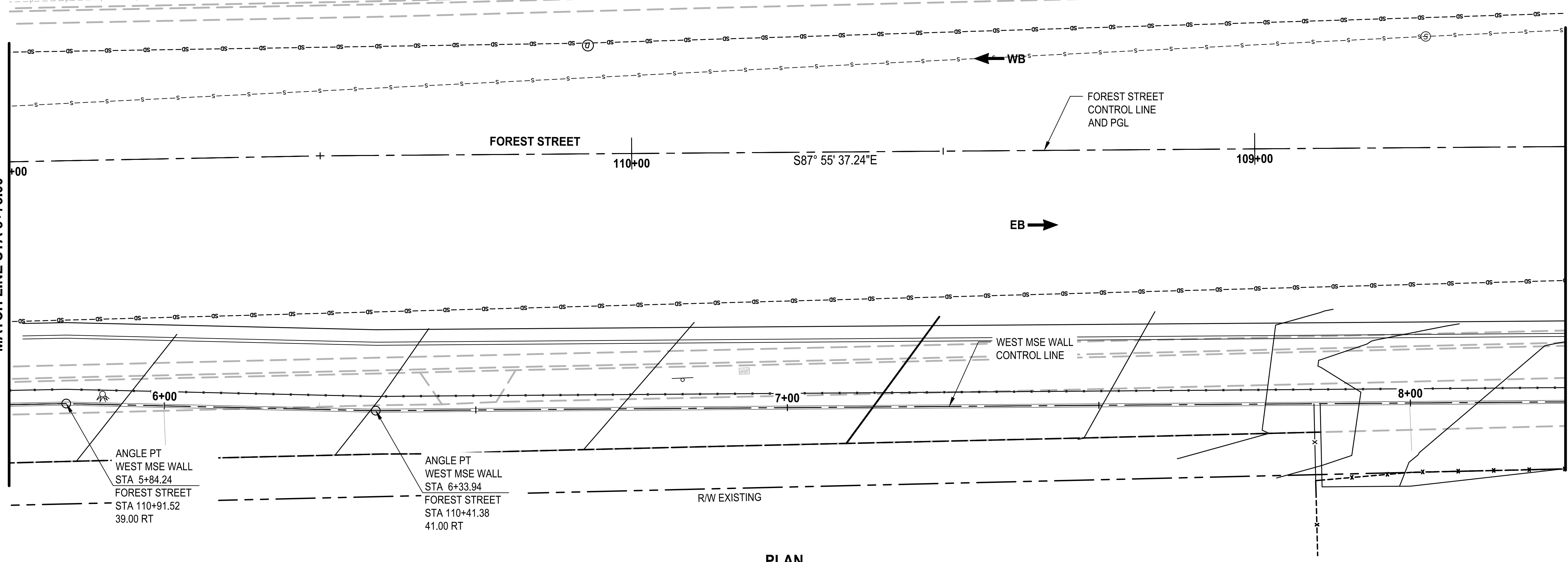
PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

**WEST MSE WALL
SITUATION
AND LAYOUT
3 OF 5**

DRAWING NO.
4 OF 11
W04

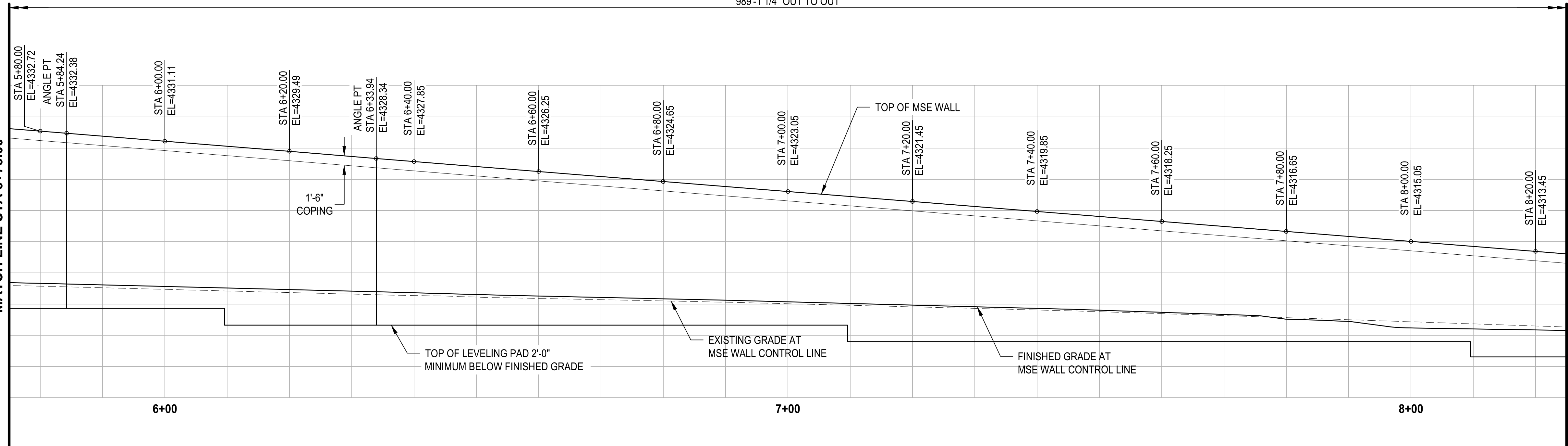
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SEE "WEST MSE WALL SITUATION AND LAYOUT 3 OF 5"
MATCH LINE STA 5+75.00



PLAN
SCALE: 1" = 10'

SEE "WEST MSE WALL SITUATION AND LAYOUT 3 OF 5"
MATCH LINE STA 5+75.00



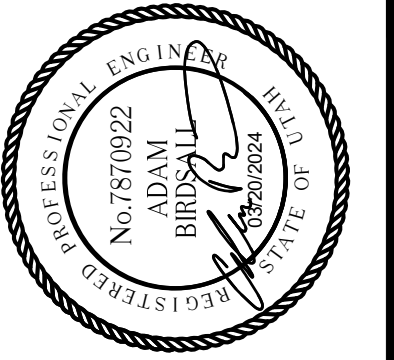
* MEASURED ALONG WEST MSE WALL CONTROL LINE
DEVELOPED ELEVATION
SCALE 1" = 10'

MATCH LINE STA 8+25.00
SEE "WEST MSE WALL SITUATION AND LAYOUT 5 OF 5"

| REVISIONS | DATE | BY |
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ONE INCH
AT FULL
SCALE IF
NECESSARY
ACCORDINGLY

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| Parametrix | DESIGNED | TWP | CHECKED | NCC |
| | DATE | 03/20/2024 | DRAWN | SLO |
| | JOB No. | 344-8541-002 | APPROVED | AJB |



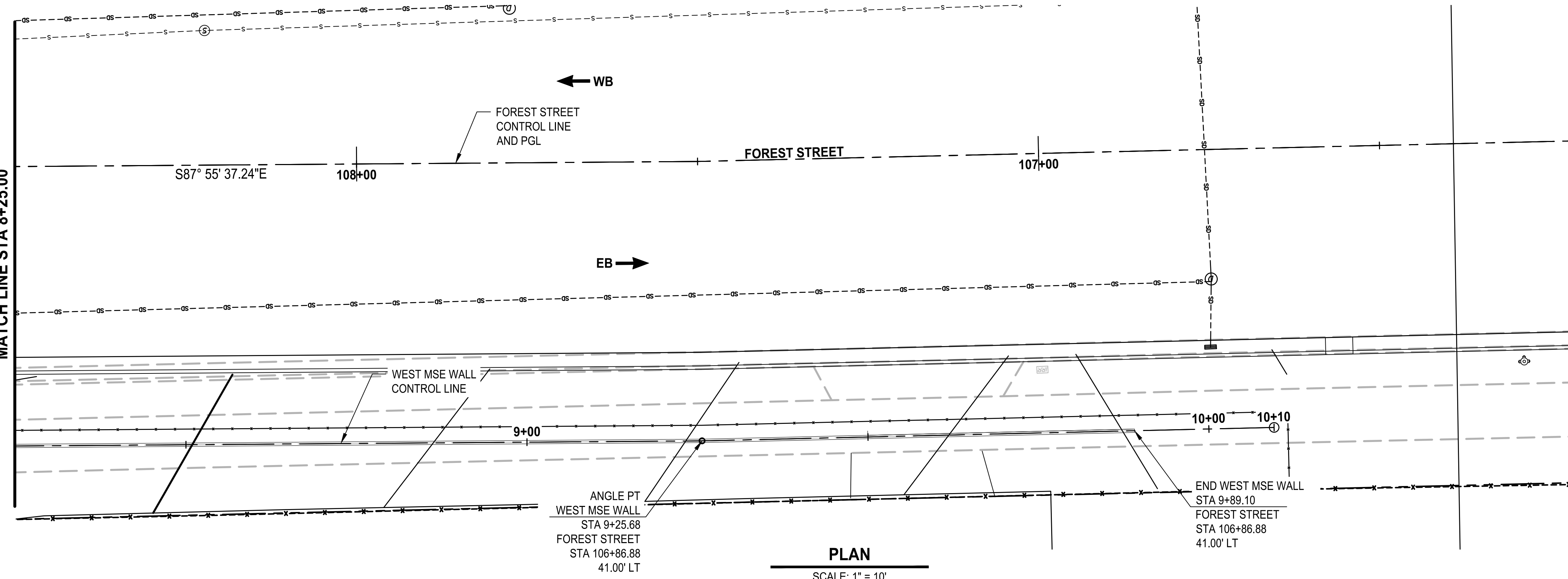
PROJECT NAME
**BRIGHAM CITY
CONNECTION PROJECT**

**WEST MSE WALL
SITUATION
AND LAYOUT
4 OF 5**

LAYOUT: 6W PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Sves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 10:00:21 AM

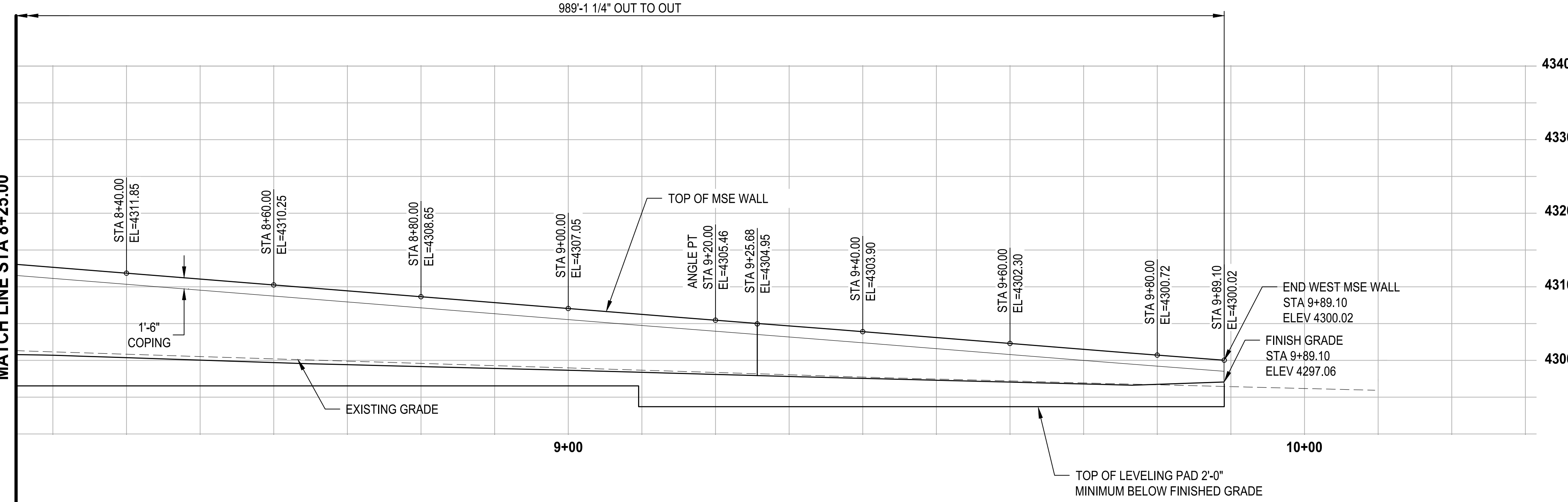
SEE "WEST MSE WALL SITUATION AND LAYOUT 4 OF 5"

MATCH LINE STA 8+25.00



SEE "WEST MSE WALL SITUATION AND LAYOUT 4 OF 5"

MATCH LINE STA 8+25.00



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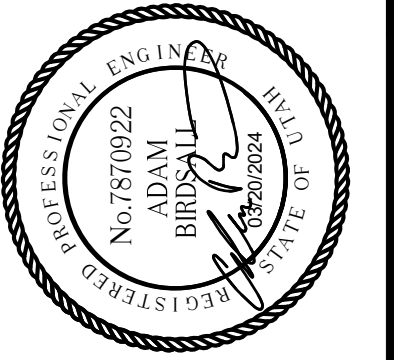
ONE INCH AT FULL SCALE IF NOT SHOWN OTHERWISE

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DATE: 03/20/2024
JOB No.: 344-8541-002

DESIGNED: TWP
DRAWN: SLO

CHECKED: NICC
APPROVED: AUB



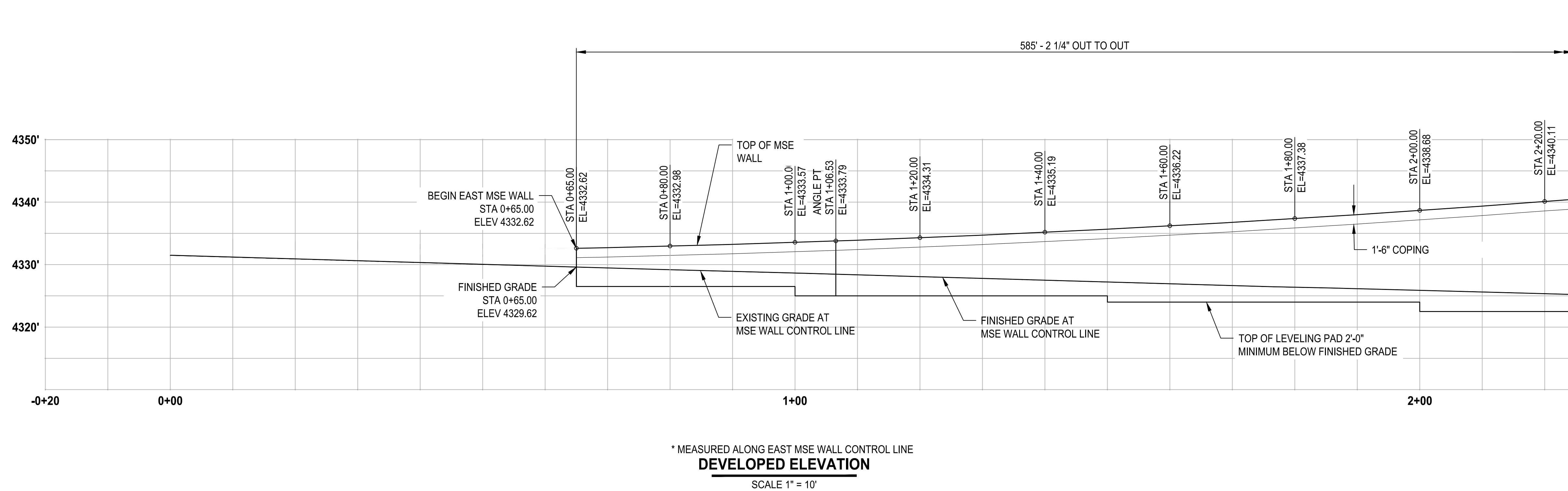
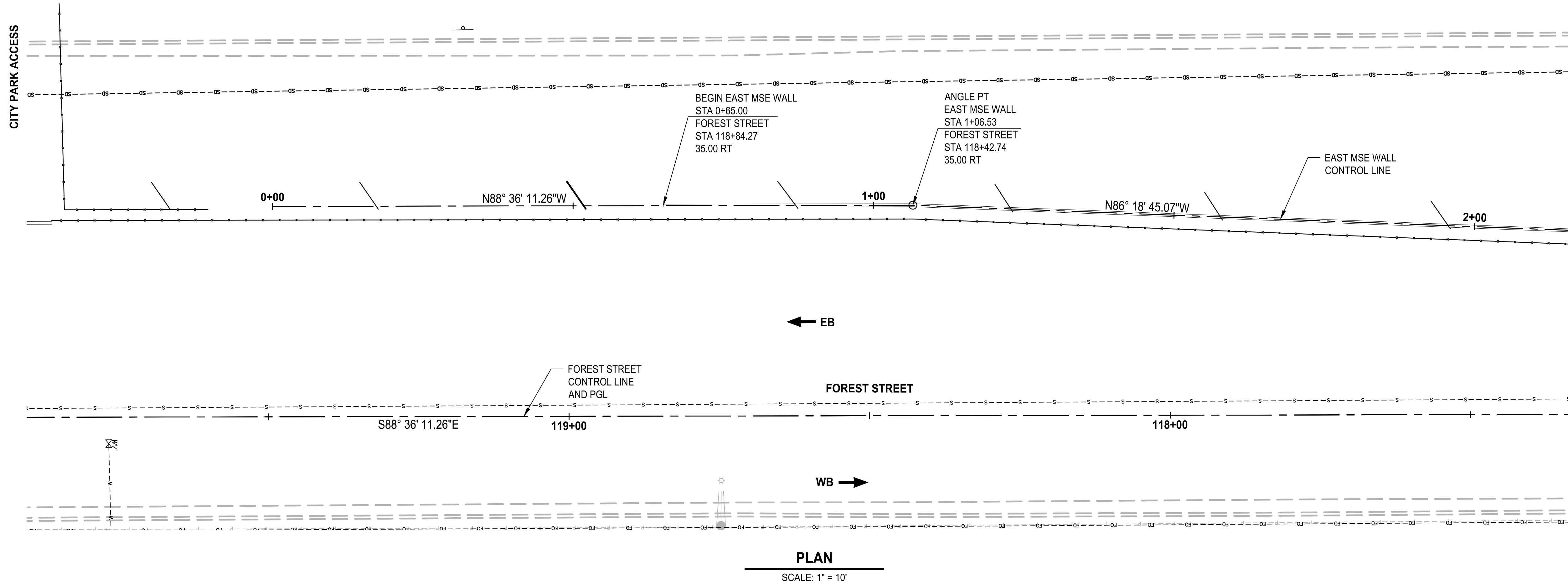
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

WEST MSE WALL SITUATION AND LAYOUT 5 OF 5

DRAWING NO.
W06

6 OF 11

LAYOUT: 1E PATH: U:\Sof\Projects\Clients\8541-Brigham City\344-8541-002_Forest St Final Design\995vcs\CADD\DWG\Structure PLOTTED BY: OllweSta DATE: Friday, March 22, 2024 10:00:43 AM

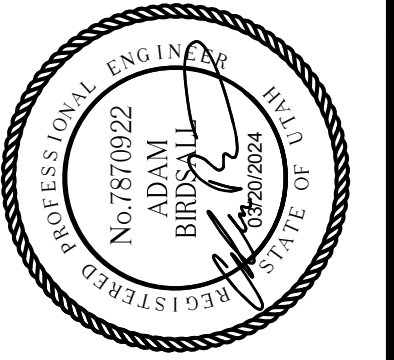


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ONE INCH AT FULL SCALE IF NOT OTHERWISE NOTED ACCORDINGLY

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| 344-8541-002 | SLO | AJB |

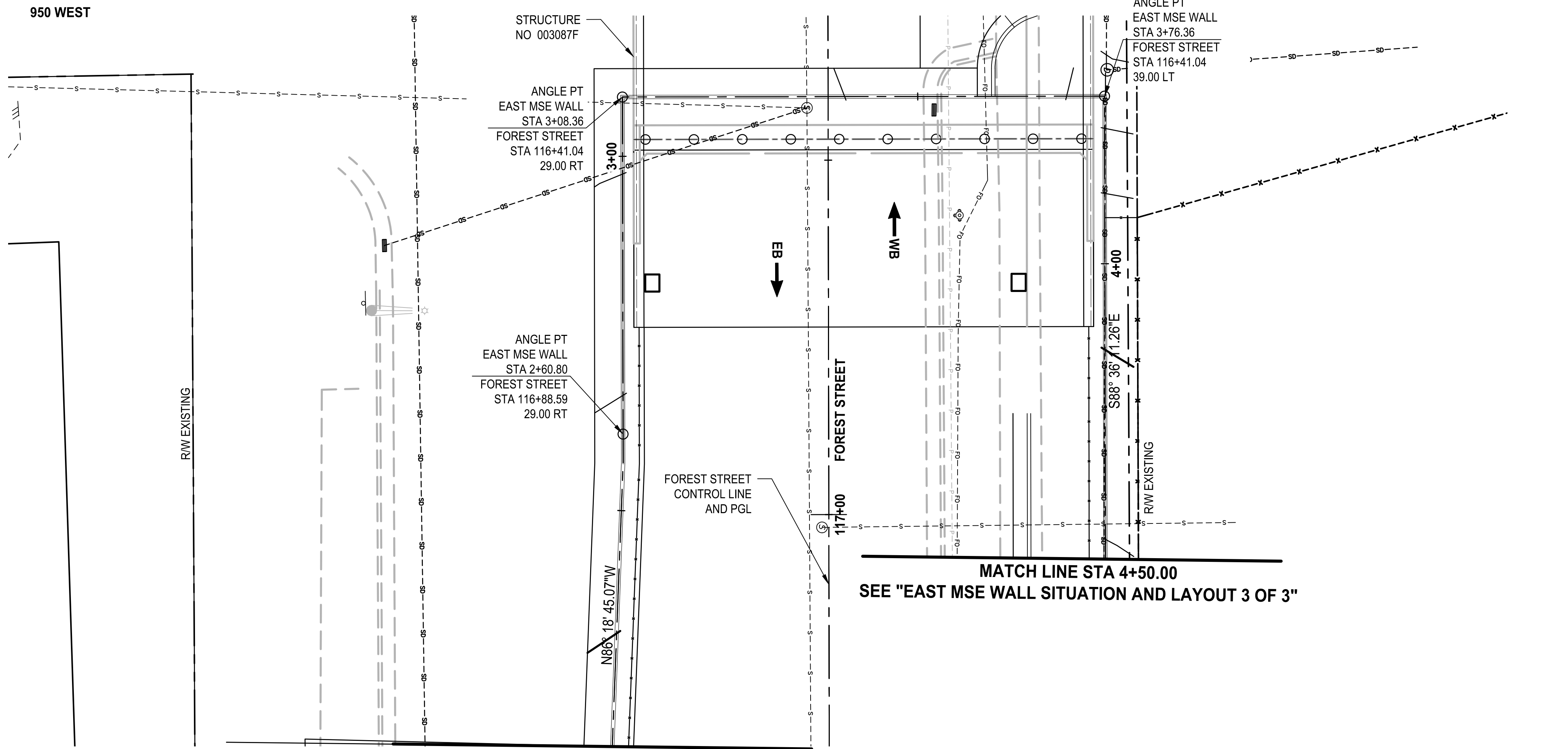


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

EAST MSE WALL SITUATION AND LAYOUT 1 OF 3

DRAWING NO.
7 OF 11
W07

LAYOUT: 2E PATH: U:\Sak\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Sves\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 10:01:11 AM

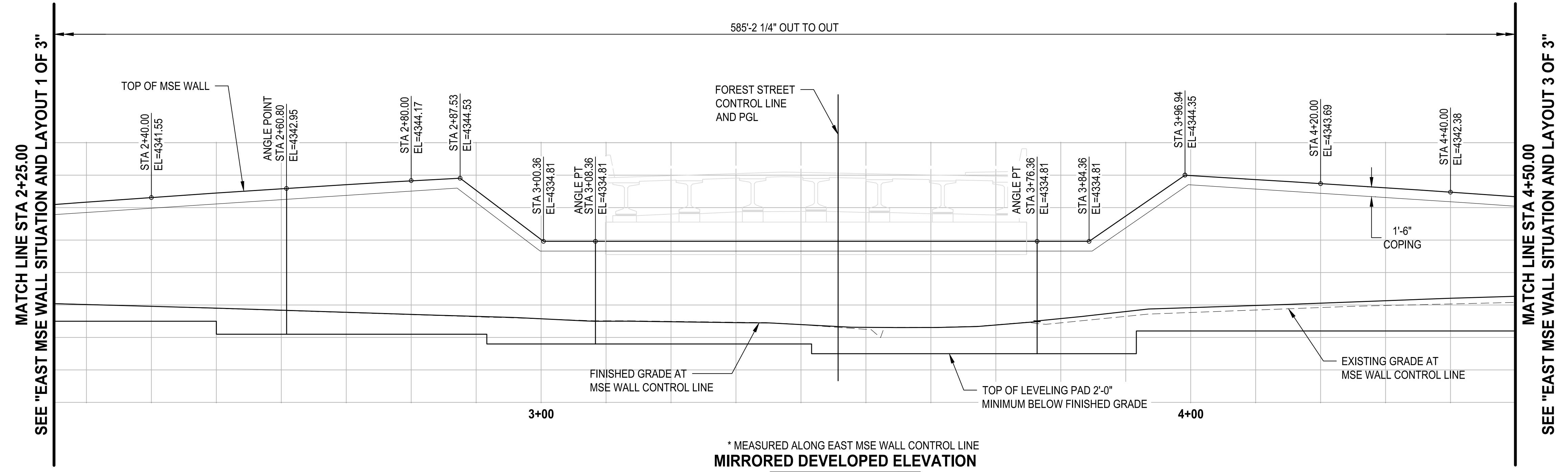


MATCH LINE STA 2+25.00
SEE "EAST MSE WALL SITUATION AND LAYOUT 1 OF 3"

MATCH LINE STA 4+50.00
SEE "EAST MSE WALL SITUATION AND LAYOUT 3 OF 3"

PLAN

SCALE 1" = 10'



* MEASURED ALONG EAST MSE WALL CONTROL LINE
MIRRORED DEVELOPED ELEVATION

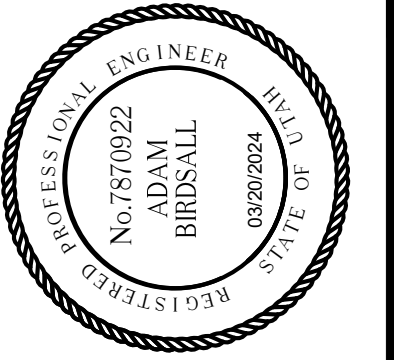
SCALE 1" = 10'

| REVISIONS | DATE | BY |
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ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

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|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | TWP | NCC |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | SLO | AJB |



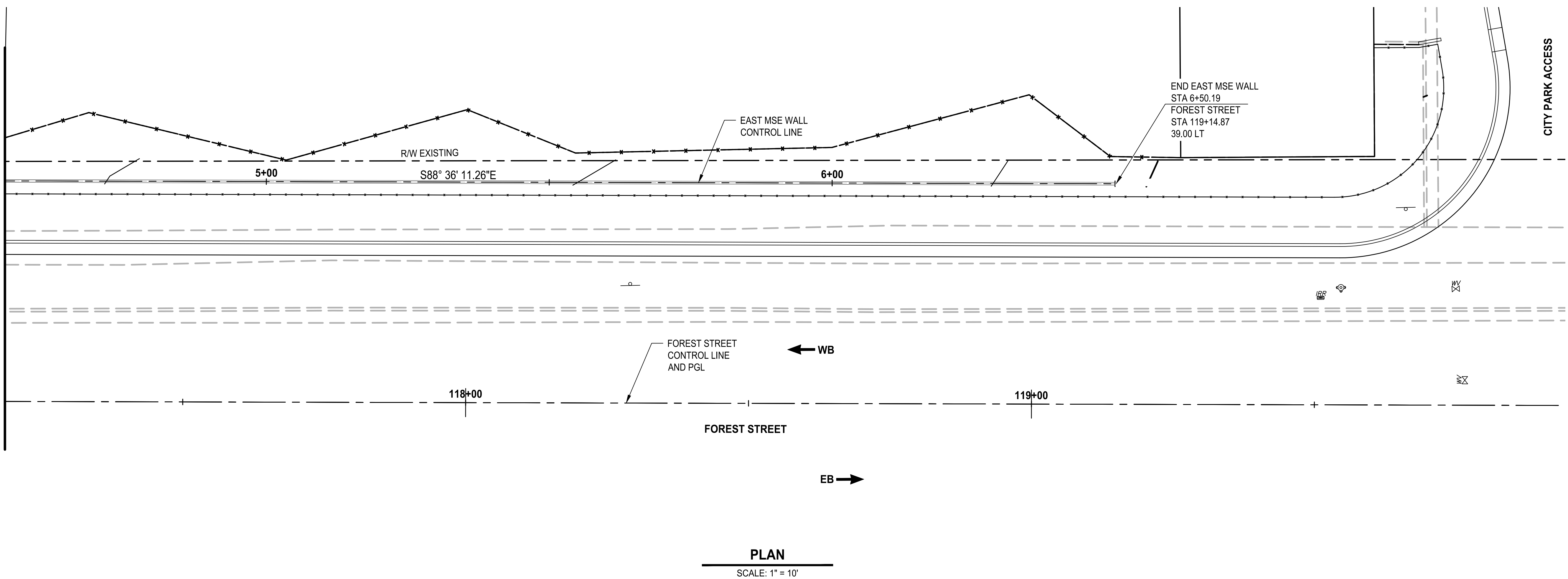
PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

EAST MSE WALL SITUATION AND LAYOUT 2 OF 3

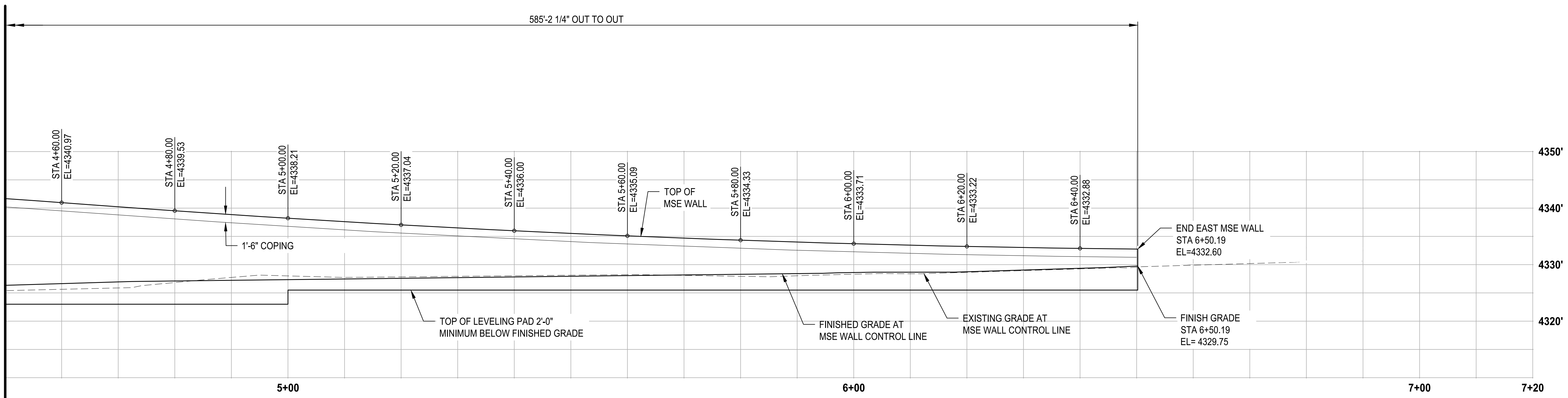
LAYOUT: 3E PATH: U:\Sat\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Svcs\CADD\DWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 10:01:36 AM

SEE "EAST WALL SITUATION AND LAYOUT 2 OF 3"
MATCH LINE STA 4+50.00

SEE "EAST MSE WALL SITUATION AND LAYOUT 2 OF 3"
MATCH LINE STA 4+50.00



PLAN
SCALE: 1" = 10'



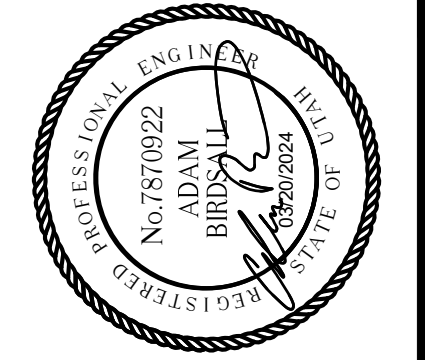
* MEASURED ALONG EAST MSE WALL CONTROL LINE
DEVELOPED ELEVATION
SCALE 1" = 10'

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NOT ACCORDINGLY

Parametrix

| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | TWP | NCC |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | SLO | AJB |

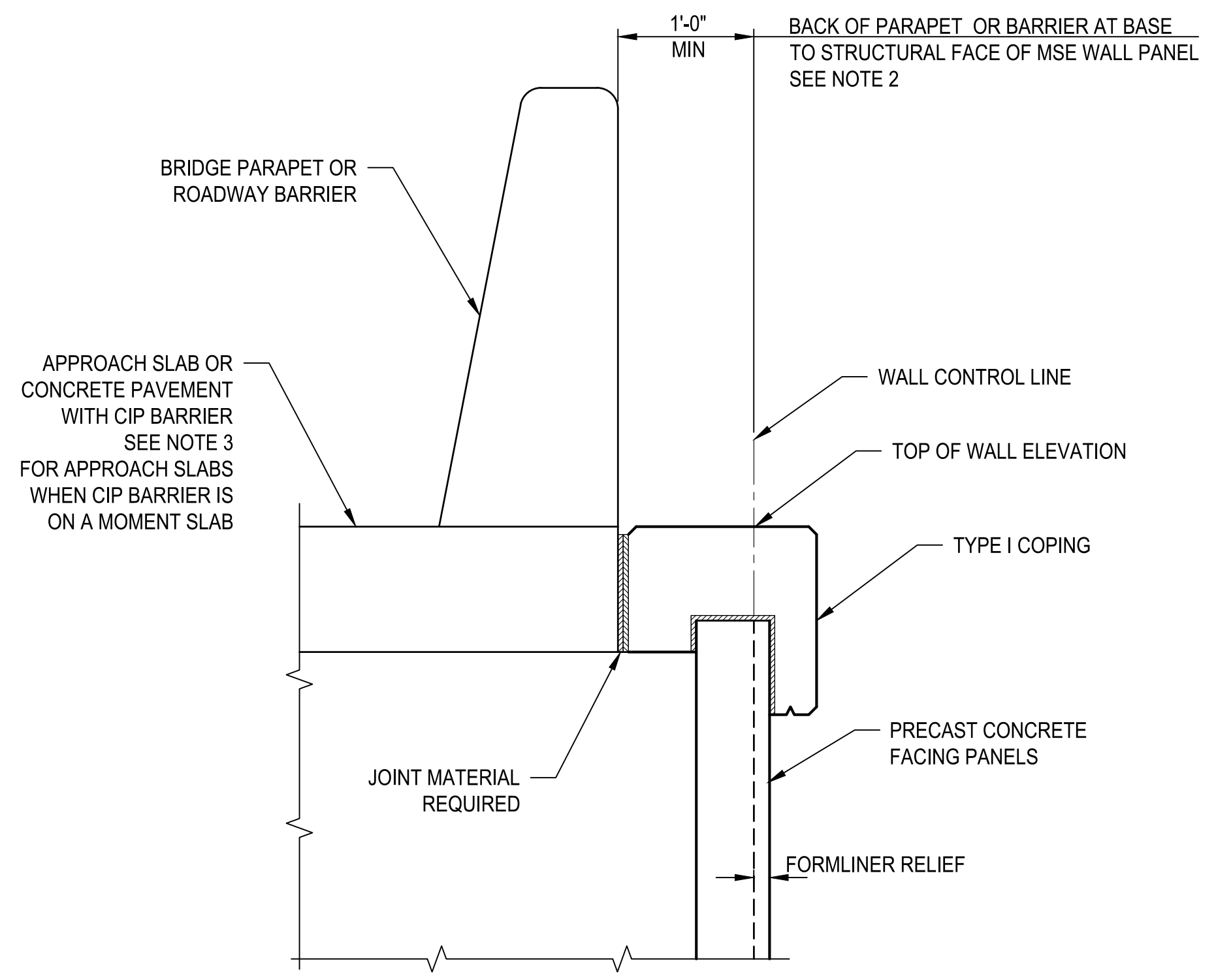


PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

EAST MSE WALL SITUATION AND LAYOUT 3 OF 3

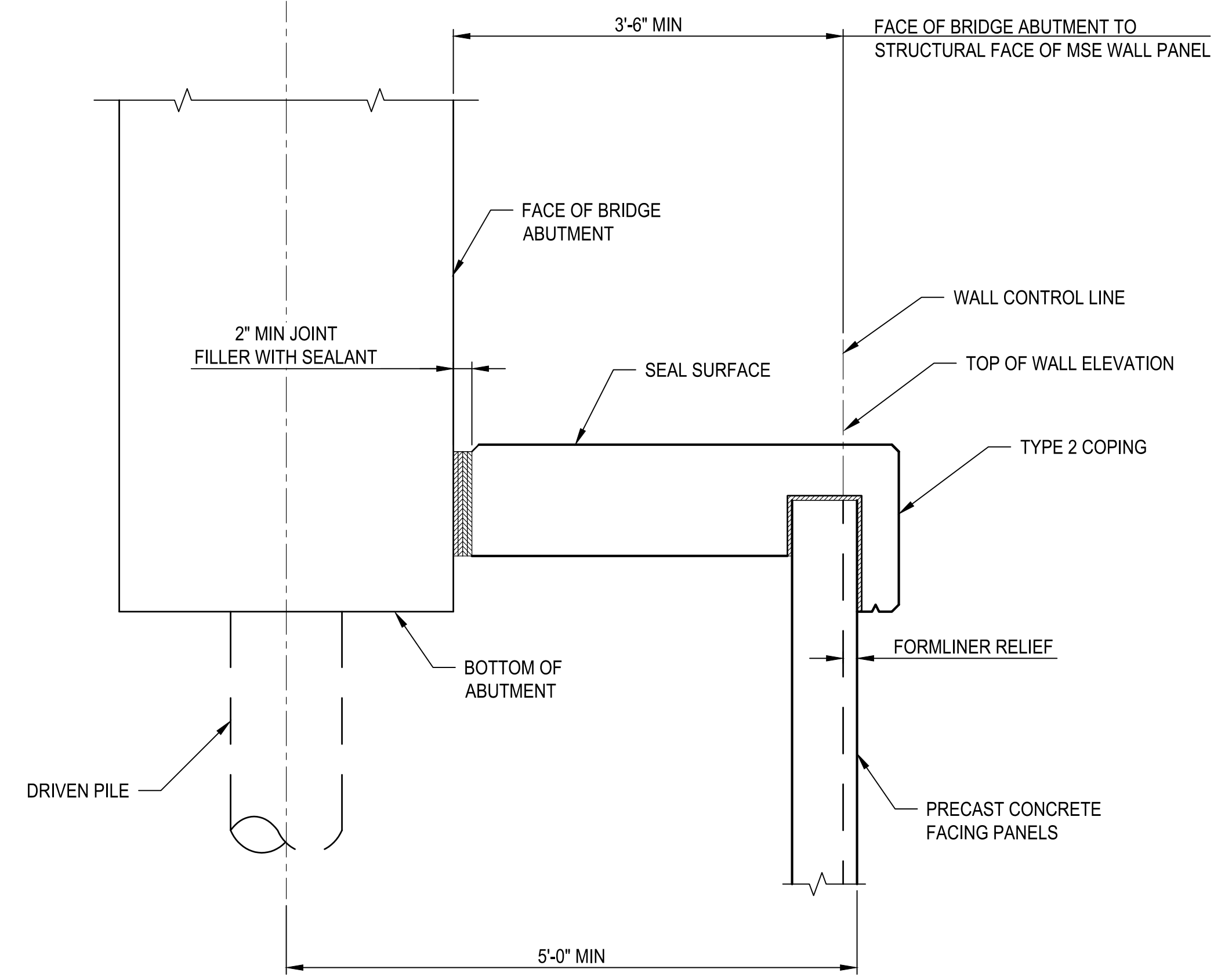
DRAWING NO.
9 OF 11
W09

LAYOUT: wall det
 PATH: U:\Sol\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\99Sves\CADD\DWG\Structure
 PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 10:01:42 AM



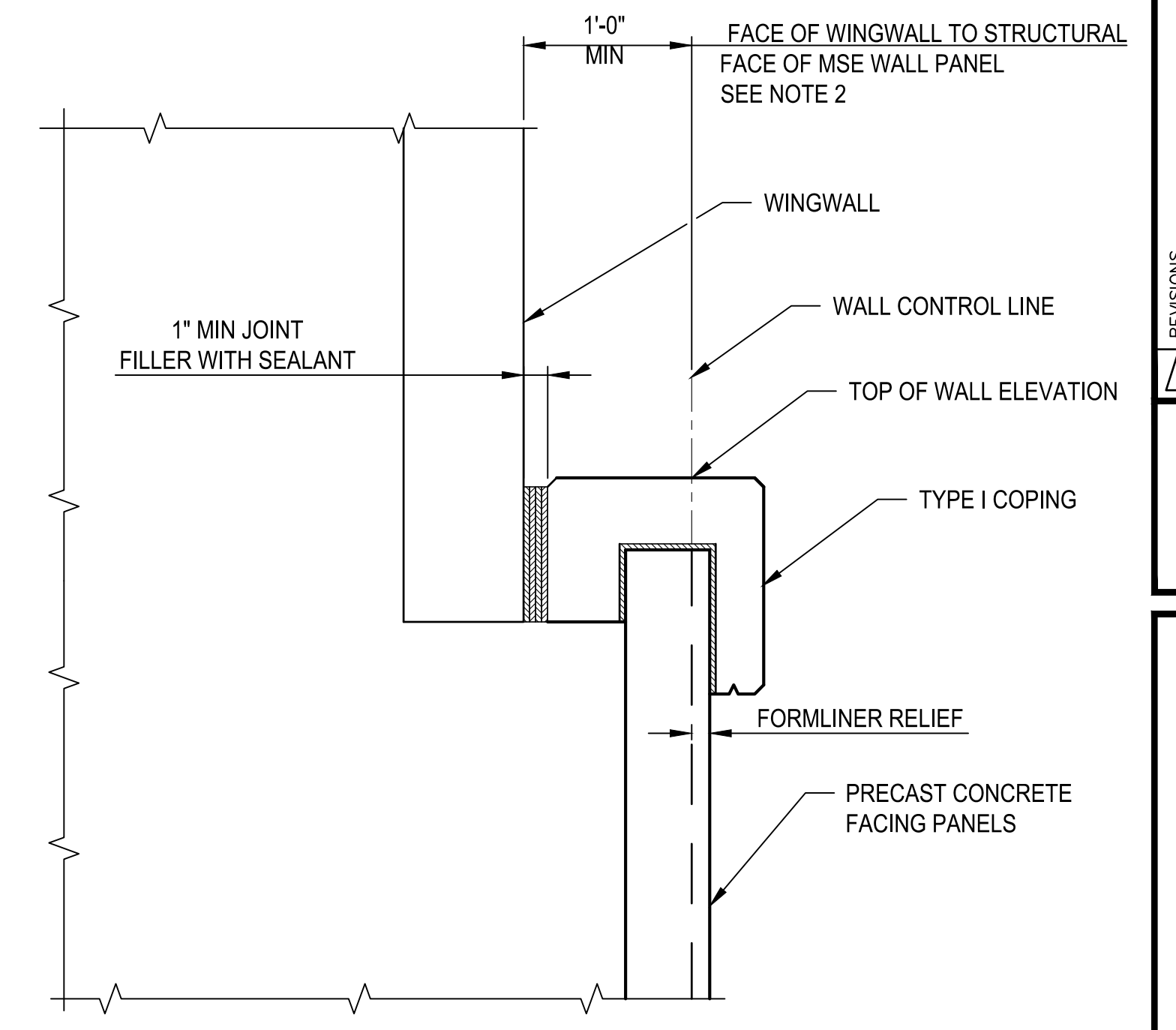
DETAIL A

SINGLE STAGE MSE WALL BEHIND CIP BARRIER
 OR APPROACH SLAB PARAPET



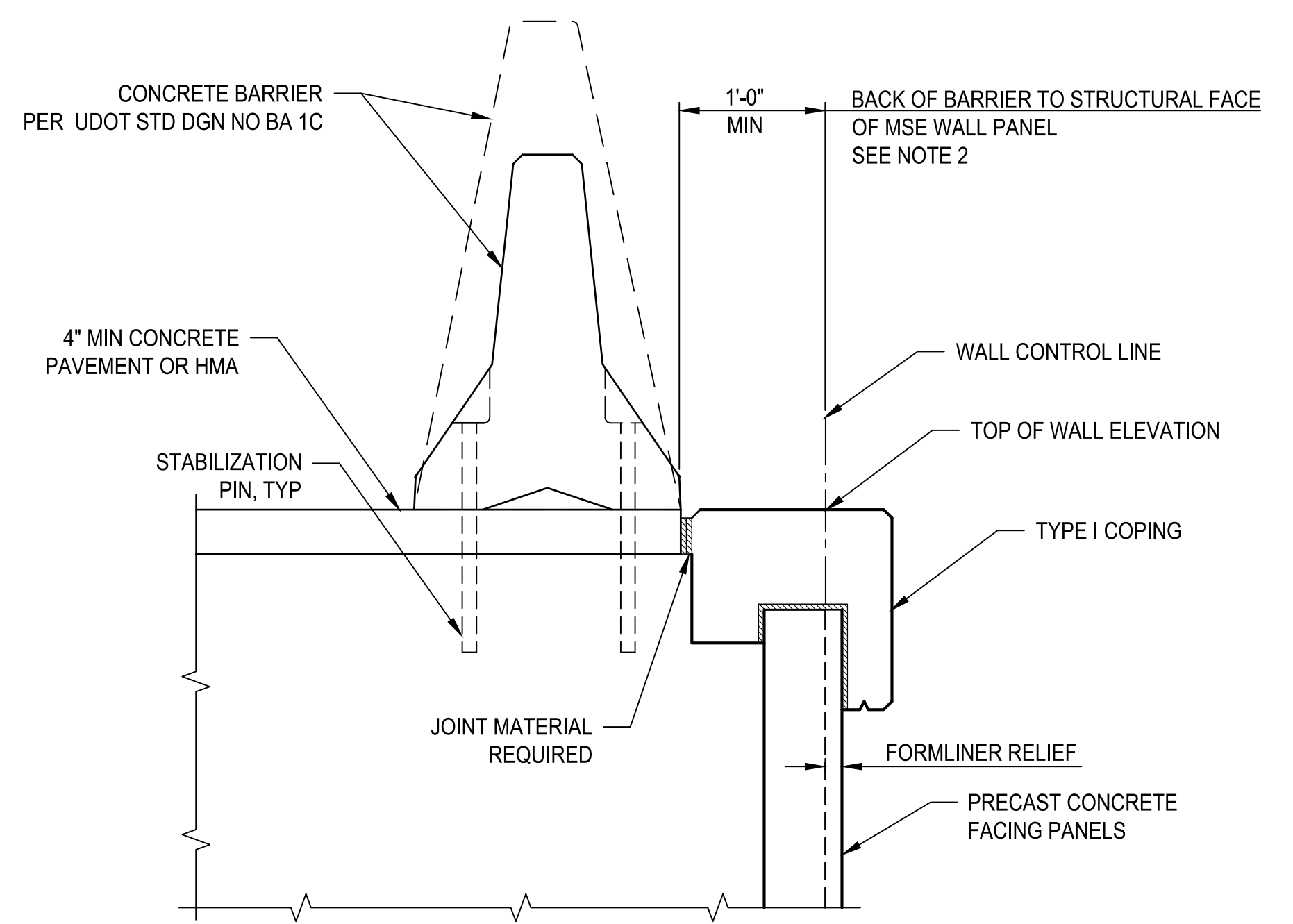
DETAIL B

SINGLE STAGE MSE WALL IN FRONT OF ABUTMENT



DETAIL C

SINGLE STAGE MSE WALL IN FRONT OF WINGWALL



DETAIL D

SINGLE STAGE MSE WALL BEHIND BARRIER

NOTES

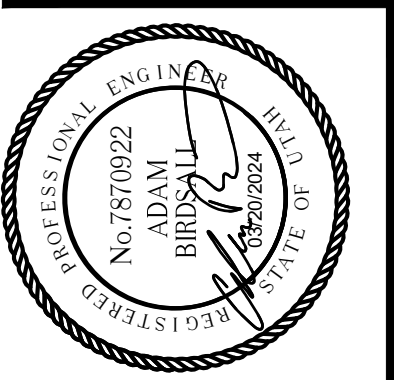
1. THE STRUCTURAL PORTION OF THE MSE WALL PANEL DOES NOT INCLUDE THE FORMLINER RELIEF.
2. DETAILS A, E AND F ARE NOT APPLICABLE WHEN USING A MOMENT SLAB. SEE WS-106A AND WS-106B FOR MOMENT SLABS.
3. WHEN THE APPROACH SLAB TIES INTO A MOMENT SLAB USE AN EDGE DETAIL SIMILAR TO THE MOMENT SLAB EDGE DETAIL ON WS-106A.

| REVISIONS | DATE | BY |
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ONE INCH
 AT FULL
 SCALE IF
 NOT
 ACCORDINGLY

Parametrix

| | | |
|--------------|----------|----------|
| DATE | DESIGNED | CHECKED |
| 03/20/2024 | TWP | NCC |
| JOB No. | DRAWN | APPROVED |
| 344-8541-002 | SLO | AJB |



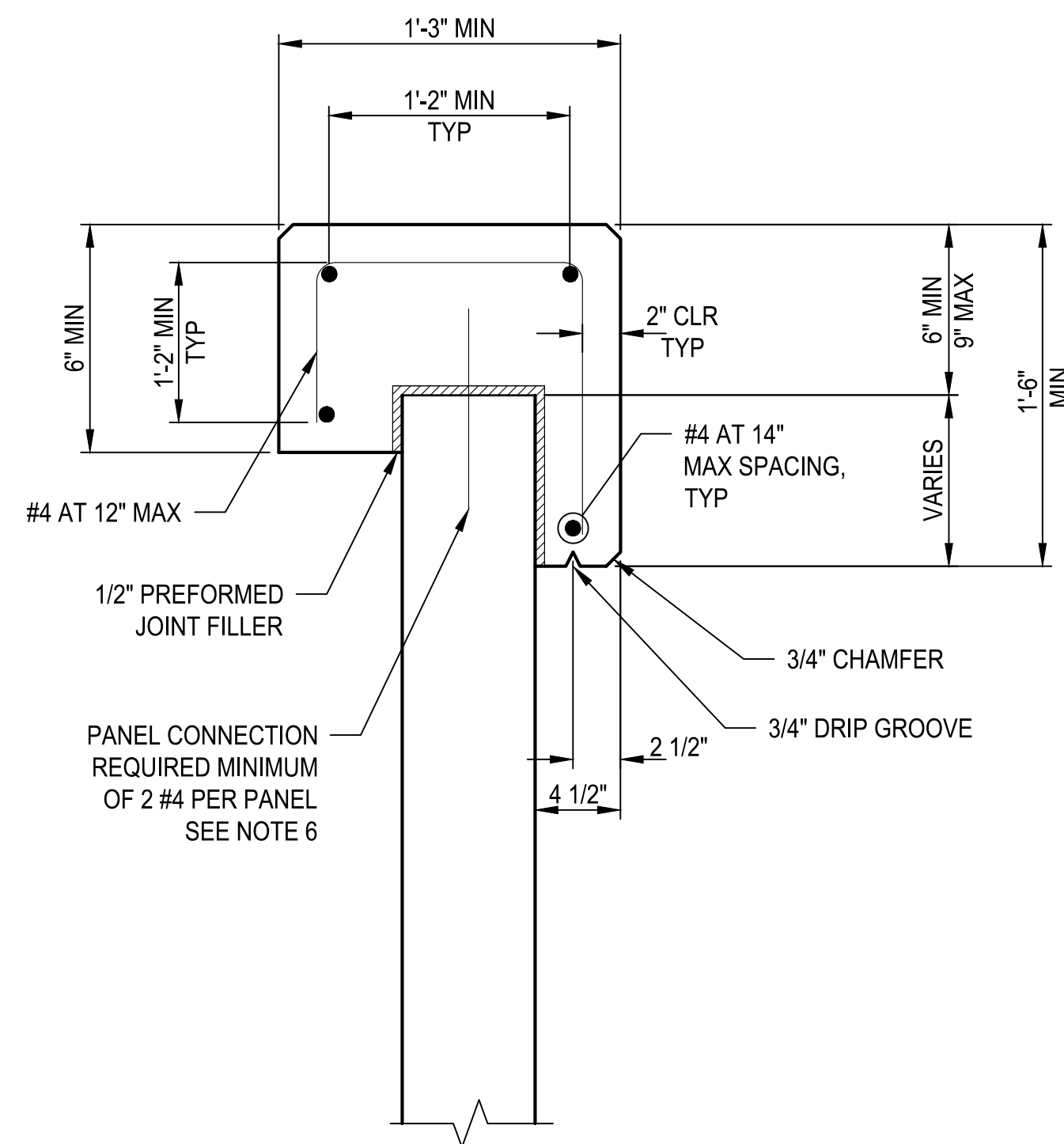
PROJECT NAME
**BRIGHAM CITY
 CONNECTION PROJECT**

**SINGLE STAGE
 MSE WALL COPING
 REQUIREMENTS
 1 OF 2**

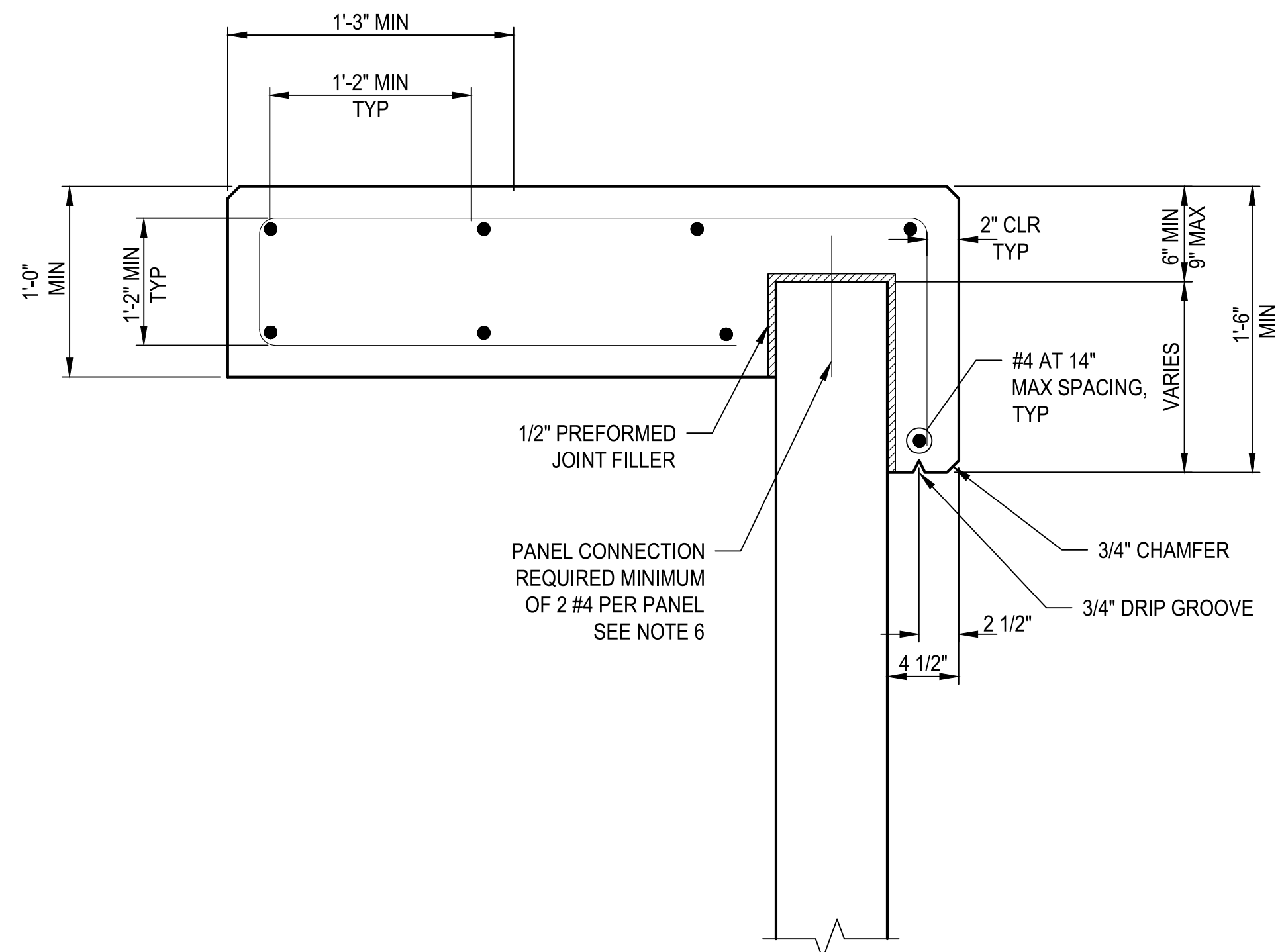
DRAWING NO.
 10 OF 11

W10

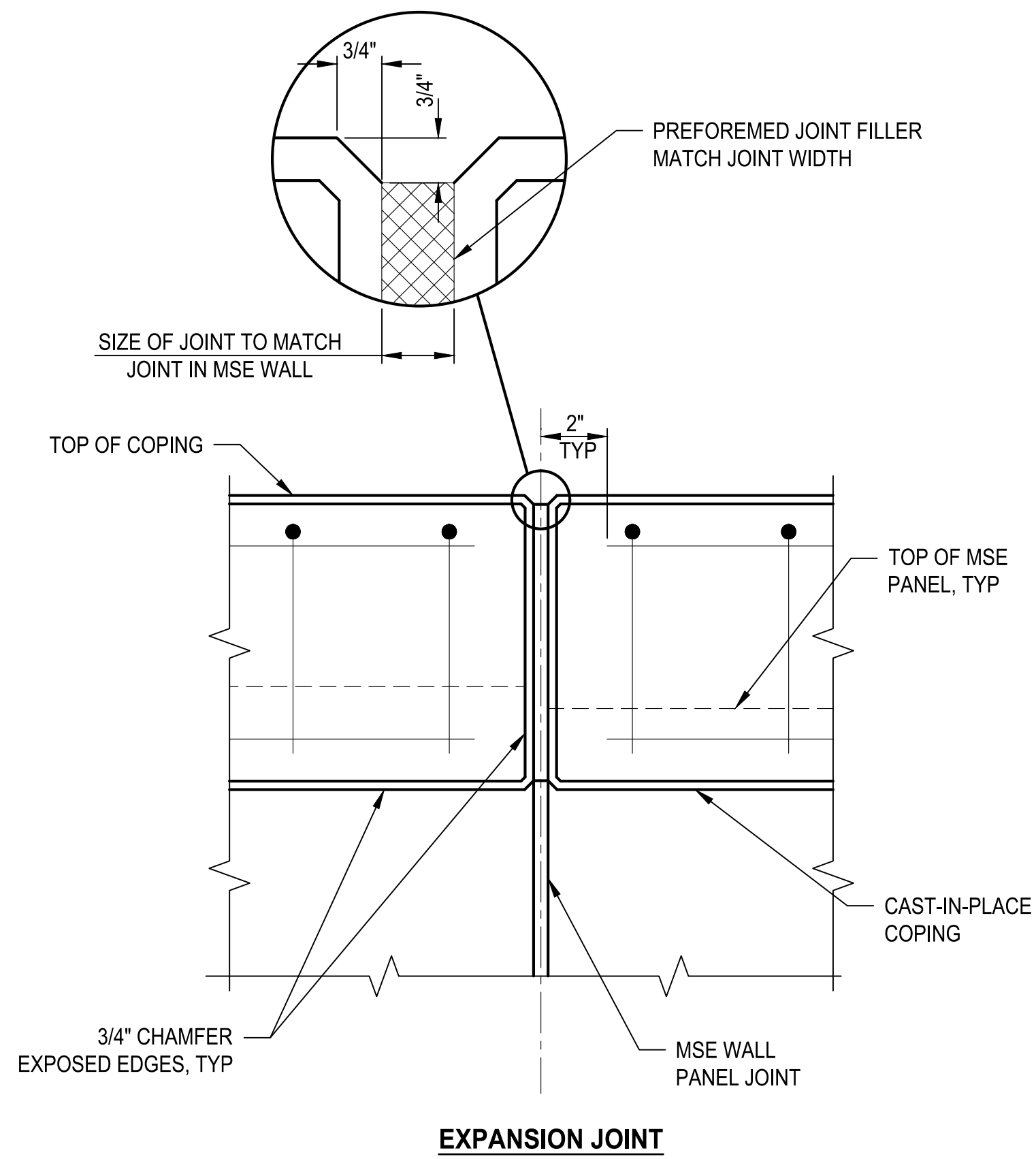
LAYOUT: reinf det PATH: U:\Soft\Projects\Clients\8541-Brigham City\344-8541-002 Forest St Final Design\995secs\CADD\CWG\Structure PLOTTED BY: OliveSta DATE: Friday, March 22, 2024 10:01:47 AM



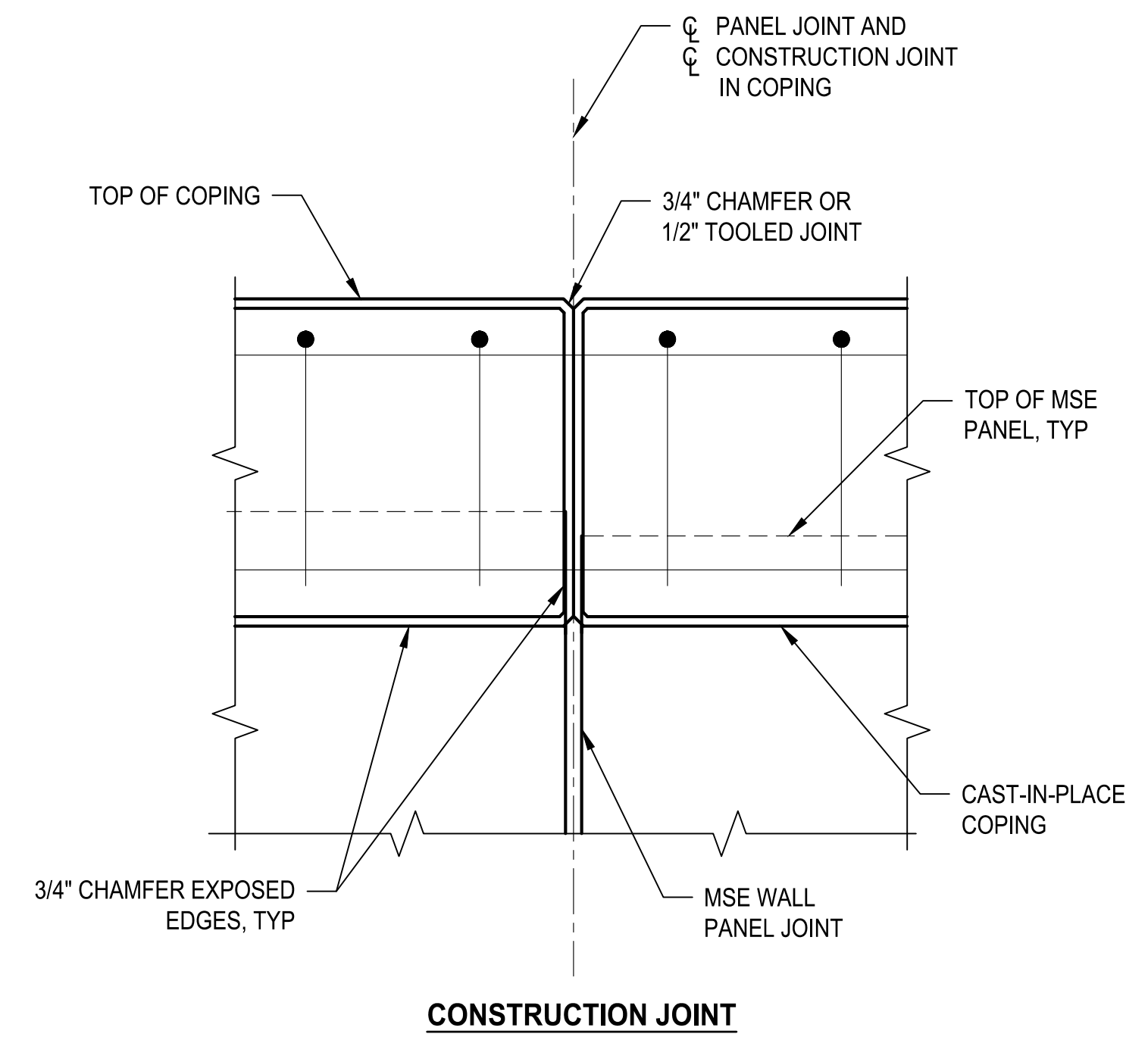
TYPE 1 COPING REINFORCING



TYPE 2 COPING REINFORCING



EXPANSION JOINT



CONSTRUCTION JOINT

COPING JOINT DETAIL

NOTES

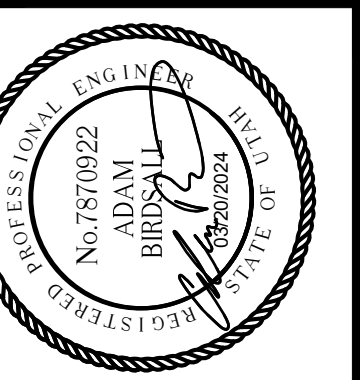
1. CUT LONGITUDINAL REINFORCING 2 INCH CLEAR OF EACH SIDE OF THE EXPANSION CONTINUE LONGITUDINAL REINFORCING THROUGH CONSTRUCTION JOINTS.
2. PLACE A 1 FT WIDE FILTER CLOTH BEHIND JOINT TO PREVENT SOIL PENETRATION THROUGH JOINT WHERE EARTH FILL IS ADJACENT TO EXPANSION JOINT. SEE WALL COMPANY'S RETAINING WALL FOR ADDITIONAL INFORMATION.
3. SLOPE TOP OF COPING SMOOTHLY ACROSS JOINTS. DO NOT STEP COPING AT JOINTS.
4. PLACE EXPANSION JOINTS AT PANEL JOINTS AT 100 FT MAX SPACING AND WHERE WALL DIRECTION CHANGES. PLACE CONSTRUCTION JOINTS AT ALL OTHER PANEL JOINTS.
5. PROVIDE CONTINUOUS LONGITUDINAL REINFORCING BETWEEN EXPANSION JOINTS. PROVIDE 2 FT LAP SPLICES WHERE REQUIRED. STAGGER LAP SPLICES MINIMUM OF 4 FT.
6. DETAIL PANEL CONNECTION REINFORCEMENT TO BE FULLY DEVELOPED INTO WALL PANEL AND COPING.

| REVISIONS | DATE | BY |
|-----------|------|----|
| | | |

ONE INCH AT FULL SCALE IF NECESSARY ACCORDINGLY

Parametrix

DATE: 03/20/2024
 JOB No.: 344-8541-002
 DESIGNED: TWP
 DRAWN: SLO
 CHECKED: NICC
 APPROVED: AUB



PROJECT NAME
BRIGHAM CITY CONNECTION PROJECT

SINGLE STAGE MSE WALL COPING REQUIREMENTS 2 OF 2

DRAWING NO.
 11 OF 11
W11

DOCUMENT 00 41 23

BID FORM

ADDENDUM 1

BRIGHAM CITY CONNECTION PROJECT

TABLE OF CONTENTS

| | Page |
|---|-------------|
| Article 1 – Bid Recipient | 2 |
| Article 2 – Bidder’s Acknowledgements | 2 |
| Article 3 – Bidder’s Representations..... | 2 |
| Article 4 – Bidder’s Certification | 3 |
| Article 5 – Basis of Bid..... | 4 |
| Article 6 – Time of Completion | 10 |
| Article 7 – Attachments to this Bid | 10 |
| Article 8 – Defined Terms..... | 10 |
| Article 9 – Bid Submittal..... | 11 |

ARTICLE 1 – BID RECIPIENT

1.01 This Bid is submitted to:

***Brigham City
20 North Main Street
Brigham City, UT 84302***

1.02 The undersigned Bidder proposes and agrees, if this Bid is accepted, to enter into an Agreement with Owner in the form included in the Bidding Documents to perform all Work as specified or indicated in the Bidding Documents for the prices and within the times indicated in this Bid and in accordance with the other terms and conditions of the Bidding Documents.

ARTICLE 2 – BIDDER’S ACKNOWLEDGEMENTS

2.01 Bidder accepts all of the terms and conditions of the Instructions to Bidders, including without limitation those dealing with the disposition of Bid security. This Bid will remain subject to acceptance for 60 days after the Bid opening, or for such longer period of time that Bidder may agree to in writing upon request of Owner.

ARTICLE 3 – BIDDER’S REPRESENTATIONS

3.01 In submitting this Bid, Bidder represents that:

A. Bidder has examined and carefully studied the Bidding Documents, and any data and reference items identified in the Bidding Documents, and hereby acknowledges receipt of the following Addenda:

| <u>Addendum No.</u> | <u>Addendum, Date</u> |
|---------------------|-----------------------|
| _____ | _____ |
| _____ | _____ |
| _____ | _____ |
| _____ | _____ |

B. Bidder has visited the Site, conducted a thorough, alert visual examination of the Site and adjacent areas, and become familiar with and satisfied itself as to the general, local, and Site conditions that may affect cost, progress, and performance of the Work.

C. Bidder is familiar with and has satisfied itself as to all Laws and Regulations that may affect cost, progress, and performance of the Work.

D. Bidder has carefully studied all: (1) reports of explorations and tests of subsurface conditions at or adjacent to the Site and all drawings of physical conditions relating to existing surface or subsurface structures at the Site that have been identified in the Supplementary Conditions, especially with respect to Technical Data in such reports and drawings, and (2) reports and drawings relating to Hazardous Environmental Conditions, if any, at or adjacent to the Site that have been identified in the Supplementary Conditions, especially with respect to Technical Data in such reports and drawings.

- E. Bidder has considered the information known to Bidder itself; information commonly known to contractors doing business in the locality of the Site; information and observations obtained from visits to the Site; the Bidding Documents; and any Site-related reports and drawings identified in the Bidding Documents, with respect to the effect of such information, observations, and documents on (1) the cost, progress, and performance of the Work; (2) the means, methods, techniques, sequences, and procedures of construction to be employed by Bidder; and (3) Bidder's safety precautions and programs.
- F. Bidder agrees, based on the information and observations referred to in the preceding paragraph, that no further examinations, investigations, explorations, tests, studies, or data are necessary for the determination of this Bid for performance of the Work at the price bid and within the times required, and in accordance with the other terms and conditions of the Bidding Documents.
- G. Bidder is aware of the general nature of work to be performed by Owner and others at the Site that relates to the Work as indicated in the Bidding Documents.
- H. Bidder has given Engineer written notice of all conflicts, errors, ambiguities, or discrepancies that Bidder has discovered in the Bidding Documents and confirms that the written resolution thereof by Engineer is acceptable to Bidder.
- I. The Bidding Documents are generally sufficient to indicate and convey understanding of all terms and conditions for the performance and furnishing of the Work.
- J. The submission of this Bid constitutes an incontrovertible representation by Bidder that Bidder has complied with every requirement of this Article, and that without exception the Bid and all prices in the Bid are premised upon performing and furnishing the Work required by the Bidding Documents.

ARTICLE 4 – BIDDER'S CERTIFICATION

4.01 Bidder certifies that:

- A. This Bid is genuine and not made in the interest of or on behalf of any undisclosed individual or entity and is not submitted in conformity with any collusive agreement or rules of any group, association, organization, or corporation;
- B. Bidder has not directly or indirectly induced or solicited any other Bidder to submit a false or sham Bid;
- C. Bidder has not solicited or induced any individual or entity to refrain from bidding; and
- D. Bidder has not engaged in corrupt, fraudulent, collusive, or coercive practices in competing for the Contract. For the purposes of this Paragraph 4.01.D:
 - 1. "corrupt practice" means the offering, giving, receiving, or soliciting of anything of value likely to influence the action of a public official in the bidding process;
 - 2. "fraudulent practice" means an intentional misrepresentation of facts made (a) to influence the bidding process to the detriment of Owner, (b) to establish bid prices at

artificial non-competitive levels, or (c) to deprive Owner of the benefits of free and open competition;

3. “collusive practice” means a scheme or arrangement between two or more Bidders, with or without the knowledge of Owner, a purpose of which is to establish bid prices at artificial, non-competitive levels; and
4. “coercive practice” means harming or threatening to harm, directly or indirectly, persons or their property to influence their participation in the bidding process or affect the execution of the Contract.

ARTICLE 5 – BASIS OF BID

5.01 Bidder will complete the Work in accordance with the Contract Documents for the following price(s):

Bid Schedule on following page(s)

**BID SCHEDULE
BRIGHAM CITY
BRIGHAM CITY CONNECTION PROJECT**

CONTRACTOR: _____

BASE BID

| Item # | M&P Reference* | Bid Item Description | Estimated Quantity | Unit | Unit Price** | Bid Price |
|--------|----------------|--|--------------------|------|--------------|-----------|
| 1 | MP 1 | Mobilization | 1 | ls | \$ | \$ |
| 2 | MP 2 | UPDES Storm Water Regulations Compliance (over 1 Acre) | 1 | ls | \$ | \$ |
| 3 | MP 3 | Traffic Control | 1 | ls | \$ | \$ |
| 4 | MP 4 | Survey | 1 | ls | \$ | \$ |
| 5 | MP 5 | Remove Existing Asphalt | 84,152 | sf | \$ | \$ |
| 6 | MP 6 | Remove Existing Fence | 619 | lf | \$ | \$ |
| 7 | MP 7 | Remove Mailbox | 1 | ea | \$ | \$ |
| 8 | MP 8 | Remove Concrete Sidewalk | 10,339 | sf | \$ | \$ |
| 9 | MP 9 | Remove Concrete Driveway | 3,251 | sf | \$ | \$ |
| 10 | MP 10 | Remove Curb | 15 | lf | \$ | \$ |
| 11 | MP 11 | Remove Curb and Gutter | 2,551 | lf | \$ | \$ |
| 12 | MP 12 | Remove Pipe | 143 | lf | \$ | \$ |
| 13 | MP 13 | Remove Catch Basin | 5 | ea | \$ | \$ |
| 14 | MP 14 | Borrow | 5,444 | cy | \$ | \$ |
| 15 | MP 15 | Granular Borrow | 5,153 | ton | \$ | \$ |
| 16 | MP 16 | Roadway Excavation | 8,861 | cy | \$ | \$ |
| 17 | MP 17 | Relocate Mailbox | 3 | ea | \$ | \$ |
| 18 | MP 18 | Cold Milling Bituminous Pavement, Depth = 3" | 3,950 | sy | \$ | \$ |
| 19 | MP 19 | Cold Milling Bituminous Pavement, Variable Depth | 205 | sy | \$ | \$ |
| 20 | MP 20 | Untreated Base Course, Grade 1 | 3,275 | ton | \$ | \$ |
| 21 | MP 21 | Pulverized Pavement Base Course | 785 | sy | \$ | \$ |
| 22 | MP 22 | Seal Coat | 14,441 | sy | \$ | \$ |
| 23 | MP 23 | Tack Coat | 14,441 | sy | \$ | \$ |
| 24 | MP 24 | Prime Coat | 10,287 | sy | \$ | \$ |
| 25 | MP 25 | Hot Mix Asphalt Pavement: SP-3/8, PG 64-34 | 3,420 | ton | \$ | \$ |
| 26 | MP 26 | Concrete Curb and Gutter, City Standard | 1,911 | lf | \$ | \$ |
| 27 | MP 27 | Concrete Curb, Type P | 118 | lf | \$ | \$ |
| 28 | MP 28 | Concrete Curb, Type M2 | 170 | lf | \$ | \$ |

| | | | | | | |
|----|-------|--|-------|----|----|----|
| 29 | MP 29 | Concrete Curb and Gutter Transition | 2 | ea | \$ | \$ |
| 30 | MP 30 | Concrete Curb and Gutter Access Transition | 2 | ea | \$ | \$ |
| 31 | MP 31 | Concrete Curb Type M2 Plowable End Section | 3 | ea | \$ | \$ |
| 32 | MP 32 | 6-Ft Precast Concrete Parking Stop Block | 16 | ea | \$ | \$ |
| 33 | MP 33 | 4" Thick Concrete Flatwork | 301 | sf | \$ | \$ |
| 34 | MP 34 | 4" Thick Concrete Sidewalk | 7,974 | sf | \$ | \$ |
| 35 | MP 35 | 6" Thick Concrete Sidewalk | 225 | sf | \$ | \$ |
| 36 | MP 36 | Driveway Approach | 1,188 | sf | \$ | \$ |
| 37 | MP 37 | Driveway, 7" Thick | 741 | sf | \$ | \$ |
| 38 | MP 38 | Concrete Pedestrian (ADA) Access Ramp | 2 | ea | \$ | \$ |
| 39 | MP 39 | 8-Ft Black Chain Link Fence, Type III | 96 | lf | \$ | \$ |
| 40 | MP 40 | 6-Ft Black Chain Link Fence, Type III | 26 | lf | \$ | \$ |
| 41 | MP 41 | 4.5-Ft Black Chain Link Barrier Mounted Fence, Type III | 1,462 | lf | \$ | \$ |
| 42 | MP 42 | 8-Ft to 4.5-Ft Black Chain Link Barrier Mounted Fence Transition, Type III | 48 | lf | \$ | \$ |
| 43 | MP 43 | 6-Ft Galvanized Chain Link Fence, Type III | 414 | lf | \$ | \$ |
| 44 | MP 44 | Right-of-Way Fence, Type B (Metal Post) | 534 | lf | \$ | \$ |
| 45 | MP 45 | 8-Ft X 4.5-Ft Wide Black Chain Link Man Gate, Type III | 1 | ea | \$ | \$ |
| 46 | MP 46 | 8-Ft X 8-Ft Wide Black Chain Link Man Gate, Type III | 1 | ea | \$ | \$ |
| 47 | MP 47 | 6-Ft X 12-Ft Wide Galvanized Chain Link Gate, Type III | 2 | ea | \$ | \$ |
| 48 | MP 48 | 6-Ft X 16-Ft Wide Galvanized Chain Link Gate, Type III | 2 | ea | \$ | \$ |
| 49 | MP 49 | Cast-In-Place Concrete Constant Slope Barrier - 42 Inch Stepped Median Barrier | 84 | lf | \$ | \$ |
| 50 | MP 50 | Cast-In-Place Concrete Constant Slope Half Barrier 42 Inch | 1,202 | lf | \$ | \$ |

| | | | | | | |
|----|-------|---|--------|----|----|----|
| 51 | MP 51 | Cast-In-Place Concrete Constant Slope Half Barrier - 42 Inch in Front of Retaining Wall BA 3K13 | 499 | lf | \$ | \$ |
| 52 | MP 52 | Cast-In-Place Concrete Constant Slope Barrier - 42 Inch Trailing Sloped End Section | 1 | ea | \$ | \$ |
| 53 | MP 53 | Cast-In-Place Concrete Constant Slope Half Barrier - 42 Inch Trailing Sloped End Section | 1 | ea | \$ | \$ |
| 54 | MP 54 | Cast-In-Place Concrete Constant Slope Half Barrier - 42 Inch to 42 Inch Bridge Parapet End Section with Moment Slab BA 3K14 | 2 | ea | \$ | \$ |
| 55 | MP 55 | Cast-In-Place Concrete Constant Slope Half Barrier - 42 Inch, Full Height End Section With Moment Slab Foundation BA 3K2 | 2 | ea | \$ | \$ |
| 56 | MP 56 | Cast-In-Place Constant Slope Barrier - 42 Inch, Lighting Foundation | 9 | ea | \$ | \$ |
| 57 | MP 57 | Cast-In-Place Constant Slope Stepped Median Barrier - 42 Inch Lighting Foundation | 1 | ea | \$ | \$ |
| 58 | MP 58 | Modified Sloped End Section (Northwest End) | 1 | ea | \$ | \$ |
| 59 | MP 59 | Modified Sloped End Section (Northeast End) | 1 | ea | \$ | \$ |
| 60 | MP 60 | Retaining Wall To Retaining Stepped Barrier Transition (Northeast) | 1 | ea | \$ | \$ |
| 61 | MP 61 | Retaining Wall To Retaining Stepped Barrier Transition (Southeast) | 1 | ea | \$ | \$ |
| 62 | MP 62 | Check Dam – Fiber Roll | 53 | lf | \$ | \$ |
| 63 | MP 63 | Silt Fence | 193 | lf | \$ | \$ |
| 64 | MP 64 | Drop-Inlet Barrier – Fiber Roll | 456 | lf | \$ | \$ |
| 65 | MP 65 | Granular Backfill Borrow (Plan Quantity) | 2,311 | cy | \$ | \$ |
| 66 | MP 66 | Temporary Retaining Wall | 1 | ls | \$ | \$ |
| 67 | MP 67 | Pile Driving Equipment | 1 | ls | \$ | \$ |
| 68 | MP 68 | Driven Piles, 16 Inch | 18,558 | lf | \$ | \$ |
| 69 | MP 69 | Chain Link Fence on Structure | 1,126 | lf | \$ | \$ |

| | | | | | | |
|-----|--------|---|---------|------|----|----|
| 70 | MP 70 | Reinforcing Steel – Uncoated CM (Plan Quantity) | 70,660 | lb | \$ | \$ |
| 71 | MP 71 | Reinforcing Steel – Uncoated CS (Plan Quantity) | 182,404 | lb | \$ | \$ |
| 72 | MP 72 | Reinforcing Steel – Coated (Plan Quantity) | 411,548 | lb | \$ | \$ |
| 73 | MP 73 | Structural Concrete (Est Qty 1,575 CY) | 1 | ls | \$ | \$ |
| 74 | MP 74 | Structural Concrete – Low Shrinkage Fiber (Est Qty 1,373 CY) | 1 | ls | \$ | \$ |
| 75 | MP 75 | Partial Depth Precast Concrete Deck Panel | 20,178 | sf | \$ | \$ |
| 76 | MP 76 | Thin Bonded Polymer Overlay, Type I | 30,402 | sf | \$ | \$ |
| 77 | MP 77 | Concrete Coating Parapet | 1,126 | lf | \$ | \$ |
| 78 | MP 78 | Structural Steel | 1,872 | lb | \$ | \$ |
| 79 | MP 79 | Prestressed Concrete Member, 109 Ft 0 Inch Type UBT58 | 21 | ea | \$ | \$ |
| 80 | MP 80 | Prestressed Concrete Member, 90 Ft 4 Inch Type UBT58 | 14 | ea | \$ | \$ |
| 81 | MP 81 | Compression Seal Joint (Type A) | 130 | lf | \$ | \$ |
| 82 | MP 82 | Concrete Coating (Plan Quantity) | 23,438 | sf | \$ | \$ |
| 83 | MP 83 | Electrical Work Bridges | 1 | ls | \$ | \$ |
| 84 | MP 84 | MSE Retaining Wall (Est Lump Qty: 26,759 SQ FT) | 1 | ls | \$ | \$ |
| 85 | MP 85 | Weed Barrier Geotextile | 158 | sy | \$ | \$ |
| 86 | MP 86 | Clear and Grub Site | 1 | acre | \$ | \$ |
| 87 | MP 87 | Remove Tree | 31 | ea | \$ | \$ |
| 88 | MP 88 | Green Vase Zelkova Tree, 2" Caliper | 9 | ea | \$ | \$ |
| 89 | MP 89 | Irrigation System and Landscape Restoration | 1 | ls | \$ | \$ |
| 90 | MP 90 | Decorative Rock Mulch | 158 | sy | \$ | \$ |
| 91 | MP 91 | Remove Sign | 7 | ea | \$ | \$ |
| 92 | MP 92 | Regulatory Sign, Post, and Base | 4 | ea | \$ | \$ |
| 93 | MP 93 | Warning Sign, Post, and Base | 2 | ea | \$ | \$ |
| 94 | MP 94 | Sign Relocation | 2 | ea | \$ | \$ |
| 95 | MP 95 | Remove Pavement Striping | 1,254 | lf | \$ | \$ |
| 96 | MP 96 | Remove Pavement Symbol | 1 | ea | \$ | \$ |
| 97 | MP 97 | Pavement Marking Paint (4 Inch) | 12,475 | lf | \$ | \$ |
| 98 | MP 98 | Pavement Marking Paint (8 Inch) | 532 | lf | \$ | \$ |
| 99 | MP 99 | Pavement Marking Paint (12 Inch) | 245 | lf | \$ | \$ |
| 100 | MP 100 | Pavement Marking Paint (24 Inch) | 138 | lf | \$ | \$ |

| | | | | | | |
|-----|--------|---|-----|----|----|----|
| 101 | MP 101 | Pavement Symbol Paint | 31 | ea | \$ | \$ |
| 102 | MP 102 | Roadway Electrical Work | 1 | ls | \$ | \$ |
| 103 | MP 103 | Connect New Storm Drain to Existing Structure | 3 | ea | \$ | \$ |
| 104 | MP 104 | 15" Reinforced Concrete Pipe, Leak Resistant | 730 | lf | \$ | \$ |
| 105 | MP 105 | 18" Reinforced Concrete Pipe, Leak Resistant | 305 | lf | \$ | \$ |
| 106 | MP 106 | Precast Manhole – 341.2-A | 2 | ea | \$ | \$ |
| 107 | MP 107 | 30" Frame and Cover – 302 | 2 | ea | \$ | \$ |
| 108 | MP 108 | 44" Frame and Cover – 303 | 1 | ea | \$ | \$ |
| 109 | MP 109 | Cleanout Box 331.1 – 305 | 8 | ea | \$ | \$ |
| 110 | MP 110 | Collar Cover – 362 | 25 | ea | \$ | \$ |
| 111 | MP 111 | 48" Grid Grate and Frame – 310 | 8 | ea | \$ | \$ |
| 112 | MP 112 | Raise Frame to Grade – 360.1 | 14 | ea | \$ | \$ |
| 113 | MP 113 | Precast Box – 332 | 1 | ea | \$ | \$ |

Total Base Bid (Items# 1-113): \$ _____

***To go directly to Measurement and Payment click here.**

****Unit Price shall contain no more than 2 decimal points (e.g., \$0.00)**

Bidder acknowledges that (1) each Bid Unit Price includes an amount considered by Bidder to be adequate to cover Contractor’s overhead and profit for each separately identified item, and (2) estimated quantities are not guaranteed, and are solely for the purpose of comparison of Bids, and final payment for all unit price Bid items will be based on actual quantities, determined as provided in the Contract Documents.

ARTICLE 6 – TIME OF COMPLETION

- 6.01 Bidder agrees that the Work will be substantially complete and will be completed and ready for final payment in accordance with Paragraph 15.06 of the General Conditions on or before the dates or within the number of calendar days indicated in the Agreement.
- 6.02 Bidder accepts the provisions of the Agreement as to liquidated damages.

ARTICLE 7 – ATTACHMENTS TO THIS BID

- 7.01 The following documents are submitted with and made a condition of this Bid:
- A. Required Bid security;
 - B. List of Proposed Subcontractors (see Document 00 43 36 for form);
 - C. Copy of current business license;
 - D. Copy of current Utah contractor's license; and
 - E. E-Verify Form¹ (see Document 00 45 39 for form).
- 7.02 The following documents shall be submitted upon request and made a condition of this Bid:
- A. List of Proposed Suppliers;
 - B. List of Project References; and
 - C. Required Bidder Qualification Statement with supporting data, upon request².

ARTICLE 8 – DEFINED TERMS

- 8.01 The terms used in this Bid with initial capital letters have the meanings stated in the Instructions to Bidders, the General Conditions, and the Supplementary Conditions.

[CONTINUED ON NEXT PAGE]

¹ Required for those Bidders with 15 or more employees. If not applicable, write "NA" on the form and submit the form as an attachment to the Bid.

² Standard forms will be provided.

ARTICLE 9 – BID SUBMITTAL

BIDDER: _____

Submittal Date: _____

(Indicate correct name of bidding entity)

License Number: _____

BY: _____

Signature: _____

Title: _____

ATTEST: _____

Signature: _____

Title: _____

(If Bidder is a corporation, a limited liability company, a partnership, or a joint venture, attach evidence of authority to sign.)

Address for Giving Notices: _____

Phone: _____

POINT OF CONTACT FOR PROJECT

Name: _____

Title: _____

Email: _____

Phone: _____

Is the Point of Contact authorized to sign documents on behalf of the Bidding Entity? **YES** **NO** *(If no, please complete information below)*

AUTHORIZED SIGNATORY

(If different from the point of contact listed above)

Name: _____

Email: _____

Title: _____

END OF BID FORM

DOCUMENT 00 43 13
BID BOND

Any singular reference to Bidder, Surety, Owner or other party shall be considered plural where applicable.

BIDDER (*Name and Address*):

SURETY (*Name, and Address of Principal Place of Business*):

OWNER (*Name and Address*):

BID

Bid Due Date:

Description (*Project Name— Include Location*):

BOND

Bond Number:

Date:

Penal sum

\$

(Words)

(Figures)

Surety and Bidder, intending to be legally bound hereby, subject to the terms set forth below, do each cause this Bid Bond to be duly executed by an authorized officer, agent, or representative.

BIDDER

SURETY

(Seal)

(Seal)

Bidder's Name and Corporate Seal

Surety's Name and Corporate Seal

By:

By:

Signature

Signature (Attach Power of Attorney)

Print Name

Print Name

Title

Title

Attest:

Attest:

Signature

Signature

Title

Title

Note: Addresses are to be used for giving any required notice.

Provide execution by any additional parties, such as joint venturers, if necessary.

Bidder and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors, and assigns to pay to Owner upon default of Bidder the penal sum set forth on the face of this Bond. Payment of the penal sum is the extent of Bidder's and Surety's liability. Recovery of such penal sum under the terms of this Bond shall be Owner's sole and exclusive remedy upon default of Bidder.

1. Default of Bidder shall occur upon the failure of Bidder to deliver within the time required by the Bidding Documents (or any extension thereof agreed to in writing by Owner) the executed Agreement required by the Bidding Documents and any performance and payment bonds required by the Bidding Documents.
2. This obligation shall be null and void if:
 - 2.1 Owner accepts Bidder's Bid and Bidder delivers within the time required by the Bidding Documents (or any extension thereof agreed to in writing by Owner) the executed Agreement required by the Bidding Documents and any performance and payment bonds required by the Bidding Documents, or

- 2.2 All Bids are rejected by Owner, or
- 2.3 Owner fails to issue a Notice of Award to Bidder within the time specified in the Bidding Documents (or any extension thereof agreed to in writing by Bidder and, if applicable, consented to by Surety when required by Paragraph 5 hereof).
3. Payment under this Bond will be due and payable upon default of Bidder and within 30 calendar days after receipt by Bidder and Surety of written notice of default from Owner, which notice will be given with reasonable promptness, identifying this Bond and the Project and including a statement of the amount due.
4. Surety waives notice of any and all defenses based on or arising out of any time extension to issue Notice of Award agreed to in writing by Owner and Bidder, provided that the total time for issuing Notice of Award including extensions shall not in the aggregate exceed 120 days from the Bid due date without Surety's written consent.
5. No suit or action shall be commenced under this Bond prior to 30 calendar days after the notice of default required in Paragraph 4 above is received by Bidder and Surety and in no case later than one year after the Bid due date.
6. Any suit or action under this Bond shall be commenced only in a court of competent jurisdiction located in the state in which the Project is located.
7. Notices required hereunder shall be in writing and sent to Bidder and Surety at their respective addresses shown on the face of this Bond. Such notices may be sent by personal delivery, commercial courier, or by United States Registered or Certified Mail, return receipt requested, postage pre-paid, and shall be deemed to be effective upon receipt by the party concerned.
8. Surety shall cause to be attached to this Bond a current and effective Power of Attorney evidencing the authority of the officer, agent, or representative who executed this Bond on behalf of Surety to execute, seal, and deliver such Bond and bind the Surety thereby.
9. This Bond is intended to conform to all applicable statutory requirements. Any applicable requirement of any applicable statute that has been omitted from this Bond shall be deemed to be included herein as if set forth at length. If any provision of this Bond conflicts with any applicable statute, then the provision of said statute shall govern and the remainder of this Bond that is not in conflict therewith shall continue in full force and effect.

The term "Bid" as used herein includes a Bid, offer, or proposal as applicable.

EJCDC® C-430, Bid Bond (Penal Sum Form). Published 2013.
Prepared by the Engineers Joint Contract Documents Committee.

DOCUMENT 00 43 36
LIST OF PROPOSED SUBCONTRACTORS

| Subcontractor Company Name and Contact Person | Type of Work to be Performed | Estimated Percentage of Work |
|--|-------------------------------------|-------------------------------------|
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Additional information to be provided upon Owner's request.

**DOCUMENT 00 45 39
E-VERIFY FORM**

COMPLETE IF COMPANY EMPLOYS OVER 15 EMPLOYEES

Private Employer Affidavit of Compliance Pursuant to Utah Code 63G-12-302

By executing this affidavit, the undersigned private employer verifies its compliance with Utah Code 63G-12-302, stating affirmatively that the individual, firm or corporation has registered with and utilizes the federal work authorization program commonly known as E-Verify, or other authorized Status Verification System, in accordance with the applicable provisions and deadlines established in Utah Code. Furthermore, the undersigned private employer hereby attests that its federal work authorization user identification number and date of authorization are as follows:

Federal Tax ID #

Name of Private Employer on File with E-Verify

I hereby declare under penalty of perjury that the foregoing is true and correct. I also acknowledge that the company will indemnify Brigham City from all fines, penalties, and costs associated with the company's non-compliance with Utah Code 63G-12-302.

Signature of Authorized Officer or Agent

Date

Printed Name and Title of Authorized Officer or Agent

STATE OF UTAH)
):SS
County of _____)

On _____, _____, 20____, personally appeared before me

_____ the signer of the within instrument, who duly acknowledged to me that she executed the same.

Notary Public